

Kerin Bay Pty Ltd

Demolition of a local heritage place and construction of a six (6) storey hotel building comprising under-croft car parking, outdoor dining, pool, restaurant, gaming area, gym and drive-through liquor shop.

2-6 Bridge Street, MURRAY BRIDGE

DA 415/E014/16

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OVERVIEW

Application No	415/E014/16
Unique ID/KNET ID	Unique ID: 12331374 File Part: 2016/15242/01
Applicant	Kerin Bay Pty Ltd
Proposal	Demolition of existing local heritage place (Bridgeport Hotel) and construction of a six (6) storey hotel building including under-croft car parking, outdoor dining, pool, restaurant, gaming area, gym and drive-through liquor shop.
Subject Land	2-6 Bridge Street MURRAY BRIDGE
Zone/Policy Area	Regional Town Centre Zone Bridge Street Policy Area 11
Relevant Authority	State Commission Assessment Panel (SCAP)
Lodgement Date	30 August 2016
Council	Rural City of Murray Bridge
Development Plan	Consolidated 11 August 2016
Type of Development	Merit
Public Notification	Category 1
Representations	Not Applicable
Referral Agencies	<p><u>Mandatory (Schedule 8):</u> Liquor & Gaming Commissioner Commissioner of Highways Heritage SA</p> <p><u>Non-mandatory:</u> Government Architect Australian Rail Track Corporation</p>
Report Author	Matthew Fielke
RECOMMENDATION	Development Plan Consent be granted

EXECUTIVE SUMMARY

The proposed development comprises the demolition of a local heritage place and the construction of a six (6) storey hotel building and standalone drive through liquor shop. The application is considered to be a Merit form of development in accordance with Section 35 (5) of the *Development Act 1993* as it is neither complying nor non-complying. Category 1 applies for the purposes of public notification pursuant with Schedule 9 Part 1 (6) of the *Development Regulations 2008*.

The key planning matters considered in this report can be summarised as follows:

- Demolition of a local heritage place and impacts on adjacent State heritage listed land
- Bulk and scale of the proposed building
- Design and appearance
- Access, egress and car parking requirements
- The contribution of the development to the Regional Town Centre Zone and Bridge Street Policy Area.

The primary function of the Regional Town Centre Zone is to cater for the business and commercial needs of the community and region more broadly, whilst the Bridge Street Policy Area seeks to guide development in a way that allows for it to contribute to the traditional 'main street' character of Bridge Street. Assessment of the proposal against the relevant provisions of the Zone and Policy Area has demonstrated that many of the desired outcomes of the policy can be achieved, with the exception of building height and access requirements.

The application was referred to the Liquor & Gaming Commissioner, Commissioner of Highways and DEWNR – Heritage SA in accordance with the relevant Schedule 8 requirements of the *Development Regulations 2008*. The Government Architect (and Associated Government Architect) was significantly involved throughout the evolution of the proposal and a non-mandatory referral was also sent to the Australian Rail Track Corporation for comment. No significant objections were raised by the above referral agencies.

Notwithstanding the removal of an existing local heritage place, on balance it is recommended that Development Plan Consent be granted for the proposal subject to conditions.

ASSESSMENT REPORT

1. BACKGROUND

1.1 Strategic Context

Since the application was formally lodged with the State Commission Assessment Panel on 30 August 2016, the following three (3) Development Plan Amendments (DPA's) have been completed:

- Integrated Water Management Regional DPA (gazetted 24 November 2016);
- Regional Town Centre Expansion DPA (gazetted 26 April 2017); and
- Primary Production (Central Policy Area 3) Value Adding DPA (Interim) (gazetted 23 January 2018).

Considering the location of the proposed development, the most significant of the above DPA's is the Regional Town Centre Expansion DPA. Among other changes, this DPA re-defined Bridge Street Policy Area 11, creating the 'Retail Core Policy Area 11' and two (2) complimentary Precincts (Bridge Street Precinct 1 and Sixth Street

Precinct 2). The subject land now falls within the Retail Core Policy Area 11 – Bridge Street Precinct 1. Precinct 1 defines the importance of the subject land and the need for any future development to reflect a ‘very high standard of design and... to reflect the significance of the location and provide an entrance statement that truly reflects the desired character as a vibrant, energetic and progressive centre that embraces its heritage’.

Various policy changes were undertaken that directly affect the subject land. Maximum building heights have been increased from 8m to 12m (or higher if demonstrating exemplary design merit), whilst the subject land is directly addressed in PDC 28, which states:

PDC 28: Buildings that are located at the intersection of Bridge Street and East Terrace should:

- (a) Be medium to high scale developments of high quality
- (b) Be integrated with other land uses on the same site, with shops, cafes or restaurants located on lower levels, to increase street-level activation and on upper levels to provide surveillance to both Bridge Street and the River
- (c) Maintain the prominence and visibility of the Bridge, River Murray and main street
- (d) Ensure the flow of traffic to and from Bridge Street be carefully managed, to minimise the impact on pedestrian movement along Bridge Street and South Terrace, and that both pedestrian and traffic flow to the Bridge and River are not restricted. Well-designed pedestrian walkways to Sturt Reserve and the surrounding locality should also be provided
- (e) Demonstrate a high quality of design that takes into consideration adjoining land uses and mitigates potential interface issues
- (f) Be articulated and terraced to follow the slope of the land to maintain overall river and main street views
- (g) Contain appropriate car parking areas at the rear, with access from a secondary street.

1.2 Pre-Lodgement Process

The proposed development was called-in by the State Coordinator-General on 15 September 2015 with the suggestion that it be subject to a series of pre-lodgement panel (PLP) meetings and design review. The initial PLP was subsequently held on 19 October 2015, with a Design Reviewing following on 10 December 2015. No further meetings were held prior to the formal lodgement of the application on 30 August 2016. Staff from SCAP, the Office of Design and Architecture (ODASA) and DPTI Transport Services were in attendance at the pre-lodgement meeting.

Further consideration of the design merits of the proposal were continually undertaken following lodgement of the application, with numerous variations being provided to SCAP, ODASA and DPTI Transport Services for comment.

2. DESCRIPTION OF PROPOSAL

Application details are contained in the ATTACHMENTS.

The proposed development is for the demolition of a local heritage place and construction of a six (6) storey hotel building comprising ground level and under-croft car parking and laundry; ground level restaurant, bar, function room, lounge, gaming room and detached drive through liquor store; first floor function room, terrace, gym, swimming pool and 12 accommodation units, and; a further 87 accommodation units over the second to fifth levels, each with their own balcony. The hotel building will include those ancillary functions normally associated with hotel & restaurant type land uses, including cold

rooms, stores, kitchen and food preparation areas, toilets, kids play area, foyer, staff rooms and an administrative / reception area.

Proposed materials include (but are not limited to) pre-cast concrete panels finished with Alucobond Pure White 10-100 and Black 326 to upper storey external walls, tiled entry statements finished in vein cut beige, galvanised steel perforated screens to verandah façade and cream painted finish to ground level external walls. Additional materials include Kingwood Eternal cladding to the external walls of all accommodation units, and Colorbond metal roof sheeting finished in Surf Mist.

The drive through liquor store will be finished in a combination of pre-cast concrete (painted in Colorbond Surf Mist) and battened aluminium screening (powder coated in Dulux Silver Pearl). A detailed schedule of materials has been prepared by CED Building Design and is provided in the ATTACHMENTS.

A summary of the proposal is as follows:

Land Use Description	Hotel with associated restaurant, gaming and function rooms. Detached drive through liquor store (shop).	
Building Height	<p><u>Hotel</u> Six (6) storeys (excluding undercroft car parking) 23.53m in height (Bridge Street elevation) 29.03m maximum height above existing ground level (corner of South & East Terrace)</p> <p><u>Liquor Store</u> One (1) storey with total wall height of 7m (parapet height) above finished floor level.</p>	
Schedule of areas	Ground Level	Area (m²)
	Function	373.5
	Dining	586.8
	Sports	214.9
	Bars	87.2
	Gaming	154.2
	Lounge	353.5
	Kitchen	124.6
	Covered footpath	215.3
	Ancillary	322.2
	Level 1	Area (m²)
	Function	541.1
	Kitchen	63.5
	Terrace	339.3
	Pool	243.2
	Gym	25.8
	Accommodation units	482.9 (40.2 – 40.3m ² per unit – 12 units)
	Ancillary	241
	Level 2	Area (m²)
	Accommodation units	884.9 (40.22m ² per unit – 22 units)
	Ancillary	164.5
	Level 3	Area (m²)
	Accommodation units	884.9 (40.22m ² per unit – 22 units)
	Ancillary	164.5
	Level 4	Area (m²)
	Accommodation units	884.9 (40.22m ² per unit – 22 units)

		units)
	Ancillary	164.5
	Level 5	Area (m²)
	Accommodation units	884.9 (40.22m ² per unit – 21 units)
	Ancillary	164.5
Site Access	<u>Existing</u> <ul style="list-style-type: none"> Two-way movements from Bridge Street – left turn in/out only. Multiple movements from South Terrace via 21m wide cross-over. One-way movements from Bridge Street through existing drive through liquor store. <u>Proposed</u> <ul style="list-style-type: none"> Two-way movements from Bridge Street including proposed new right-turn treatment. Two-way movements from South Terrace – ground level car park. Two-way movements from South Terrace – under-croft car park. One-way exit only movements on to South Terrace from the drive through liquor shop. 	
Car and Bicycle Parking	<u>Required</u> 387 car parking spaces (in accordance with Table MuBr/2 – Off Street Vehicle Parking Requirements. <u>Proposed</u> 160 proposed car parks. 12 proposed bicycle parks at ground level adjacent the southwestern entrance, with additional secure facilities in the north-western corner of the under-croft car park, the total number of which has not been provided.	
Encroachments	Potential to impact on the existing underground train tunnel – located within approximately 7m of the northern corner of the subject land. TMK Consulting Engineers have provided comment on this matter – concluding that the proposed development works are unlikely to have negative impact on the tunnel. However, further investigation is required during the preparation of details drawings of Building Rules Consent. No easements currently exist over the land.	
Staging	The proposed staging is as follows: Stage 1 – Civil works Stage 2 – Demolition of the existing hotel Stage 3 – Bottle shop construction Stage 4 – Hotel building construction.	

3. SITE AND LOCALITY

3.1 Site Description

The subject land is legally defined as follows:

Lot No	Street	Suburb	Hundred	Title Reference
A94, 95, 96, 97, 98 & 99 in TP 170702	Bridge Street	Murray Bridge	Murray Bridge	CT 6124/82

The subject site comprises six (6) regular shaped allotments, totalling approximately 6070m² of land and has direct frontage to Bridge Street to the northwest, East Terrace to the northeast and South Terrace to the southeast. The southwestern boundary abuts an existing single-storey office development (boundary construction). The land is wholly located within the Regional Town Centre Zone (Bridge Street Policy Area 11) of the Murray Bridge (RC) Development Plan, consolidated 11 August 2016.

Existing improvements on the land include the Local Heritage listed two-storey hotel building to the north, associated alterations and additions, existing drive through liquor store abutting the hotel building to the southwest, hotel car park to the south and southwest, and associated open space including some mature vegetation in the eastern corner. Photographs of the site can be found in the ATTACHMENTS.

The site is relatively flat when viewed from Bridge Street, at which point the land begins to slope down toward the east to southeast, with its lowest point being in the eastern corner at the junction of East Terrace and South Terrace.

3.2 Locality

The immediate locality comprises a wide variety of land uses that are complimentary to the functions of a Regional Town Centre such as shopping, municipal offices (including the former police station, Magistrates Court and Town Hall) and public open space. Bridge Street has been historically established to provide a traditional 'main street' function and comprises shopping and restaurant facilities in a predominantly 1-2 storey character. Opposite the site to the south is the recently completed (circa 2011) Murray Bridge Market Place – a large formal enclosed 'mall' development comprising retail and restaurant facilities as well as the local Murray Bridge Library. Public open space (Wharf Hill Reserve) to the northeast comprises car parking, public garden, existing boat sheds and open park land, providing pedestrian access to the river-front and historic wharf area.

The broader locality takes in to consideration the recently heritage listed Murray Bridge Transport Precinct, which includes (but is not limited to) the existing road bridge, train bridge, Wharf Hill Reserve, railway station and round house (former Murray Bridge Works Superintendent House). The precinct was included in the South Australian Heritage Register in May 2015 and is recognised for its role in the development of river and rail transport in the colony of South Australia. Associated with the transport functions of this precinct, it is noted that two (2) train tunnels are located beneath Bridge Street, immediately north of the subject land.

Figure 1 – Location Map



4. STATUTORY REFERRAL BODY COMMENTS

Referral responses are contained in the ATTACHMENTS.

4.1 Commissioner of Highways (DPTI – Traffic Operations)

A mandatory referral to DPTI – Traffic Operations was undertaken in accordance with Schedule 8 Part 2 Item 3 of the *Development Regulations 2008* as the proposed development is likely to alter an existing access and change the nature of movement through and existing access from a secondary arterial road (Bridge Street).

It is understood that the subject land is currently serviced by two individual access points from Bridge Street – one of which being solely dedicated to the existing drive through liquor store. Traffic Operations staff have identified their strong support for the proposed closure of this access point due to its undesirable location and function, resulting in queues onto to Bridge Street. Staff have also noted the modification of the remaining access – and provision of a right hand turn lane for north-bound traffic – as being the most suitable outcome for the site.

All conditions as recommended have been adopted with the exception of condition 8, which restricts service vehicle movements to 'outside of peak traffic hours'. This requirement is covered in further detail in a condition 20 attached.

4.2 DEWNR – State Heritage

Due to the location of the subject land in close proximity to the State heritage listed Murray Bridge Transport Precinct and given the unique scale of the proposal, it was considered that the proposed development may materially affect the context within which this place is situated. As such, a mandatory referral to DEWNR – State Heritage was undertaken in accordance with Schedule 8 Part 2 Item 5(1) of the *Development Regulations 2008*.

It was concluded that the impact of the proposed development would be limited and that the setting, context and meaning of the State heritage place would not be diminished or adversely affected by the proposed development. No conditions were recommended and two advisory notes will form part of the approval documentation as recommended.

4.3 Liquor and Gambling Commissioner

Schedule 8 Part 2 Item 22 defines the need for a referral to the Liquor and Gambling Commissioner (the Commissioner) where development involves the construction or extension of a gaming area within the meaning of the *Gaming Machines Act 1992*. A response was received on 20 November 2017 which stated that the Commissioner has no comment to make in relation to the development. However, it was also advised that 'an application for alterations will need to be made and approved by [the Commissioner] prior to the commencement of any building works at the premises. An advisory note will be included in any approval documentation as required.

4.4 Australian Rail Track Corporation (ARTC)

A non-mandatory referral was sent to ARTC due to the location of the site in close proximity to an existing underground rail tunnel (located beneath the intersection of Bridge Street and East Terrace). No objections were raised as a result of this referral, however it is noted that ARTC are to be consulted throughout the development of engineering documentation, providing certainty that the existing rail tunnel will not be impacted as a result of the proposed building and excavation works.

4.5 Government Architect – Office for Design and Architecture South Australia (ODASA)

Whilst a referral to ODASA is not a mandatory requirement for this application under Schedule 8 of the *Development Regulations 2008*, the State Coordinator-General – as well as SCAP staff – concluded that the involvement of the Government Architect would significantly benefit the outcome of the proposed development. Delivering a positive design outcome is key concern of SCAP staff when considering the heritage impacts of the development, bulk and scale of the proposal and the landmark nature of the site.

ODASA were closely involved in the progression of the application – providing comment (in writing and in person) on all versions of the plans. Final revised comments were received from the Associate Government Architect on 28 February 2018 and can be summarised as follows:

- Acknowledgement that the heritage value of the existing hotel building has been significantly compromised and that the continued use of the landmark corner for

hotel accommodation is supported. The proposal will act as a benchmark for development of this size in Murray Bridge.

- Notwithstanding the height restrictions of the Development Plan, the proposed height is – on balance – supported given the design and siting of the taller elements on the land. The corner presence of the existing heritage listed hotel building has been adopted into the design proposed.
- Support is given to the ground floor design, including active uses at all four frontages. The design and function of entrance features to Bridge Street and internal car park are also supported, however it is noted that the Bridge Street entrance configuration could be improved.
- The design gives due consideration to the provision of natural light to all levels, including shared corridors and lift lobbies.
- The architectural expression is supported on balance. Further refinement of the perforated metal screening is recommended, including the final images and scale in which they will be presented. It was recommended that SCAP consider the further development of this design aspect.
- Support is given to the proposed landscaping plan.
- The simple design and material palette of the drive through liquor store is considered to be appropriate relative to its context.

5. COUNCIL TECHNICAL ADVICE

5.1 Rural City of Murray Bridge

Two referral responses have been received from the Rural City of Murray Bridge, dated 17 November 2017 and 28 February 2018. In principle support was provided for the concept of further developing the Bridgeport Hotel site, noting its economic and social benefits for the township and surrounding areas. However, Council also voiced the following key concerns:

- Further justification required regarding demolition of the Local heritage place
- Proponent should further explore methods of providing additional off-street car parking, suggesting that they seek an agreement with the adjacent Marketplace to utilise a portion of their car park.
- The provision of a pedestrian link between the Marketplace and the subject land has been investigated by Council. SCAP are requested to consider establishing an infrastructure agreement to allow for contribution to this pedestrian link by the applicant. In lieu of this, payment to Council's car parking fund (established under Section 50A of the *Development Act 1993*) is recommended due to the deficiency in off-street car parking numbers.

Council also requested additional clarification on matters regarding development within the public realm, building height, removal of the existing gas bullet, plant equipment, car parking, access and egress and stormwater management. All of these matters have been addressed in the applicant's letter dated 13 February 2018.

Council provided a total of 17 conditions to be attached to any approval issued by SCAP. Included in my recommendation are the eight conditions considered to be the most relevant. Conditions considered to be superfluous and not required related to the provision of a waste management plan, control of access and egress

requirements, car parking design and matters regarding detailed building design that can be satisfactorily addressed in the Building Rules Consent documentation. Many of these requirements have been addressed in conditions recommended by the Commissioner for Highways and through the provision of further information provided by MFY Traffic Engineering Consultants and Veolia Environmental Services.

It is noted that one recommended condition has been incorporated as an advisory note, ensuring that the applicant understands their need to obtain an Encroachment Permit from Council once Development Approval has been granted.

6. PUBLIC NOTIFICATION

The application is considered to be a Category 1 form of development in accordance with Schedule 9 Part 1 Clause 6(h) of the *Development Regulations 2008*, which states:

- 6 Any development which consists of any of the following, other than where the site of the development is adjacent land to land in a zone under the relevant Development Plan which is different to the zone that applies to the site of the development or where the development is classified as *non-complying* under the relevant Development Plan:

- (h) Any kind of development within a... Regional Town Centre... as delineated in the relevant Development Plan...

As such, no public notification was undertaken pursuant with Section 38 of the *Development Act 1993*.

7. POLICY OVERVIEW

The subject site falls wholly within the Regional Town Centre Zone and the Bridge Street Policy Area 11 as described within the Murray Bridge Council Development Plan Consolidated 11 August 2016. Whilst it is noted that this is the relevant Development Plan for the purposes of the assessment of this application, the changes to policy occurring as part of the Regional Town Centre Expansion DPA (26 April 2017) should also be considered. Analysis of these policies is strictly to inform the Panel of the vision for the Regional Town Centre Zone as it exists in the current Development Plan and how the proposed development would fit in to its future local context. This analysis can be found in the ATTACHMENTS.

Relevant planning policies are contained in the ATTACHMENTS and summarised below.

Figure 2 – Zoning Map (Consolidated 11 August 2016)

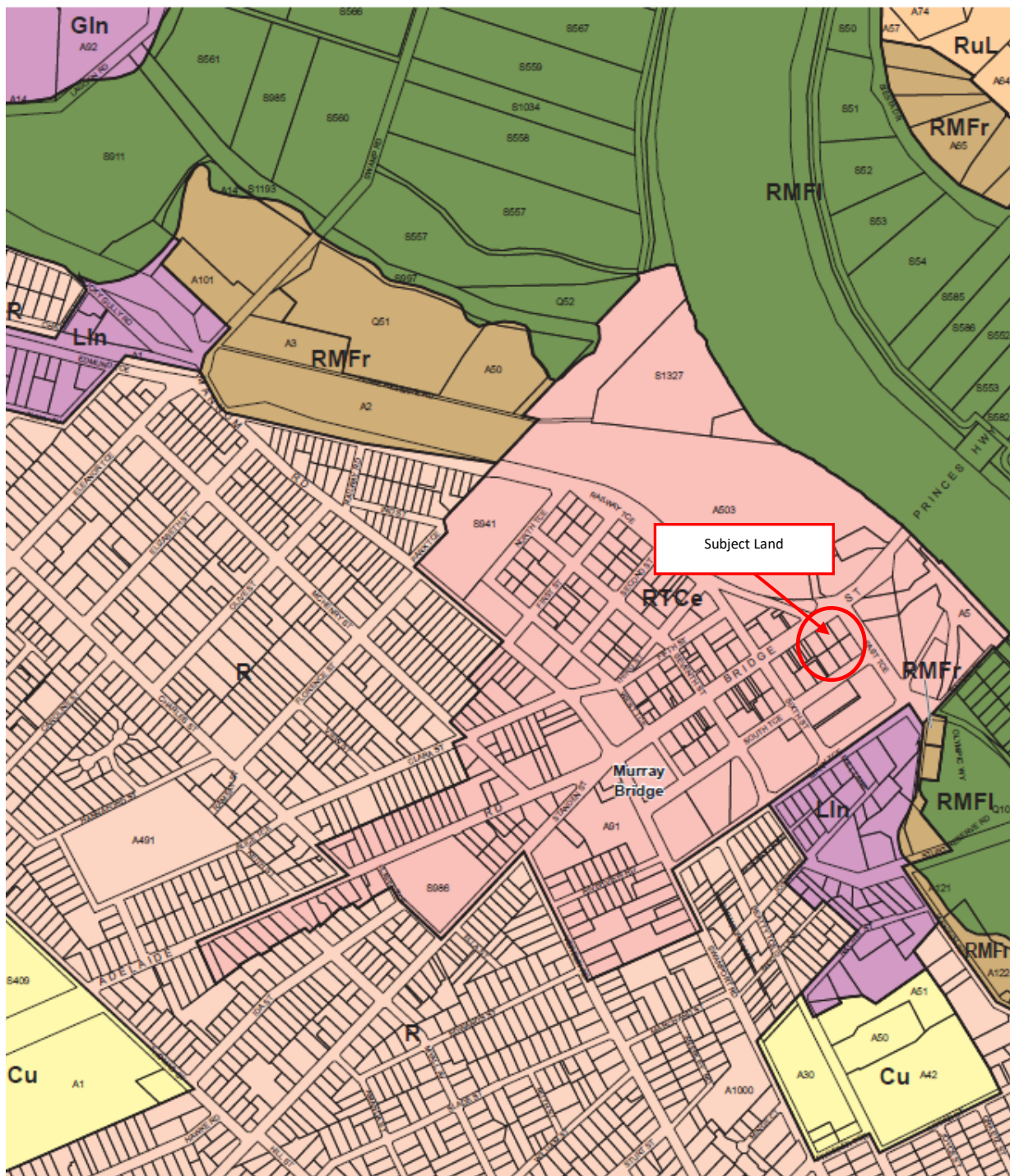


Table 1 – Relevant Objectives and PDC's

Zone	Objectives	PDC's
Regional Town Centre	1, 3, 5.	1, 4, 5, 6, 8, 9, 10, 11, 12, 14.
Bridge Street Policy Area 11	1, 2, 3.	1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16.
General Provisions	Objectives	PDC's
Advertisements	1, 2, 3.	1, 2, 4, 5, 6, 7, 8, 10, 13.
Centres and Retail Development	1, 2, 3, 5.	1, 2, 3, 4, 5, 6, 8.
Crime Prevention	1.	1, 2, 3, 4, 5, 6, 7, 8, 10.
Design and Appearance	1.	1, 2, 3, 5, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 18.
Energy Efficiency	1.	1, 2, 3.
Heritage Places	1, 2, 3.	1, 2, 3, 6.
Infrastructure	1, 5.	1, 2, 4, 5.
Interface between Land Uses	1, 2, 3.	1, 2, 9, 10, 11, 12.
Landscaping, Fences and Walls	1, 2.	1, 2, 3, 4.
Natural Resources	1, 2, 3, 5, 6, 7.	1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15.
Orderly and Sustainable Development	1, 2, 3, 4, 6, 7.	1, 3, 4, 6, 7.
Siting and Visibility	1.	1, 2, 5, 6, 8.
Sloping Land	1.	1, 2, 3, 4, 5.
Tourism Development	2, 3, 7.	1, 2, 3, 4, 5, 6.
Transportation and Access	2.	2, 5, 6, 8, 9, 10, 13, 14, 19, 21, 22, 23, 25, 26, 27, 28, 29, 30, 31, 32, 33, 35, 36, 37, 38.
Waste	1.	1, 2, 3, 4, 5, 10, 11.

7.1 Bridge Street Policy Area 11

The Bridge Street Policy Area seeks to reinforce Bridge Street as the traditional 'main street' and the historical retail centre of Murray Bridge. The parameters of the Policy Area support this objective, defined by West Terrace, East Terrace, South Terrace and Fifth Street. The desired character statement envisages the use of land for retailing, entertainment, cultural and tourist services, whilst encouraging new development to reinforce a cohesive lineal streetscape appearance. Proposed and future development along Bridge Street is encouraged to provide retail tenancies with direct street frontage at ground level.

An emphasis is placed on delivering a high standard of built form and design, noting that future development will respect the character and integrity of the locality. The appropriateness of the design will take into consideration the buildings siting, scale, bulk, height, materials, colours, signs and external illumination. A human scale is desirable within the Policy Area with policies speaking to pedestrian accessibility and the reduction of conflict with automobiles through appropriate car parking design.

Principles of development control (PDC's) seek to establish suitable access to and from future development sites that is sympathetic with the 'main street' character. Bridge Street, whilst approximately 30m in width, is a pedestrian focused environment where heavy vehicle movements are discouraged and buildings of one to two storeys predominate. Advertisements are encouraged to be simple in design and restrained in size.

7.2 Regional Town Centre Zone

The Murray Bridge Regional Town Centre Zone seeks to accommodate for the retail, entertainment, business, administrative, educational and cultural needs of the community, including rural areas such as Lake Alexandrina, Keith, Pinnaroo and Swan Reach. The varied functions of the zone are reflected in their corresponding policy areas, however it is a key objective that the zone as a whole functions in a cohesive and integrated manner. This objective is supported by a desired character statement that promotes pedestrian accessibility throughout the zone and the restriction and calming of vehicular traffic.

Relevant PDC's of the zone speak to access and egress, provision of off-street car parking (including under-croft car parking), reinforcement of the townscape character through appropriate design, materials and finishes, and the retention of items that display a historic or cultural significance.

7.3 Council Wide

General provisions of the Development Plan establish a broad set of goals applied to all development applications (where applicable), the most relevant of which have been identified in Appendix One. These policies reflect the significance of the proposed development and its potential impact on the locality through traffic generation and car parking, waste production and management, demolition of a local heritage place, modification and provision of infrastructure and building height. Additionally, the Development Plan seeks to guide development toward providing a high quality of design that is sensitive to its surrounding environment through minimising impacts on adjoining land or any existing or envisaged sensitive land uses in the area.

Crime Prevention Through Environmental Design (CPTED) principles have been broadly adopted by the Development Plan and seek development outcomes that provide for a suitable level of passive surveillance and limited opportunities for entrapment. Provisions that guide the allocation of car parking, advertising and landscaping also form part of the relevant Council wide provisions considered in the assessment of this application.

7.4 Current Development Plan

A brief outline of the provisions of the current Development Plan (Consolidated 23 January 2018) can be found in the ATTACHMENTS.

8. PLANNING ASSESSMENT

The proposal seeks to undertake the demolition of a Local heritage place (existing Bridgeport Hotel) and construction of a six (6) storey hotel building comprising ground level and under-croft car parking and laundry; ground level restaurant, bar, function room, lounge, gaming room and detached drive through liquor store; first floor function room, terrace, gym, swimming pool and 12 accommodation units, and; a further 87 accommodation units over the second to fifth levels, each with their own balcony.

8.1 Quantitative Provisions

The Murray Bridge (RC) Development Plan is mostly silent on quantitative requirements, particularly for commercial types of development – preferring a qualitative range of assessment criteria.

	Development Plan Guideline	Proposed	Guideline Achieved	Comment
Building Height	8 metres	23.5 to 29 metres	YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> PARTIAL <input type="checkbox"/>	
Car Parking	387 car parks	160 car parks	YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> PARTIAL <input type="checkbox"/>	Justification provided by MFY Pty Ltd – Traffic Consultants.
Bicycle Parking	Nil	12 at ground level. 12 + under-croft.	YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> PARTIAL <input type="checkbox"/>	

8.2 Land Use and Character

The Regional Town Centre Zone envisages a range of development types that will maintain and reinforce the primary retail, business and recreational function of the zone. Hotels and restaurants are defined as envisaged developments in Zone PDC 1 – consistent with both the existing and proposed uses of the land. The function of a hotel is defined in Schedule 1 of the *Development Regulations 2008* as a licensed premises under the *Liquor Licensing Act 1985*, and includes, among other things, the sale of liquor on site, preparation and sale of meals, live entertainment and accommodation. Other associated land uses such as a function centre, gaming area and detached drive through liquor store are considered appropriate and reflect the current use of the land.

The character of the locality is defined by the main street function and hierarchy of Bridge Street and it's predominantly one to two storey built form. Development addressing Bridge Street is envisaged to abut the road frontage and provide visual interest and permeability through the incorporation of windows and shopfronts at ground level, whilst ensuring that a cohesive lineal streetscape is provided. This main street shop-front character is reflected in the podium element of the building, being up to two-storeys in height at the boundaries of Bridge Street and East Terrace, whilst also providing a pedestrian canopy to the corner, regularly spaced columns and an entry feature that emphasises the Bridge Street pedestrian entry point. Active uses have been provided along the East Terrace frontage to provide a human scale and contribute to the vibrancy of the Policy Area. While the proposal does not provide a continuous built form to Bridge Street, ODASA have resolved that the landscaped timber screen provided is suitable given its height and role in screening the proposed ground-level car park.

The desired character statement of the Bridge Street Policy Area seeks to ensure that future development exhibits a high quality of design and provides economic vitality to the region. It is also encouraged that development provides a sense of place and a focal point for the town. The subject land is a landmark site, prominently placed on the north-eastern end of Bridge Street, with additional frontages to East Terrace and South Terrace. Its prominence, coupled with the unique scale and design of the building will provide a new focal point for the town, whilst also providing guests with a unique view of the historic riverfront and township more broadly. The proposed building will retain and expand upon many of the existing uses of the land in a modern setting. The increased accommodation capacity of the proposed hotel will aim

to support the tourism sector throughout the region. The envisaged land uses are set to function throughout the day and night, providing a sense of vibrancy to the township.

The proposed land uses are reasonably envisaged in the Regional Town Centre Zone and the built form will contribute positively to the desired character of the township and Bridge Street Policy Area.

8.3 Design and Appearance

The Bridge Street Policy Area guides the design of future development by establishing some qualitative built form principles such as:

- Buildings should abut all road frontages where there is more than one road frontage.
- Design elements such as verandahs and balconies consistent with local features should be included in building design.
- Buildings should maintain the existing horizontal massing of built-form while incorporating vertical proportioning in the composition of facades.
- Provision of a visually permeable ground level through the provision of display windows and shops.
- Large expanses of bright colours are to be avoided.
- The existing spatial character of Bridge Street should inform appropriate building heights and massing.
- Design that is compatible with traditional architecture.
- Advertisements should be simple in design and restrained in size.

Notwithstanding the Policy Area provisions, the general provisions of the Development Plan seek a design outcome that is contemporary in nature and sympathetic with the scale of development throughout the locality. Design and Appearance PDC 3 envisages a building design that reduces visual bulk and provides visual interest through suitable articulation, finishes, vertical and horizontal highlights and the variation of facades (among other aspects). Plant equipment, service and storage areas are envisaged to be screened from view, with screening forming part of the overall design concept of the building (PDC's 8 and 14). Additionally, it is desirable for pedestrian entry points to form part of the overall building design, allowing for clear and direct access from the public realm (PDC 9).

The importance of delivering a high quality of design for the proposed development is demonstrated through the continued involvement of ODASA during pre-lodgement and as a non-mandatory referral body following lodgement of the application. Given their expertise in the matter, considerable weight should be placed on ODASA's referral response – prepared by the Associate Government Architect on 28 February 2018.

It is noted that ODASA have provided support for the proposed design, advising that its height, location, intensity of uses and overall concept are acceptable given the site context and will contribute to the revitalisation of the area. Its landmark location has provided an opportunity for the proposal to become a benchmark for development of this size in Murray Bridge and will be a 'defining structure'. The overall height of the development is mitigated through the use of separate 'podium' and 'tower' elements, each provided with appropriate set-backs and materiality. The established main street shop-front character is reflected in the horizontal emphasis of the podium and provision of a verandah / canopy feature over the footpath, whilst also reflecting the historical 'corner presence' of the existing hotel building.

The overall architectural expression is complimented by perforated metal screening to the podium edges, alfresco dining with fixed umbrellas and landscaping at ground level, vertically proportioned north-west and south-east upper storey facades, covered in white and black metal cladding as well as vertical glazing. The north-east and south-west frontages of the hotel building are horizontally proportioned with expressed floor plates, white/black metal panel cladding accents and timber façade cladding. Signage is simple in nature, with large 'B' lettering at the car park entrance and on the podiums northern corner, as well as 'BRIDGEPORT HOTEL' lettering to the Bridge Street frontage (pedestrian entry point) and north-eastern frontage (top of the building). The materiality of the proposed drive through liquor store is of neutral tones and considered acceptable in the locality.

Notwithstanding the height and scale of the proposed building, as well as some departures from the provisions of the Development Plan, I am of the opinion that on balance, the proposed design concept is appropriate.

8.4 Heritage

The Development Plan identifies portions of the Bridgeport Hotel as a local heritage place in *Table MuBr/4 – Local Heritage Places*, as demonstrated below:

2-6 Bridge Street MURRAY BRIDGE	Bridgeport Hotel; Elements to be retained - Original large two-storey Victorian Hotel section, including original rear single - storey storeroom and later two- storey Victorian extensions	99	CT 5187/857	a c d f	14209
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Heritage PDC 1 seeks to prevent the demolition or re-development of local and State heritage places unless either of the following apply:

- That portion of the place to be demolished, destroyed or removed is excluded from the extent of the place...
- The structural condition of the place represents an unacceptable risk to public or private safety.

Further provisions of the Development Plan allow for complimentary development to be undertaken, provided the elements of heritage value are retained.

As discussed, the proposal seeks to completely demolish the existing local heritage place on the land and construct a new building in its place. Whilst the Development Plan speaks against such development, detailed investigations have been undertaken by Dash Architects (on behalf of the applicant) in an effort to investigate the justification of the heritage listing. The advice takes into consideration those relevant criteria identified in Section 23(4) of the *Development Act 1993*, with assistance from Planning SA's 'Planning Bulletin – Heritage' as well as a draft copy of the Murray Bridge Town Centre and Environs Local Heritage Register (dated November 2001), and is a result of a site visit and structural interrogation dated 15 March 2017. The following is a summary of the findings of this report:

- Historic imagery of the site from 1879 to the present day, coupled with a visit to the site, has demonstrated that substantial modification of the building has taken place.
- The integrity of the place has been significantly compromised through the loss of ground floor facades and spaces.

- The capacity for the building to reflect its prominent role in the lives of local residents has not been suitably established or defined.
- Due to the historical modification of the building, it is concluded that it no longer demonstrates an aesthetic merit of significance to the local area.
- Whilst the site of the building is prominent in nature (location and topography), the building itself is not considered to retain any notable landmark features.

Mr Craig Eyles of CED Building Design attended the site on 6 March 2017 and has provided a detailed outline of building modifications over time (with images) in an attempt to demonstrate the degradation of the buildings heritage fabric. This report and its associated images can be found in the ATTACHMENTS.

Heritage advice was also sought from Flightpath Architects by the Rural City of Murray Bridge. A report was provided with Council's comments dated 17 November 2017, which disagreed with Dash Architects determination that the existing building does not meet the requirements of Section 23 (4) of the *Development Act 1993*. Most significantly, it is argued that the construction of external ground level walling is such that a sufficient level of original wall remains and that the social and cultural values of the community should carry more weight in the justification of its heritage listing. Furthermore, the report continues to reflect an opposition to the findings and opinions of Dash Architects.

Notwithstanding the differing opinions identified in the above advice, it is clear from all reports that substantial modification has been undertaken to the building since its opening in 1885.

Given its presence in *Table MuBr/4*, the local heritage listing of the building cannot be ignored, however the weight given to the Heritage provisions of the Development Plan under these circumstances have been carefully considered. Whilst heritage places should be retained where possible and regeneration of an existing heritage buildings is encouraged, the modifications to the current building have significantly diminished the Heritage value of the place to the extent that it no longer provides a positive contribution to the community from a heritage perspective, not to mention the negative impacts in terms of presentation and amenity to streetscape which differ to the wider aspirations for the Regional Town Centre Zone.

As such, the advice provided by Dash architects in conjunction with an assessment against the intention of the overall zone and its future design aspirations to provide a vibrant and attractive town centre, demolition of the existing heritage place is considered to be appropriate in exchange for a quality design outcome and the retention of the existing uses of the site.

ODASA have provided a response to the heritage impacts in their referral advice, stating that they 'acknowledge the findings by the heritage consultant that over time the heritage value of the existing hotel has been significantly compromised' and have – on balance – endorsed the proposed building design.

8.5 Traffic Impact, Access and Parking

The Development Plan speaks to the provision of safe and convenient access for all anticipated modes of transport, the retention/provision of suitable site lines for vehicles and pedestrians entering and exiting the site, the provision of end-of-trip facilities to encourage active transport and the provision of parking areas that can meet anticipated demand and where used during non-daylight hours, are fitted with a suitable lighting system that causes minimal nuisance to adjoining land. In addition to the above, Transport and Access PDC 32 adds the following detail in an effort to guide the design of vehicle parking areas, in that they should:

- Facilitate safe and convenient pedestrian linkages to the development and areas of significant activity or interest
- Not inhibit safe and convenient traffic circulation
- Avoid the need to use public roads when moving from one part of the parking area to another
- Minimise the number of vehicle access points to public roads
- Integrate car parking areas with other land uses in the locality to reduce the total extent of vehicle parking
- Not dominate the character and appearance of a centre, and
- Provide landscaping that will shade and enhance the appearance of vehicle parking areas.

Bridge Street Policy Area 11 provides additional guidance, stating that 'direct vehicular access to car parking... should not be provided from Bridge Street'. Projected car parking demand is outlined in *Table MuBr/2 – Off Street Vehicle Parking Requirements* and identifies that the proposed hotel and associated uses would require a total of 387 car parking spaces. No quantitative requirements are provided for the provision of bicycle parking.

The proposed development seeks to provide a total of 160 off-street car parks for guests and other hotel patrons, with direct access from Bridge Street (via a two-way crossover) and South Terrace (via two x two-way crossovers and one single exit-only crossover). Pedestrian access to the site is provided along all road frontages, with direct building access at Bridge Street, East Terrace and from the car park to the south-west, whilst a pedestrian walkway is provided from South Terrace. Included in the car park design is the provision for 12 bicycle parks at ground level as well as additional secure bicycle parking in the northern corner of the under-croft car park.

The applicant engaged the services of MFY (Traffic Engineering Consultants), who undertook a car parking assessment of the proposal. It has been concluded that the current proposal will sufficiently cater for future car parking requirements when assisted by local on-street car parking. It was concluded that all uses of the site (used to define the car parking requirements in *Table MuBr/2*) will not generate peak demand at the same time. It was demonstrated in the MFY report dated 15 August 2017 that the car parking provided will sufficiently meet peak requirements throughout the day, whilst some on-street car parking would be required through the evenings when retail businesses in the locality will be closed. MFY also provided turning paths that demonstrate the sites ability to cater for service, waste collection and delivery vehicles.

A landscaping plan was prepared by Oxigen on 29 June 2017, which identifies the provision of shade trees to the car parking area coupled with ground cover plantings within garden beds. Provision has also been made for 'a timber batten screen and planting adjacent the Bridge Street access point to assist with softening and screening the car park. A car park lighting plan was also provided as prepared by TMK Consulting Engineers expressing that the proposed lighting arrangement will meet relevant Australian Standards.

It is considered that the proposed car parking and vehicle access arrangement is acceptable when assessed against the relevant provisions of the Development Plan. Whilst access will still be gained from Bridge Street, the existing nature of this crossover must be considered. The access arrangements to South Terrace, whilst impacting upon existing on-street car parking, is considered to provide a more functional and cohesive arrangement than that which currently exists. DPTI Safety and Service Division have reviewed the proposed development and have provided conditional support to the access arrangements, including the provision of a right turn lane in place of an existing median strip on Bridge Street.

Matters such as the internal function of the car parking area are acceptable and will be in accordance with relevant Australian Standards (enforced by condition) and the suitability of landscaping and provision of bicycle parking and pedestrian access points is considered appropriate.

8.6 Environmental Factors

8.6.1 Crime Prevention

The Development Plan seeks to ensure that development can support a crime-free environment through passive surveillance and the provision of lighting in appropriate locations - see Crime Prevention PDC's 1, 2 & 5.

To assist with passive surveillance, the proposed hotel will provide an active ground level comprising outdoor dining, two (2) key access points, glazing to most ground level facades and the provision of an outdoor children's play area. Accommodation units are provided with balconies, which overlook the locality to the north-east and south-west.

Lighting is proposed to the entire car park area and the perimeter of the building, supporting Crime Prevention PDC 5.

8.6.2 Noise Emissions

An environmental noise assessment was undertaken by Sonus Pty Ltd on behalf of the applicant, and considered the environmental noise impact of the proposed development on the closest noise sensitive locations (dwellings located approximately 160m to the north). The noise generating components that were considered include:

- Patrons in outdoor areas
- Mechanic services plant
- Vehicle movements and activities associated with the use of the car park and liquor drive through, and
- Music.

It was concluded that the proposed development could suitably meet the requirements of the *Environment Protection (Noise) Policy 2007* and the Interface provisions of the Development Plan, provided the applicant complies with a number of recommendations by Sonus Pty Ltd. These recommendations will form a condition(s) on any approval granted by SCAP.

8.6.3 Stormwater Management

Stormwater management is envisaged by the Development Plan to be undertaken in a manner that will not result in the pollution of receiving water ways and where post-development flows are consistent with pre-development flows off the land (see Natural Resources PDC's 5, 7, 8 & 11 in particular). It is also noted that where development is to be undertaken on steep sloping land, that site drainage systems should minimise the chance of erosion and avoid impacts on slope stability (see Sloping Land PDC 5).

The proposal has been accompanied by a stormwater management plan and associated civil drawings prepared by TMK Consulting Engineers on 17 July 2017. This information identifies that the proposed development will result in peak flows from the site that are consistent with pre-development flow rates. An Ecosol RSF 4200 Gross Pollutant Trap is proposed as the primary treatment method for stormwater.

The proposed development sufficiently meets the Stormwater requirements of the Development Plan in that no additional impacts are envisaged on receiving water ways following the completion of the development.

8.6.4 Waste Management

The Development Plan requires that future development demonstrate suitable waste control measures that are environmentally sensitive and prevent visual and physical impacts on adjoining land or stormwater management systems. Waste PDC 6 seeks specific treatments to waste collection and storage areas, stating that they should be screened from view, located on impervious surfaces and designed in a way that ensures all waste is contained within the boundaries of the subject land.

The proposed development includes two separate waste storage areas, one in the southern corner of the under-croft car park and another immediately adjacent the proposed drive through liquor store along the south-western boundary of the site. A waste management plan was prepared by Veolia Environmental Services in February 2018 and is included in the package of information for approval. The waste management plan highlights that the proposed development can be sufficiently serviced in a cost effective and environmentally sensitive manner, with general waste collection occurring three times per week and recyclable materials collected two times per week.

The site plans identify that both bin store locations will be secure, located on a hard stand surface and sufficiently screened by way of 2.4m high concrete walls and galvanised steel framed gates.

8.6.5 Energy Efficiency

While the provision of on-site solar photovoltaic panels is not defined on the plans provided, it is noted that the development will not jeopardise the ability for buildings on adjoining land to accommodate for solar energy generation in the future - complimenting Energy Efficiency PDC 3.

The proposed hotel building will have a roof form and pitch suitable to accommodate solar photovoltaic panels in the future.

8.7 Interface

Interface provisions of the Development Plan seek to mitigate impacts on adjoining land owners by limiting noise, vibration, the emission of effluent or airborne pollutants, light spill, traffic impacts and hours of operation. Methods adopted to assist with the reduction of interface issues can vary and can depend greatly on the location of sensitive receptors.

As discussed above, Sonus Pty Ltd have undertaken an assessment of the proposed development and estimated that the noise impacts associated with the use of the land will not detrimentally impact on sensitive receptors in the locality. This assessment included any noise impacts associated with traffic generation to and from the site.

Traffic impacts on the local road network are envisaged to be minimal given the existing use of the site for hotel and other associated land uses. MFY have also provided justification that the site can suitably cater for the safe and efficient movement of all expected vehicle types, including access to and from the land by service, waste collection and delivery vehicles.

The site is provided with existing access to a potable water supply and sewerage system. This, coupled with the proposed stormwater drainage plan identifies that the site is capable of managing waste and stormwater flows without impacting adjoining land owners or the public realm. Matters regarding the ventilation of proposed kitchen areas must meet the relevant Australian Standard 1668.2-2012. Detailed drawings prepared for the purposes of an assessment against the Building Code— will be required to address this Australian Standard. Given the existing nature of the hotel use, I am of the opinion that any treatments installed in accordance with AS 1668.2-2012 will sufficiently mitigate any likely odour impacts on adjoining land. Hours of operation of the development are defined by the existing Hotel Licence 50108345 and allow the site to operate until midnight on Sundays and until 2:00am on all other days. It is understood that the existing operating hours are to remain (enforced by condition).

Interface concerns that are unique to this proposal are as a result of the proximity of the land to the State heritage listed Murray Bridge Transport Precinct and an existing train tunnel under the intersection of Bridge Street and East Terrace. These concerns have been previously deemed acceptable and are addressed in Sections 4.2 & 4.3 of this report.

Further investigation was carried out by TMK Consulting Engineers as to the developments impact on the existing underground train tunnel by way of construction vibration. It is envisaged that the proposed development will have minimal impact on the tunnel, however further investigation will be required during the preparation of detailed engineering drawings. The Australian Rail Track Corporation were referred the application details and have subsequently confirmed their satisfaction with the information provided by TMK.

Based on the above information, I am of the opinion that the proposal satisfactorily meets the interface requirements of the Development Plan.

9. CONCLUSION

Having undertaken an assessment of the proposal against the relevant provisions of the Development Plan and in particular the key objectives of the Regional Town Centre Zone and Bridge Street Policy Area 11, I am of the opinion that the development achieves, on balance, the provisions of the plan.

In particular, the provision of a primary focus for business and commercial services throughout the region and reinforce the role of Bridge Street as the traditional 'main street' of Murray Bridge – as well as the plans and supporting information provided in this application,

Significant departures from the Development Plan, including the removal of the existing Local heritage place and a discrepancy in car parking provision, have been suitably justified and mitigated through careful analysis of the traffic impacts associated with the development, the continuation of the existing (historic) use of the land and the resulting high quality design outcome. The final design was continually improved through engagement with ODASA, receiving written endorsement from the Associate Government Architect.

Assessment of the proposal against the Council Wide provisions of the Development Plan demonstrates that the development will not have a detrimental impact on adjoining land, the local road network or other sensitive land uses in the locality. The town centre location of the development is such that hotel patrons will be awarded with prominent views of the river and Murray Bridge Township, whilst having little to no impact on adjoining land by way of overshadowing and overlooking.

10. RECOMMENDATION

It is recommended that the State Commission Assessment Panel:

- 1) RESOLVE that the proposed development is NOT seriously at variance with the policies in the Development Plan.
- 2) RESOLVE that the State Commission Assessment Panel is satisfied that the proposal generally accords with the related Objectives and Principles of Development Control of the Murray Bridge (RC) Development Plan.
- 3) RESOLVE to grant Development Plan Consent to the proposal by Kerin Bay Pty Ltd for the demolition of an existing local heritage place and construction of a new hotel building and standalone drive through liquor store at 2-6 Bridge Street Murray Bridge subject to the following reserved matters and conditions of consent.

PLANNING CONDITIONS

1. That except where minor amendments may be required by other relevant Acts, or by conditions imposed by this application, the development shall be established in strict accordance with the following details and plans submitted in Development Application No 415/E014/17.

Architectural Plans (CED Building Design)

- Existing Site Plan Rev DA1 – 17 Jul 2017 – C1201 Sheet 1.01/DA1.
- Basement Plan Rev DA3 – 18 Dec 2017 – C1201 Sheet 1.03/DA3.
- Site Plan Rev DA3 – 18 Dec 2017 – C1201 Sheet 1.04/DA3.
- Ground Floor Plan Rev DA3 – 18 Dec 2017 – C1201 Sheet 1.05/DA3.
- First Floor Plan Rev DA3 – 5 Sep 2017 – C1201 Sheet 1.06/DA3.
- Second Floor Plan Rev DA1 – 17 Jul 2017 – C1201 Sheet 1.07/DA1.
- Third Floor Plan Rev DA1 – 17 Jul 2017 – C1201 Sheet 1.08/DA1.
- Fourth Floor Plan Rev DA1 – 17 Jul 2017 – C1201 Sheet 1.09/DA1.
- Fifth Floor Plan Rev DA1 – 17 Jul 2017 – C1201 Sheet 1.10/DA1.
- Part Roof Drainage Plan Rev DA2 – 10 Aug 2017 – C1201 Sheet 1.11/DA2.
- Apartment Types Rev DA2 – 10 Aug 2017 – C1201 Sheet 1.12/DA2.
- Proposed Elevations Rev DA2 – 18 Dec 2017 – C1201 Sheet 2.01/DA2.
- Proposed Elevations Rev DA2 – 18 Dec 2017 – C1201 Sheet 2.02/DA2.
- Bottleshop Proposed Elevations & Cross Section Rev DA2 – 18 Dec 2017 – C1201 Sheet 2.03/DA2.
- Streetscape Elevations Rev DA2 – 18 Dec 2017 – C1201 Sheet 2.04/DA2.
- Bin Store Plan Rev DA1 – 4 Apr 2018 – C1201 Sheet 2.06/DA1.

Reports & Supporting Documentation

- Letter from Botten Levinson – dated 13 Feb 2018.
- Letter from Botten Levinson – dated 07 Feb 2018.
- Letter from Eureka Hotel Group – dated 07 Dec 2017.

- Letter from Botten Levinson – dated 22 Sep 2017.
- Email from Botten Levinson – dated 5 April 2018
- External Colour and Finishing Schedule prepared by CED Building Design – dated 14 Aug 2017.
- Schedule of Plan Amendments prepared by CED Building Design – dated 22 Sep 2017.
- Changes made to Bridgeport Hotel – Observations of CED Building Design – dated 6 Mar 2017.
- Landscaping Plan Report prepared by Oxigen Pty Ltd – dated 29 Jun 2017.
- Landscape Planting Plan – Drawing 17.027.101 Rev A – dated 02 Jun 2017.
- Landscape Planting Plan (detail) – Drawing 17.027.103 Rev A – dated 02 Jun 2017.
- Landscape Planting Plan (upper level terrace) – Drawing 17.027.104 Rev A – dated 02 Jun 2017.
- Landscape Species & Screen – Drawing 17.027.105 Rev A – dated 02 Jun 2017.
- Review of Proposed Demolition of Local Heritage Place (report) prepared by Dash Architects – Issue B – dated 15 Aug 2017.
- Contextual Analysis (report) prepared by Dash Architects – Issue B – dated 28 Aug 2017.
- Traffic and Car Parking Access Review prepared by MFY Traffic Engineering Consultants – dated 15 February 2017.
- Traffic and Car Parking Assessment prepared by MFY Traffic Engineering Consultants – dated 15 Aug 2017.
- Revised planning report prepared by URPS – dated 15 Sep 2017.
- Assessment of Impact of Proposed Development on Existing Railway Tunnel prepared by TMK Consulting Engineers – dated 22 Aug 2017.
- Waste Management Plan prepared by Veolia Environmental Services – dated February 2018.
- Proposed Perforated Screen Images prepared by CED Building Design – dated 17 Jul 2017.
- Exterior Lighting Modelling Report prepared by TMK Consulting Engineers – dated 21 Jun 2017.
- Stormwater Management Report prepared by TMK Consulting Engineers – dated 17 Jul 2017.
- Wind Loading Report prepared by TMK Consulting Engineers – dated 14 Sep 2017.
- Email from Benoit Hopkins (Engineer – TMK Consulting Engineers) – response to Council's stormwater concerns – dated 22 Jan 2018.
- Environmental Noise Assessment prepared by Sonus Pty Ltd – dated September 2017.

STORMWATER

2. Stormwater run-off shall be collected on-site and discharged without jeopardising the safety and integrity of the adjoining arterial road. Any alterations to road drainage infrastructure required to accommodate the development shall be at the expense of the applicant.
3. All stormwater design and construction shall be in accordance with Australian Standards and recognised engineering best practices to ensure that stormwater does not adversely affect any adjoining property or public road. Final details of the stormwater design, including onsite retention and reuse, shall be provided to the satisfaction of the State Commission Assessment Panel prior to the approval of any building works on site.

TRAFFIC AND CAR PARKING

4. The removal of the existing line marked bays and the provision of new line marked bays on South Terrace shall be undertaken at the applicant's expense.
5. Waste disposal vehicles and general delivery vehicles shall only service the development between the hours of 7:00am and 7:00pm Monday to Saturday (inclusive), and shall only load or unload within the confines of the subject land.

ENVIRONMENT

6. All external lighting of the site, including car parking areas and buildings, shall be designed and constructed to conform with Australian Standards and must be located, directed and shielded and of such limited intensity that no nuisance or loss of amenity is caused to any person beyond the site.
7. Mechanical plant or equipment shall be designed, sited and screened to minimise noise impact on adjacent premises or properties.
8. A Construction Environment Management Plan (CEMP) shall be prepared and implemented in accordance with current industry standards – including EPA publication 'Environmental Management of On-site Remediation' – to minimise environmental harm and disturbance during construction. The management plan must incorporate, without being limited to, the following matters:
 - a) Air quality, including odour and dust
 - b) Surface water including erosion and sediment control
 - c) Soils, including fill importation, stockpile management and prevention of soil contamination
 - d) Groundwater, including prevention of groundwater contamination
 - e) Noise
 - f) Occupational health and safety.

A copy of the CEMP shall be provided to the State Commission Assessment Panel prior to the commencement of site works for both the hotel and car park.

9. Exhaust from the kitchen must be dispersed in accordance with Australian Standards AS/NZS 1668.1 and AS 1688.2.

INFRASTRUCTURE

10. All Council, utility or state-agency maintained infrastructure (i.e. roads, kerbs, drains, crossovers, footpaths etc) that is demolished, altered, removed or damaged during the construction of the development shall be reinstated to Council, utility or state agency specifications. All costs associated with these works shall be met by the proponent.

COMMISSIONER OF HIGHWAYS CONDITIONS

11. Operating hours shall remain consistent with the current Hotel Licence 50108345 following completion of the development.
12. The site shall be accessed via a single access point to/from Bridge Street, with the balance of access being via South Terrace. The Bridge Street access shall provide left-in, left-out and right-in movements only.

13. The Bridge Street access shall be provided with a channelised right turn treatment that is designed and constructed to DPTI and Council satisfaction. All costs (including design, construction, project management and any changes to road drainage, lighting etc. required) shall be borne by the applicant. Prior to undertaking detailed design, the applicant shall contact Mr Justin Henderson, Asset Enhancement Engineer, Road and marine Assets on telephone (08) 7223 6029 or via email Justin.Henderson@sa.gov.au to progress this.
14. The applicant shall enter into a Developer Agreement with DPTI to undertake and complete the required road works.
15. All loading and unloading, parking and manoeuvring areas shall be designed and constructed to ensure that all vehicles can safely enter and exit the subject land in a forward direction.
16. Signage and line marking shall be utilised to reinforce the desired flow of traffic to, from and through the site.
17. The largest vehicle permitted on the site shall be an 11 metre long rigid truck. This vehicle shall enter the site via the Bridge Street access and exit the site via the South Terrace access point.
18. All car parking and internal manoeuvring areas shall be in accordance with AS/NZS 2890.1:2004 and 2890.6:2009.
19. All commercial vehicle facilities shall be designed in accordance with AS 2890.2-2002.
20. All redundant crossovers to/from the site shall be reinstated to Council standard kerb and gutter at the applicant's expense prior to the operation of the development.
21. All materials and finishes shall not be permitted to result in glare or other effects that will result in the discomfort or impairment of road users.
22. Signage on this site that is viewable from the adjacent or nearby roads shall not utilise any element of LED/LCD display (the use of LED lighting for the internal illumination of a sign box is permissible).
23. Signage on this site shall not contain any element that flashes, scrolls, moves or changes.
24. Illuminated signage on this site shall be limited to a low level of illumination (<200 cd/m²) so as to minimise distraction and discomfort to motorists.
25. Non-illuminated signage on this site shall be finished in a material of low reflectivity to minimise the risk of sun/headlamp glare that may dazzle or distract motorists.
26. The utilisation of Trailer Mounted Variable Message Displays for advertising purposes shall not occur on or adjacent the subject land.

ADVISORY NOTES

- a. This Development Plan Consent will expire after 12 months from the date of this Notification, unless final Development Approval from Council has been received within that period or this Consent has been extended by the State Commission Assessment Panel.
- b. The applicant is also advised that any act or work authorised or required by this Notification must be substantially commenced within 1 year of the final Development Approval issued by Council and substantially completed within 3 years of the date of final Development Approval issued by Council, unless that Development Approval is extended by the Council.
- c. The applicant has a right of appeal against the conditions which have been imposed on this Development Plan Consent. Such an appeal must be lodged at the Environment, Resources and Development Court within two months from the day of receiving this notice or such longer time as the Court may allow. The applicant is asked to contact the Court if wishing to appeal. The Court is located in the Sir Samuel Way Building, Victoria Square, Adelaide, (telephone number 8204 0289).
- d. An application for alterations will need to be made and approved by the Liquor and Gambling Commissioner prior to the commencement of any building works at the premises.
- e. An Encroachment Permit will be separately issued by Council for the proposed encroachments into the public realm once Development Approval is granted. In particular, your attention is drawn to the following:
 - i) An annual fee may be charged in line with the Encroachment Policy
 - ii) Permit renewals are issued on an annual basis for those encroachments that attract a fee.
 - iii) Unauthorised encroachments will be required to be removed.

DEWNR – HERITAGE SA ADVISORY NOTES

- f. The applicant is reminded of the following requirements of the *Heritage Places Act 1993*:
 - i) If an archaeological artefact believed to be of heritage significance is encountered during excavation works, disturbance in the vicinity shall cease and the SA Heritage Council shall be notified.
 - ii) Where it is known in advance (or there is reasonable cause to suspect) that significant archaeological artefacts may be encountered, a permit is required prior to commencing excavation works.

For further information, contact the Department of Environment, Water and Natural Resources.

- g. The applicant is also reminded of the following requirements of the *Aboriginal Heritage Act 1988*:
 - i) If Aboriginal sites, objects or remains are discovered during excavation works, the Aboriginal Heritage Branch of the Aboriginal Affairs and Reconciliation Division of the Department of the Premier and Cabinet (as delegate of the Minister) should be notified under Section 20 of the *Aboriginal Heritage Act 1988*.



Matthew Fielke
PLANNING OFFICER









EAST TERRACE

PLANT LEGEND

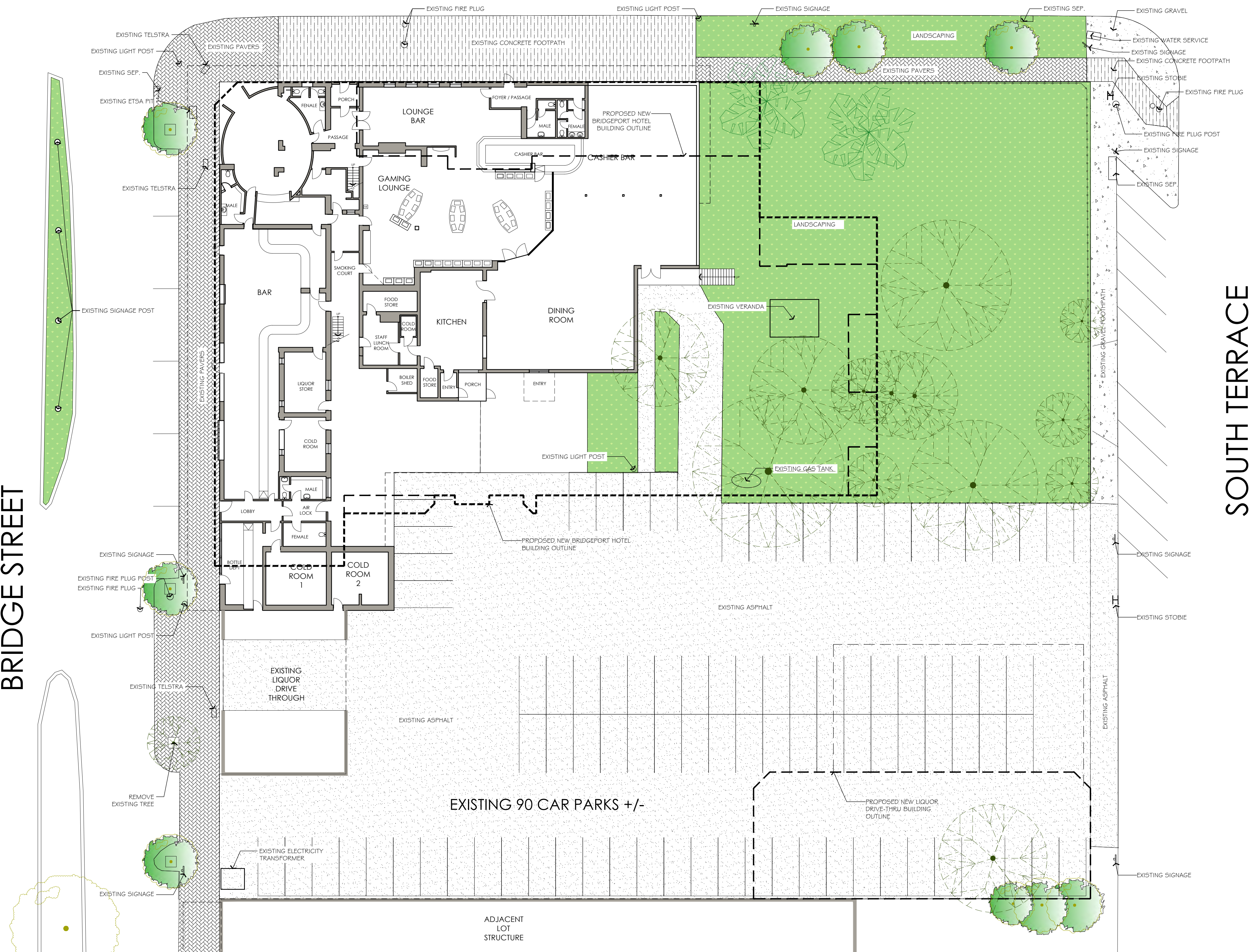
- EXISTING TREE TO REMAIN IN PLACE
- EXISTING PALM TREE TO BE RELOCATED ON SITE
- EXISTING TREE TO BE REMOVED

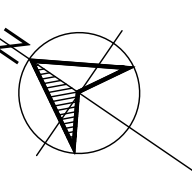

FLOOR FINISH

- EXISTING PAVERS
- EXISTING CONCRETE
- EXISTING GRAVEL
- EXISTING ASPHALT
- EXISTING LANDSCAPE

BRIDGE STREET

SOUTH TERRACE

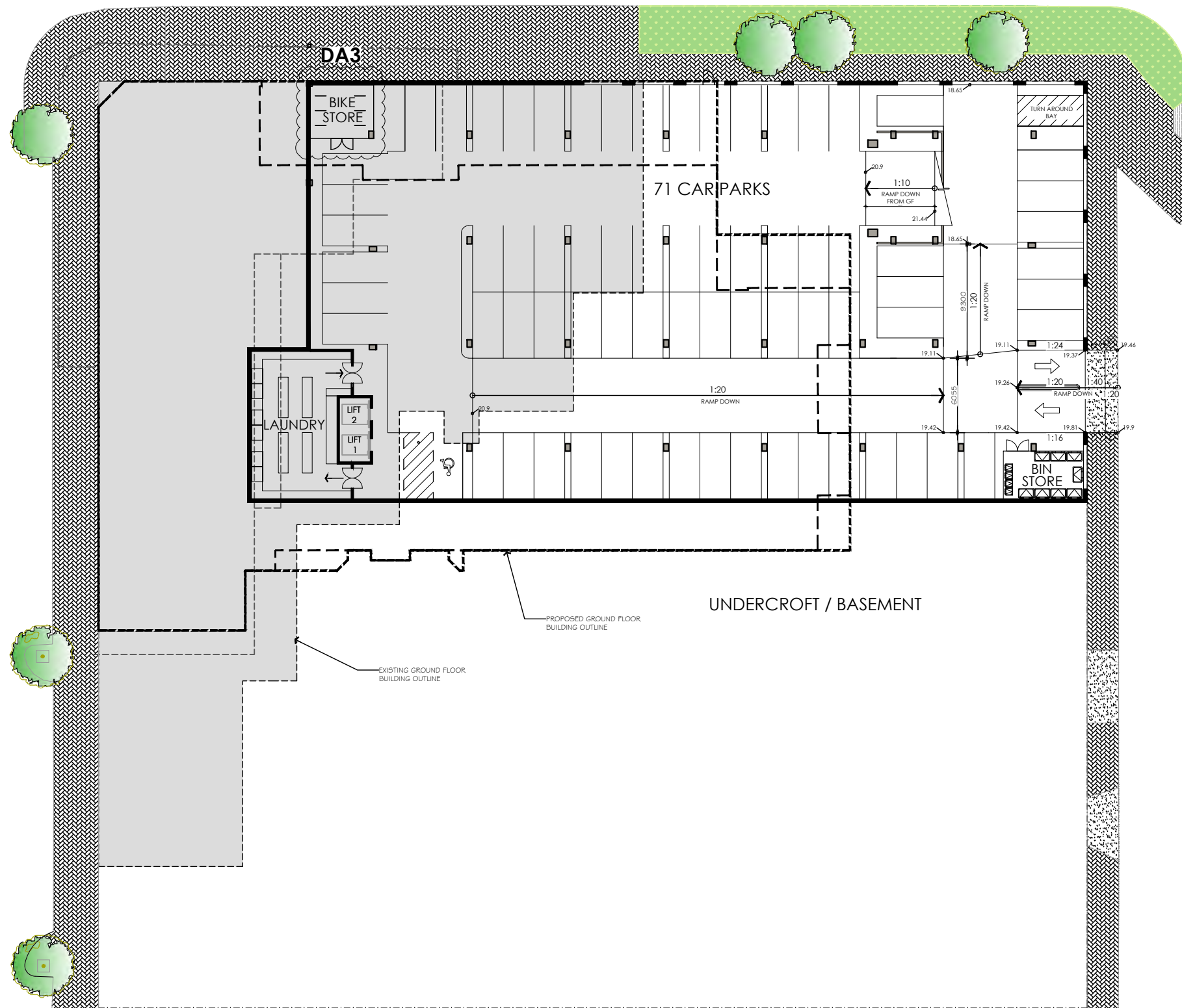


	REVISIONS		General Notes: - Check drawings prior to ordering materials or quoting of project. - Check any discrepancies with designer. - Verify all dimensions and levels prior to commencement of any shop details, fabrication or construction. - Do not scale drawings. - Verify all dimensions on site.	 building design	Client: EUREKA GROUP Project: BRIDGEPORT HOTEL	Site Address: 2 BRIDGE STREET, MURRAY BRIDGE SA 5253 Drawing Title: BRIDGEPORT HOTEL EXISTING SITE PLAN	Structural Engineers: - Issue: DAC Date: 17/07/2017 Project No. C1201	Engineers job No. - Other Consultants: - Scale: 1:200 @A1 1:400 @A3 Sheet No. 1.01 / DA1
	REV	DATE						
	DA1	17/07/17	REVISED DEVELOPMENT APPLICATION					

BRIDGE STREET

EAST TERRACE

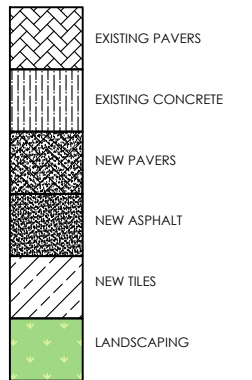
SOUTH TERRACE



PLANT LEGEND



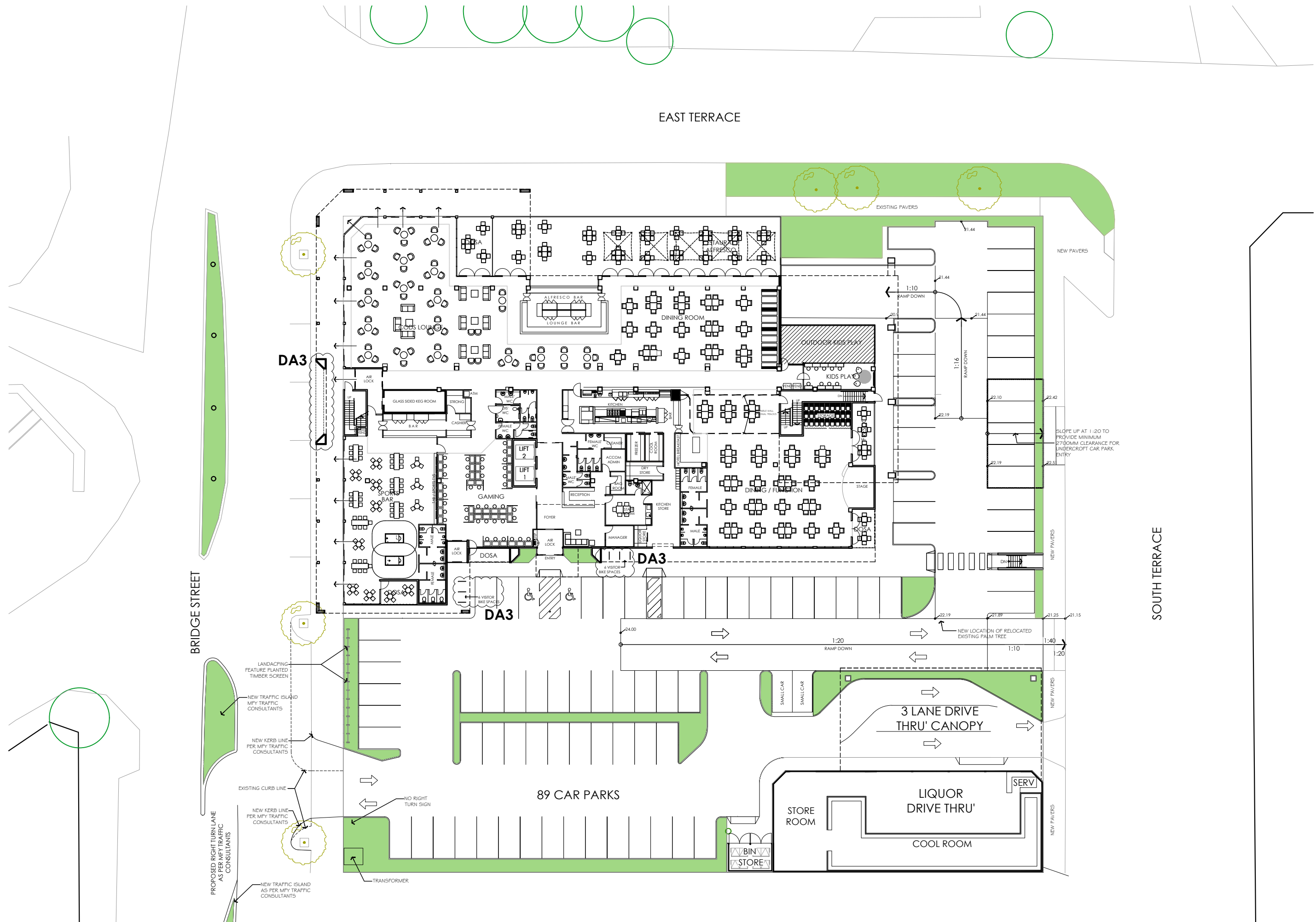
FLOOR FINISH



AREA TABULATION

OVERALL UNDERCROFT PARKING AREA 2035 SQ.M

	REVISIONS		<p>General Notes:</p> <ul style="list-style-type: none">- Check drawings prior to ordering materials or quoting of project.- Check any discrepancies with designer.- Verify all dimensions and levels prior to commencement of any shop details, fabrication or construction.- Do not scale drawings.- Verify all dimensions on site.		<p>3 /81 Grange Road Welland SA 5007</p> <p>T : 08 7087 6880 E : ceyles@hcs.on.net</p>	<p>Client: EUREKA GROUP</p> <p>Project: BRIDGEPORT HOTEL</p>	<p>Site Address: 2 BRIDGE STREET, MURRAY BRIDGE SA 5253</p> <p>Drawing Title: BRIDGEPORT HOTEL BASEMENT PLAN</p>	<p>Structural Engineers: -</p> <p>Issue: DAC</p> <p>Date: 18/12/2017</p> <p>Project No. C1201</p>	<p>Engineers job No. -</p> <p>Other Consultants: -</p> <p>Scale: 1:200 @A1 1:400 @A3</p> <p>Sheet No. 1.03 / DA3</p>	
	REV	DATE								DESCRIPTION
	DA1	17/07/17								REVISED DEVELOPMENT APPLICATION
	DA2	10/08/17								CAR PARKS REVISED TO MATCH MPT TRAFFIC CONSULTANTS
	DA3	18/12/17								BICYCLE PARKING REVISED



REVISIONS			General Notes: - Check drawings prior to ordering materials or quoting of project. - Check any discrepancies with designer. - Verify all dimensions and levels prior to commencement of any shop details, fabrication or construction. - Do not scale drawings. - Verify all dimensions on site.		3 /81 Grange Road Welland SA 5007 T : 08 7087 6880 E : ceyley@hcs.on.net	Client: EUREKA GROUP Project: BRIDGEPORT HOTEL	Site Address: 2 BRIDGE STREET, MURRAY BRIDGE SA 5253 Drawing Title: SITE PLAN	Structural Engineers: - Issue: DAC Date: 18/12/2017 Project No. C1201	Engineers job No. - Other Consultants: - Scale: 1:200 @A1 1:400 @A3 Sheet No. 1.04 / DA3
REV	DATE	DESCRIPTION							
DA1	17/07/17	REVISED DEVELOPMENT APPLICATION							
DA2	10/08/17	CAR PARKS REVISED TO MATCH MPY TRAFFIC CONSULTANTS							
DA3	18/12/17	BRIDGE STREET ENTRY REVISED. - BICYCLE PARKING ADDED.							

AREAS - 2nd to 5th FLOOR

LIFT FOYER	34.8 m ²
SUITE PASSAGE	123.3 m ²
CLEANERS RM	6.4 m ²
SUITES	884.9 m ²
TOTAL	1049.4 m²



SECOND FLOOR

	REVISIONS		General Notes: - Check drawings prior to ordering materials or quoting of project. - Check any discrepancies with designer. - Verify all dimensions and levels prior to commencement of any shop details, fabrication or construction. - Do not scale drawings. - Verify all dimensions on site.		3 / 81 Grange Road Welland SA 5007 T : 08 7087 6880 E : ceyles@hcs.on.net	Client: EUREKA GROUP Project: BRIDGEPORT HOTEL	Site Address: 2 BRIDGE STREET, MURRAY BRIDGE SA 5253 Drawing Title: BRIDGEPORT HOTEL SECOND FLOOR PLAN	Structural Engineers: - Issue: DAC Date: 17/07/2017 Project No. C1201	Engineers job No. - Other Consultants: - Scale: 1:100 @A1 1:200 @A3 Sheet No. 1.07 / DA1
	REV	DATE							
	DA1	17/07/17	REVISED DEVELOPMENT APPLICATION						

AREAS - 2nd to 5th FLOOR

LIFT FOYER	34.8 m ²
SUITE PASSAGE	123.3 m ²
CLEANERS RM	6.4 m ²
SUITES	884.9 m ²
TOTAL	1049.4 m²



THIRD FLOOR

	REVISIONS		General Notes: <ul style="list-style-type: none">- Check drawings prior to ordering materials or quoting of project.- Check any discrepancies with designer.- Verify all dimensions and levels prior to commencement of any shop details, fabrication or construction.- Do not scale drawings.- Verify all dimensions on site.		3 / 81 Grange Road Welland SA 5007 T : 08 7087 6880 E : ceyles@hcs.on.net	Client: EUREKA GROUP Project: BRIDGEPORT HOTEL	Site Address: 2 BRIDGE STREET, MURRAY BRIDGE SA 5253 Drawing Title: BRIDGEPORT HOTEL THIRD FLOOR PLAN	Structural Engineers. -	Engineers job No. -
	REV DATE	DESCRIPTION						Issue. DAC	Other Consultants. -
	17/07/17	REVISED DEVELOPMENT APPLICATION						Date. 17/07/2017	Scale. 1:100 @A1 1:200 @A3
								Project No. C1201	Sheet No. 1.08 / DA1

AREAS - 2nd to 5th FLOOR	
LIFT FOYER	34.8 m²
SUITE PASSAGE	123.3 m²
CLEANERS RM	6.4 m²
SUITES	884.9 m²
TOTAL	1049.4 m²



FOURTH FLOOR

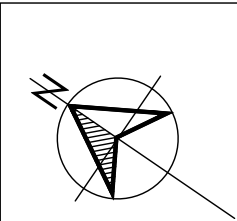
	REVISIONS		General Notes: <ul style="list-style-type: none">- Check drawings prior to ordering materials or quoting of project.- Check any discrepancies with designer.- Verify all dimensions and levels prior to commencement of any shop details, fabrication or construction.- Do not scale drawings.- Verify all dimensions on site.		3 / 81 Grange Road Welland SA 5007 T : 08 7087 6880 E : ceyles@hcs.on.net	Client: EUREKA GROUP Project: BRIDGEPORT HOTEL	Site Address: 2 BRIDGE STREET, MURRAY BRIDGE SA 5253 Drawing Title: BRIDGEPORT HOTEL FOURTH FLOOR PLAN	Structural Engineers. -	Engineers job No. -
	REV DATE	DESCRIPTION						Issue. DAC	Other Consultants. -
	17/07/17	REVISED DEVELOPMENT APPLICATION						Date. 17/07/2017	Scale. 1:100 @A1 1:200 @A3
								Project No. C1201	Sheet No. 1.09 / DA1

AREAS - 2nd to 5th FLOOR

LIFT FOYER	34.8 m ²
SUITE PASSAGE	123.3 m ²
CLEANERS RM	6.4 m ²
SUITES	884.9 m ²
TOTAL	1049.4 m²



FIFTH FLOOR



REVISIONS		
REV	DATE	DESCRIPTION
01	17/07/17	REVISED DEVELOPMENT APPLICATION

General Notes:

- Check drawings prior to ordering materials or quoting of project.
- Check any discrepancies with designer.
- Verify all dimensions and levels prior to commencement of any shop details, fabrication or construction.
- Do not scale drawings.
- Verify all dimensions on site.



3 /81 Grange Road
Welland SA 5007
T : 08 7087 6880
E : ceyles@hcs.on.net

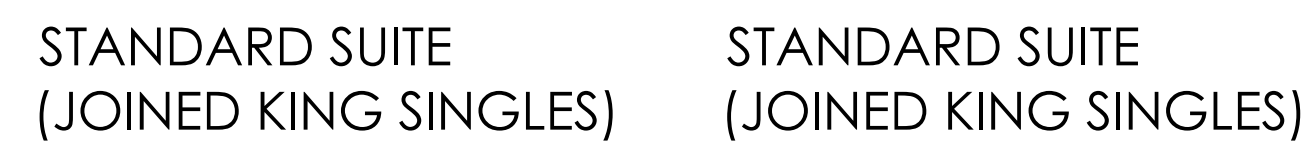
Client:
EUREKA GROUP

Project:
BRIDGEPORT HOTEL

Site Address:
2 BRIDGE STREET, MURRAY BRIDGE SA 5253

Drawing Title:
**BRIDGEPORT HOTEL
FIFTH FLOOR PLAN**

Structural Engineers.	Engineers job No.
-	-
Issue.	Other Consultants.
DAC	-
Date.	Scale: 1:100 @A1
17/07/2017	1:200 @A3
Project No.	Sheet No.
C1201	1.10 / DA1



STANDARD JOINED SUITES

SCALE 1:50 @A1

STANDARD AREA	
LIVING	35.2 m ²
BALCONIES	4.6 m ²
TOTAL	39.8 m ² (each)



PENTHOUSE LEVEL - RIVER SIDE SUITES

SCALE 1:50 @A1

<u>AREA:</u>	
BALCONIES	4.6 m ²
LIVING	35.2 m ²
TOTAL	39.8 m ² (each)



1st FLOOR ONLY


SCALE 1 : 50 @A1

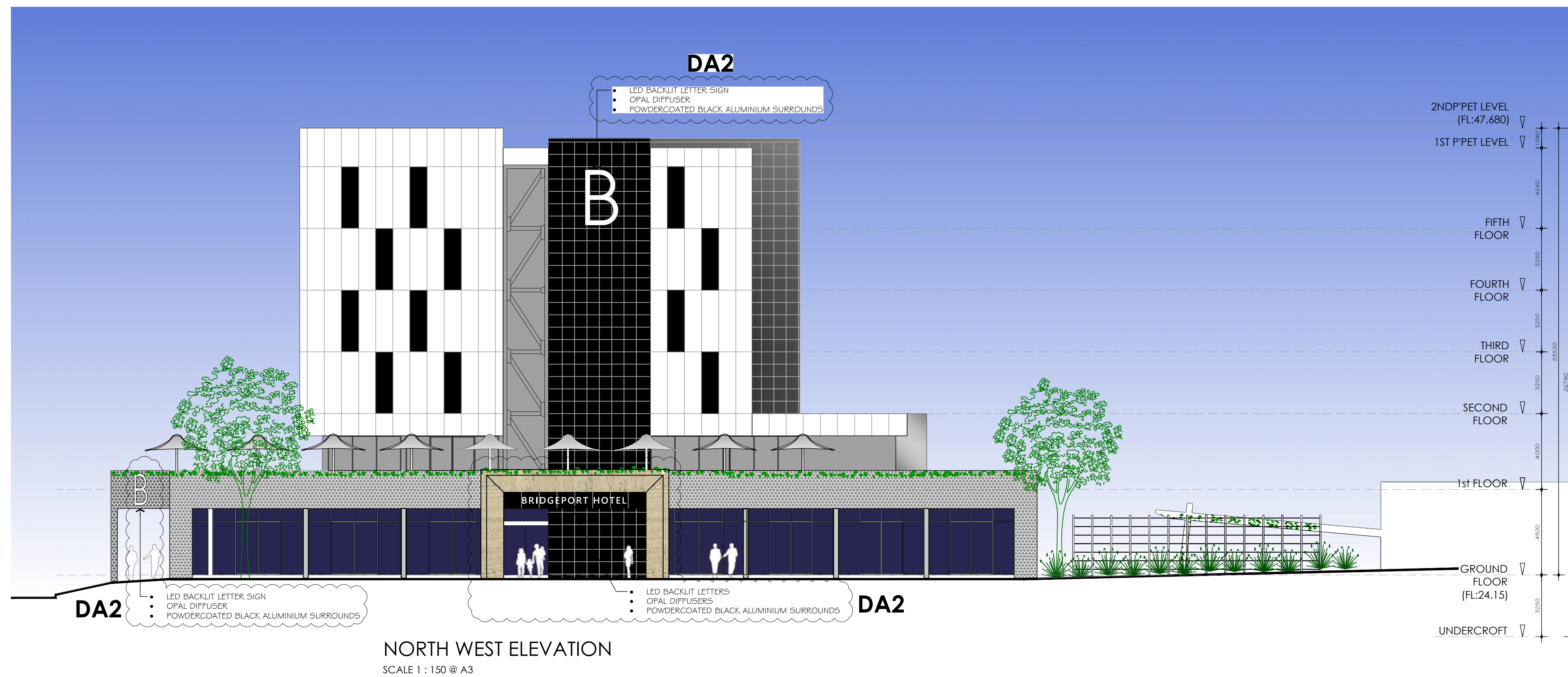
AREA:	
LIVING	39.8 m ²
TOTAL	39.8 m ²



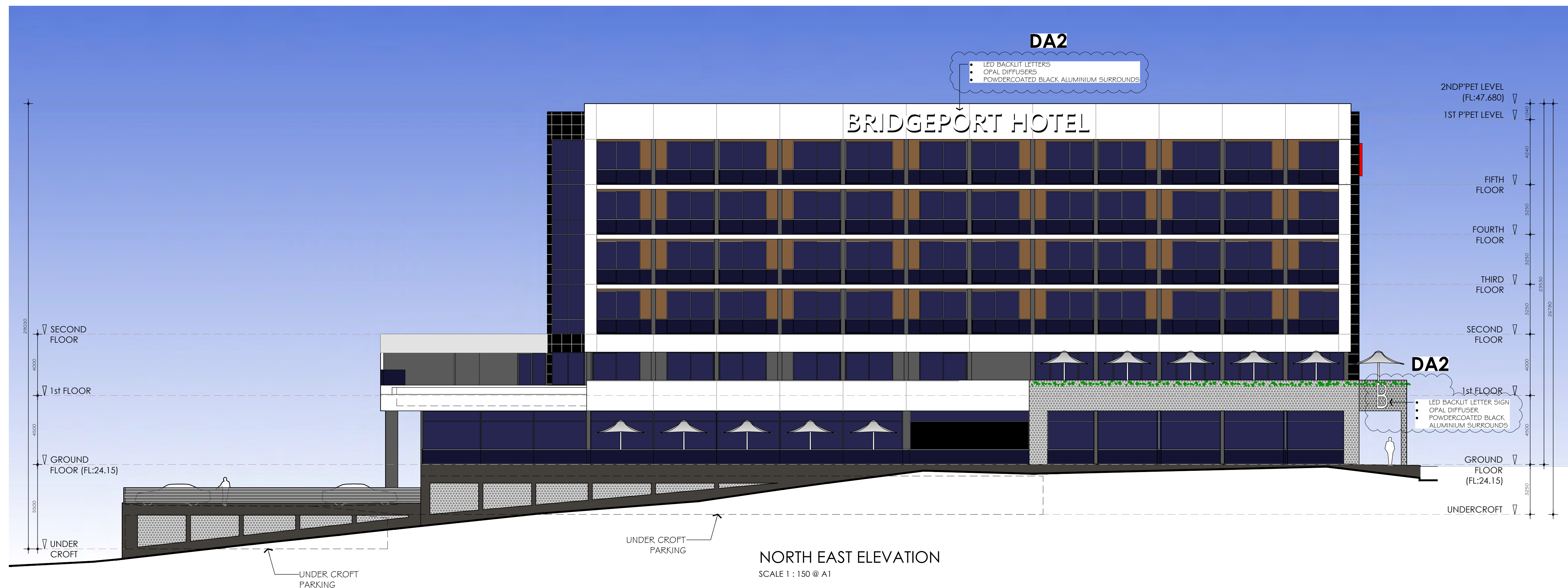
SCALE 1 : 50 @A1

<u>AREA:</u>	
LIVING	35.2 m ²
BALCONY	13.9 m ²
<u>TOTAL</u>	<u>49.1 m²</u>


REVISONS		General Notes:	 3 /81 Grange Road Welland SA 5007 T : 08 7087 6880 E : ceyles@bcd.on.net	Client: EUREKA GROUP	Site Address: 2 BRIDGE STREET, MURRAY BRIDGE SA 5253	Structural Engineers.	Engineers Job No.
REV DATE	DESCRIPTION	<ul style="list-style-type: none"> - Check drawings prior to ordering materials or quoting of project - Check any discrepancies with designer. - Verify all dimensions and levels prior to commencement of any shop details, fabrication or construction. - Do not scale drawings. - Verify all dimensions on site. 		Project: BRIDGEPORT HOTEL	Drawing Title: BRIDGEPORT HOTEL APARTMENTS TYPE	Issue: DAC	Other Consultants.
DA1 17/12/17	REVISD DEVELOPMENT APPLICATION				Date: 10/08/2017	Scale: 1:50 @A1 1:100 @A3	
DA2 10/8/17	AIR CONDITIONERS ADDED TO BALCONIES				Project No: C1201	Sheet No. LJ2 / DA2	



NORTH WEST ELEVATION
SCALE 1 : 150 @ A3

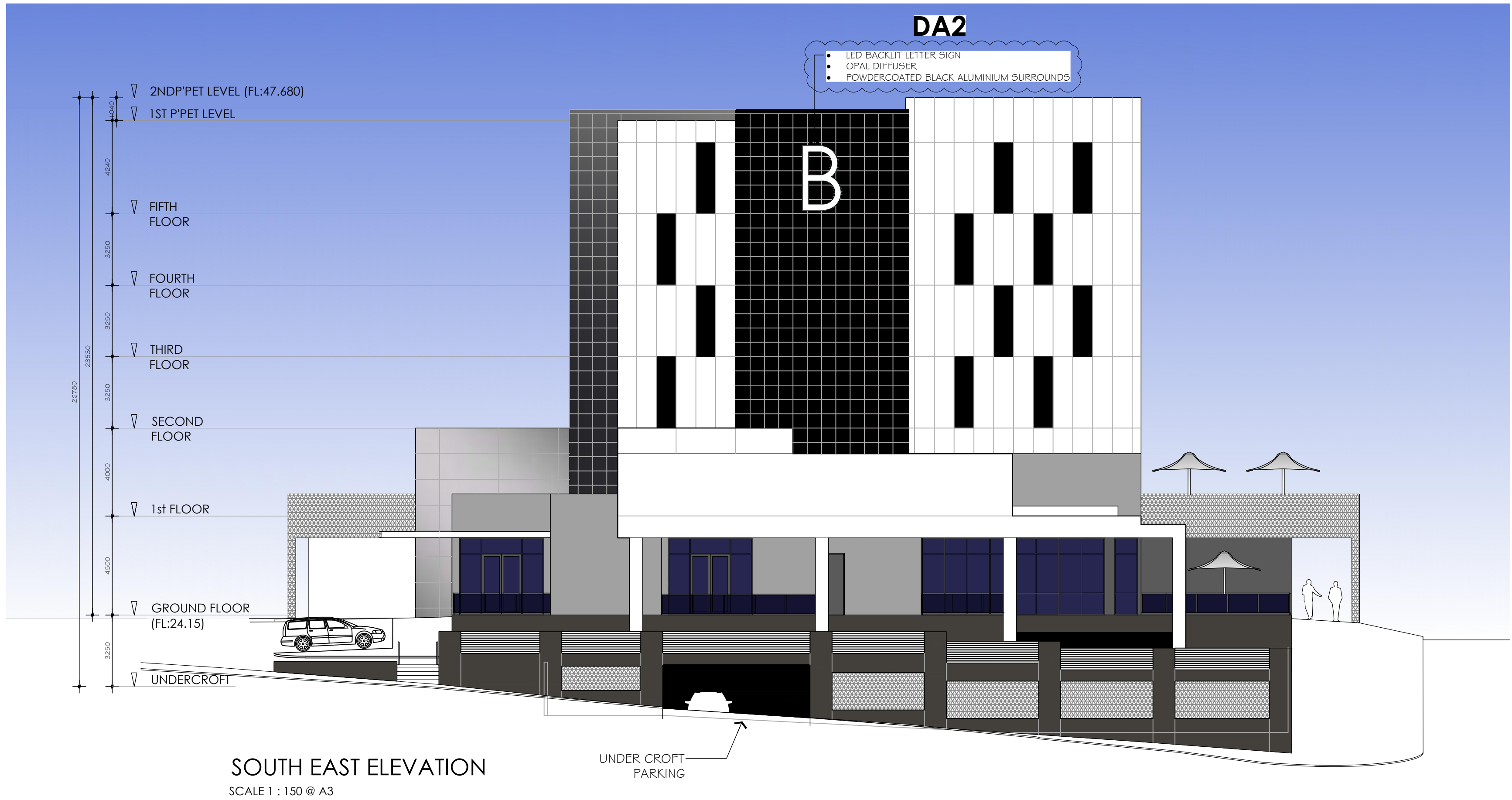


NORTH EAST ELEVATION
SCALE 1 : 150 @ A1

REVISIONS			NOTES		540 Port Road, Allenby Gardens South Aust 5009 T: 08 8241 5133 (ext 5) F: 08 8241 5899 E: ceyles@hcs.on.net	Client: EUREKA GROUP Project: BRIDGEPORT HOTEL	Site Address: 2 BRIDGE STREET, MURRAY BRIDGE SA 5253	Structural Engineers: -	Engineers job No. -
REV	DATE	DESCRIPTION							
DA1	17/07/17	REVISED DEVELOPMENT APPLICATION	1. These designs and plans are subject to the Copyright Act of 1968 and the Copyright Amendment (Moral Rights) Bill 1999 and are not to be used or reproduced, wholly or in part without the written consent of the Designer. The copyright for these drawings and any construction produced from these drawings is vested with CED building design. 2. Do not scale from drawings 3. Verify all dimensions on site 4. Check any discrepancies with the designer.				Drawing Title: BRIDGEPORT HOTEL PROPOSED ELEVATIONS	Issue: DAC	Other Consultants: -
DA2	18/12/17	BRIDGE STREET ENTRY REVIEWED. - 8 SIGN ADDED TO HOTEL CORNER. - SIGNAGE DETAILS ADDED.							
							Date: 18/12/2017	Scale: 1:100 @A1 1:200 @A3	
							Project No. C1201	Sheet No. 2.01 / DA2	



SOUTH WEST ELEVATION
SCALE 1 : 150 @ A1

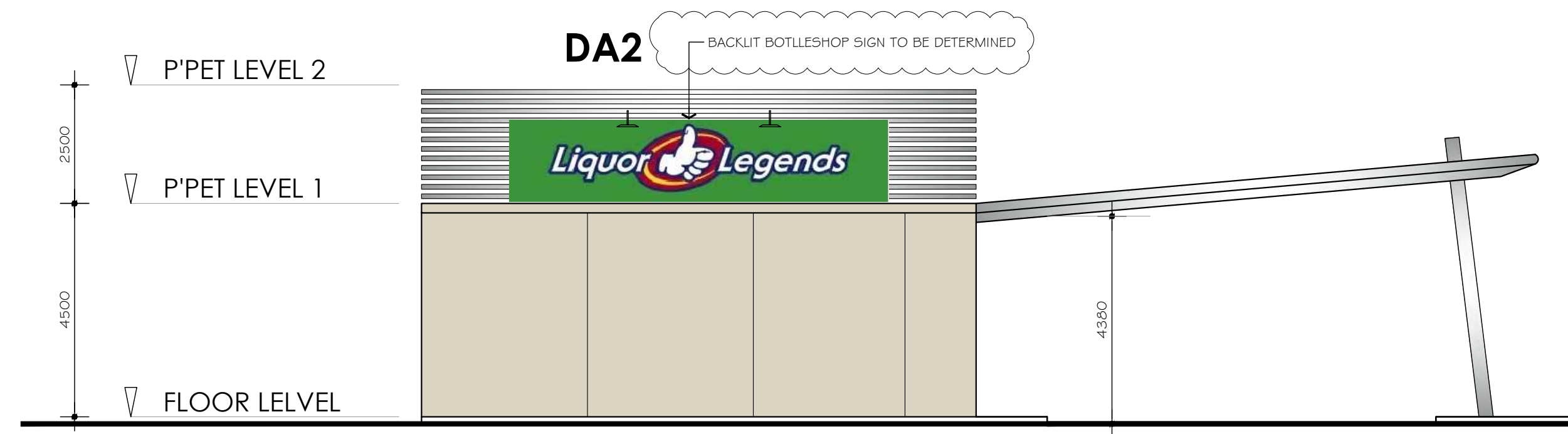


SOUTH EAST ELEVATION
SCALE 1 : 150 @ A3

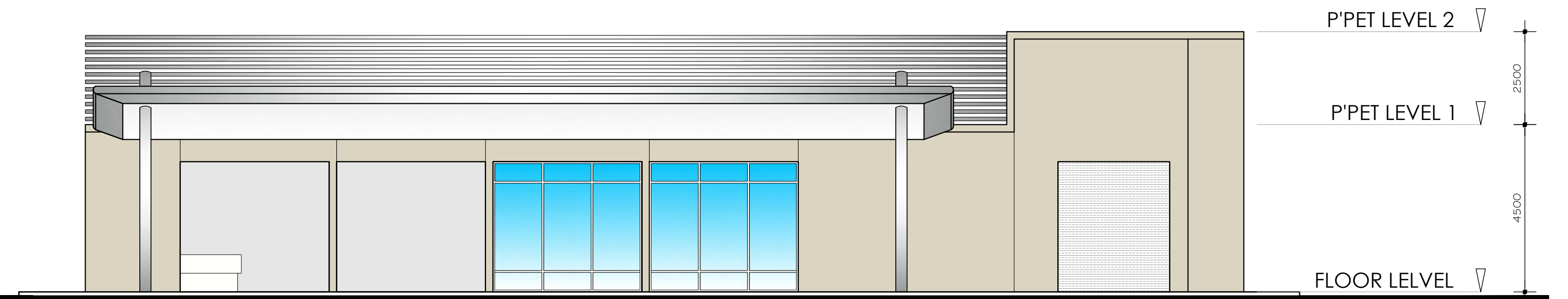
REVISIONS				NOTES	Client:	Site Address:	Structural Engineers:	Engineers job No.
REV	DATE	DESCRIPTION	REVISIONS					
DA1	17/07/17	REVISED DEVELOPMENT APPLICATION	AS NOTED.	1. These designs and plans are subject to the Copyright Act of 1968 and the Copyright Amendment (Moral Rights) Bill 1999 and are not to be used or reproduced, wholly or in part without the written consent of the Designer. The copyright for these drawings and any construction produced from these drawings is vested with CED building design.	EUREKA GROUP	2 BRIDGE STREET, MURRAY BRIDGE SA 5253	-	-
DA2	18/12/17	REVISED DEVELOPMENT APPLICATION	AS NOTED.	2. Do not scale from drawings 3. Verify all dimensions on site 4. Check any discrepancies with the designer.				
					Project:	Drawing Title:	Date:	Scale:
					BRIDGEPORT HOTEL	BRIDGEPORT HOTEL PROPOSED ELEVATIONS	18/12/2017	1:150 @A1 1:300 @A3
							Project No.	Sheet No.
							C1201	2.02 / DA2

EXTERNAL COLOUR AND FINISHES SCHEDULE

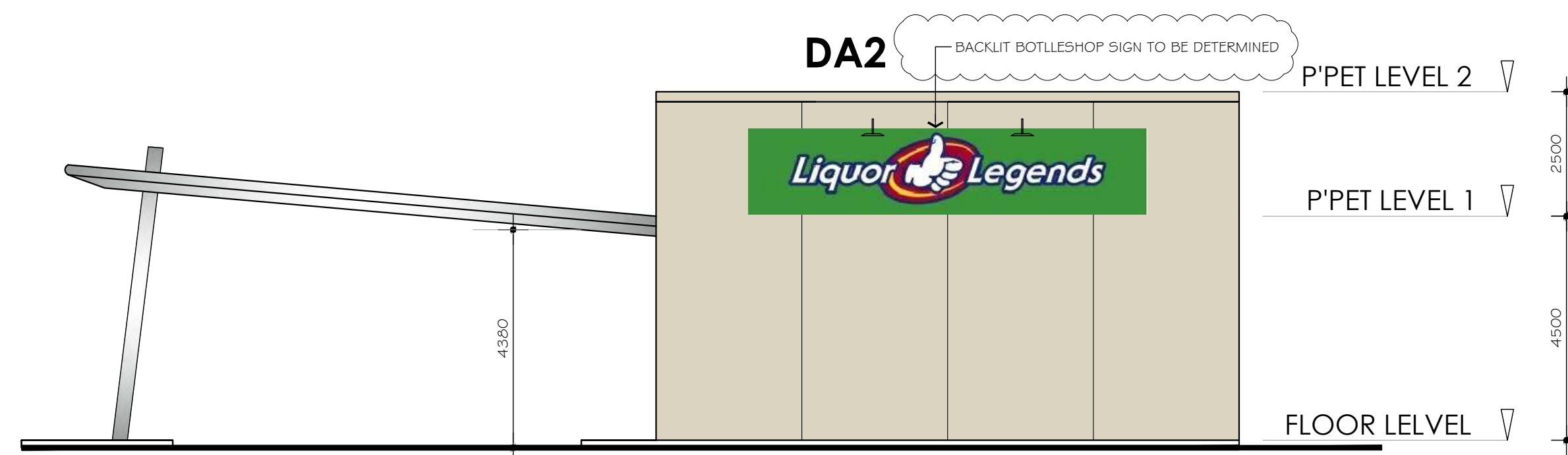
EXTERNAL FINISHES	MATERIAL	COLOUR
Roof	Colorbond metal	Surfmist
Drive-thru' canopy soffit lining	Painted Versilux - Flat acrylic	Dulux Lexicon (Quarter) - PN2D1
Walls generally	High-build paint	Dulux Hog Bristle - P14D1
Battened screen wall	Powder coated Aluminium	Dulux Precious Silver Pearl
Parapet capping	Colorbond steel	Surfmist
Window glazing	Viridian Glass	Clear Energytech insulating glass
Window frames	Powder coated Aluminium	Dulux Precious Silver Pearl
Auto roller shutter	Galvanised steel	
Canopy posts	Stainless steel cladding	Brushed
Canopy edge capping	Stainless steel cladding	Brushed



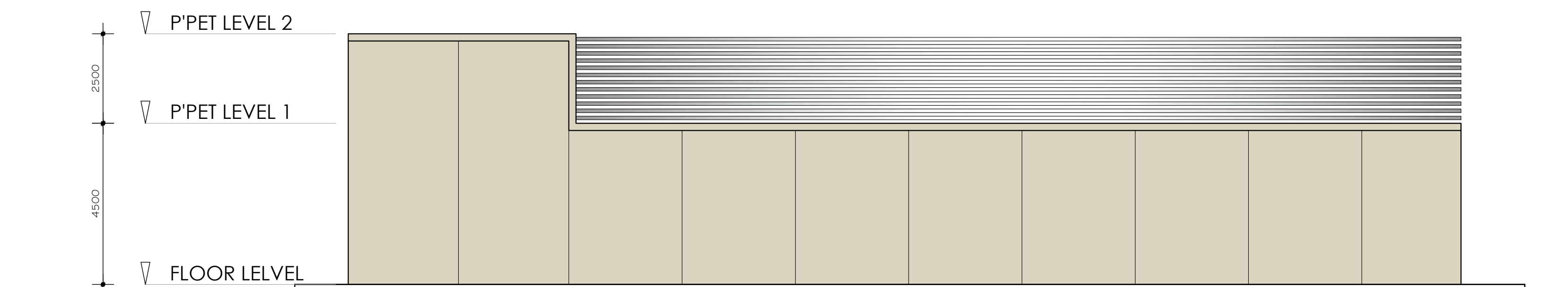
SOUTH EAST ELEVATION
SCALE 1 : 100 @ A1



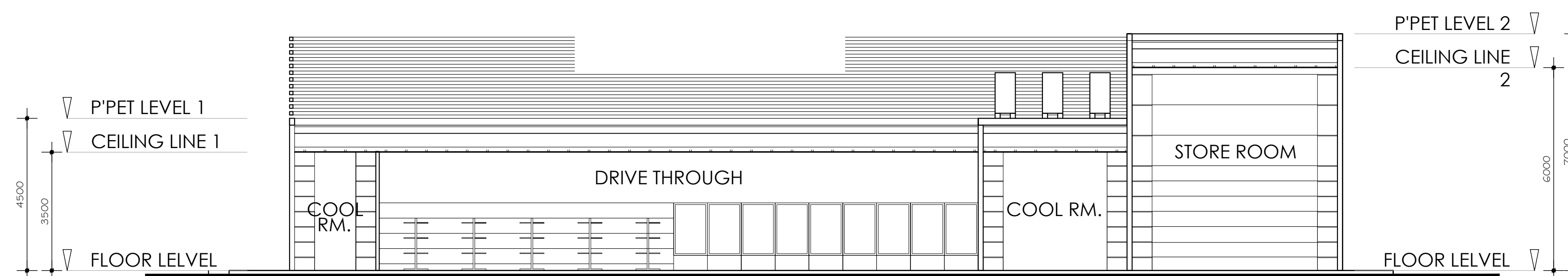
NORTH EAST ELEVATION
SCALE 1 : 100 @ A1




NORTH WEST ELEVATION
SCALE 1 : 100 @ A1



SOUTH WEST ELEVATION
SCALE 1 : 100 @ A1

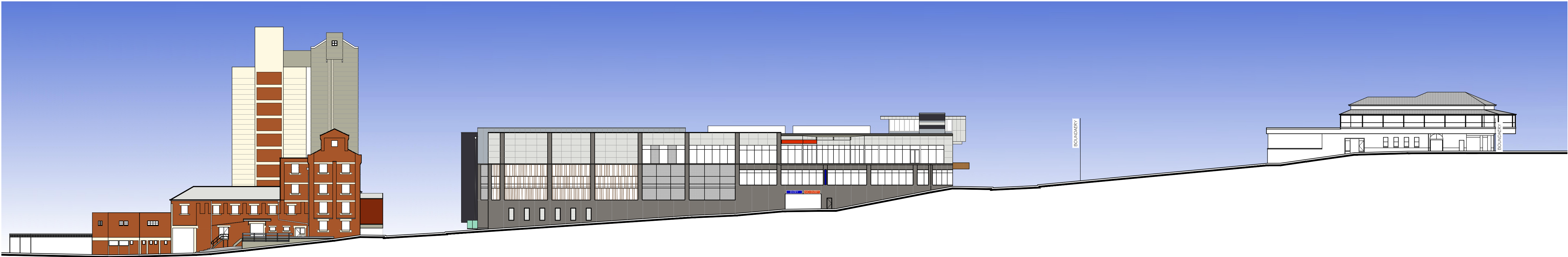


CROSS SECTION
SCALE 1 : 100 @ A1

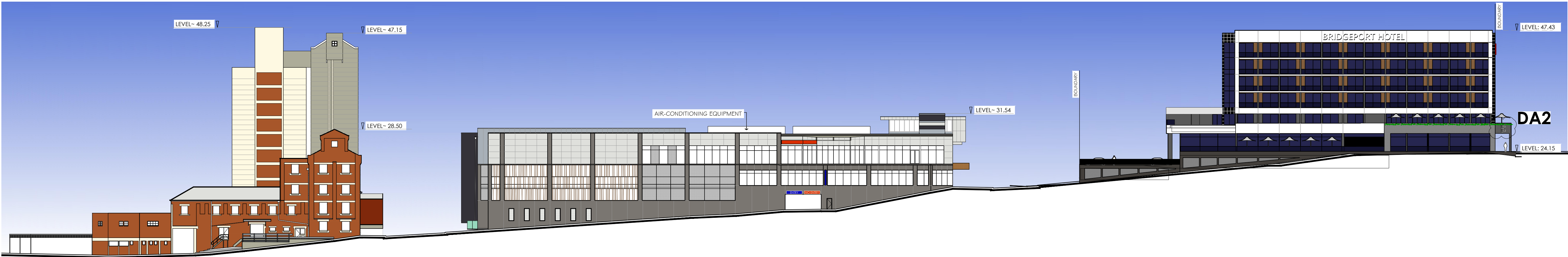
REVISIONS			NOTES <div>1. These designs and plans are subject to the Copyright Act of 1968 and the Copyright Amendment (Moral Rights) Bill 1999 and are not to be used or reproduced, wholly or in part without the written consent of the Designer. The copyright for these drawings and any construction produced from these drawings is vested with CED building design.</div> <div>2. Do not scale from drawings</div> <div>3. Verify all dimensions on site</div> <div>4. Check any discrepancies with the designer.</div>	 <div>540 Port Road Allenby Gardens South Aust 5009</div> <div>T: 08 8241 5133 (ext 5) F: 08 8241 5899 E: ceylex@hcs.on.net</div>	Client: EUREKA GROUP	Site Address: 2 BRIDGE STREET, MURRAY BRIDGE SA 5253	Structural Engineers.	Engineers job No.
REV	DATE	DESCRIPTION			Project: BRIDGEPORT HOTEL	Drawing Title: BOTTLESHOP PROPOSED ELEVATIONS & CROSS SECTION	Issue: DAC	Other Consultants.
DA1	17/07/17	REVISED DEVELOPMENT APPLICATION AS NOTED					Date: 18/12/2017	Scale: 1:100 @A1 1: 200 @A3
DA2	18/12/17						Project No: C.1201	Sheet No: 2.03 / DA2




PROPOSED BRIDGE STREET
STREETSCAPE ELEVATION
SCALE 1 : 250 @ A1

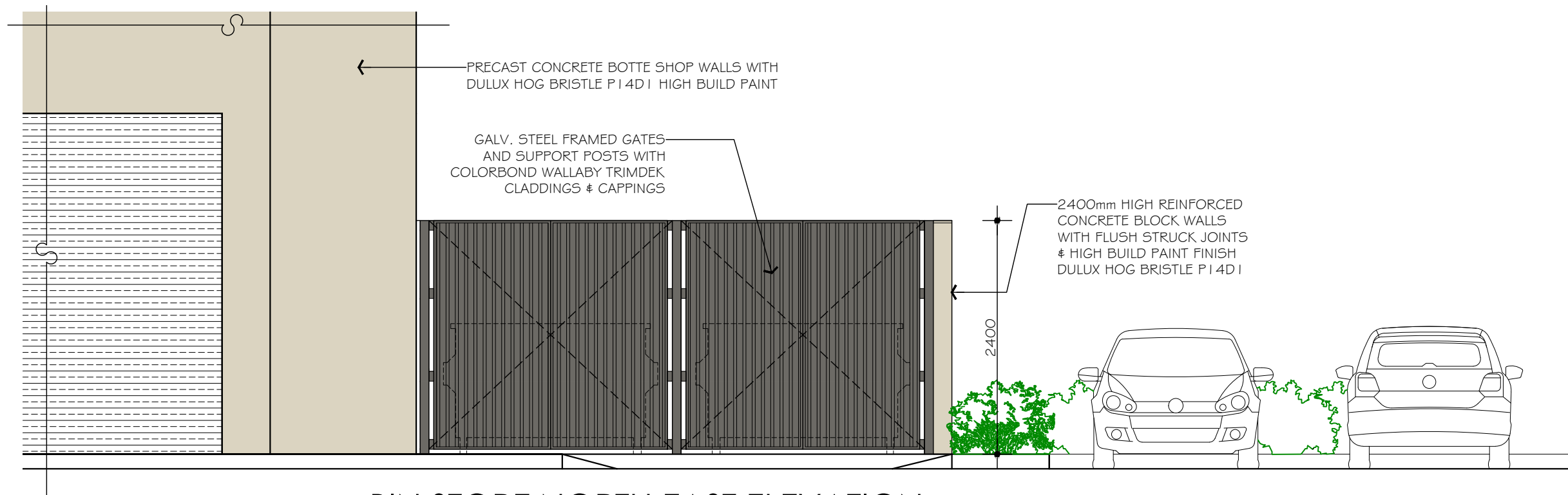


EXISTING EAST TERRACE
STREETSCAPE ELEVATION
SCALE 1 : 400 @ A1



PROPOSED EAST TERRACE
STREETSCAPE ELEVATION
SCALE 1 : 400 @ A1

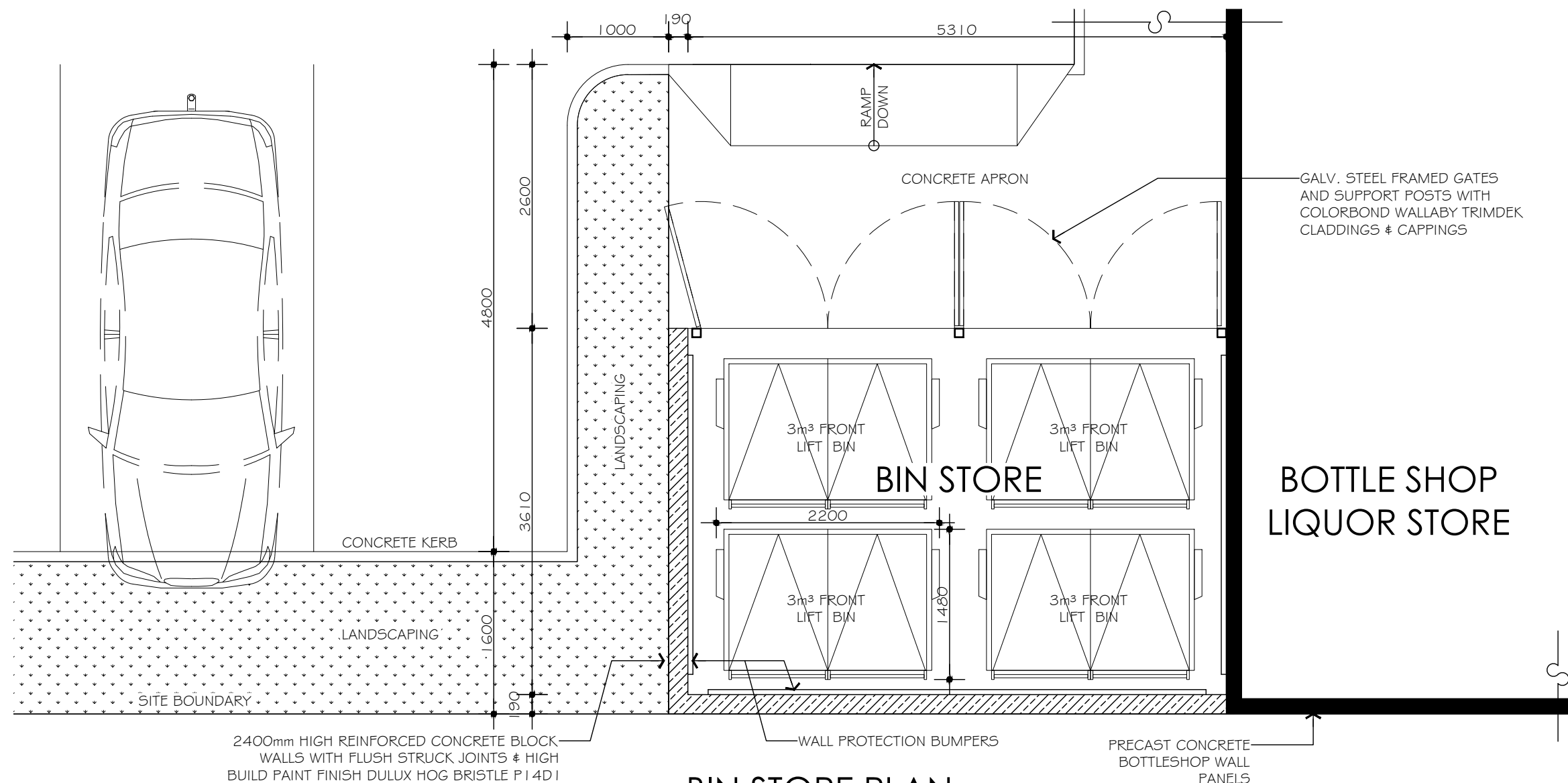
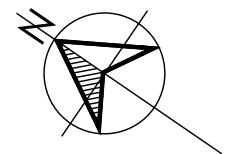
REVISIONS			NOTES		540 Port Road Allenby Gardens South Aust 5009 T: 08 8241 5133 (ext 5) F: 08 8241 5899 E: ceyley@hcs.on.net	Client: EUREKA GROUP	2 BRIDGE STREET, MURRAY BRIDGE SA 5253	Structural Engineers. -	Engineers job No. -				
REV	DATE	DESCRIPTION	1. These designs and plans are subject to the Copyright Act of 1968 and the Copyright Amendment (Moral Rights) Bill 1999 and are not to be used or reproduced, wholly or in part without the written consent of the Designer. The copyright for these drawings and any construction produced from these drawings is vested with CED building design.			Project: BRIDGEPORT HOTEL	Drawing Title: BRIDGEPORT HOTEL STREETSCAPE ELEVATIONS	Issue: DAC	Other Consultants. -				
DA1	17/07/17	REVISED DEVELOPMENT APPLICATION	2. Do not scale from drawings 3. Verify all dimensions on site 4. Check any discrepancies with the designer.							Date: 18/12/2017	Scale @ A1. AS SHOWN		
DA2	18/12/17	BRIDGE STREET ENTRY & SIGN ADDED TO HOTEL CORNER.										Project No. C1201	Sheet No. 2.04 / DA2




BIN STORE NORTH EAST ELEVATION
SCALE 1 : 50 @ A3

NOTE:

BIN STORE HAS BEEN SIZED IN ACCORDANCE WITH RECOMMENDATIONS OF THE WASTE MANAGEMENT PLAN PREPARED BY VEOLIA ENVIRONMENTAL SERVICES (AUSTRALIA) P/L DATED FEBRUARY 2018



BIN STORE PLAN
SCALE 1 : 50 @ A3

 <div> 3 /81 Grange Rd Welland SA 5007 T : 08 7087 6880 E : ceyles@hcs.on.net </div>	
Client: EUREKA GROUP	
Project: BRIDGEPORT HOTEL	
Site Address: 2 BRIDGE STREET, MURRAY BRIDGE SA 5253	
Drawing Title: BIN STORE DETAILS	
Structural Engineers. -	Engineers job No. -
Issue. DAC	Other Consultants. -
Date. 04/04/2018	Scale. 1:50 @ A3
Project No. C1201	Sheet No. 2.06 / DA1

Bridgeport Hotel - DA 415/E014/16

Subject Land

This product was generated by PLB Pro

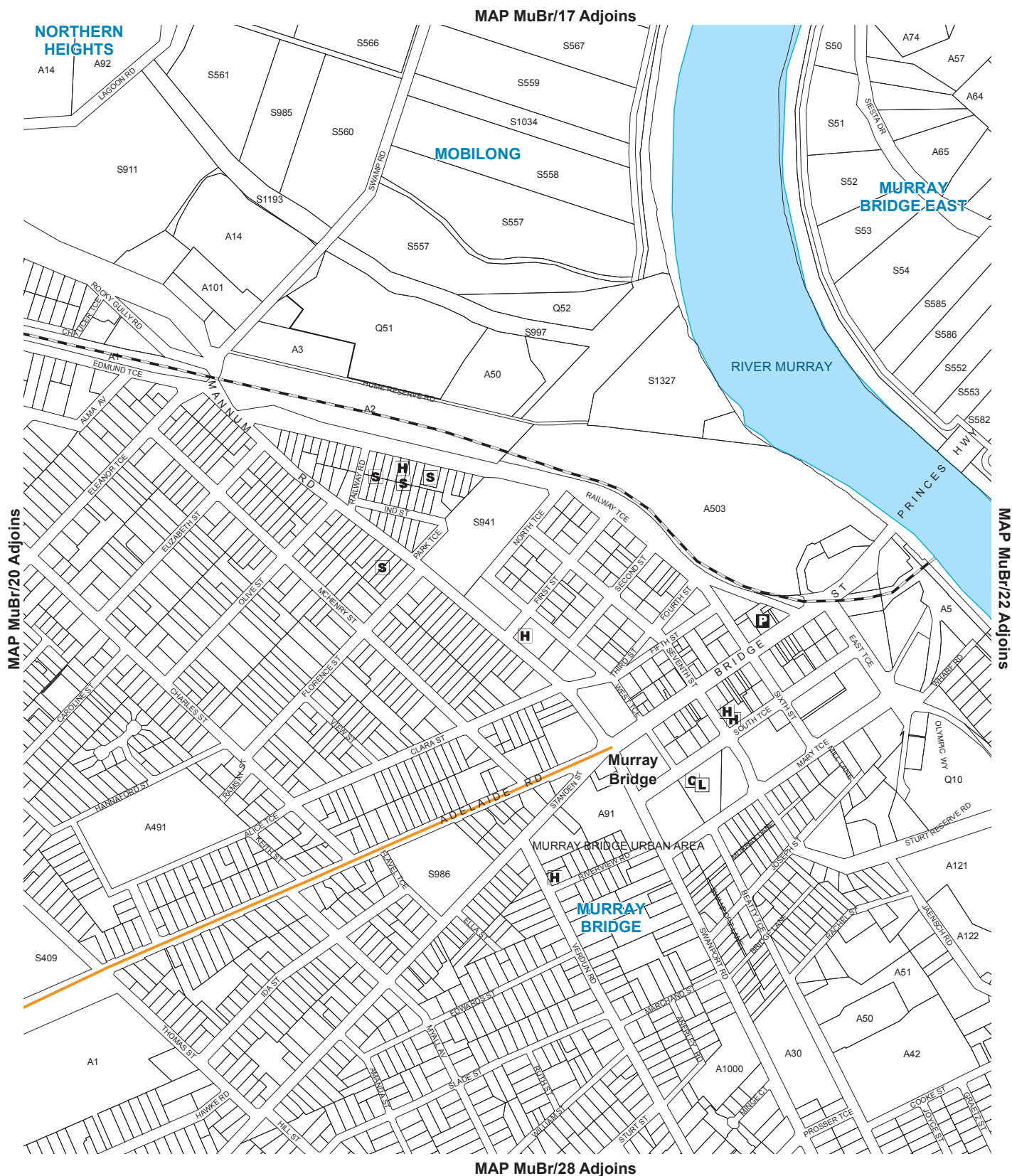
Date created: April 4, 2018



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PLB Pro - developed by DPTI GIS Office

DPTI.GISOffice@sa.gov.au



MURRAY BRIDGE TOWNSHIP

Location Map MuBr/21

MURRAY BRIDGE COUNCIL

Consolidated - 11 August 2016

MAP MuBr/17 Adjoins



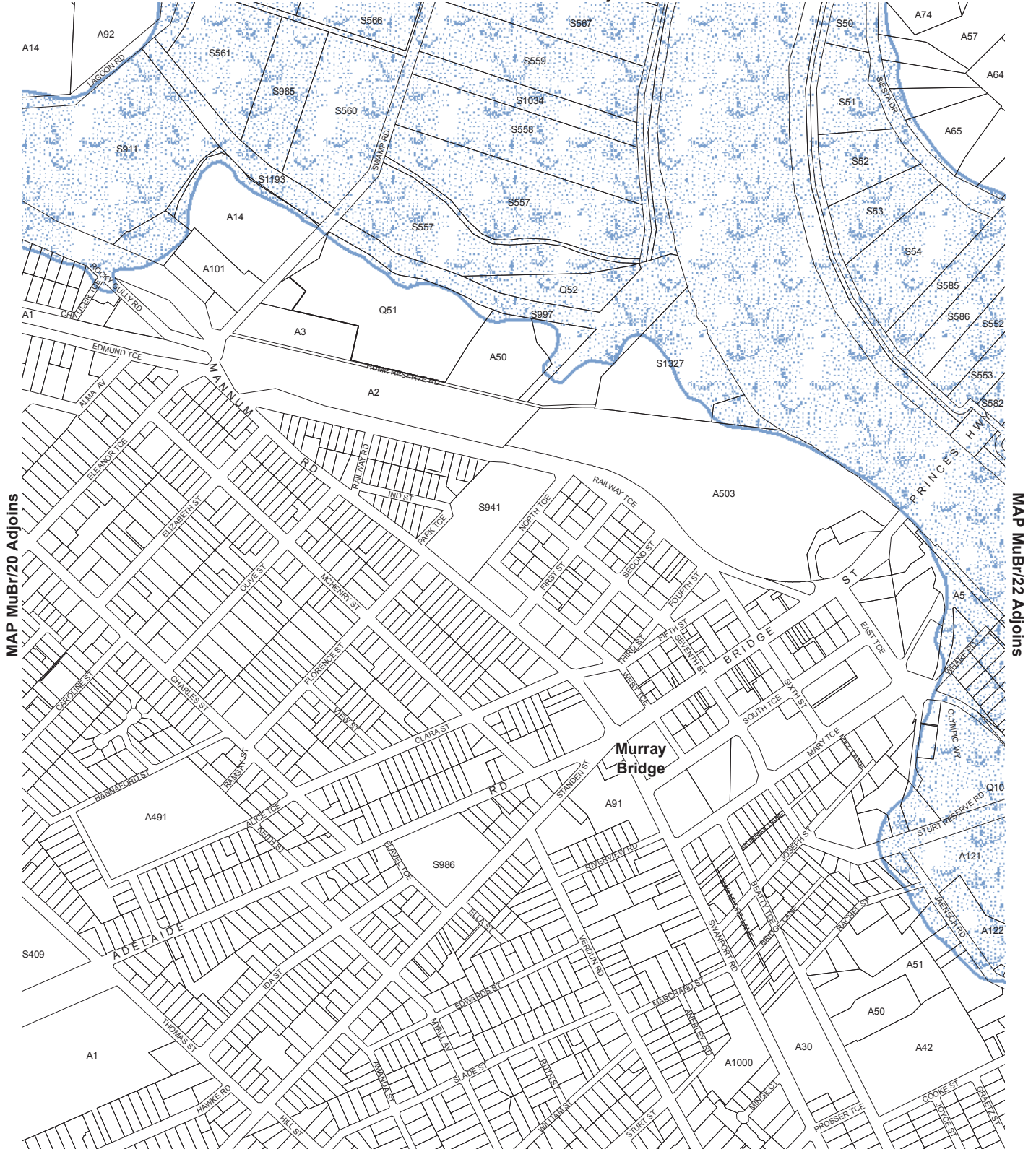
MURRAY BRIDGE TOWNSHIP

Overlay Map MuBr/21 TRANSPORT

- Primary Arterial Roads
- Secondary Arterial Roads

MURRAY BRIDGE COUNCIL
Consolidated - 11 August 2016

MAP MuBr/17 Adjoins



MAP MuBr/28 Adjoins



MURRAY BRIDGE TOWNSHIP

Overlay Map MuBr/21 DEVELOPMENT CONSTRAINTS

MURRAY BRIDGE COUNCIL

Consolidated - 11 August 2016

MAP MuBr/17 Adjoins



Murray Bridge

Heritage points are indicative only.
For further information on State and Local Heritage Places and Contributory Items please refer to the relevant tables within this document.

- Historic Conservation Area**
 1 Historic Conservation Area
 2 Historic Conservation Area



MURRAY BRIDGE TOWNSHIP

Overlay Map MuBr/21

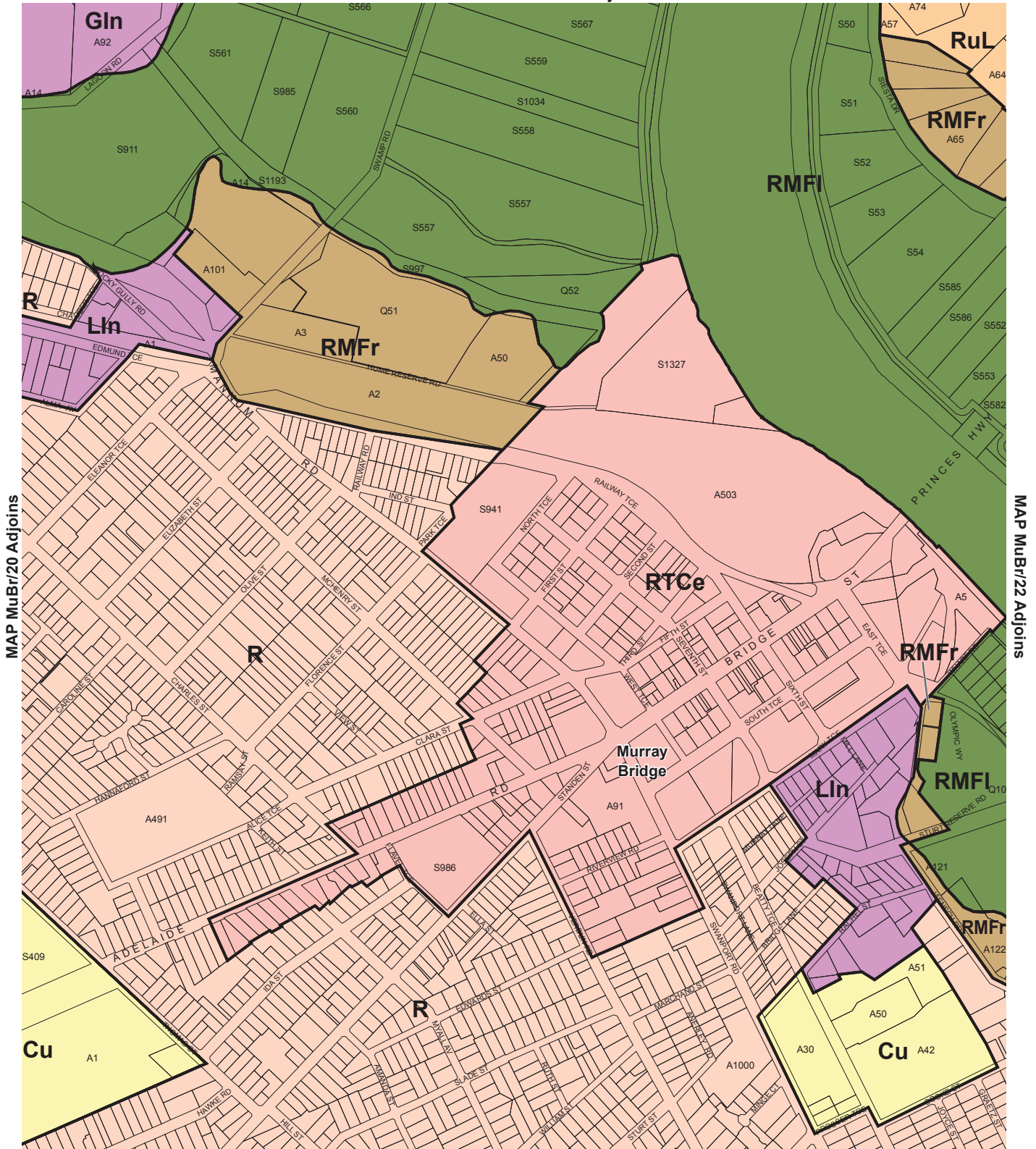
HERITAGE

- State heritage place
- Local heritage place
- Historic Conservation Area
- Contributory item

MURRAY BRIDGE COUNCIL

Consolidated - 11 August 2016

MAP MuBr/17 Adjoins



MAP MuBr/28 Adjoins

Lamberts Conformal Conic Projection, GDA94

Zones	
Cu	Community
Gln	General Industry
Lln	Light Industry
RTCe	Regional Town Centre
R	Residential
RMFI	River Murray Flood
RMFr	River Murray Fringe
RuL	Rural Living
	Zone Boundary



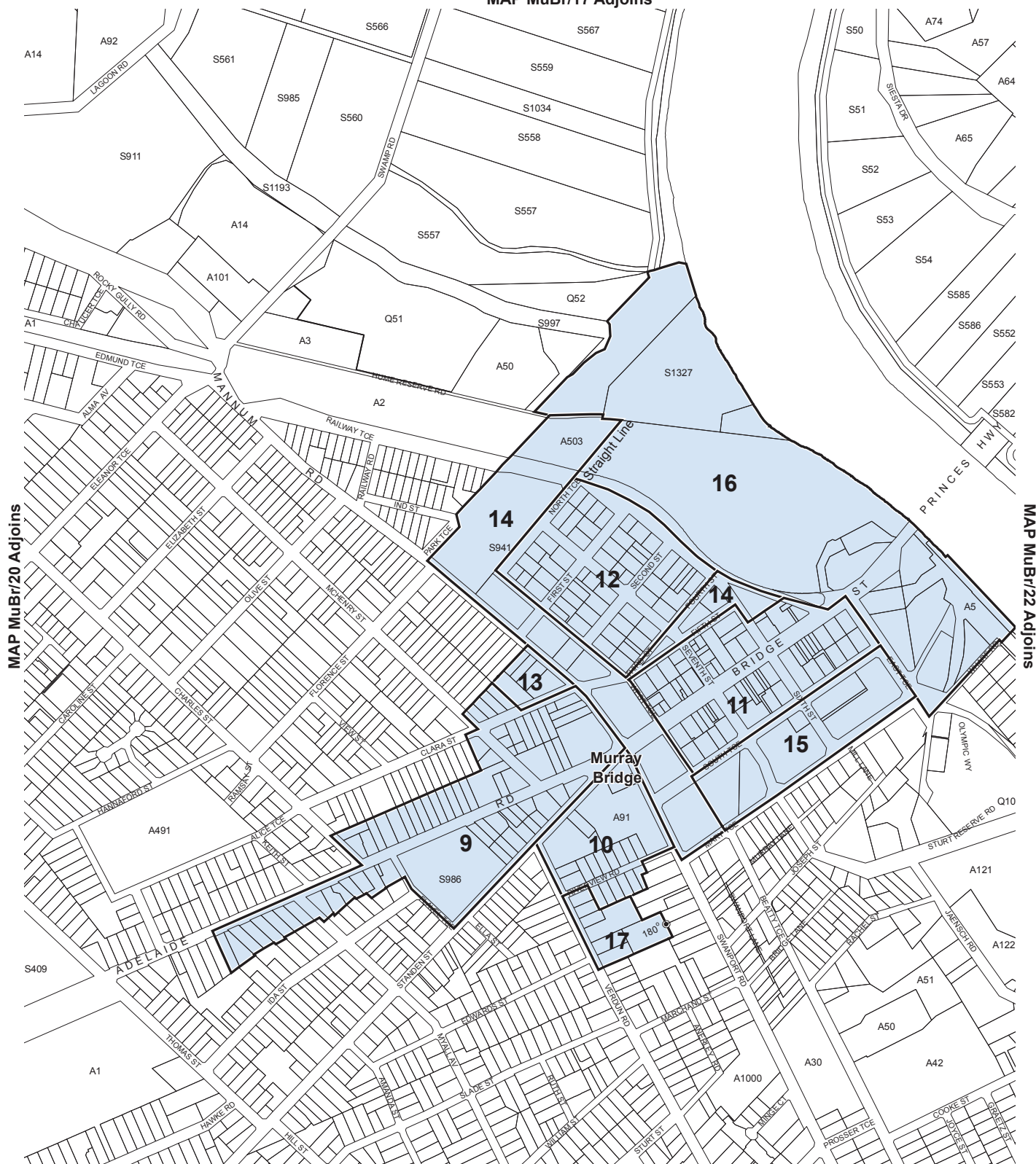
MURRAY BRIDGE TOWNSHIP

Zone Map MuBr/21

MURRAY BRIDGE COUNCIL

Consolidated - 11 August 2016

MAP MuBr/17 Adjoins



Lamberts Conformal Conic Projection, GDA94

Policy Area

- 10 Adelaide/Swanport Road Retail
- 11 Bridge Street
- 12 Business
- 13 Mannum Road Retail
- 14 Parklands
- 15 South Terrace
- 16 Urban Waterfront
- 17 Verdun Road
- 9 Adelaide Road



MURRAY BRIDGE TOWNSHIP

Policy Area Map MuBr/21

 Policy Area Boundary

MURRAY BRIDGE COUNCIL

Consolidated - 11 August 2016

MAP MuBr/17 Adjoins



MAP MuBr/20 Adjoins

MAP MuBr/22 Adjoins

MAP MuBr/28 Adjoins

Lamberts Conformal Conic Projection, GDA94

Precinct
11 One Hectare



MURRAY BRIDGE TOWNSHIP

Precinct Map MuBr/21

 Precinct Boundary

MURRAY BRIDGE COUNCIL
Consolidated - 11 August 2016



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29 JUNE 2017

Mr Jamie Botten
Botten Levinson Lawyers
Level 1, Darling Building
28 Franklin Street,
Adelaide SA 5000

Dear Jamie

BRIDGEPORT HOTEL - DA 415/E014/16. LANDSCAPE PLAN

I refer to your email dated 18 May 2017 and our subsequent meeting where you requested Oxygen prepare a landscape plan and report to accompany the development application for a proposed development on the current Bridgeport Hotel site in Murray Bridge. My response comprises this letter of description and accompanying drawings.

DESCRIPTION

The landscape proposals are illustrated on the following drawings:

1. Drawing No. 17.027.101 Revision A Landscape Planting Plan
2. Drawing No. 17.027.103 Revision A Landscape Planting Plan - Detail
3. Drawing No. 17.027.104 Revision A Landscape Planting Plan - Upper Level Terrace
4. Drawing No. 17.027.105 Revision A Landscape Species + Screen

LANDSCAPE PLAN

The key elements of the plan are:

1. Provision of shade trees within the carpark. These are intended to visually soften the sealed surface carpark, provide scale to the proposed built form and provide amenity for site users.
2. Provision of low shrub and groundcover plantings within garden beds.
3. Provision of a timber batten screen and planting adjacent to Bridge Street to visually soften and enclose the carpark when viewed from the street.
4. Provision of an automatic irrigation system.

SPECIES SELECTION

1. Medium-height deciduous trees are proposed to provide an appropriate scale and visual amenity to the site.
 - a. Species include *Acer buergerianum*, *Lagerstroemia indica* 'Tuscarora', *Jacaranda mimosifolia*, *Pistacia chinensis* and *Sophora japonica*.
 - b. The above species are deciduous and are considered suitable for the growing conditions likely to be experienced on this site. All of the species are well-proven in similar urban locations.
2. Generally, deciduous trees are preferred for their horticultural and amenity qualities, including resilience in urban planting locations, seasonality, shade in summer and sunlight penetration in winter.
3. Two of the existing palms on site are proposed to be relocated as indicated on the plans.

RECEIVED
03/10/2017
SCAP

4. Shrubs are selected for their amenity value, support to Crime Prevention through Environmental Design (CPTED) principles, and horticultural characteristics that includes low maintenance.
 - a. Species include *Berberis thunbergii* Atropurpurea, *Cornus alba sibirica*, *Escallonia* Pink Pixie, *Euphorbia rigida*, *Hibbertia scandens*, *Nandina* Moonbay, *Rosmarinus officinalis prostratus*, *Raphiolepis umbellata*, *Trachelospermum jasminoides* and *Westringia* Smokey.
 - b. The above species are mostly exotic with two species (the *Hibbertia* and *Westringia*) native. All are well-proven species used in urban horticulture. They are all readily available in appropriate pot sizes from nurseries in South Australia.
5. Planting is also proposed within a planter on the upper level terrace.
 - a. Species include *Chlorophytum comosum*, *Eremophila glabra* 'Kalbarri Carpet', *Grevillea* 'Robyn Gordon', *Russelia equisetiformis* Reggae, *Trachelospermum jasminoides*, *Arthropodium cirhatum* 'Matapouri Bay', *Bulbine glauca*, *Liriope muscari* 'Evergreen Giant' and *Philodendron* Xanadu.
 - b. The above species are suited to the growing conditions of a raised planter in a relatively exposed location. The planting will be attractive when viewed from the terrace and will be visible when viewed externally from the site.
6. In proposing species suitable for this site and its function, we were mindful of the Landscaping, Fences and Walls provisions of Council's Development Plan, including Objective 1 "The amenity of land and development enhanced with appropriate planting and other landscaping works, using locally indigenous plant species where possible" and the various Principles of Development Control.
7. We have also taken into consideration Table MuBr/6 - Landscaping Schedule of the Development Plan. Whilst we note that there are no provisions within the Development Plan that connect this table to the subject land or the zone and policy area, for completeness we have had regard to the table in the formulation of the landscape plans.
8. The table referred to above, Objective 1 and PDC 2(a) of the Landscaping, Fences and Walls provisions in particular favour the use of locally indigenous plant species where possible. Whilst some native species are proposed in the landscape plans, we do not consider the species identified in Table MuBr/6 suitable for a commercial development in an urban area such as that proposed for aesthetic and practical reasons, the latter referring to supply of stock at an appropriate size from nurseries, growth characteristics, soil requirements, form and habit, and maintenance requirements. In short, in this matter we do not consider the use of locally indigenous plant species appropriate.
9. In our opinion, the species proposed in the landscape plans are more likely to be successful horticulturally and contribute towards the overall development and precinct in a positive way.

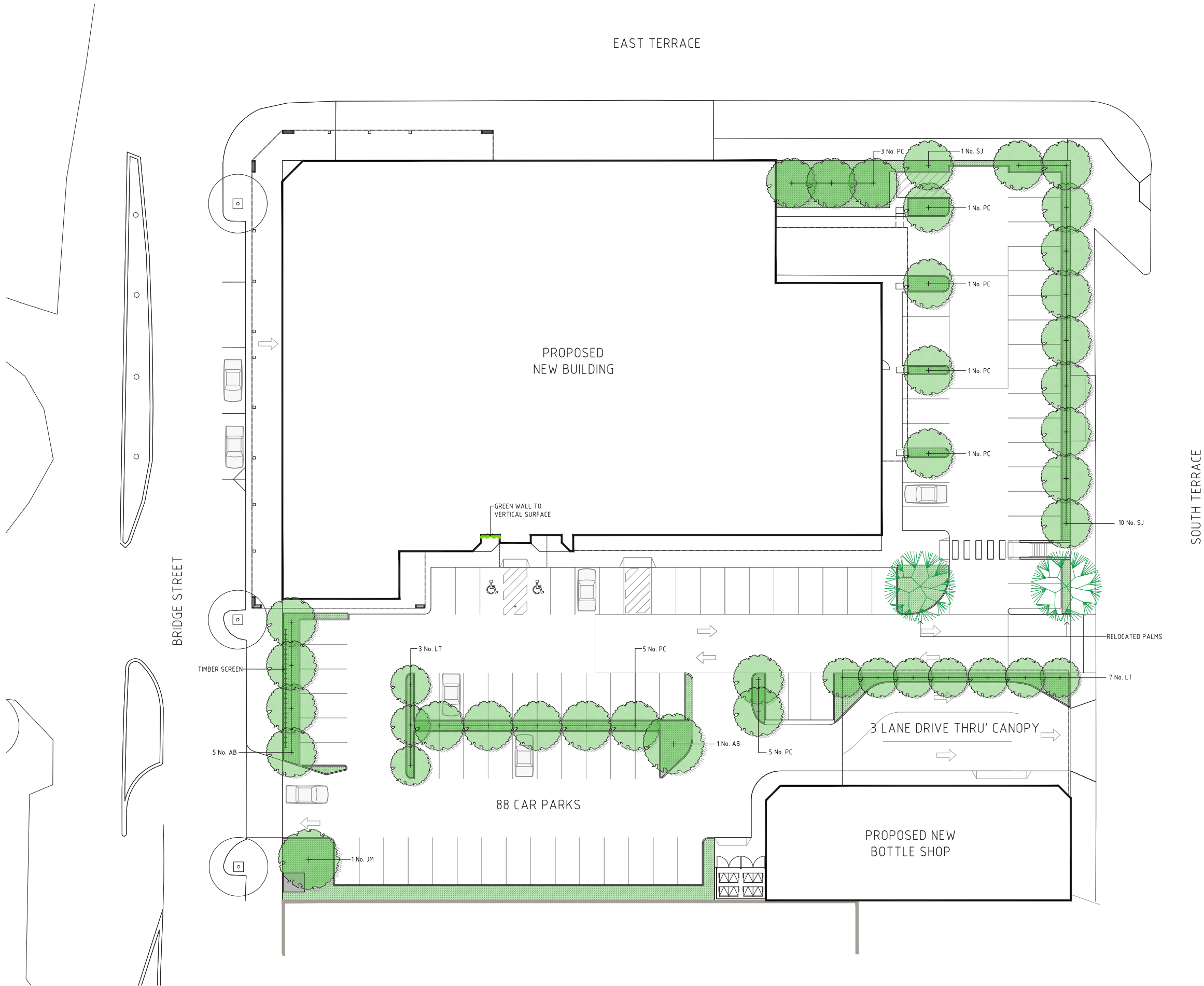
If we can provide any further information or explanation in relation to the proposed landscaping please let me know.

Regards,

Professor James Hayter FAILA

Director, Oxigen >Registered Landscape Architect

TRAVIS WRIGHT
ON BEHALF OF
JAMES HAYTER



LEGEND

NEW

EXISTING

+

NEW TREE

•

EXISTING TREE

PLANTING

BUILDING

TIMBER BATTEN

BARRIER

SPECIES LIST

TREE SPECIES

REF.	BOTANIC NAME
AB	Acer buergerianum
LT	Lagerstroemia indica 'Tuscarora'
JM	Jacaranda mimosifolia
PC	Pistacia chinensis
SJ	Sophora japonica

SHRUB AND GROUNDCOVER SPECIES

REF.	BOTANIC NAME
BA	Berberis thunbergii atropurpurea
CS	Cornus alba sibirica
EP	Escallonia Pink Pixie
ER	Euphorbia rigida
HS	Hibbertia scandens
NM	Nandina Moonbay
RP	Rosmarinus officinalis prostratus
RU	Rhaphiolepis umbellata
TJ	Trachelospermum jasminoides
WS	Westringia Smokey

AVERAGE 3 PLANTS / M²

NOTES

EXISTING SERVICES

THE CONTRACTOR MUST LOCATE AND MARK ALL UNDERGROUND SERVICES BEFORE COMMENCING WORK ON SITE.

TREE PLANTING

PREPARE TREE HOLES TO A MINIMUM SIZE OF THE DEPTH OF THE ROOTBALL, x 1m WIDE AND BREAK THE SUBGRADE TO A MINIMUM DEPTH OF 200MM BELOW. TAKE PARTICULAR CARE TO BREAK UP ANY GLAZING TO SIDES OF TREE HOLE. FINISH THE ROOTBALL LEVEL WITH THE FINAL SURROUNDING SOIL LEVEL AND BACKFILL THE PLANTING HOLE WITH SITE TOPSOIL BLENDED WITH 40% ORGANIC MATTER. PROVIDE A 1m DIAMETER MULCHED WATERING BOWL TO THE BASE OF THE TREE. STAKE TREES WITH 4No. 2500x50x50 HARDWOOD STAKES AND TIE WITH 50mm HESSIAN TIES SECURELY STAPLED TO THE STAKES. ENSURE STAKES AND TIES REMAIN CLEAR OF BRANCHES, FOLIAGE AND ROOTBALL.

PLANTING BEDS

PLANT INTO EXISTING TOPSOIL WHERE TOPSOIL IS NOT PRESENT, CULTIVATE EXISTING GROUND AND PLACE 300 MM DEPTH IMPORTED MT COMPASS SANDY LOAM. CULTIVATE ALL PLANTING BEDS TO A MINIMUM DEPTH OF 300MM. PLACE PLANTS IN THE CENTRE OF THE PLANTING HOLE AND FINISH THE TOP OF THE ROOT BALL LEVEL WITH THE FINISHED SURFACE OF THE SURROUNDING SOIL. APPLY FERTILISER AT TIME OF PLANTING AND AFTER PLANTING PLACE A 100MM MINIMUM DEPTH OF PEATS COTTAGE MULCH. THOROUGHLY WATER PLANTS BEFORE AND IMMEDIATELY AFTER PLANTING, AND AS REQUIRED TO MAINTAIN HEALTH AND VIGOR.

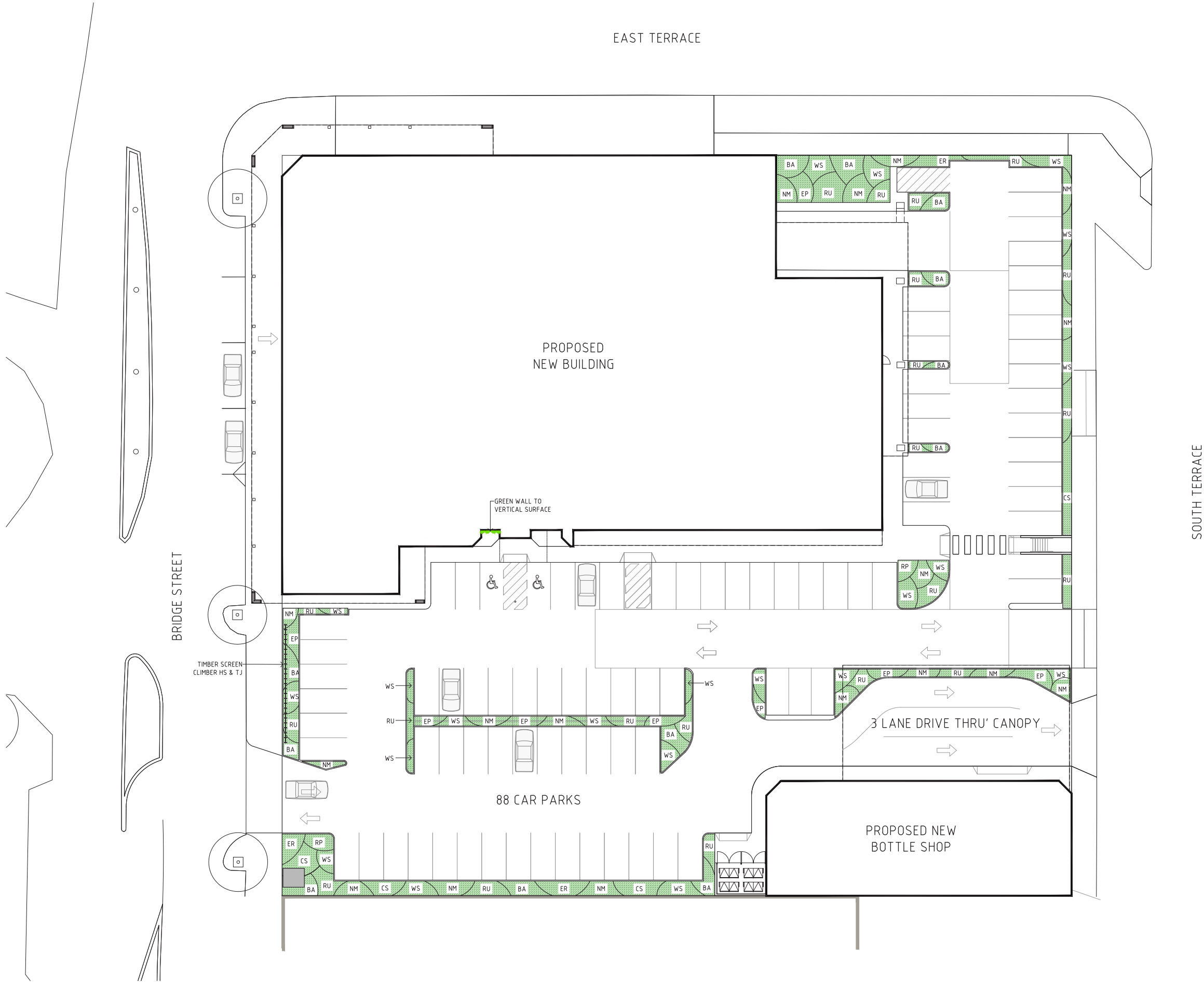
IRRIGATION

PROVIDE AN AUTOMATIC IRRIGATION SYSTEM TO ALL PLANTING BEDS AND LAWN AREAS COMPRISING POP-UP SPRINKLERS IN LAWNS AND IN-LINE DRIP IRRIGATION IN PLANTING BEDS.

DRIP IRRIGATION SPECIFIED AS NETAFIM TECHLINE 16 POLY TUBE 1.6Lph @ 0.5M SPACINGS OR SIMILAR APPROVED.

FOR ALL TREE PLANTING INSTALL AT BASE OF TREE 4No 4Lph PC DRIP EMITTERS ON 13MM POLY LOOP (OR INLINE EQUIVALENT).

ALL POLY TUBING TO BE LAID ON SURFACE AND COVERED WITH MULCH. PROVIDE BACKFLOW PREVENTION, AUTOMATIC CONTROLLER AND OTHER DEVICES AS REQUIRED.



LEGEND

NEW

NEW TREE

PLANTING

BUILDING

TIMBER BATTEN BARRIER

EXISTING

EXISTING TREE

SPECIES LIST

TREE SPECIES

REF.	BOTANIC NAME
AB	Acer buergerianum
LT	Lagerstroemia indica 'Tuscarora'
JM	Jacaranda mimosifolia
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WS	Westringia Smokey

AVERAGE 3 PLANTS / M²

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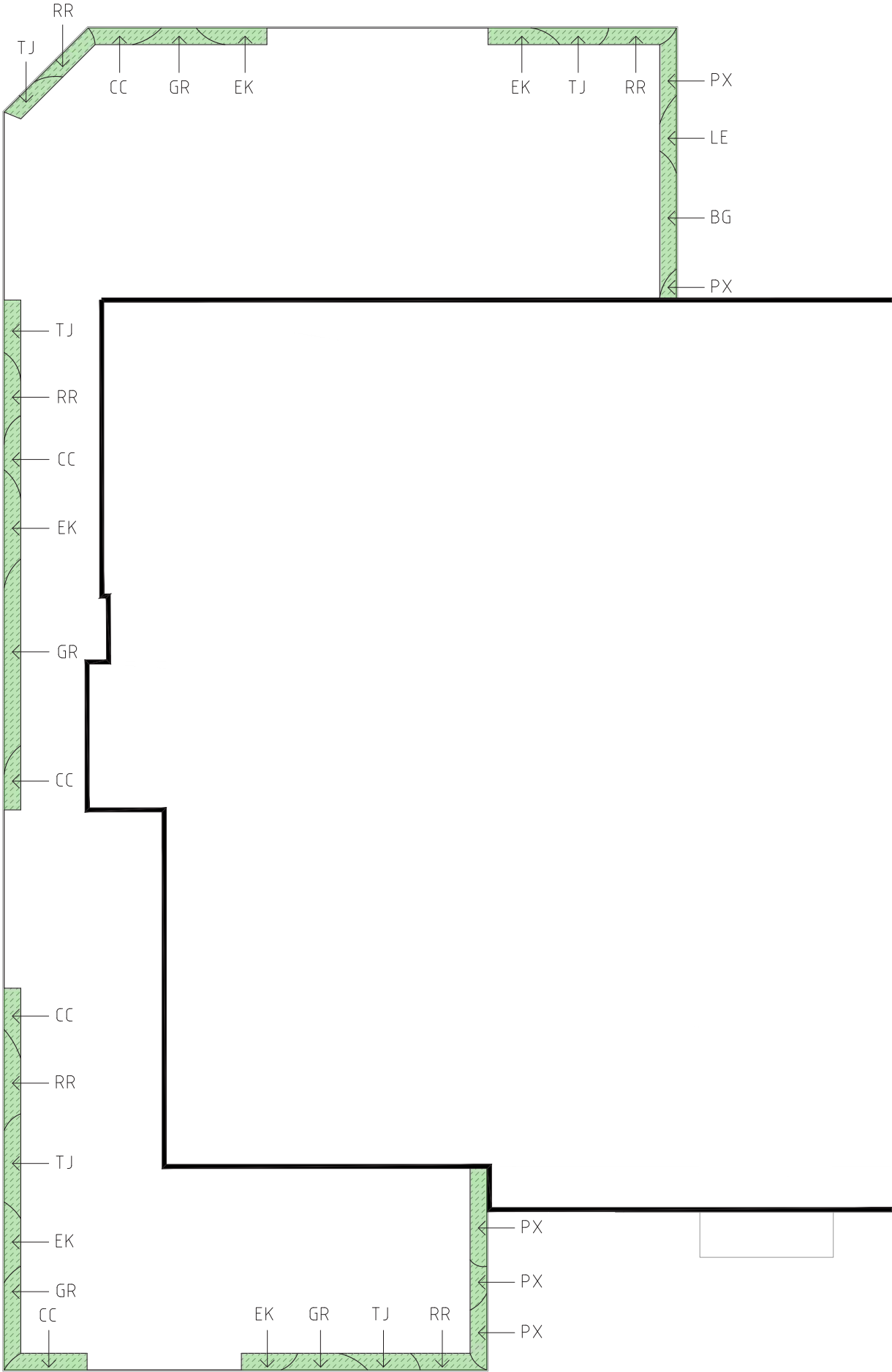
ALL POLY TUBING TO BE LAID ON SURFACE AND COVERED WITH MULCH.

PROVIDE BACKFLOW PREVENTION, AUTOMATIC CONTROLLER AND OTHER DEVICES AS REQUIRED.

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LEGEND

NEW

PLANTING

BUILDING

EDGE PLANTS SPECIES LIST

POSITION - SUN

REF.	BOTANIC NAME
CC	Chlorophytum comosum
EK	Eremophila glabra 'Kalbarri Carpet'
GR	Grevillea 'Robyn Gordon'
RR	Russelia equisetiformis Reggae
TJ	Trachelospermum jasminoides

POSITION - SHADE

REF.	BOTANIC NAME
AM	Arthropodium cirrhatum 'Matapouri Bay'
BG	Bulbine glauca
LE	Liriope Muscari 'Evergreen Giant'
PX	Philodendron xanadu

AVERAGE 3 PLANTS / M²

NOTES

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PROVIDE BACKFLOW PREVENTION, AUTOMATIC CONTROLLER AND OTHER DEVICES AS REQUIRED.

PLANTS SPECIES

TREES SPECIES
Botanical Name
<i>Acer buergerianum</i>
<i>Lagerstroemia indica 'Tuscarora'</i>
<i>Jacaranda mimosifolia</i>
<i>Pistacia chinensis</i>
<i>Sophora japonica</i>
SHRUBS & GROWDCOVERS SPECIES
Botanical Name
<i>Berberis thunbergii atropurpurea</i>
<i>Cornus alba sibirica</i>
<i>Escallonia Pink Pixie</i>
<i>Euphorbia rigida</i>
<i>Hibbertia scandens</i>
<i>Nandina Moonbay</i>
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<i>Rhaphiolepis umbellata</i>
<i>Trachelospermum jasminoides</i>
<i>Westringia fruticosa 'Smokey'</i>



Acer buergerianum
Trident maple



Lagerstroemia indica 'Tuscarora'
Crape Myrtle



Jacaranda mimosifolia
Blue jacaranda



Pistacia chinensis
Chinese pistache



Sophora japonica
Japanese Pagoda Tree



Berberis thunbergii atropurpurea
Japanese barberry



Cornus alba sibirica
Dogwood



Escallonia Pink Pixie
Pink Pixie



Euphorbia rigida
Narrow Leaf Spurge

PLANTED TIMBER SCREEN TO CARPARK



Hibbertia scandens
Guinea Flower



Nandina Moonbay
Sacred Bamboo



Rosmarinus officinalis 'Prostratus'
Creeping Rosemary



Rhaphiolepis umbellata
Japanese hawthorn



Trachelospermum jasminoides
Star Jasmine



Westringia fruticosa 'Smokey'
Coastal Rosemary

DASH Architects is one of Australia's leading practices in the provision of specialist heritage services and has been at the forefront of the development of a sustainable paradigm for the conservation of cultural heritage.

Operating across the full range of the architectural disciplines enables DASH Architects an appreciation of the role of cultural heritage within the broader design process, as one of many factors that influence project outcomes.

This flexible and integrated approach is based primarily on contemporary community values and traditions. Within this framework there is an acknowledgement that while the preservation of heritage fabric is important, it is only one of many considerations when assessing the cultural significance of a place.

Bridgeport Hotel Redevelopment - Review of Proposed Demolition of Local Heritage Place (DA2014/20674/01)

DA153176 – Issue B – 15.08.2017

1.0 Introduction

Since late 2015 I have been engaged by the Kerin Bay Pty Ltd (a member of the Eureka Hotel Group) (the Applicant) to provide heritage advice in relation to its property, the Bridgeport Hotel, Murray Bridge (the Subject Property, or the Hotel).

This report is aimed at summarising the advice I have provided relating to the proposed demolition of the Local Heritage place on that site.

In doing so, I have also responded to various of the issue raised within a report prepared for the Rural City of Murray Bridge by Flightpath Architects, dated October 2016, titled: "Heritage Advice Bridgeport Hotel" (the Flightpath Report).

In preparing this report I have:

- Visited the site and locality;
- Reviewed the following documents:
 - Draft copy of the Murray Bridge Town Centre and Environs Local Heritage Register by Bruce Harry & Associates, Nov 2001¹;
 - the Heritage Data Sheet (Data Sheet) from within the above Register;
 - Section (23)4 criteria of the Development Act;
 - Planning SA's "Planning Bulletin – Heritage";
- Reviewed a report that I prepared in February 2016, titled "Bridgeport Hotel, Murray Bridge Review of Local Heritage Listing (Issue A) (1st DASH Report);
- Reviewed a letter that I prepared in March 2017, titled "Further Review of Heritage Value of Bridgeport Hotel" (2nd DASH Report);
- Reviewed the Flightpath Report; and
- Undertaken further detailed inspections of the property on Wednesday 15th March 2017, and specifically inspected areas within the ceiling space, between the ground and first floors, adjacent to the external walls.

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ABN 82 059 685 059

¹ I was not able to locate a final issue of this document.

2.0 About the author

I am an architect and heritage consultant with over 20 years of consulting experience. Since 2000, I have been a Director of DASH Architects (Danvers Schulz Holland Architects Pty Ltd), a multi-disciplinary practice providing professional services in the fields of Architecture & Interiors, Heritage, and Urban Design. Of the professional services it offers, DASH Architects specialise in contextual architecture and urban design within zones of heritage significance, adjoining heritage items or as extensions and modifications to heritage items themselves.

As part of DASH Architects, I have been responsible for, or overseen, numerous significant heritage projects (including multiple award winners), significant architectural projects, Conservation Management Plans and conservation projects.

As part of Local Heritage PARs/DPAs, or as part of Development Applications or appeals, I have prepared numerous Heritage Significance Reports and statements (assessing properties against the relevant listing criteria and Development Plan provisions).

I am also regularly asked to provide expert heritage and design advice to other architects, designers and applicants in relation to proposed developments.

I am Fellow of the Australian Institute of Architects (RAIA). I have previously been its State President, a National Director, and a member of the National Practice Committee. I was also a Chapter Councilor and chair of the State Practice Committee. I have sat on the Architectural Practice Board of South Australia. My practice is a member of the Association of Consulting Architects - Australia and I have been a member of its SA Branch Committee. I was also formerly a member of Seymour College's Property and Building Committee.

I have also been, and continue to be, a member of various State and National Visiting Panels responsible for the accreditation of the Architectural courses at the University of Adelaide and at the University of South Australia.

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3.0 Review of the Local Heritage Listing of the Place Assessment

3.1 Heritage Listing

The Development Plan identifies portions of the Bridgeport Hotel (the Subject Place) as a Local Heritage Place in Table MuBr/4 as follows:

Property Address	Description and/or Extent of Listed Place	Lot No or Part Sec	Plan No	Certificate of Title	Section 23(4) Criteria	DPLG ID
2-6 Bridge Street MURRAY BRIDGE	Bridgeport Hotel; Elements to be retained - Original large two-storey Victorian Hotel section, including original rear single - storey storeroom and later two- storey Victorian extensions	99		CT 5187/857	a, c, d, f	14209

Further background to this listing was obtained from the November 2001 Murray Bridge Town Centre & Environs Local Heritage Register (the Heritage Survey) prepared by Bruce Harry & Associates for the Local Council. Only a draft copy of this report could be sourced, and it is unclear whether a final version was ever issued.

The ultimate listing of the Bridgeport Hotel was a listing of the portion described in Table MuBr/4, referenced above.

3.2 Data Sheet

The Data Sheet for the Subject Property, sourced from within the above Register, has been attached for reference. Relevant sections of the data sheet have been quoted below.

The Data Sheet for the description of the Subject Property is as follows:

"This large two storeyed building is typical of High Victorian corner Hotel architecture. It has undergone numerous stylistic and structural changes, mostly at Ground Floor level.

The original stone walls are embellished with quoined opening surrounds and Italianate bracket pairs to the eaves, Progress of building changes are clearly evident, the least original fabric being at ground level. Although the changes have been extensive, a sympathetic reconstruction of the principal facades would be possible."

Regarding this description, I do not believe that use of the term 'High Victorian' is appropriate. To my mind, 'High Victorian' would describe a highly

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ornate and classically proportioned building. The Bridgeport Hotel is not that. Examples of Hotel buildings, from the same period, that could be described as 'High Victorian' would be the Botanic Hotel, Adelaide or the Newmarket Hotel, Adelaide. I would suggest that the Bridgeport Hotel would be better described as simply 'Victorian'.

The Statement of Heritage Significant within the Heritage Data Sheet describes the Bridgeport Hotel as being typical of 'High Victorian' Style. It is my contention that it is not. The Newmarket and Botanic Hotels have been cited as examples of buildings that are of 'High Victorian' style. In doing so, I am not suggesting that they are comparable from a heritage significance standpoint.

I also suggest that much of the second paragraph of the Statement of Heritage Significance could be deleted, or at least significantly reduced, as it is either redundant or irrelevant.

As such I suggest that the Description could be revised to be:

*"This large two-storeyed building is **representative** typical of High Victorian ~~corner~~ Hotel architecture. **Its remaining original stone walls are embellished with quoined opening surrounds and Italianate bracket pairs to the eaves.** It has undergone numerous stylistic and structural changes, mostly **notably** at Ground Floor level. ~~The original stone walls are embellished with quoined opening surrounds and Italianate bracket pairs to the eaves, Progress of building changes are clearly evident, the least original fabric being at ground level. Although the changes have been extensive, a sympathetic reconstruction of the principal facades would be possible."~~*

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Image 1: Photograph of the Subject Property taken by Author 2016



Image 2: Botanic Hotel, Adelaide (built c.1877)



Image 3: Newmarket Hotel, Adelaide (built c.1883)

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3.3 Integrity

Integrity is an important consideration when assessment to the degree to which a place meets the requirements for listing under many of the relevant criteria. Following is discussion of the integrity of the Bridgeport Hotel.

3.3.1 c1879-c1937

Early photographs (c1879 through to c1937) sourced of the Bridgeport Hotel (images 4-8 below) show an imposing two storey face stone building, with first floor balcony and cast iron detailing to all visible sides (no early images could be sourced of the rear of the building). This balcony was cantilevered to East Terrace, but supported off posts to other facades. It included cast iron corner brackets to the first floor posts, and cast iron balustrading. Quoining to the building's fenestration and corners appears to be face red brick, while the hipped roof accommodates up to six chimneys (as visible from the surrounding streets).

The most notable change when comparing the early images (c1879 through to c1937) of the hotel was the reconfiguration of the Bridge Street balcony, that originally did not extend across the full length of this façade (compare images Image 5 and 6).

These issues aside, the external appearance of the building appears to have changed very little during the period covered by the above images (c1879 through to c1937).

3.3.2 c1937 on

Since then, however, the building has undergone numerous and notable change associated with various refurbishments / redevelopments, including the complete concealment / reconfiguration / reconstruction of the ground floor façade.

While the extent of any remaining original fabric at ground floor is unclear from visual inspection, it is likely to be minimal, as all doors and windows have been reconfigured, and face cream brick (c. 1970) now presents to both street elevations at this level.

Most notable of this ground floor level reconfiguration, is the removal of the corner portion (fronting the intersection of Bridge Street and East Terrace) to create a deep, widened recessed entry. The original alignment of the first floor façade remains, supported (somewhat awkwardly) over this new recessed entry (ref Images 10 and 11).

The ground floor level has also been extended down the steeply sloping East Terrace, creating a two storey high form at its end.

Above ground floor, much of the original face stonework and brickwork appears to remain, albeit now painted. The upper verandah of the balcony remains in generally the same post-1920s configuration, however the main structure and balustrading has been heavily modified.

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Of the six original chimneys visible from the surrounding streets, only four appear to remain.

The Subject Place has been identified as a Local Heritage item as it was considered to adequately fulfill the following Development Act (1993) Section 23(4) criteria:

- *(a) it displays historical, economic or social themes that are of importance to the local area;*
- *(c) it has played an important part in the lives of local residents;*
- *(d) it displays aesthetic merit, design characteristics or construction techniques of significance to the local area; and*
- *(f) it is a notable landmark in the area*

3.3.3 Summary

Overall, the integrity of the place has been significantly compromised, particularly through the loss of the ground floor facades and spaces.

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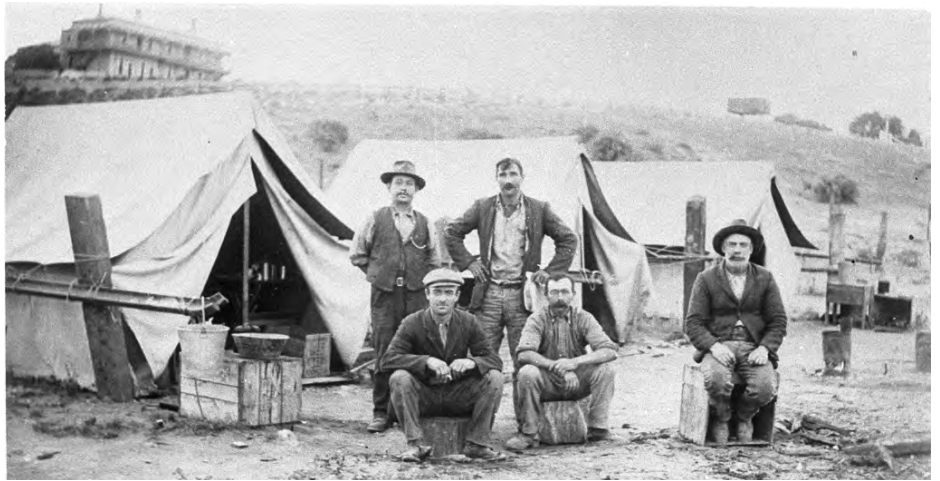


Image 4 - Bridgeport Hotel, c1879. Source: State Library of South Australia, PRG1258/2/1652



Image 5 - Bridgeport Hotel, c1900, showing early balcony configuration to Bridge Street. Source: Across the Mighty Murray, p79



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Image 6 - Bridgeport Hotel, c1920. Source: State Library of South Australia, B44623



Image 7 - Bridgeport Hotel, c1925. Source: State Library of South Australia, B61788/75



Image 8 - Bridgeport Hotel, c1937. Source: State Library of South Australia, B12192



Image 9 - Bridgeport Hotel, 2015, Viewed from East Terrace

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Image 10 - Bridgeport Hotel, 2015, Viewed from East Terrace



Image 11 - Bridgeport Hotel, 2015, showing enlarged corner entry



Image 12 - Bridgeport Hotel, 2015, corner of Bridge Street and East Terrace

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Image 13 - Bridgeport Hotel, 2015, viewed from Bridge Street



Image 14 - Bridgeport Hotel, 2015, viewed from Bridge Street



Image 15 - Bridgeport Hotel, 2015, showing cream brick ground floor façade, later awning, balustrading and some verandah posts.

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3.4 Assessment of The Listing

As noted above, the Data Sheet proposed portions of the property for listing under criteria (a) (c), (d) and (f), and included a "Statement of Heritage Value" for it. As noted previously, the ultimate listing of the Bridgeport Hotel was a listing of the portion described in Table MuBr/4, referenced above.

Following is a discussion of the merits of this proposition.

3.4.1 Criterion (a)

The data sheet proposes the Hotel for listing under criterion (a) by suggesting that:

"it displays historical and economic themes that are of importance to Murray Bridge."

"Planning Bulletin - Heritage" provides the following commentary to this provision:

"To determine places under this criterion a local community must have a clear idea of the historical development of the area and region and the overview history prepared for a heritage survey will provide this knowledge."

Examples:

- *Groups of buildings, which display the close concentration of commercial and residential buildings in the early settlement of the area (eg Mitcham Village). Farmhouses which have been engulfed by spreading residential and other development.*
- *Industrial complexes or buildings which have been the basis of settlement or economic activity in the area (eg Onkaparinga Mill, Lobethal; stone quarries, Mount Gambier; shearers' factory, Woodville).*
- *Residential development on specific subdivisions creating new areas of development of towns (eg Whinham Street, Prospect)."*

The Draft Murray Bridge Town Centre and Environs Local Heritage Register, contains an "Overview History of Murray Bridge" that establishes the key periods of importance to the Town. Following is this history:

"Until the building of a bridge over the River Murray was begun in 1873, the area surrounding the river crossing was largely open farmland with scattered buildings. The nearest township was at Wellington, on the main road between Adelaide and Melbourne. The substantial development of Murray Bridge township followed the building of the first bridge and has primarily occurred in three distinct historical periods:

- *1873-1886: the period of establishment, intense land speculation, and brief building boom during which the first bridge was constructed, large farm holdings began to be subdivided, the town was formally surveyed, the initial period*

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of town building occurred, the railway link to Melbourne was completed, and the river trade was buoyant. After 1886, the South Australian economy fell into a recession worsened by drought and the worldwide depression of the 1890s, the effects of which lasted until 1910;

- *1910-1926: the period of consolidation, and renewed growth after WW1, development of community infrastructure and services and construction of the second bridge. This period of rapid development ended with the removal of the railway workshops to Tailem Bend in 1926 and the subsequent onset of another severe drought, and worldwide Depression, and the collapse of the river trade. The 1934 Jubilee, and 1936 South Australian Centenary provided brief opportunities for celebrations.*
- *1950-1980: the post WW2 period of resumed growth, triggered by the end of rationing, renewed optimism, and post war immigration, up to the completion of the Swanport Bridge and freeway bypass, and final closure of the Railway Station complex.*

These were the periods of most intensive building and landscaping, and they are evident in the townscape in the architectural styles of the buildings and botanical species popular in each period..."

The Hotel was built c.1879 and it is reasonable to suggest that sections of the Hotel that remain from this period can be considered to represent the 1873-1886 period of development.

That said however, building integrity plays an important role in fulfilling criterion (a), as the heritage place needs to “display” the cited *historical, economic or social themes that are of importance to the local area*. As noted in previous sections, there have been numerous and substantial changes to the building since the period that it represents. Most notable of these changes are those to the ground floor level facades that face Bridge Street and East Terrace. Almost all of these sections of the building have been altered and are now stylistically and functionally different. The location and extent of the changes (and particularly as they relate to the public interface of the building at ground floor street level) has compromised the integrity of the Building and have reduced the degree to which the Hotel displays the important period of development in the area between 1873-1886.

The compromised integrity of the Subject Building is illustrated in the photographs within this report. Images 5, 6, 7, and 8 in the previous section show the façade before the changes, while Images 9, 10, 11, 12, 13, 14, and 15 show it as it is now.

It is likely that if the subject building was in ‘original’ (c.1937) condition it would meet the requirements for listing under criterion (a). While it would be possible to reconstruct the missing sections of the ground floor of the building (as suggested in the description within the Data Sheet), the degree to which the item meets the criteria for listing must be assessed on the basis of the current

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condition of the building, and not on what it was, or what it has the potential to be.

In assessing the degree to which the place meets the requirements for listing under the criterion, I have considered (hypothetically) the impact that the changes to the ground floor of the building have had. In particular, I have reflected on how these changes would be viewed if they were proposed to an otherwise 'original' (c.1937) building today. Having done so I have formed a view that the changes would not be supportable as they would result in the loss of extensive areas of physical fabric including the almost total loss of the public interface of the building. While this is an hypothetical exercise it served to highlight for me that the changes that have been made to the ground floor of the building have had a profound impact of the value of the place and particularly on its ability to demonstrate that value.

While it is not the intention of this report to address all the points raised in the Flightpath Report, a critical point of difference between that Report and my views relates to the role that the 'integrity' of place plays in that place's ability to meet the criteria for listing and thus how it affects its Heritage Value.

While I appreciate the "deeper meaning of heritage value" (to quote from the Flightpath report), under our legislation these meanings need to be vested in some form of physical fabric. This concept is readily understood at the 'extremes'. At one end of the spectrum, where a place has remained unchanged from its period of significance, it clearly retains its heritage value. Whereas, at the other end, where all the 'original' fabric has been removed or replaced, it clearly does not. It is less clear within the spectrum. The difficulty is in determining the point at which so much fabric has been lost that what is left no longer adequately meets the requirements for listing. It is my view that, due to the many and varied changes, the Bridgeport Hotel has reached that point.

On this basis, I have formed a view that the significant and prominent changes that have been made to the Hotel have reduced its integrity to the point where it no longer adequately displays historical and economic themes that are of importance to Murray Bridge and does not meet the requirements for listing under this criterion.

3.4.2 Criterion (c)

The data sheet proposes the Hotel for listing under Criterion (c) by suggesting that:

"it has played an important part in the lives of local residents"

"Planning Bulletin - Heritage" provides the following commentary to this provision:

c) It has played an important part in the lives of local residents.

Commentary:

Many types of buildings, which form the basis of community structure, can

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be assessed under this criterion. These building types are often repeated in each local government area, but each community will also have specific places to which they will have special attachment due to the particular circumstances of local development and sense of place. Examples:

- *Early schools, both primary and secondary.*
- *Hotels.*
- *Mechanics institutes and libraries.*
- *Hospitals (particularly early sections).*
- *Churches.*

While the building can be considered to have played an important part in the lives of local residents through its use as a hotel, this is true, to varying degrees, of all Hotels. To meet this criterion there needs to be a “*special attachment due to the particular circumstances of local development and sense of place*”. While the period in which the Hotel was established is of significance to the town (as established above in the discussion of criterion (a) above), neither the History within the Local Heritage Register, nor the data sheet for the property, provides any further support to the argument for listing under this criterion. Without this support it is not possible for me, within the confines of this report, to form an argument for listing under this criterion simply based on observations of the site and my thoughts on the how the community might feel about the place.

Overriding these discussions, as with criterion (a), are also considerations of the degree to which the compromised integrity of the physical fabric of the place, particularly as they relate to the public interface of the Hotel to East Terrace and Bridge Street, has undermined the ability of the place to demonstrate any importance it might have.

Based on this, I believe that there has not been a strong argument presented that establishes the significance of the role that the Hotel has played in the lives of local residents. Further, the significant and prominent changes that have been made to the Hotel have reduced its integrity to the point where it capacity to reflect this role (whatever significance it might have) has been substantially eroded. On balance therefore, I find that this place does not adequately meet the requirements for listing under this criterion.

3.4.3 Criterion (d)

The data sheet proposes the Hotel for listing under Criterion (d) by suggesting that:

“it displays aesthetic merit and design characteristics of significance to the local area.”

"Planning Bulletin - Heritage" provides the following commentary to this provision:

“Local areas will have places that are particularly characteristic of the conditions or materials available within the district. These places will often immediately come to mind when the locality is mentioned, as being ‘typical’ of the area. Examples:

- *Mount Gambier limestone buildings and houses.*
- *North Adelaide bluestone villas.*

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- *Port Adelaide timber-framed corrugated iron buildings.*
- *Half-timbered buildings in German settlements.*
- *Buildings designed by significant local architects/designers.”*

With reference to this, a place would be considered to fulfil this criterion if its particular design characteristics and construction techniques immediately come to mind as being 'typical' of the local area. The Victorian style and materials used in the construction (face stonework, brick quoins, timber windows and doors, lacework to balconies, corrugated iron roofing and verandahs) of the Bridgeport Hotel are not unique or significant to the local area and indeed are seen throughout South Australia in buildings of this kind, from this period. Examples are numerous but include: Pretoria Hotel, Mannum (Image 16), Jens Hotel, Mount Gambier (Image 17), and Belalie Hotel, Jamestown (Image 18).

I have suggested that the style of construction and materials used in the Bridgeport Hotel are seen elsewhere South Australia in buildings from this era and are therefore not unique or significant to this region. I cite examples of this. In doing so, I have not suggested that the significance of the examples cited is comparable to that of the Bridgeport Hotel. I have simply noted that they are from a similar era, of similar style and use similar materials.

Further the aesthetic merit and design characteristics have been significantly compromised by the modifications and additions made to the building, as previously discussed.

Based on the above, I do not believe that the subject building meets the requirements for listing under criterion (d).

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Image 16 – Pretoria Hotel, Mannum



Image 17 – Jens Hotel, Mount Gambier



Image 18 – Belalie Hotel, Jamestown

3.4.4 Criterion (f)

The data sheet proposes the property for listing under Criterion (f) by suggesting that:

"It is a notable landmark in the area".

"Planning Bulletin - Heritage" provides the following commentary to this provision:

"Places assessed as fulfilling this criterion need to be visually prominent and a reference point for the whole community or a significant part of it.

Examples:

- *Memorials at road intersections or prominently placed in town centres.*
- *Large buildings within lower scale settlements."*

The Subject Building is two-storey in a streetscape of a similar scale. It has no significant features that breaks this pattern or provides landmark characteristics. While it is prominent, this prominence is a result of the characteristics of the site: its location (at the end of the main street and aligned with the bridge crossing) and the topography (it is at a high point in the terrain), rather than of any inherent characteristic of the building itself.

Based on the above, I do not believe that the subject building adequately meets the requirements of for listing under criterion (f)

3.5 Conclusion of Assessment

In the above sections, I have concluded that, largely through the significant and prominent changes that have been made to it, the Hotel does not adequately meet the requirements for listing under criteria (a) and (c). I also have concluded that it does not adequately meet the requirements for listing under either criteria (d) or (f)

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4.0 Further Investigations

On page 21, the Flightpath report suggests (that on the):

“Ground Floor: external walls are very thick and there is every indication that the original walls still exist behind a more recent external skin.”

Having undertaken further investigations on site, including inspection within the ceiling spaces adjacent the Bridge Street and East Terrace facades, I note that:

Bridge Street Elevation

Refer *Image 1* below.

- It appears that the entire length of the ground floor section of the ‘original’ (c1879-1937) sections of stone wall has been removed and replaced with: (load bearing) brickwork, concrete columns and beams, and windows.
- We were not able to establish if the brickwork encroaches onto the footpath (over the boundary).
- Based on my 20 plus years of architectural consulting experience, I suggest that the removal of the brickwork and reinstatement of stonework (using new stone) would neither be readily achieved nor cost effective.

Corner

Refer *Image 2* below.

- As per the DASH Report, the entire ‘original’ lower section of the corner has been removed.

East Terrace Elevation

Refer *Image 3* below.

- Except at the corner where it has clearly been removed, there is evidence to suggest that there are sections of original stonework remaining behind the brickwork wall along this facade. We are not sure of the condition or extent of this stonework however we assume that it was only removed where required for the works at the corner and for new fenestration etc. I am also not sure whether the concealed stonework retains the capacity to be load bearing (this capacity may have been compromised either because of the design of the newer walls or due to its condition).
- Again, I was not able to establish if the newer brickwork wall encroaches onto the footpath (over the boundary).

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Image 1 - Annotated image of Bridge Street elevation of Hotel (photo by author)



Image 2 - Annotated image of corner elevation of Hotel (photo by author)



Image 3 - Annotated image of East Terrace elevation of Hotel (photo by author)

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4.1 Adaptive Reuse

As an extension to the above discussions about integrity, while there is often potential for the adaptive reuse of a building, there are several factors in this instance that would likely limit the viability of this approach and incur excessive time, cost and/or risk penalties. These include:

- Much of the remaining 'original' fabric is at first floor level (potentially limiting the ability of new work to be built 'over and through' the existing building);
- The layout of the upper floor does not lend itself to reuse (it compromises multiple small compartments);
- There is no setback to the main street frontages (meaning that new work would likely need to be positioned behind this section of building)
- There is a need to replace sections of the lower floor facades (adding cost and risk); and
- The need to upgrade the existing building to meet current construction codes (again, adding cost and likely requiring physical intervention in remaining fabric).

Based on the compromised integrity of the place, and the location and nature of the fabric that is left, it is my view that this approach is not warranted in this instance.

5.0 Sign Off

This Report has been prepared for and on behalf of DASH Architects



David Holland
Architect
Director
DASH Architects

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DASH Architects is one of Australia's leading practices in the provision of specialist heritage services and has been at the forefront of the development of a sustainable paradigm for the conservation of cultural heritage.

Operating across the full range of the architectural disciplines enables DASH Architects an appreciation of the role of cultural heritage within the broader design process, as one of many factors that influence project outcomes.

This flexible and integrated approach is based primarily on contemporary community values and traditions. Within this framework there is an acknowledgement that while the preservation of heritage fabric is important, it is only one of many considerations when assessing the cultural significance of a place.

Bridgeport Hotel Redevelopment – Contextual Analysis Report (DA2014/20674/01)

DA153176 – Issue B – 28.08.2017

1.0 Introduction

Since late 2015 I have been engaged by the Kerin Bay Pty Ltd (a member of the Eureka Hotel Group) (the Applicant) to provide heritage advice in relation to its property, the Bridgeport Hotel, Murray Bridge (the Subject Property, or the Hotel).

Following a meeting on site with the Government Architect, and receipt of feedback from ODASA, dated 13 April 2017, the Applicant has also now engaged me to:

- Review the design of the new works proposed as part of the development, and to provide feedback to its designer, Craig Eyles of CED Building Design, concerning the relationship of this new work with the public realm and the locality generally, and how it responds to the issues raised by ODASA; and
- Prepare a 'Contextual Analysis Report' aimed at:
 1. Reviewing the Impact of the new works proposed as part of the development on the State and Local Heritage places in the vicinity; and
 2. Reviewing how the revised proposal prepared by the Applicant relates to the public domain and the locality generally.

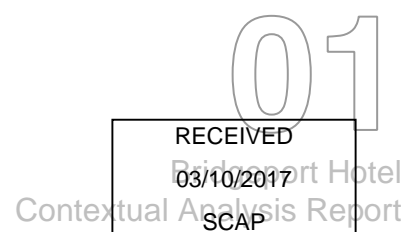
In preparing this report, I have:

- Revisited the site and locality;
- Attended meetings with the Government Architect (on site) and the Associate Government Architect (at DPTI);
- Met with the designer, Craig Eyles of CED Building Design;
- Reviewed a Report by Andrew Martin of TMK Consulting Engineers (dated 22.08.17);
- Reviewed various iterations of previous and revised design proposals prepared by CED Building Design; and
- Reviewed the final design proposal (dated August 2017) that is to be assessed by the Development Assessment Commission (DAC).

Having done so, I offer the following comments.

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2.0 About the author

I am an architect and heritage consultant with over 20 years of consulting experience. Since 2000, I have been a Director of DASH Architects (Danvers Schulz Holland Architects Pty Ltd), a multi-disciplinary practice providing professional services in the fields of Architecture & Interiors, Heritage, and Urban Design. Of the professional services it offers, DASH Architects specialise in contextual architecture and urban design within zones of heritage significance, adjoining heritage items or as extensions and modifications to heritage items themselves.

As part of DASH Architects, I have been responsible for, or overseen, numerous significant heritage projects (including multiple award winners), significant architectural projects, Conservation Management Plans and conservation projects.

As part of Local Heritage PARs/DPAs, or as part of Development Applications or appeals, I have prepared numerous Heritage Significance Reports and statements (assessing properties against the relevant listing criteria and Development Plan provisions).

I am also regularly asked to provide expert heritage and design advice to other architects, designers and applicants in relation to proposed developments.

I am Fellow of the Australian Institute of Architects (RAIA). I have previously been its State President, a National Director, and a member of the National Practice Committee. I was also a Chapter Councilor and chair of the State Practice Committee. I have sat on the Architectural Practice Board of South Australia. My practice is a member of the Association of Consulting Architects - Australia and I have been a member of its SA Branch Committee. I was also formerly a member of Seymour College's Property and Building Committee.

I have also been, and continue to be, a member of various State and National Visiting Panels responsible for the accreditation of the Architectural courses at the University of Adelaide and at the University of South Australia.

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Bridgport Hotel
Contextual Analysis Report

3.0 Assessment

3.1 Impact of proposed development on State and Local Heritage Places in the vicinity

There are two components to this Application:

- (a) the proposed demolition of the Local Heritage Place; and
- (b) the proposed new works.

Having considered the potential impact of each of these components on the heritage values of the State and Local Heritage Places in the vicinity, I offer the following comments.

The aerial image below (image 08) shows the site, surrounding area and the various Heritage Places in the locality of the Subject Site. Of the Heritage places shown on the aerial image, I have considered the impact of the Application on the following State Heritage Places:

- Murray Bridge Transport Precinct; and
- Murray Bridge Hotel.

I have also considered the impact of the Application on the following Local Heritage Places:

- Former Town hall and Municipal Offices;
- Flour Mill;
- Row of Canary Island Palms Trees;
- Ruges Beehive Corner; and
- Railway and Wharf Precinct.

Having assessed the potential impact that the proposed demolition of the Hotel (and thus that portion of it that is on the Local Heritage List) currently on the Subject Site would have on other Heritage places in the locality, I have formed a view that:

- The nature of the Hotel site is such that it is isolated and does not relate strongly to the other heritage elements within the area. While there are other places of a similar age, and that represents a similar period of development, these places do not 'read' as either a cohesive streetscape, or area. As such the demolition of the existing Hotel would have little adverse impact on the heritage values of the other Heritage Places within the Vicinity of the site.

Having assessed the potential impact that the new works proposed on the Subject Site may have on other Heritage places in the locality, I have formed a view that:

- Other than for the Hotel itself (that is discussed above), the proposed Development does not propose the removal or alteration of the physical fabric of any of the State or Local Heritage places in the area;
- Other than for the Railway Tunnel within the Murray Bridge Transport Precinct (that is discussed below), there is a significant physical separation between the Subject Site and the other Heritage Places in

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- its vicinity. Given the nature of the development, I do not foresee any risk of physical damage to any other Heritage Places in the locality arising from the construction of the proposed development;
- As noted above, the Railway Tunnel within the Murray Bridge Transport Precinct (see Images 09-11) is near the Subject Site. The Applicant has commissioned Andrew Martin, of TMK Consulting Engineers, to review the potential impact that the new building might have on the Tunnel. Mr Martin has indicated in his Report that neither the demolition of the existing structure, nor the construction of the new ones, need have an adverse impact on the Railway Tunnel. He also indicated that, once constructed, the proposed new building will not have an adverse impact on the Railway Tunnel as it is located outside its 'line of influence'. Based on this advice I accept that the proposed Development (the demolition of the existing Hotel and the new works) would have no detrimental impact on the physical fabric of the adjacent Railway Tunnel and thus would not have an impact on its heritage values;
 - The Murray Bridge Transport Precinct is also designated as a place of archaeological significance. Although I am not an archaeologist, given there are no works proposed as part of this Application that would require excavation within the Precinct, I do not envisage any impact on that aspect of the significance of the place.

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3.1.1 Summary of Assessment

In summary, the proposed Development (the demolition of the existing Hotel and the new works) will not, in my opinion, directly affect any of the other Heritage Places, or in any meaningful sense, affect the setting in which those other heritage Places are situated.

Place name	Heritage Listing	Assessment of Impact of the proposed development on the heritage values of the place	
		Physical Impact	Contextual Impact
Murray Bridge Transport Precinct	State	<p>No physical work is proposed to the site of the Murray Bridge Transport Precinct as part of the Application, therefore, no loss of significant fabric, or impact on the archaeological significance of the site is envisaged.</p> <p>The proposed works will not have any impact of the physical fabric of this site.</p>	<p>The nature of the Hotel site is such that it is isolated and does not relate strongly to other heritage elements within the area.</p> <p>While there are other places of a similar age, and that represents a similar period of development, these places do not 'read' as either a cohesive streetscape. or area.</p>
Murray Bridge Hotel	State	<p>No physical work is proposed to these sites as part of the Application, therefore, no loss of significant fabric is envisaged.</p> <p>The Heritage Place is sufficiently distant from the site of the proposed works such that it is not envisaged that the construction will impact on the physical fabric of the Heritage Places, or on the Canary Island Palm Trees, themselves.</p>	<p>As such the demolition of this place would have little adverse impact on the heritage values of the other Heritage Places within the Town.</p>
Former Town hall and Municipal Offices	Local		
Flour Mill	Local		
Row of Canary Island Palms Trees	Local		
Ruges Beehive Corner	Local		
Railway and Wharf Precinct	Local		

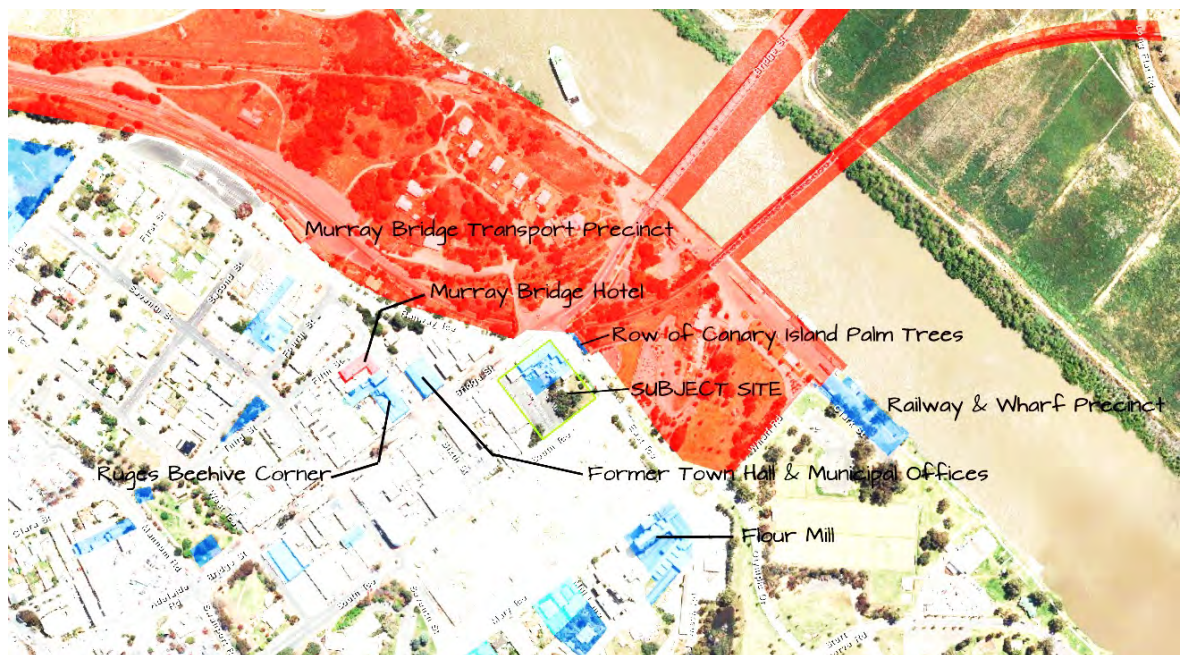


IMAGE 08 - taken from location.sa.gov.au, with annotations by the author, showing the Subject Site (in green), various State Heritage places (in red) and various Local Heritage places (in blue) in the locality, and beyond (noting also contributory items are shown in aqua).

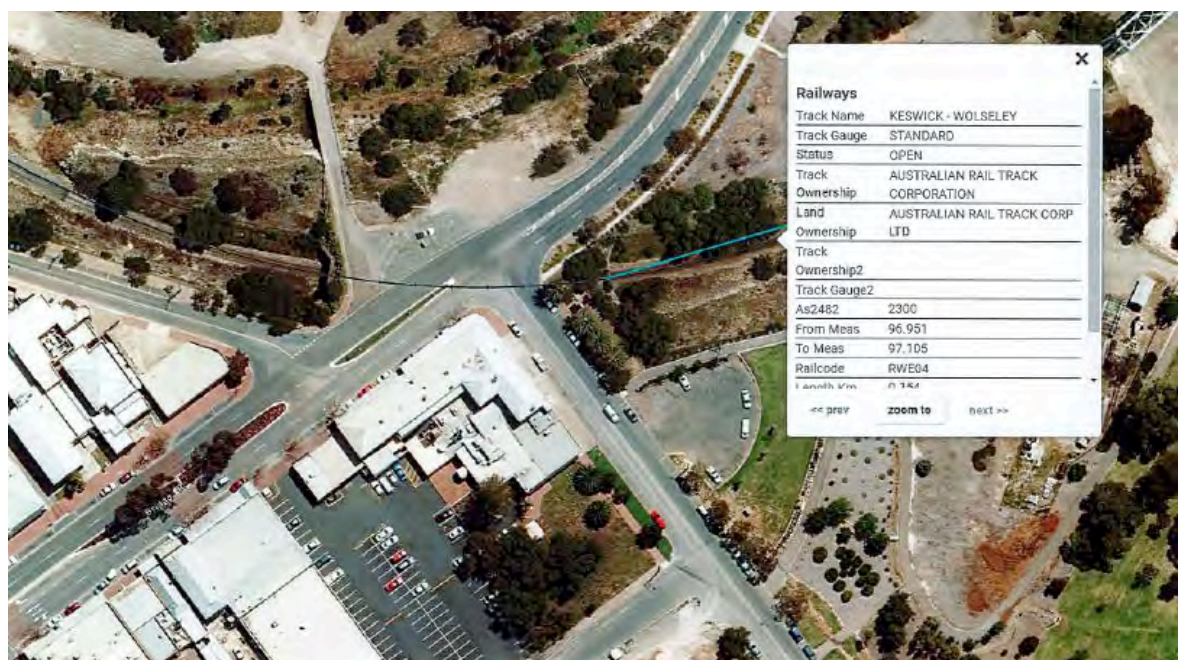


IMAGE 09 - taken from location.sa.gov.au showing the approximate line of the Rail Tunnel.

3.2 Designer's response to feedback from ODASA

Following receipt of feedback from ODASA, the Applicant engaged me to review the design and to liaise with its designer, CED Building Design, to revise the concept to review revisions to the proposed plans lodged with the Development Application in the context of the ODASA Comments. These comments were (loosely):

- Review the mass distribution and scaling, with the intent to reinterpret the demolished Hotel building;
- Further refine the architectural expression, with the intent to provide a more cohesive streetscape response along Bridge Street; and
- Review the external finishes.

Following this liaison, revised plans were prepared and I subsequently attended a meeting with the Associate Government Architect to discuss the changes. In summary, the changes were:

- Changes to the ground floor plan aimed at increasing the active frontages along Bridge Street;
- Preparation of drawings showing the wider streetscape context (to both Bridge Street and East Terrace) to confirm that suitable connections have been established along these roadways;
- Changes to the overall design approach to establish: a 'base' level that addressed the sloping site (particularly along East Terrace); a 'podium' level that addressed the predominantly single storey streetscape along Bridge Street; and an 'upper' level that has been redesigned with a more horizontal presentation and revised materials.



IMAGE 12 – Extract from CED Building Design drawings –Streetscape elevation along Bridge Street

In my view, these changes have contributed positively to the overall design quality of the proposal and particularly the way it relates to the physical and cultural context of the site.

4.0 Conclusion

Having assessed the potential impact that the new works proposed on the Subject Site would have on other Heritage places, and other Development, in the locality, I have formed a view that the proposed works will not have detrimental impact on the heritage values of the other Heritage places, or other Development, in the Locality.

Regarding the changes made to the design of the new works proposed for the site, I believe that they have resulted in a proposal that relates, in a contextual sense, satisfactorily with the adjacent public domain and addressed the concerns raised by ODASA.

5.0 Sign Off

This Report has been prepared for and on behalf of DASH Architects

A handwritten signature in black ink, appearing to read 'David Holland', is positioned above the printed name and title.

David Holland
Architect
Director
DASH Architects

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Bridgeport Hotel
Contextual Analysis Report



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MLM/16-0112

15 February 2017

Mr Jamie Botten
Botten Levinson
GPO Box 1042
Adelaide SA 5001

Dear Jamie

PROPOSED BRIDGEPORT HOTEL DEVELOPMENT ACCESS REVIEW

I am in receipt of a copy of correspondence from the Rural City of Murray Bridge to the Development Assessment Commission (DAC) relating to the above proposal and the minutes of Council's Development Assessment Panel (DAP) in regard to this development. You have asked that I consider Council's comments in relation to the proposed access to/from the subject site. Further to this request, I have reviewed the report to the Development Assessment Panel (Friday 9 December 2016), and the independent report commissioned by Council prepared by GTA Consultants dated 28 November 2016.

The DAP report identified that Council's Engineers had raised concern in respect to the proposed access points on South Terrace. Specifically, the following was reported to the DAP:

"The Council's Engineers have raised concerns regarding the three vehicle access/egress points located on South Terrace (a public road under the ownership, care & control of Council). They have stated that the location of the drive-through bottle shop exit adjacent the main entrance/exit on South Terrace (to the at grade car park) will create conflicting traffic movements, whilst removing a significant number of on-street car parks.

Furthermore this design will adversely impact on the Council's ability to provide a pedestrian link across South Terrace between the Bridgeport Hotel and the Marketplace (entrance). The Council's Engineers provided guidance to the proponent on how to redesign the vehicular access for South Terrace, removing the drive-through exit and redirecting the traffic through the main access/egress point."

The "solution" as suggested by Council's Engineer was previously provided as a suggestion to which I responded (my letter to you on 7 November 2016). Council reported the following in regard to my previous response.

“The applicant provided this information from Council’s engineers to their traffic engineer who provided the following response:

- *The revised layout would create a number of safety and operational issues on the site and, in particular, that it would transfer the existing collision potential which currently occurs on Bridge Street to South Terrace and create a potential crash risk on that road;*
- *That there would also be compliance issues with the parking design, and additional spaces would need to be removed to cater for the delivery vehicle;*
- *While modifications to the Council design could be made to address the non-compliance issues, there would be congestion created in the car park and resolution of that issue would result in the loss of additional spaces;*
- *With respect to the pedestrian crossing, if Council were to install such a facility, it would not compromise the operation of the proposed development but it is to be noted that it may impact the Australia Post loading area opposite and that Council may need to consider this issue prior to confirming installation of the crossing.”*

In relation to the assessment I made of Council’s “solution”, the DAP report included the following statement.

“The Council’s Engineers have stated that the response by the traffic engineer is still not satisfactory as design issues regarding safety and operational issues on the site, (i.e. the applicant’s land) is a matter for the developer / proponent to resolve. These issues should not be transferred to the Council to deal with through the removal of on-street car parking, the disruption to traffic through dealing with three access points or compromising the achievement of the Council’s Town Centre Traffic Management Plan. With regards to on-street car parking, it is pointed out that a lack of on-street car parking is already seen as an issue by other businesses within the vicinity.”

The above statement is misleading in that it implies that there are design issues with the proposed car park. This is not the case. The issues identified were those which would have been created should Council’s option have been adopted. Importantly Council’s option would have been substandard in design when compared with relevant Australian Standards and would have resulted in high risk conflict points between pedestrians and delivery vehicles which would be highly undesirable.

In support of my earlier review, I note that the independent review of GTA Consultants concurred with my earlier assessment. Specifically, the GTA report included the following advice:

- *They “concur with the findings of the applicant with regards to the difficulties in implementing the reverse flow options and problems with designing entry and exit points within the site”.*

Further, the GTA report included the following conclusion in respect to the bottleshop access:

- *"Parking will be impacted by the proposed access point generally, but not by the bottle shop access point, as there appears to be clearance between the existing spaces to the south of the site."*
- *The proposed bottle shop access point will operate satisfactorily on South Terrace given there will be adequate sight distance and clearance to existing parking spaces."*

In respect to access, the GTA report only included one recommendation (not several as documented in the DAP report) in that the bottleshop egress should be in a single lane. I concur with this recommendation and confirm that it is the intent of the design to provide a single traffic lane at the exit. Other design suggestions included in the GTA review related to preferred detailed design outcomes, I raise no issues with these suggestions.

The advice from MFY and GTA relating to safety and impact on parking associated with the proposed bottleshop egress is consistent. The DAP report summary, however, has failed to acknowledge that GTA does not concur with Council's Engineer in respect to these matters. Further, the report has not provided justification for the concerns identified by Council's Engineer. I consider that this has resulted in a misleading report as the assertions are not only inconsistent with relevant documents and guidelines but do not reflect the actual impact on the road.

In order to provide a clear understanding of the impact of the proposed bottleshop access, I have documented a comparison of the existing situation compared with the proposed situation to/from the at-grade parking area.

The existing car park is serviced by a wide crossover on South Terrace of approximately 22 m in length. While this crossover may be a 'single' access, it directly services two parking aisles. It, therefore, acts as a double crossover, as illustrated in Figure 1.

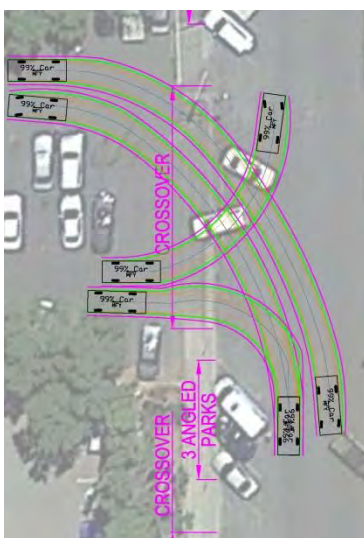


Figure 1: Turning movements at existing crossover

The above situation has the following safety issues:

- drivers enter/exit the site along the full length of the crossover rather than defined entry/exit points for the two aisles;
- drivers enter/exit the crossover on the incorrect side of the road (contrary to the Australian Road Rules); and
- pedestrians have a very long crossing distance, created by the wide crossover.

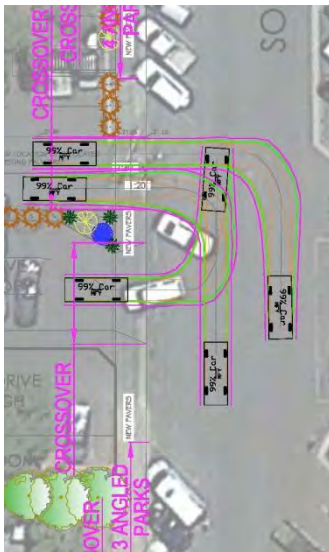
In effect, therefore, the single crossover is currently acting as a double crossover without the benefit of additional delineation for drivers or a safe refuge for pedestrians.

In regard to on-street parking, there are currently 3 spaces along the subject area of South Terrace, as illustrated in Figure 2 (note that there are additional spaces adjacent the eastern end of the site which will not be impacted by the proposed bottleshop).



Figure 2: Existing on-street parking spaces

The proposed crossovers will be 6 m and 8 m in width respectively (i.e. a total of 14 m in crossover length). They will provide for a single entry movement and two egress movements, as illustrated in Figure 3.



The proposed access points will comply with the design requirements of the relevant Australian Standard and will improve safety in that they will define traffic movements entering and exiting the site, restrict the manoeuvring area across the footpath to approximately 90° (as recommended in Australian/New Zealand Standard, *Parking Facilities Part 1: Off-street car parking (AS/NZS 2890.1:2004)*), provide improved delineation for drivers and provide a safe pedestrian refuge area between crossovers,. Importantly, the total crossover length will be reduced and the entry movements will be restricted to a single location along the footpath.

[illegible]

Figure 4: Proposed on-street parking spaces



The only impact on on-street parking would occur if Council was to install a pedestrian crossing on South Terrace. This is a matter for Council and would not impact the access for the proposed car park, but would impact existing on-street parking, particularly on the southern side of the street where prohibition zones have been nominated for Australia Post Facility.

In summary, therefore, I believe it is important for the Development Assessment Commission to understand that the information documented in Council's report, in relation to the proposed access is spurious in that:

- the proposal will not result in compliance or safety issues within the car park (as confirmed in the GTA review);
- the proposed egress from the bottleshop would not create safety issues;
- there would, in fact, be a reduction in crossover lengths and improved safety as a result of the formalisation of defined access points;
- there will be no loss of on-street parking as a result of the proposed bottle egress; and
- there would only be a loss of parking on-street if Council were to construct a pedestrian crossing on South Terrace. The proposed development would not inhibit the construction of such a device.

It is particularly relevant to note that GTA Consultants concur with the MFY design assessment. The single issue raised by GTA in relation to provision of a single egress lane at the bottleshop exit is consistent with the intent of the proposal.

Of particular importance to note is that the access solution for the site was developed with a holistic view of improving safety for the site and the adjacent road network. This was endorsed in DPTI's referral response to DAC dated 7 October 2016. Introduction of substandard design parameters and conflict points to create a single crossover which would decrease safety for road users generally is not the preferred outcome for any development

Yours sincerely,
MFY PTY LTD

A handwritten signature in black ink, appearing to read 'Melissa Mellen'.

MELISSA MELLEN
Director



2010 NATIONAL WINNER
2010 TELSTRA SOUTH AUSTRALIAN
BUSINESS WOMAN OF THE YEAR



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MFY Pty Ltd

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MLM/16-0112

15 August 2017

Mr Jamie Botten
Botten Levinson
GPO Box 1042
Adelaide SA 5001

Dear Jamie

BRIDGEPORT HOTEL, 2 BRIDGE STREET, MURRAY BRIDGE

I am in receipt of plans by CED Building Design dated August 2017 which relate to a proposal to redevelop the Bridgeport Hotel at Murray Bridge. MFY has previously prepared a traffic and parking report (July 2016) relating to a proposal for the redevelopment of the hotel. The current plans (Project No. C1201, sheets 1.03/DA 2 and 1.04/DA 1) illustrate the parking levels of the most recent revision of the subject proposal.

You have requested that I review the most recent set of plans for the proposal with a view to updating my earlier traffic and parking advice to reflect the current proposal. In doing this, I have also considered the advice provided by GTA Consultants to Council in relation to the earlier proposal.

I note that the proposal incorporates some variation in the size of areas with the proposed hotel and that there will be an increase provision of parking on the site (from 151 to 160 spaces).

1 DESIGN

The current plans have only included limited change in relation to the traffic operation of the site, including:

- conversion of the (previously) proposed drop-off area in the car park to parking spaces. This will have no impact on the functionality of the car park and will increase the parking space provision on-site;
- modification of the taper within the drive-through facility. GTA Consultants queried whether the intent was to promote a dual exit arrangement from the car park. While the initial design always intended that the drive-through narrow to a single lane, the current plans provide clarity that this will be the case; and

- greater separation between the car park access and the drive-through egress, as recommended in the GTA report.

The car park also includes the addition of blades adjacent the Bridge Street frontage. I have, therefore, reviewed the sightlines at the access and the blades will not impact on sightlines for exiting drivers, as illustrated in Figure 1.

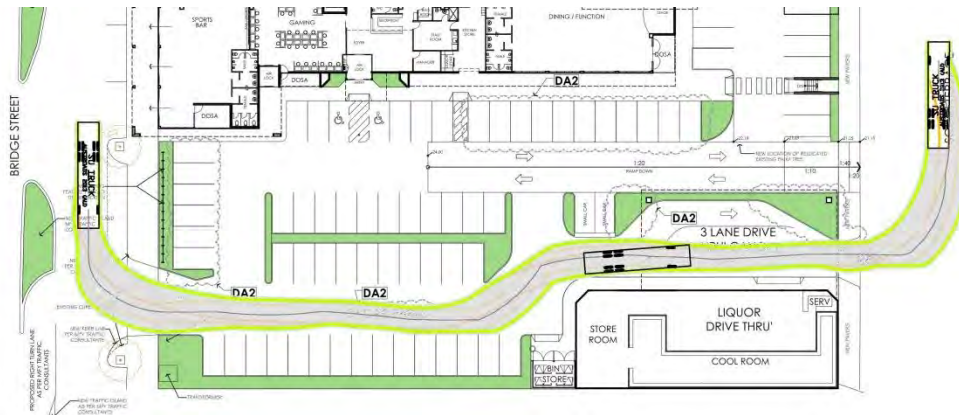


Figure 1: Sightlines for exiting drivers at Bridge Street access

In regard to car park design, access, delivery and refuse vehicle requirements and bicycle provision, the proposal is consistent with the earlier design, as detailed in the July 2016 report.

2 PARKING ASSESSMENT

The 2016 parking assessment included an assessment of a number of anticipated usage scenarios which could occur at the proposed hotel, given that each use will not peak simultaneously. It concluded that there will be adequate parking on the site to cater for the forecast peak demand during the day but that some parking on-street will occur during some evening periods.

The GTA assessment concurred with the view that there would be a requirement for some on-street parking during the peak evening periods. It recommended that an assessment of the requirement for existing on-street parking during peak hotel demand periods be undertaken. However, given the initial assessment undertaken by MFY identified that there is ample on-street parking within the Murray Bridge proximity to the subject site during evening periods. It is likely that some of the existing on-street parking will relate to hotel patrons, given that the majority of businesses are closed during the evening. This existing demand will only off-set the future demand assessment, resulting in more availability and hence, any identification of existing hotel parking on-street will improve the parking associated with the proposal.

Accordingly, I have completed an updated assessment of the forecast parking demand for the current proposal based on the alternative peak scenarios as identified in Table 2 of the July 2016 report, which is documented as Table 1 in this report.



Table 1: Modified Table 2 from MFY July 2016 Report.
Peak Parking Demand Assessment for Alternative Peak Scenarios

Use	Quantity	Rate	Period 1		Period 2		Period 3	
			Per cent occupancy	Peak evening dining period	Per cent occupancy	Peak bar occupancy period	Per cent occupancy	Peak lunch period
Hotel Room	100	1 per 3 rooms	80%	27	80%	27	50%	17
Gaming Lounge	40 machines	1 per 2 gaming machines	15%	3	80%	16	85%	17
Sports Lounge	177 m ²	1 per 6 m ²	65%	19	85%	25	15%	4
Lounge Bar	326 m ²	1 per 6 m ²	65%	35	85%	46	15%	8
Dining area	300 seats	1 per 3 seats	90%	90	65%	65	50%	50
Outdoor dining	150 seats	1 per 5 seats	90%	27	65%	20	50%	15
Function*	200 persons	1 per 3 seats	50%	33	10%	7	25%	17
Total				234		206		128

Table 2 provides a comparison of the earlier assessment and the current proposal.

Table 2: Comparison from previous assessment

Period	Previous Assessment (No Spaces)	Current Proposal (No Spaces)
1 (peak evening dining)	223	234
2 (peak evening bar)	191	206
3 (peak lunch)	125	128

It can be seen from the above assessment that the current proposal is forecast to generate a marginally greater peak parking demand when compared with the earlier forecast.

However, the proposal also includes nine additional spaces on the subject site. Accordingly, there will only be a small increase in on-street parking at night when compared with the previous assessment. As such, it is anticipated that the proposed car park will adequately cater for the forecast demand during the day and that there will be adequate on-street parking to accommodate any additional parking required during peak evening periods.



3 ACCESS AND TRAFFIC

The varied proposal will not alter the intended access arrangements or traffic distribution associated with the proposal. These arrangements are consistent with the earlier plans which were endorsed by the DPTI and provide appropriate and safe circulation for the site. All vehicles, including the delivery vehicles, will enter and exit the site in a forward direction and appropriate simultaneous movements and sightlines will be maintained, as requested by DPTI. Importantly, a number of safety improvements proposed on the road network will be maintained.

4 SUMMARY

In summary, I have reviewed the current plans for the Bridgeport Hotel redevelopment at Murray Bridge as they relate to traffic and parking matters. I am of the view that they are generally consistent with the previous traffic and parking assessment, as documented in the MFY report dated July 2016.

Minor variations in areas will not substantially impact the parking assessment, as confirmed in this report. Importantly, the criteria relating to adequate parking on-site and on the adjacent street network within close proximity of the site is still valid.

Yours sincerely,

MFY PTY LTD

A handwritten signature in black ink, appearing to read 'Melissa Mellen'.

MELISSA MELLEN

Director





Kerin Bay P/L
2015-0190
15 September 2017



BRIDGEPORT HOTEL REDEVELOPMENT

Demolition and Redevelopment of the Bridgeport Hotel

Development Application 415/E014/16
Revised Planning Report

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Bridgeport Hotel Redevelopment

15 September 2017

Lead consultant	URPS
Prepared for	Kerin Bay Pty Ltd
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1.0 Introduction

1.1 Background

URPS has been engaged to assess the proposed development against the relevant provisions of the Murray Bridge Council Development Plan (consolidated 11 August 2016).

This assessment is an updated version of the Planning Report that addresses the changes made to the proposed development following the Design Review Process and addresses the additional reporting/investigations undertaken in support of the proposal.

Kerin Bay, the applicant in this matter, owns the land that comprises the Bridgeport Hotel (the Hotel) and its associated car parking and outdoor area. Kerin Bay proposes the total demolition of the Hotel and the development of a new 6 storey hotel facility, complete with accommodation rooms, function area, associated guest facilities and car parking together with freestanding bottle shop.

The following has been prepared and taken into account as part of the preparation of this report:

- the initial proposal plans prepared by CED Building Design dated 27 June 2016 and the amended proposal plans prepared by CED Building Design (refer attached document transmittal for drawing revisions and dates)
- the *Traffic and Parking Report* prepared by MFY dated August 2016 and the additional advice prepared by MFY dated 15 August 2017
- the *Traffic and Parking Review* prepared by GTA Consultants on behalf of the Rural City of Murray Bridge dated 28 November 2016
- the *Bridgeport Hotel, Murray Bridge Review of Local Heritage Listing* prepared by DASH Architects dated 29 February 2016 and the *Further Review of Heritage Value of the Bridgeport Hotel* prepared by DASH Architects dated 15 August 2017
- the *Heritage Advice* prepared by Flightpath on behalf of the Rural City of Murray Bridge dated October 2016
- the *Referral Response – Development Application 415/E014/16 (Redevelopment of Bridgeport Hotel)* prepared by the Rural City of Murray Bridge dated 16 December 2016
- the *Bridgeport Hotel Redevelopment – Contextual Analysis Report* prepared by DASH Architects dated 28 August 2017
- the TMK advice in relation to the potential impact on the nearby railway tunnel dated 22 August 2017
- the *Landscape Plan* prepared by Oxigen Pty Ltd dated 29 June 2017
- the lighting modelling prepared by TMK Consulting Engineers dated 21 June 2017
- the *Waste Management Plan* prepared by CED Building Design dated 5 July 2017



- review of updated *External Colour and Finishes Schedule* prepared by CED dated 14 August 2017
- an inspection of the site and locality
- the relevant provisions of the Murray Bridge Council Development Plan (consolidated 11 August 2016), and
- the relevant sections and regulations of the *Development Act 1993* (the Act) and the *Development Regulations 2008* (the Regulations).

A consolidated set of the revised and updated documentation is submitted in parallel with this revised Planning Report by Botten Levinson Lawyers.

1.2 Grounds for Approval

The proposed development is appropriate for this site and locality and it complies with the provisions of the Development Plan to warrant Development Plan Consent for the following reasons.

DASH Architects has undertaken a detailed assessment of the existing building and have concluded that it has been “significantly compromised” as a result of the number of changes to the building. Therefore, it is appropriate to place considerably less weight on the Development Plan provisions that anticipate retention of Local Heritage Places, and demolition of this building is appropriate.

The proposed new development responds to its location within the Regional Town Centre. It preserves the historic hotel use of the site at a new scale that will make a significant economic contribution to the town and region. It will play a significant role in the day-to-day entertainment and social activities of the community, as well as making an important contribution to the Regional Town Centre through its expanded tourist accommodation and function centre components.

The six storey height of the proposed building is appropriate because:

- it will create a new focal point within the town while supporting the role/presence of existing smaller scale buildings elsewhere in Bridge Street given that the land is currently substantially separated at the north-eastern end away from the more traditional main street
- the building incorporates a podium with veranda to maintain a comfortable human scale at street level
- angled columns along the Bridge Street frontage complement the scale of the existing built form in Bridge Street and the proposed podium/veranda, creating an improved streetscape connection
- it has a street width to building height of less than 1:1 which maintains an appropriate scale relationship with Bridge Street
- it will not enclose the public realm or have any unreasonable overshadowing impacts on any sensitive land uses, and
- it is sufficiently separated from other Heritage Places, in particular the State Heritage listed Round Houses, the road and rail bridges and the Murray Bridge Transport Precinct (railway precinct).

The proposed development is of a high quality design, addressing both Bridge Street and East Terrace as shared main frontages, while at the same time providing an appropriate interface with South Terrace.



The main building is appropriately articulated with balconies, first floor podium and other design features such as verandahs that are characteristic of the Town Centre. A range of high quality materials and finishes, such as steel/aluminium, variation in the colour and texture of pre-cast concrete walls, extensive glazed facades and feature umbrellas/shade structures are also proposed.

The development also includes improved vehicular access to the site and sufficient car parking to cater for the day-to-day operations of the Hotel and, with the use of immediately adjoining and underutilised on-street car parking, can accommodate parking demand during the peak patronage at the site.



2.0 Subject Land and Locality

2.1 Subject Land

The subject land comprises 6 regular shaped allotments, together forming a total site area in the order of 6,070m².

The 6 allotments are more particularly identified as 94-99 in Town Plan 170702, Hundred of Mobilong, Certificate of Title Volume 6124 Folio 82 (as already provided to Council)

The land has a frontage to Bridge Street, East Terrace and South Terrace. The existing Hotel is constructed in the northern corner of the site, directly adjacent Bridge Street and East Terrace. A drive-through bottle shop is attached to the Hotel on the Bridge Street frontage and there is also a two-way crossover for access to the site on this frontage. The Hotel occupies approximately half of the East Terrace frontage with an outdoor area associated with the Hotel at the rear of the site, sharing a frontage with South Terrace.

The Hotel building is a two storey, stone faced building with a presence to both Bridge Street and East Terrace. The building has been substantially altered from its original Victorian design. At ground level, the original structure has almost completely been replaced, including brickwork in the place of the stone and new doors and windows along each façade. Most recently, the street facing corner frontage at ground level was completely reworked to provide a deep recessed front entry. A more detailed description of the original building and its later works is described in the DASH Architects Review of Local Heritage Listing report.

While the land is generally flat at the Bridge Street frontage, the land slopes quite substantially to the eastern corner of the site (at the intersection of East and South Terraces). At this corner, the land is approximately 6 metres below the finished floor level of the existing Hotel.

2.2 Locality

The subject site sits on the periphery of the “built” town centre where the site also adjoins a fragmented ring of park lands and railway lands leading down to the river. It is the first “town centre” building from the approach over the historic bridge.

To the west and south, land is used for a range of municipal, retail and other commercial activities including the Murray Bridge Magistrates Court on the opposite side of Bridge Street, as well as Centrelink and other retail shops directly to the west. The Murray Bridge “Marketplace” building is located to the south which accommodates a number retail shops, including a supermarket and discount department store, as well as the Murray Bridge Council’s library.

To the east of the site, on the opposite side East Terrace is the Wharf Hill Reserve providing a car parking area, gardens and pedestrian access to the riverfront.

The site and locality are illustrated on the following pages.



Figure 2-1 The Hotel as viewed from the north-east of the site



Figure 2-2 View of the site from the north-east looking along Bridge Street





Figure 2-3 View of the Hotel as viewed from the car park on the eastern side of East Terrace



Figure 2-4 View of the Hotel from within the car park on the subject land





Figure 2-5 View south showing the edge of the subject land (on the right side of the image) and the Murray Bridge Green and Flour Mill and silos



Figure 2-6 View of the town hall and former police station to the west of the site along Bridge Street





Figure 2-7 Development directly to the west of the subject site along Bridge Street



Figure 2-8 View of the rear of the subject land along South Terrace looking East to the River





3.0 Proposed Development

3.1 Proposal

The proposed development involves the demolition of the existing Bridgeport Hotel (including all structures on the land) and the construction of a 6 storey hotel building and an associated, but detached, drive-through bottle shop. The proposed hotel building works comprise:

- a 6 storey hotel building with undercroft level car parking, consisting of:
 - > at the undercroft – 71 car parking spaces and direct vehicle access to South Terrace
 - > at ground level – a TAB/sports bar, lounge bar, two dining/function areas, outdoor children’s play area, alfresco bar and seating areas, gaming area, foyer, kitchen and other associated amenities and storage areas
 - > at the first floor – a function room, bar, kitchen, alfresco terrace (extending out over the road reserve), 12 accommodation rooms, gymnasium, outdoor pool and bar for hotel guests, and
 - > at the second to fifth floors – 22 accommodation rooms on each level.
- 89 at grade car parks, the retention of one of the site’s existing access points to the site on Bridge Street (modified) and a two-way access at the rear of the site to South Terrace, and
- a new retail liquor building at the rear of the site facing South Terrace comprising a store room, retail floor space, cool room and 3 lane vehicle drive-through with exit onto South Terrace.

The Hotel is to be constructed of pre-fabricated concrete with a painted finish in white, bronze and dark grey colours. All other detailing and finishes will be a high quality to complement the main façade. The bottle shop will be constructed of similar materials in a sandstone colour with a metal awning extending over the 3 drive-through bays.

The proposed development does not involve any change in use of the land. The proposed development constitutes a “hotel” as defined by *the Development Regulations 2008* and it is proposed that it will continue to operate under its current hotel licence under the *Liquor Licensing Act 1985* (subject to appropriate amendments to that licence).

3.2 Staging

The application is to be staged in order to commence site works prior to the construction of the buildings. The staging will be as follows:

Stage 1 – Civil works

Stage 2 – Demolition of the hotel

Stage 3 – Bottle shop

Stage 4 – Hotel Building



4.0 Procedural Considerations

4.1 Development Plan

The subject land is located within the Bridge Street Policy Area 11 of the Regional Town Centre Zone of the Murray Bridge Council Development Plan (consolidated 11 August 2016).

A portion of the Bridgeport Hotel is listed as a Local Heritage Place within Table MuBr/4 of the Development Plan.

Bridge Street is identified as a secondary arterial road on Overlay Map MuBr/21 – Transport.

4.2 Assessment Pathway

The proposed development is not identified as either a complying or non-complying kind of development within the Regional Town Centre Zone. The proposal is therefore a merit development.

4.3 Public Notification

The proposed development is not listed as a Category 1 or 2 development within the Regional Town Centre Zone Development Plan provisions. Instead, the Zone provisions defer to Schedule 9 of the Regulations for the prescription of public notification categories.

The Regulations prescribe the activity as Category 1 kind of development as it involves:

Part 1 - Category 1 development

6 (1) Any development which consists of any of the following, other than where the site of the development is adjacent land to land in a zone under the relevant Development Plan which is different to the zone that applies to the site of the development or where the development is classified as non-complying under the relevant Development Plan:

(h) any kind of development within a Local Shopping, District Shopping, Specialty Goods Centre, Local Centre, Town Centre, City Centre, Neighbourhood Centre, District Centre, Regional Centre, Regional Town Centre, District Business, Local Town Centre or District Town Centre zone as delineated in the relevant Development Plan; or

The site of the proposed development is not adjacent land to land within a different zone to the Regional Town Centre Zone. Additionally, the development is not listed as a non-complying kind of development.

Therefore, the proposed development involving the demolition of the existing building and the construction of the new hotel building is a Category 1 matter for the purpose of public notification.

4.4 Amended Plans

As indicated previously, the proposal has been through review and amendment via the Design Review Process. This process has focused on achieving design excellence in the detail of the proposed development and has not changed the essential nature of the proposal.



5.0 Development Assessment

Having considered the relevant provisions of the Development Plan, the matters central to the assessment of this application can be addressed under the following headings:

- demolition of the existing hotel
- land use and the Regional Town Centre
- character and nature of the development
- building height
- building appearance
- traffic access and trip generation
- vehicle parking, and
- additional and ancillary matters.

5.1 Demolition of the Existing Hotel

The proposed development involves the demolition of a Local Heritage Place. The Development Plan's Objectives with respect to Heritage Places generally impart support for the retention and ongoing use as follows:

General Section – Heritage Places

Objective 1 The conservation of State and local heritage places.

Objective 2 The continued use, or adaptive reuse of State and local heritage places that supports the conservation of their cultural significance.

The following provisions identify circumstances where heritage listed buildings may be demolished:

Principle 1 A heritage place spatially located on Overlay Maps - Heritage and more specifically identified in Table MuBr/5 - State Heritage Places, Table MuBr/4 - Local Heritage Places, should not be demolished, destroyed or removed, in total or in part, unless either of the following apply:

- (a) that portion of the place to be demolished, destroyed or removed is excluded from the extent of the places identified in the Table(s)
- (b) the structural condition of the place represents an unacceptable risk to public or private safety.

Additionally, Heritage Places Principle 2 states that:

Principle 2 Development of a State or local heritage place should retain those elements contributing to its heritage value, which may include (but not be limited to):

- (a) principal elevations
- (b) important vistas and views to and from the place
- (c) setting and setbacks
- (d) building materials
- (e) outbuildings and walls
- (f) trees and other landscaping elements



- (g) access conditions (driveway form/width/material)
- (h) architectural treatments
- (i) the use of the place.

DASH Architects was engaged to consider the heritage significance of the existing building/subject site. As outlined within the investigations and report by DASH Architects (*Bridgeport Hotel, Murray Bridge, Review of Local Heritage Listing*), the existing building has been “significantly compromised” through the loss of ground floor facades and spaces.

Regard has also been given to the further considerations by DASH in relation to the proposed demolition of the proposed Local Heritage Place.

In considering parts (a) and (b) of Principle 1 above, the proposed demolition of the Local Heritage Place is not limited to a portion that is excluded from the listing, nor is the building of a structural condition that could result in an unacceptable risk to public or private safety.

In the context of Principle 2, which states that development of a “heritage place should retain those elements contributing to its heritage value”, an assessment must be made as to heritage value of the Hotel in order to determine the appropriateness of any works to the building. Therefore, the specialist opinion of DASH Architects has been considered regarding the Hotel’s heritage value.

DASH Architects considered whether the existing hotel meets the criteria of a Local Heritage Place. The Data Sheet for the hotel lists its criteria as applying under parts (a), (c), (d) and (f). DASH Architects has concluded, in respect to these criteria that:

- Under part (a), further consideration has been given to the nature of the term “displays” and noted that “due to the many and varied changes, the Bridgeport Hotel has reached [the] point” that it has lost so much of its “original” fabric.

The external alterations and additions, particularly at the ground floor façade and also the upper floor balconies and verandas, mean that the building no longer “displays” the historical and economic themes that are of importance to Murray Bridge.
- Under part (c), there is no demonstration within the listing that the hotel has a “special attachment to the particular circumstances of local development and a sense of place”, and that the significant alterations have substantially reduced the hotel’s integrity to the point where its capacity to reflect this role has been substantially eroded.
- Under part (d), “the Victorian style and materials used in the construction (face stonework, brick quoins, timber windows and doors, lacework to balconies, corrugated iron roofing and verandahs) of the Bridgeport Hotel are not unique or significant to the local area and indeed are seen throughout South Australia in buildings of this kind, from this period”. Comparisons made against other hotels were only to indicate that there are other similar buildings of a similar era, style and material finish elsewhere in South Australia.

On this basis, “the aesthetic merit and design characteristics have been significantly compromised by the modifications and additions made to the buildings”, and therefore, the building does not meet the requirements for listing under part (c).



- Under part (f), the hotel building's prominence is more reflective of its siting and topography rather than any inherent characteristic of the building itself.

DASH Architects concludes that, for a number of reasons, including the number of significant changes to the building, it does not meet the criteria to be listed as a Local Heritage Place. Further, it is noted that DASH conclude in its 15 August Report (review of Dmeolition), that for all of the reasons outlined on page 21 of that report, that because of the compromised integrity of the place, and the location and nature of the fabric that is left an adaptive reuse of the building is unlikely to be viable and is not warranted in this instance.

5.2 Land Use and the Regional Town Centre Zone

The existing hotel has operated for in excess of 130 years on the land. Over time, the form of the building has changed substantially, but the use of the land has remained the same. It has provided, and will continue to provide, a range of licensed hotel services including a bar (or bars), dining areas and accommodation.

This use is consistent with both the Objectives and Principles of the Regional Town Centre Zone and Bridge Street Policy Area as follows:

Regional Town Centre Zone

Objective 1 A centre representing the primary focus for business and commercial services for the region providing a full range of shopping, administrative, cultural, community, entertainment, education, religious and recreational facilities, and public and private office development.

Principle 1 The following forms of development are envisaged in the zone: ...

- entertainment facility...
- hotel...
- motel...

Bridge Street Policy Area 11

Objective 1 The reinforcement of the role of Bridge Street as the traditional 'main street' of Murray Bridge.

Principle 1 The following forms of development are envisaged in the policy area:

- entertainment facility..
- restaurant
- tourist facilities

(underlining added)

The Development Plan recognises the Murray Bridge centre as an important focal point within the broader Murraylands region and within the town itself. This is highlighted within the Regional Town Centre Zone's Objectives and Desired Character Statement, and also Objective 7 of the Orderly and Sustainable Development:



Regional Town Centre Zone

Objective 1 A centre representing the primary focus for business and commercial services for the region providing a full range of shopping, administrative, cultural, community, entertainment, education, religious and recreational facilities, and public and private office development.

Desired Character Statement

This zone comprises the regional town centre of Murray Bridge. The primary and traditional focus of the town centre for specialty retail is Bridge Street, however the zone includes other retail, business and recreational policy areas along Mannum and Swanport Roads and the encompassing parklands. The town centre is located on a ridge overlooking the River Murray, and the two bridges crossing the river...

The policy areas within the town centre are aimed at reinforcing the town centre as the major commercial centre in the region and as a place for local residents and visitors to shop, work, meet, entertain and relax in an attractive, comfortable and safe environment...

The Murray Bridge town centre reflects the growth of the town as a regional centre over the past 150 years. The current land use patterns reflect this evolution of the past, and show a range of building forms from historic to modern buildings. Development within the town centre is intended to generate a greater cohesion of these features and to create a more attractive and better functioning centre as it continues to grow. Development will therefore have regard to policy contained within the individual policy areas.

Development will promote a vibrant, economically viable and attractive centre, which will best suit the needs of the community, increase the centre's historic role as a social meeting place, and enhance the image of the town and the level of comfort and convenience for residents and visitors.

General Section – Orderly and Sustainable Development

Objective 7 Development of the town of Murray Bridge as the main regional service and community centre within the district and Murray Mallee generally.

(underlining added)

The site is within the core retail precinct and is therefore a suitable location for a development of this scale and economic contribution to the town. It will play a significant role in the day-to-day entertainment and social activities of the community, as well as making a significant contribution through its expanded tourist accommodation and function centre components.

5.3 Character/Nature of Development

The Regional Town Centre Zone and Bridge Street Policy Area provisions establish that the retail core of the town centre is the primary focus for development of this nature.

The Policy Area's Desired Character statement recognises the diversity in built form ranging from substantial historic buildings to more modern styles. Bridge Street has been developed as a traditional "main street" predominantly comprising one and two storey buildings. Where the contours of the land permits, there are buildings that have a greater height such as Mobilong House, a four storey commercial building on the corner of Seventh Street and South Terrace within the same Policy Area.

Development should reinforce the "main street" role by:



Bridge Street Policy Area

Desired Character Statement

....

- (a) ensuring the economic viability and vitality of specialised retailing, entertainment, cultural and tourist services
- (b) encouraging new development to create a cohesive lineal streetscape appearance and where the scale of new development is compatible with the existing built-form
- (c) encouraging development to make greater use of the street for pedestrian and social activities, creating an atmosphere where people want to spend time
- (d) establishing a high visual amenity and strong local character to create a sense of place and focal point within the town

(underlining added)

The proposed development is consistent with these criteria as follows:

- in respect of part (a), through economic investment and increased utilisation of the site that will contribute strongly to the town's entertainment, cultural and tourist services, while substantially adding to the town's current central business district retail activities
- in respect of part (b), by reinforcing the prominence of the site and its geographic location, while respecting the more cohesive streetscape further south-west along Bridge Street
- in respect of part (c), by reinvigorating the street with the addition of formal outdoor dining areas (within the site) that promotes activity and interest for pedestrians, and
- in respect of part (d), establishing a building of high visual amenity featuring a range of design features at each level (such as the outdoor dining at ground and first floor levels, podium setback at the first floor and other outdoor areas above ground level), and forms a new focal point at the north-eastern end of the street.

For similar reasons, it is also considered that the proposal makes a strong, positive contribution to the street as anticipated by Regional Town Centre Zone's Desired Character Statement and Principle 11, which state:

Regional Town Centre Zone

Desired Character Statement

The policy areas within the town centre are aimed at reinforcing the town centre as the major commercial centre in the region and as a place for local residents and visitors to shop, work, meet, entertain and relax in an attractive, comfortable and safe environment.

It is desirable that the retail core of the town centre develops in a compact yet orderly manner...

Principle 11 Development should establish a cohesive townscape character by:

- (a) defining the major streets as important linear public spaces by:
 - (i) ensuring that buildings maintain or establish a relatively continuous edge of built-form abutting or situated close to those major street frontages
 - (ii) emphasizing the townscape importance of development at the intersections of major streets, with corner buildings of a scale and form appropriate to their location and situated close to or abutting both street frontages
 - (iii) ensuring that buildings fronting on to such streets are of a shape, scale and orientation which relate to and reinforce the rectilinear grid pattern of the town centre



- (b) ensuring that the design, external materials, colours and finishes of buildings have regard to their townscape context as appropriate to the desired future character of the relevant policy area
- (c) avoiding the use of large expanses of highly reflective materials
- (d) ensuring the height of any building is compatible with the desired streetscape character.

(underlining added)

With reference to Principle 11 above, the proposed building is appropriately sited on the land, establishing a built form situated either abutting or close to the major street frontages. It is also oriented on the land to maximise river views while maintaining an appropriate siting in the context of town centre's rectilinear grid pattern.

Further discussion on the nature of the building height and appearance are addressed in the following sections.

5.4 Building Height

Policy Area Principle 13 states:

Principle 13 The spatial character of Bridge Street, established by the width of the street relative to its generally one-to-two-storey building height, should predominate.

The proposed development has a building height of 6 levels, or 23.3 metres above the natural ground level at the Bridge Street frontage. This building height is taller than that generally anticipated within the Bridge Street Policy Area.

Policy Area Principle 15, which is also advisory in nature, states that buildings within Bridge Street should have a maximum height of 8 metres:

Principle 15 Buildings within Bridge Street should have a maximum height of 8 metres.

Despite the departure from the above Principles, in all other relevant surrounding circumstances, it is considered that the height of the new building is acceptable.

A maximum building height of 8 metres and one-to-two storeys is unnecessarily limiting in this case. It may be that these height guideline were adopted because of the height of the existing Local Heritage listed hotel. If the existing hotel is demolished (as proposed within this application), the relevant assessment question to be answered is whether the proposed height, having regard to all relevant Development Plan provisions and surrounding circumstances, is sufficiently compatible with the existing development in the locality.

The subject land sits somewhat isolated at the north-eastern end of Bridge Street. The existing hotel and particularly its two storey elements are separated from the nearest buildings on Bridge Street by a driveway and car park. This provides a distinct separation between the existing/proposed hotel and the remaining, more traditional, main street.

Likewise, the existing and proposed building is substantially separated from existing built form on the opposite side of Bridge Street. In fact, there is an underground railway tunnel and railway line directly opposite the subject site on the northern side of Bridge Street, beyond which the land slopes down and away towards the River to the north and east.



The proposed building incorporates a revised podium and a better articulated 'base level' and 'tower' with veranda elements along the Bridge Street frontage to create a human scale at street level that also reflects the scale of existing development elsewhere in Bridge Street. The proposed building then rises to a maximum height of 23.3 metres set further back from Bridge Street. Given that Bridge Street is approximately 30 metres wide, this creates a street width to building height ratio of 1:0.8. It is considered that this is an appropriate scale relationship, ensuring that Bridge Street is not unduly enclosed by the proposed building.

In an effort to better connect the proposed hotel to the existing built form further to the south-west along Bridge Street, the proposed development includes a series of angled columns along the Bridge Street frontage which are of a complementary scale to the existing and proposed built form in the street.

The separation of the proposed 6 storey building from other existing lower scale development in Bridge Street, combined with the street width to building height ratio of less than 1:1, and the angled columns that create a connection in scale between the proposed podium and existing buildings, means that the proposed building height is sufficiently compatible with Bridge Street and its existing built form.

The proposed building's location on the southern side of the street will not result in any overshadowing impacts on the public realm in Bridge Street, ensuring appropriate pedestrian comfort at ground level. It is also important to note that the location of the subject site and the nature of surrounding development means that amenity impacts such as overlooking, sense of enclosure and restriction of views are not relevant in this instance despite the height of the proposed development being taller one-to-two storeys.

Consideration has also been given to the height of the proposed building in relation to the nearest State Heritage Places. These Places include the "Round House", the rail and road bridges and the Murray Bridge Transport Precinct (railway precinct). Each of these places are located 100-200 metres from the subject land. The nature of these Heritage Places (including their scale and use) and their separation from the site, including by way of localised topography, ensures that their individual settings and heritage values will not be detrimentally impacted by the height of the proposed building.

The proposed building height also responds positively to the Policy Area's Desired Character statement by reinforcing Bridge Street's role as the traditional main street, establishing strong local character with an identifiable sense of place and more of a focal point within the township:

Bridge Street Policy Area

Desired Character Statement

The policy area will be developed as the principal retail area for specialised goods in the Council area. The role of Bridge Street as the traditional 'main street' of Murray Bridge will be reinforced by:...

- (d) establishing a high visual amenity and strong local character to create a sense of place and focal point within the town**

In short, while the height of the proposed building is taller than 8 metres and one-to-two storeys, it is acceptable for the following reasons:



- The subject land is separated from existing development at the north-eastern end of Bridge Street, providing greater flexibility for taller development without compromising the spatial character at the street as a whole.
- The podium with veranda elements along the Bridge Street frontage creates a human scale at street level that also reflects the scale of existing development elsewhere in Bridge Street.
- The angled columns along the Bridge Street frontage are of a complementary scale to the existing built form in Bridge Street and the proposed podium/veranda, creating an improved streetscape connection.
- The overall height of the proposed building creates a comfortable street width to building height ratio of less than 1:1.
- The proposed building's location on the southern side of the street will not result in any overshadowing of the public realm in Bridge Street.
- Amenity impacts such as overlooking, sense of enclosure and restriction of views are not relevant in this instance given the nature of surrounding development.
- There will be no negative impact on the heritage values of any existing State Heritage Places.
- The additional building height will reinforce Bridge Street's role at the traditional main street, establishing strong local character with an identifiable sense of place and making it more of a focal point within the town.

5.5 Building Appearance

The proposed building has a key façade to East Terrace – in much the same way as the original hotel building. This orientation takes advantage of views to and from the River Murray which is a major drawcard of the town and a feature of the broader locality.

The height of the building is considered appropriate and it is contended that the design of the building is suitable for the site's location within the Policy Area and the Regional Town Centre. As outlined within the preceding section of this report, the site is separated from the prevailing main street built form and this provides an opportunity to create a landmark building form that departs from the traditional built form within the locality.

In particular, the proposed development makes positive contributions to each street frontage through activation at ground level and predominantly glazed facades is consistent with Policy Area Principle 11:

Principle 11 The frontages to Bridge Street at street level should be composed predominantly of display windows, avoiding bland surfaces and solid infills.

Policy Area Principle 11 contrasts with Policy Area Principle 9 which states:

Principle 9 Buildings should exhibit a high proportion of solid-to-void construction in the composition of facades and veranda design.

This Principle anticipates a high proportion of solid to void – in other words, more walls than windows – as opposed to Principle 11 which anticipates predominantly display windows. It is contended that



Principle 11 carries more weight and is more appropriate as a design cue than Principle 9 in this circumstance because there are traditionally more retail land uses with extensive glazed facades along Bridge Street.

The proposed building is appropriately articulated with balconies, podium and other design features such as verandas and staggered facades as sought by the Bridge Street Policy Area's Desired Character Statement. Design techniques have been introduced to provide additional visual interest. This includes the use of a range of materials and finishes (such as steel/aluminium, variation in the colour and texture of pre-cast concrete walls, extensive glazed facades and feature umbrellas/shade structures). The proposed building is an attractive building which will improve the existing environment and streetscape.

5.6 Traffic Access and Trip Generation

The proposed development retains one of the two existing vehicle crossovers to Bridge Street. This access point accommodates all turning movement to and from the site except for a right-turn exit.

There are three access points at the rear of the site along South Terrace, including one for the bottle shop (egress only), a two way crossover from the main car park and a two-way crossover for the undercroft car park.

The Zone and Policy Area both seek to minimise vehicle access points from Bridge Street, promoting the use of other minor streets where possible. The proposed development achieves this by removing one of the existing Bridge Street crossovers, thereby satisfying the following Zone and Policy Area provisions:

Regional Town Centre Zone

Principle 10 Access to car parking areas should be obtained from minor streets and lanes wherever possible.

Bridge Street Policy Area

Principle 6 Direct vehicular access to car parking or service areas of the development should not be provided from Bridge Street.

(underlining added)

The General Section's Transportation and Access provisions also anticipate vehicle access that enables safe and convenient access and movement for all road users and pedestrians:

General Section – Transportation and Access

Principle 9 Development at intersections, pedestrian and cycle crossings, and crossovers to allotments should maintain or enhance sightlines for motorists, cyclists and pedestrians to ensure safety for all road users and pedestrians.

Principle 10 Driveway cross-overs affecting pedestrian footpaths should maintain the level of the footpath.

Principle 23 Development should be provided with safe and convenient access which:

- (a) avoids unreasonable interference with the flow of traffic on adjoining roads
- (b) accommodates the type and volume of traffic likely to be generated by the development or land use and minimises induced traffic through over-provision



- (c) is sited and designed to minimise any adverse impacts on the occupants of and visitors to neighbouring properties.

The proposal involves modification to the existing Bridge Street median in order to facilitate right turn movements to the site. As outlined by MFY, this modification will provide a safer road environment than the current conditions where drivers must wait within the through-traffic lane when waiting to turn.

The site has been designed to accommodate the turning movements of vehicles up to 11 metres in length which are the largest vehicles anticipated to service the site (for the bottle shop only).

All access points at the South Terrace frontage have been sited to ensure no unreasonable impact to vehicles using this street. In addition, all footpaths will be maintained or reinstated at appropriate levels.

5.7 Vehicle Parking

MFY has prepared a revised parking assessment based on the amended proposal plans, the current Development Plan parking rates, a peak parking assessment based on anticipated use/activity on the site and also an assessment of parking within the locality. The advice of MFY has been considered with respect of the car parking assessment.

The most relevant Development Plan provisions are as follows:

General Section - Transportation and Access

Principle 30 Development should provide off-street vehicle parking and specifically marked disabled car parking places to meet anticipated demand in accordance with MuBr/2 - Off Street Vehicle Parking Requirements.

Table MuBr/2 – Off Street Vehicle Parking Requirements

Form of Development	Number of Required Car Parking Spaces
Hotel	<p>1 per 2 square metres of public bar and gaming room; plus</p> <p>1 per 6 square metres of lounge or beer garden; plus</p> <p>1 per 3 seats in a dining room; plus</p> <p>1 per 3 guest rooms.</p>

MFY has concluded that:

- the proposed development has a “theoretical parking requirement” in the order of 387 spaces in accordance with the above Development Plan guidelines
- based on a “real” parking demand, which factors the rates of occupancy of each area within the hotel, there is potential demand for:
 - > 234 spaces during the peak evening dining period
 - > 206 spaces during the peak bar period, and
 - > 128 spaces during the peak lunch period.



- the proposed drive-through and walk-in bottle shop will have a negligible impact on car parking demand
- the proposal incorporates 160 car parking spaces and will therefore provide sufficient car parking during the lunch dining peak period
- the revised proposal is forecast to generate a marginally greater peak parking demand than the original proposal, however, with additional on-site car parking proposed (an additional 9 spaces), there would be only a small increase in on-street parking required during other peak times.
- during a peak function and peak bar occupancy, there would be demand for parking within the adjacent road network
- there are some 68 car parking spaces within close proximity of the Hotel (including nearby South Terrace and East Terrace only), together with additional parking in other nearby streets, and
- at peak periods, there is adequate parking available on-site and in the immediately adjoining streets to satisfy the parking demand and without having a negative impact on other land uses in the locality that are also partly dependent upon on-street parking.

For the reasons outlined within the Traffic and Parking Report, the proposed development is considered to have sufficient car parking in accordance with Transportation and Access Principle 30.

5.8 Additional Matters

5.8.1 Waste Management

The applicant has provided a waste management plan which outlines the anticipated waste streams and the methods for which these will be managed. Bin storage areas have been illustrated on the amended proposal plans and have been sited in a way in which they are obscured from public view. Bins will comprise a range of 240-1,100L “wheelie bins” which can be easily moved to the kerb for collection by the waste contractor.

In considering the relevant provisions of the Development Plan, it is considered that the waste storage area and collection complies with General Section – Waste Principles 5 and 6.

5.8.2 Landscaping

Oxygen has prepared a Landscaping Plan for the amended proposal. In preparing this Plan, Oxygen has had regard to Table MuBr/6 of the Development Plan which contains a Landscaping Schedule. This Table applies specifically to development within the Equine Recreation Policy Area 22 of the Recreation Zone, the Southern Area Policy Area 20 of the Residential Zone and the Equine Rural Living Policy Area 21 of the Rural Living Zone, but does not apply to the subject land. It is considered, however, to be instructive for landscaping planning purposes in relation to the proposed development.

The species selected for the proposed development generally comprises exotic species which are more suited to a commercial development where species are selected for aesthetic and practical reasons. Landscaping comprises a mix of medium height deciduous trees within the car park and the relocation of two existing palm trees on the site. The trees are complemented by a range of small shrubs and ground



covers with some vertical screen planting (fronting Bridge Street) and a green wall adjacent the entry from the car park.

In considering the nature of the proposed landscaping in relation to the General Section – Landscaping, Fencing and Walls provisions, we consider the landscaping to be appropriate because:

- it complements the built form of the building
- it enhances the appearance of the development, particularly along the Bridge Street frontage where feature screen planting is proposed to obscure the car park
- it includes an efficient irrigation system, and
- it will provide shade to car parking areas and assist with the minimisation of heat absorption and reflection within the car park.

5.8.3 Stormwater Management

TMK Consulting Engineers has prepared a Stormwater Management Plan and Report. This has been prepared in accordance with the following design criteria:

- drainage system designed to accommodate a 1 in 10 and 1 in 100 year ARI rainfall event
- the flow rate of stormwater discharged from the site not exceeding the pre-development flow rate for minor and major rainfall events
- the total discharge modelled as described in *Storm Drainage Design in Small Urban Catchments* and *Australian Rainfall and Runoff (ARR87)*.

These criteria are met through a design which incorporates 24kl of detention in an oversized 600mm diameter stormwater pipe. Water quality targets are met through the use of an Ecosol RSF 4200 Gross Pollutant Trap which is designed for 99% reduction of Gross Pollutants (GP), 80% reduction of Total Suspended Solids (TSS) and 45% reduction of Total Phosphorous (TP) and Total Nitrogen (TN) for the range of treatable flow.

With respect to the relevant stormwater provisions of the Development Plan, it is considered that the proposed methods of stormwater management comply with General Section – Natural Resources Principles 7, 8, 9, 10, 11 and 12.

5.8.4 External Lighting and Interface with Adjoining Development

TMK Consulting Engineers has prepared an Exterior Lighting Modelling Report. This Report outlines the proposed exterior lighting and the illumination levels. The Report provides an assessment for compliance with Australian Standard AS4282-1997 Table 2.1 (as it relates to light spill to adjoining commercial properties), and Australian Standard 1158.3.1-2005 for light level recommendations within the car park.

The assessment by TMK indicates compliance with both Australian Standards. On this basis, it is considered that the proposed development will not cause unreasonable light spill and therefore satisfies General Section – Interface between Land Uses Principle 1(e).



Our Ref: 1606131_L2 ASM
22 August 2017

Kerin Bay Pty Ltd
C/- Eureka Group
540 Port Road
ALLENBY GARDENS SA 5009

ATTENTION: Mr Mike Lunniss

Email: mlunniss@hcs.on.net

Dear Mike,

**RE: PROPOSED BRIDGEPORT HOTEL REDEVLEOPMENT
NO. 2 BRIDGE STREET, MURRAY BRIDGE**

As requested by Botten Levinson Lawyers on your behalf TMK have conducted an initial assessment of the proposed developments proximity to the existing railway tunnel to comment on and form an initial opinion as to whether the proposed development will have an adverse impact on the existing tunnel. The existing railway tunnel passes the northern corner of the site beneath the intersection of Bridge Street and East Terrace (sheet SD1 attached shows the approximate alignment of the existing tunnel).

Proposed Redevelopment

From the plans provided the development consists of the demolition of the existing double storey building and construction of a new 6 level building. In the vicinity of the railway tunnel at the northern corner of the site the proposed building structure consists of the following;

- Double storey building structure (bars, lounges and terrace) following same alignment as existing double storey building and set back approximately 7 metres from the existing tunnel.
- Six storey building structure (accommodation tower) setback approximately 12 metres from the existing tunnel.

Assessment of impact of proposed development on existing railway tunnel

The existing railway tunnel could be affected by the proposed development if there was an increase in surcharge loading on the tunnel from the weight of the new building or possibly by vibration from machinery and plant during construction activities. The impact of both are assessed separately below.

Surcharge loading

For surcharge loading to occur and the proposed development to have any effect on the existing tunnel at all the tunnel would need to be located within the line of influence. The line of influence can simply be estimated by drawing a line at 45 degrees from the bottom of the building (refer section 1 on sheet SD2) where anything below the line of influence is potentially affected by the weight of the building structure, whilst anything above the line would be unaffected.

An increase in surcharge loading would only occur on the tunnel from the proposed new development if the tunnel is located within the line of influence and either the building weight increases or the building is moved closer to the tunnel.



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TMK believe that the proposed new double storey section of building will not result in an increase in surcharge loading on the tunnel because;

- The alignment of the new double storey structure is as per the existing double storey structure and as such the proximity of building loads to the tunnel is unchanged.
- The weight of the new double storey structure is likely to be less than the existing structure as the existing structure consists of solid masonry and stone walling.
- The new building (and existing building) is situated outside the line of influence for surcharge loading to occur.

TMK believe that the proposed new accommodation tower (6 level building) will not result in an increase in surcharge loading on the tunnel because;

- The tower block is well set back from the existing tunnel and well outside the line of influence for surcharge loading to occur.

Vibration

Vibration results from ground movements caused by a driving force or activity such as the movement that can be felt on a foot path from a truck passing over an uneven road surface. Typical construction activities result in ground movements and hence vibration from excavation and the use of plant & machinery.

The level of vibration experienced at some point is influenced by;

- The type of construction activity
- The size and type of plant or machinery used.
- The way plant or machinery is operated
- The distance between the location of the construction activity and the point under consideration.

The type of construction activities which may produce ground vibration at the site are likely to be limited to those associated with demolition of the existing buildings and with the construction of the footings and earthworks (once the footings have been poured and the building is "out the ground" it is anticipated that further potential for effects from vibration due to construction activity would be minimal).

Plant used to undertake these activities would possibly include;

- Excavators.
- Trucks.
- Compaction equipment.
- Piling or drilling rigs.

The former Department for Transport, Energy and Infrastructure (now the Department for Planning, Transport and Infrastructure) has produced a document "Management of Noise and Vibration: Construction and Maintenance Activities – operational instruction 21.7" which gives guidance on vibration level targets and safe limits. As there is no Australian Standard that provides recommended vibration levels relating to structural damage the document uses German Standard DIN 4150-3 which defines limits below which damage will not occur (the limits are considered conservative in that vibrations which exceed the limits will not necessarily result in damage). These vibration limits have in turn been used to create a Table in Appendix B of the document "Typical Vibration levels from Construction Activities" which lists the order of distance to achieve damage targets from different types of construction activities.

A copy of the Table mentioned above is included at the back of this letter and it can be seen that the safe distance for most construction activities is in the order of 10 metres, apart from vibratory rollers 12 metres, compactor & excavators 15 metres and impact piling 50 metres. Further it can be seen that the safe distance for truck traffic over a maintained road is 5 metres - the existing railway tunnel runs below a road intersection and is expected to be less than 5 meters below the road.

TMK believe that whilst construction activity at the site will produce some ground vibration its effect on the tunnel structure will be limited due to;

- The distance of the site to the tunnel itself (over 90% of the site is more than 15 metres away from the tunnel with the closet point in the northern corner being approximately 10 metres away from the tunnel structure).
- The existing tunnel runs directly below a main road intersection and is already exposed to vibration effects from passing traffic which is possibly greater than any likely site construction activity induced vibration.

Further it is believed that any vibration effects can be controlled and minimised by;

- The use of appropriate plant or machinery during excavation and construction of the footings especially in the northern corner of the site.
- The use of Continuous Flight Auger piling or other low impact piling systems should deep piled footing systems be required.
- The provision of a demolition plan to allow for safe demolition and the use of low impact demolition methods.
- Monitoring of ground vibration effects with a seismograph.



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TMK have been involved a number of ground vibration monitoring projects for DPTI and private clients such as civil contractors and in our experience for normal civil construction works of this scale, vibrations levels are normally well below considered acceptable limits.

Summary

Surcharge loading

It is TMK's opinion based on an initial assessment that the proposed development is sufficiently setback from the existing railway tunnel to not have an adverse impact on the tunnel nor affect its structural integrity.

TMK also note that even if the tunnel was located within the zone of influence from the building structure it would be normal or common engineering practice to design a deep footing system such as piles which would direct loads from the building below the tunnel and prevent surcharge loading.

Vibration & Site Construction Activities

Whilst there will be vibration created from normal construction activities undertaken at the site in TMK's experience these are not likely to have an adverse impact on the tunnel structure. Vibration levels can be minimised by the use of appropriate construction techniques and plant. We also note that current vibration levels on the railway tunnel from passing traffic such as trucks is possibly greater than any site induced construction activity.

It is TMK's opinion that construction activities at the site can be effectively managed to limit vibration levels to considered acceptable limits by the preparation of a vibration management plan. Such a plan should consider the use of low impact construction techniques especially to the northern corner of the site and some form periodic monitoring most likely during demolition of the existing structures and footing construction.

For and on behalf of
TMK Consulting Engineers

ANDREW MARTIN
Senior Associate

Encl SD1, SD2, & Extract from "Management of Noise and Vibration: Construction and Maintenance Activities - Operational Instruction 21.7" - Appendix B. Typical Vibration levels from Construction Activities



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DATE: JULY '17

JOB NO: 1606131

SD1

STRUCTURAL DETAIL

AT: 2 BRIDGE STREET

MURRAY BRIDGE, SA 5253

FOR: KERIN BAY PTY LTD

SCALE NTS



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Facsimile 08 8410 1405
Berri Office: 25 Vaughan Terrace,
Berri SA 5343



DATE: JULY '17

JOB NO: 1606131

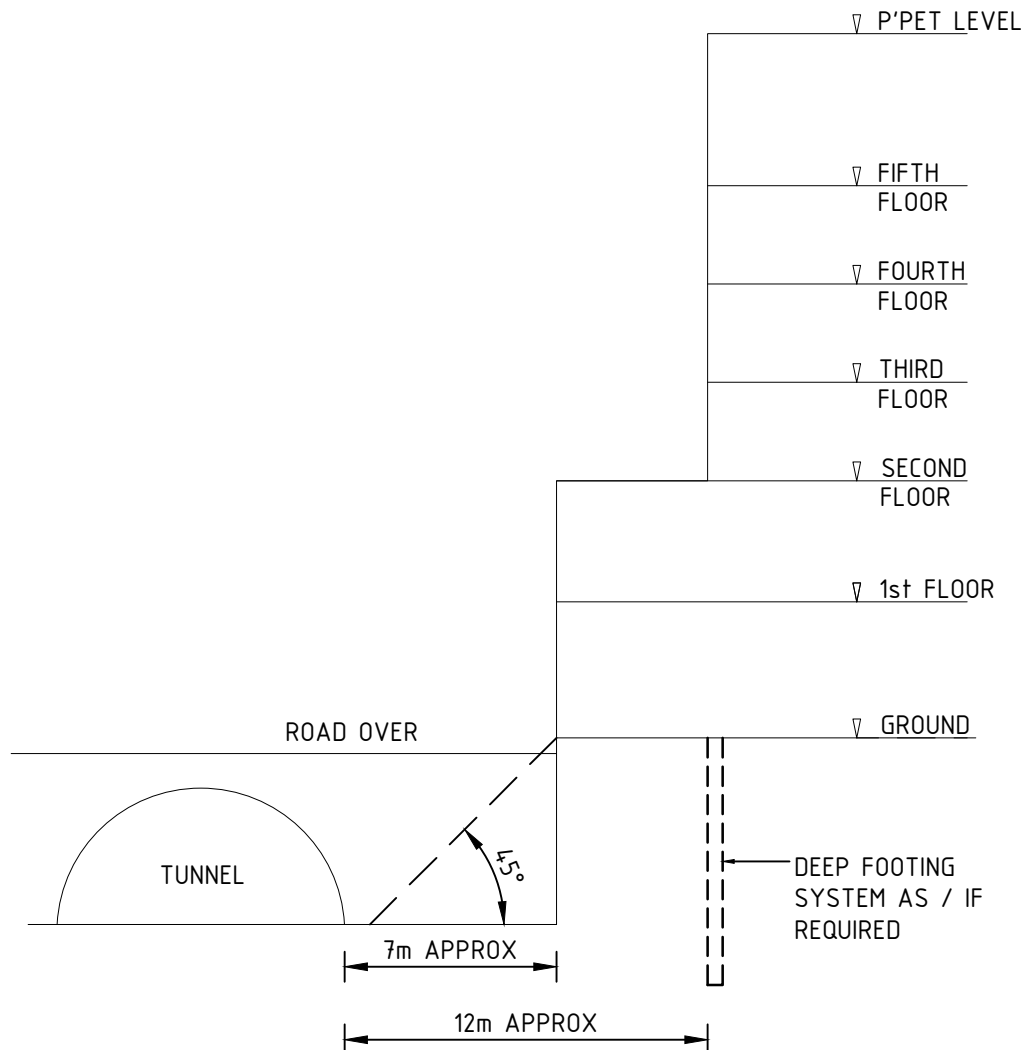
SD2

STRUCTURAL DETAIL

AT: 2 BRIDGE STREET

MURRAY BRIDGE, SA 5253

FOR: KERIN BAY PTY LTD



SECTION
NTS

1
SD1

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*Management of Noise and Vibration: Construction and Maintenance Activities
Operational Instruction 21.7*

Appendix B. Typical Vibration Levels from Construction Activities

Vibration levels are influenced by the actual operating condition of the plant and equipment being considered and the local site and geographical conditions. Therefore, this table should be used for indicative purposes only. Vibration level monitoring in the vicinity of a site in accordance with this Operational Instruction should be used in lieu of this table for construction works where vibration might occur.

Activity	Typical Levels of Ground Vibration	Order of distance to achieve damage targets
Vibratory Rollers	1.5mm/s at 25m <i>Higher levels could occur at closer distances depending on local conditions and the roller operation. For a heavy roller, it is expected that damage will not occur with a minimum 12m buffer to the foundations of a standard residential building.</i>	12m
Hydraulic Rock Breakers (levels typical of a large rock breaker in hard sandstone)	4.5mm/s at 5m 1.3mm/s at 10m 0.4mm/s at 20m 0.1mm/s at 50m	10m
Compactor	20mm/s at 5m 2mm/s at 15m 0.3mm/s at 30m	15m
Excavators	0.2mm/s at 40m	15m
Ballast Tamping	6mm/s at 3m 2mm/s at 10m	10m
Truck traffic (over maintained road surfaces)	0.2mm/s at 10m	5m
Truck traffic (over irregular surfaces)	2mm/s at 10m	10m
Impact pile driving / removal	≤ 15 mm/s at distances of 15 m ≤ 9 mm/s at distances greater than 25 m Typically below 3mm/s at 50m <i>Significant changes to the vibration levels can occur based on the soil conditions and the driving energy of the hammer</i>	50m
Continuous Flight Auger (CFA) piling	Negligible vibration at distances greater than 20 m from the piling	
Bored piling	Negligible vibration at distances greater than 20 m from the piling	
Bulldozers	2mm/s at 5m 0.2mm/s at 20m	10m
Air track drill	5mm/s at 5m 1.5mm/s at 10m 0.6mm/s at 25m 0.1mm/s at 50m	10m
Jackhammer	1mm/s at 10m	10m



Leader in sustainable waste management and recycling solutions

Bridgeport Hotel Development

Waste Management Plan

Prepared by Veolia Environmental Services (Australia) Pty Ltd

February 2018





The Manager – Building Design
Unit 3 / 81 Grange Road,
Welland. SA. 5007

Attn: Craig Eyles

Dear Craig,

Veolia is pleased to submit the following Waste Management Plan for the proposed development located in Murray Bridge.

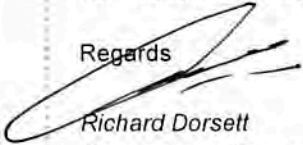
Veolia will have a strong focus on maintaining the existing waste streams to maintain cost minimisation. The following waste streams would be available

- Liquid Waste – recycled through our liquid plant.
 - grease traps.
- General Waste – for all contaminated wet waste streams
 - 3m3 Front Lift Bins from existing bin compound location.
- Cardboard & Paper - No dry recycling solutions currently exist in this region.

Please see a copy of the waste management plan below for your consideration. I am confident Veolia can implement the above services and systems to work towards achieving cost minimisation and supply the waste management services in a safe & environmentally friendly manner.

We look forward to working with you throughout this process and into the future. Should you require additional information or clarification relating to this document, please do not hesitate to contact myself on 0438446665.

Regards


Richard Dorsett
Key Account Manager



Executive Summary

Veolia's aim is to deliver viable collection, handling and transport of all waste streams.

Veolia Environmental Services (Veolia) is Australia's leading provider of environmental waste management services to industry, commerce and the public. We have worked closely with government, industry and commerce for over 42 years to satisfy people's essential daily needs while respecting natural resources. Our strong and stable management team have taken the organisation from a small operation in 1969 to the current Australia-wide and international network generating Australian revenues in excess of \$700 million per annum from in excess of 100 operating sites.

Veolia is the Australian waste management, industrial cleaning and resource recovery division of the global company Veolia Environnement (VE), generating revenue in excess of AUD \$55 billion annually.

The worldwide strength of Veolia is underpinned by a strategy of long-term investment, continuous innovation and mutual partnering with our customers. Veolia works in partnership with nationally aligned accounts such as Coles, Spotless and Health Scope. Locally, Veolia has forged strong working partnerships with ISS, Burnside Village, Makris Corporation and performs municipal services for Councils such as Mt Barker, Pt Augusta, Whyalla and Pt Lincoln. Veolia has significant experience within the Local Government sector throughout Australia in areas of environmentally recognised and sustainable waste management and recycling services.

This experience enables Veolia to provide the suite of services required by the Bridgeport Hotel Development, whilst maintaining the necessary standards of environmental health and safety compliance. Veolia is proud of its commitment and compliance to all aspects of Quality, Occupational Health Safety & Welfare and Environmental Management Systems to support our commitment to sustainable development.

Our proposal recognises the need to address the disposal of all waste streams generated from each area of Bridgeport Hotel Development. Our model will focus on effective waste minimisation strategies, including the future development of recycling or beneficial re-use of product wherever appropriate at extremely competitive rates. Veolia has adopted the principle of 'World's Best Practice' and is dedicated to achieving the highest standards in our field.

In the waste management sector, disposal of biodegradable waste will ultimately attract a higher landfill cost at poorly run landfill operations. Government and commerce are becoming increasingly aware of the environmental and economic benefits of sorting all waste streams to recover high yields of recyclable waste. The increased recycling of plastics, paper, cardboard, waste oily waters, sludges, greases and other recyclable materials will improve the Bridgeport Hotel life-cycle Greenhouse Gas (GHG) Emissions and ecological footprint. Veolia can provide monthly reports on GHG emission savings, in addition to data on volumes and weights.



Reduce Carbon Gas Emissions and result in lower costs.

A dedicated contract manager focused on exceeding your expectations

Veolia believes in conducting regular audits of its waste segregation management system to ensure that it complies with Bridgeport Hotel environmental directives. The evaluation of the effectiveness of this system may be monitored through regular agreed KPI reporting.

In this resource and carbon constrained world, it is important that the Bridgeport Hotel develops a waste and recycling management program and aligns with an environmental service provider who is strategically positioned to help the Bridgeport Hotel mitigate its environmental footprint.

The key characteristics of our proposal are:

One Contact: Veolia is able to provide a dedicated Waste Services Team and we will assign a major account executive to Bridgeport Hotel development. This provides one point of contact for Bridgeport Hotel development to monitor waste expenditure costs and recycling performance, enabling real improvements in both over the life of the contract. Veolia will provide one phone number to the Bridgeport Hotel development for all enquiries and this will be operational 24 hours a day, 7 days a week.

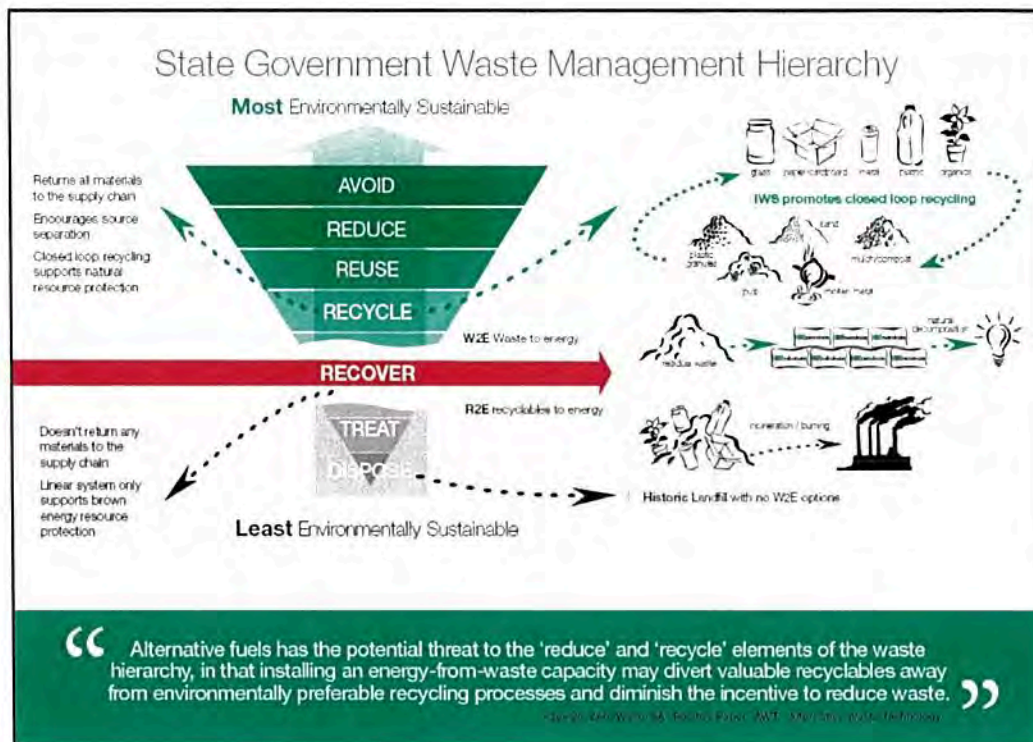
Leading Edge Reports: A monthly national report, which not only captures recycling and waste data, but calculates waste related Greenhouse Gas Emissions and savings from transport and waste disposal is available on a monthly basis for the Bridgeport Hotel development.

We are also able to provide reporting based on:

- ❖ Cost Centre volumes and costs, waste volumes & weights, waste types, recycling volumes, recycling types, disposal costs etc.
- ❖ A feature of our reporting will be a Green House Gas (GHG) calculation, which will detail what impact Market Street development has had on the environment and the benefits they have delivered through increased recycling.

Educational Material: Veolia can supply a full range of educational material to help understand and increase the recycling outcomes.





1. Waste Management Hierarchy

VEOLIA Recyclepak

Maximum diversion of resources from burning alternative fuels and historic landfill disposal.

ASK FOR A SITE TOUR OF IWS TODAY!

Dry Recyclables	Commingled Recyclables	Mixed Paper & Cardboard	Food & Garden Organics	General Waste	Mixed Waste
<p>Process Mechanically pre-sorted and then processed through the IWS Recycling Centre.</p> <p>Recycling Performance 90% of the recyclable content* is extracted by weight. Residual content is largely in garbage bags and is pulverised which cannot be opened due to OHS risks.</p> <p>Presentation to IWS Can contain between 0-20% residual waste by weight due to limited source separation.</p>	<p>Process Directly processed through the IWS Recycling Centre.</p> <p>Recycling Performance 90% of the recyclable content* is extracted by weight. Residual content is dropped out by screen and largely contains broken glass and food fines which are removed before passing through manual sorting room due to OHS risk management.</p> <p>Presentation to IWS Can contain between 0-15% residual waste by weight. Limited contamination due to high level of source separation.</p>	<p>Process Directly processed through the IWS Recycling Centre.</p> <p>Recycling Performance 100% of the recyclable content* is processed directly into mixed paper and cardboard baling.</p> <p>Presentation to IWS Required to be 100% paper and cardboard.</p>	<p>Process Processed through the IWS Composting Process.</p> <p>Recycling Performance 90% of the recyclable content* is extracted by weight. Residual content is dropped out by screen and largely contains broken glass and plastic fines.</p> <p>Presentation to IWS Required to be 100% food organics.</p>	<p>Process Pulverisable waste processed through carbon efficient Envirobake System.</p> <p>Recovery Performance Compacted into waste bales and placed in a modular landfill where methane is extracted and converted to energy.</p> <p>Presentation to IWS Contains mixed general waste, no source separation occurs.</p>	<p>Process Mechanically pre-sorted and then processed through the IWS Recycling Centre.</p> <p>Recycling Performance 90% of the recyclable content* is extracted by weight. Residual content is dropped out by screening and is minimal by weight due to the high density of recyclables e.g. brick/dirt.</p> <p>Presentation to IWS Can contain between 0-10% residual waste by weight. Limited contamination due to source separation and high density of recyclables.</p>
<input checked="" type="checkbox"/> Recyclable Content <input type="checkbox"/> 2m x 1.2m x 0.8m <input type="checkbox"/> 2m x 1.2m x 0.8m	<input checked="" type="checkbox"/> Recyclable Content <input type="checkbox"/> 2m x 1.2m x 0.8m <input type="checkbox"/> 2m x 1.2m x 0.8m	<input checked="" type="checkbox"/> Recyclable Content <input type="checkbox"/> 2m x 1.2m x 0.8m <input type="checkbox"/> 2m x 1.2m x 0.8m	<input checked="" type="checkbox"/> Recyclable Content <input type="checkbox"/> 2m x 1.2m x 0.8m <input type="checkbox"/> 2m x 1.2m x 0.8m	<input checked="" type="checkbox"/> Envirobake	<input checked="" type="checkbox"/> Recyclable Content

500 Churchill Road
Melburn, SA 5068
Telephone: 08 8356 2100
Email: sales@veolia.com.au

VEOLIA
Integrated Waste Services

Lot 254 Dry Creek and Whigley Roads
Virginia SA 5030
Telephone: 8241 2541
Facsimile: 8241 1039
Email: sales@wgs.com.au
www.wgs.com.au

creating a greener

Printed on 100% recycled paper



Triple National Certification: Bridgeport Hotel development will have peace-of-mind that their waste is being collected, recycled and disposed of in a safe and environmentally compliant manner. This is backed up by our highly enviable triple certification of ISO 14001 (Environment), ISO 9001 (Quality) and AS 4801 (Safety) management systems.



Award Winning Business: Veolia is the recipient of the Australian Business Award for Environmental Sustainability.



Veolia is also an Australian Quarantine and Inspection Service accredited service provider. The strategic direction of Veolia is one of continuous improvement in environmental technologies for the handling, processing and treatment of waste as well as improvements in education and environmental awareness programs for our customers.

As the organisation has grown, it has earned a reputation for quality, reliability, customer service and commitment to sustainable development based on 'World's Best Practice'. We look forward to working with Bridgeport Hotel development throughout this period and into the future. Should you require additional information or clarification relating to this document, please do not hesitate to contact myself on (08) 8260 2122.

Richard Dorsett

Key Account Manager SA

Waste Management Plan – Hotel Suites/Hotel/Cafe Refuse Area

Bridgeport Hotel

Subject	Details																																			
Development Details	Residential suites and Hotel (99 x bedroom suites)																																			
Type of waste Streams & Bin Sizes	Bin room in basement <ul style="list-style-type: none">Cardboard and paper – dry waste recyclingGeneral Waste – For all Contaminated wet waste <p>Bin Dimensions:</p> <p>Rear Lift Bins (Size Matrix)</p> <table><tr><th>Bin Size (Ltrs)</th><th>Wheel Diameter</th><th>Max weight in Bin</th><th>Bin Weight</th><th>Height (mm)</th><th>Width (mm)</th><th>Depth (mm)</th></tr><tr><td>140</td><td>200</td><td>48kg</td><td>11.4kg</td><td>920</td><td>535</td><td>640</td></tr><tr><td>240</td><td>200</td><td>96kg</td><td>15.5kg</td><td>1060</td><td>580</td><td>730</td></tr><tr><td>660</td><td>200</td><td>265kg</td><td>45kg</td><td>1200</td><td>1360</td><td>770</td></tr><tr><td>1100</td><td>200</td><td>440kg</td><td>58kg</td><td>1390</td><td>1360</td><td>1090</td></tr></table> <p>Smaller wheelie bins for internal use as required.</p>	Bin Size (Ltrs)	Wheel Diameter	Max weight in Bin	Bin Weight	Height (mm)	Width (mm)	Depth (mm)	140	200	48kg	11.4kg	920	535	640	240	200	96kg	15.5kg	1060	580	730	660	200	265kg	45kg	1200	1360	770	1100	200	440kg	58kg	1390	1360	1090
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Service Frequency & Waste Volumes	<p>Residential bedroom suites (99 x Bedrooms)</p> <p>General Waste (20ltr per bedroom per week required & kitchen waste)</p> <ul style="list-style-type: none">Volume – 2985 ltr per week1 x 3m3 Front Lift binsService schedule 3 days x per week = 9m3 capacity (1 x 3m3 bin) <p>Cardboard & Paper</p> <ul style="list-style-type: none">Serviced schedule 2 days x per week = 6m3 capacity (1 x 3m3 bin)																																			
	<table><tr><th>Bin</th><th>Height</th><th>Front Height</th><th>Depth</th><th>Width</th></tr><tr><td>1.5 m3</td><td>1120mm</td><td>N/A</td><td>950mm</td><td>2200mm</td></tr><tr><td>3.0m3</td><td>1340mm</td><td>N/A</td><td>1480mm</td><td>2200mm</td></tr><tr><td>4.5m3</td><td>1650mm</td><td>1200mm</td><td>1850mm</td><td>2200mm</td></tr><tr><td>6.0m3</td><td>1950mm</td><td>1500mm</td><td>1835mm</td><td>2200mm</td></tr></table>	Bin	Height	Front Height	Depth	Width	1.5 m3	1120mm	N/A	950mm	2200mm	3.0m3	1340mm	N/A	1480mm	2200mm	4.5m3	1650mm	1200mm	1850mm	2200mm	6.0m3	1950mm	1500mm	1835mm	2200mm										
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Bin Storage Locations & movement of bins	<ul style="list-style-type: none"> • Bins will be stored in the ground level bin waste compound. The bins will be clearly labelled with signage encouraging the staff to recycle as much as possible. • The bins will be managed by the Cleaners/Facility Manager– the bins will be serviced via South Terrace. • Bin storage area needs to be large enough to store bins allocated above.
Collection Points	<p>Waste & Recycling</p> <ul style="list-style-type: none"> • A Veolia truck will service the refuse compound at side of Hotel bottle shop driveway, entering off South Terrace. Should extra bins be required these can be supplied and stored in the under croft area and brought out by forklift on collection days. Alternatively we can collect 5 days per week if required. We currently already service the waste at the existing facility.
Specialised Facilities & Equipment	<ul style="list-style-type: none"> • There will be no specialised equipment for this project
Account Management & Customer Education	<ul style="list-style-type: none"> • If awarded the waste contract, Veolia will have a dedicated Account Manager to oversee the waste management services for the Bridgeport Hotel development. We can supply signage to help achieve improved recycling for Cardboard and Paper. This service is currently not on site.



Waste Management Plan – Kitchen / Restaurant Refuse Area

Subject	Details																																			
Development Details	Hotel rooms & Restaurant/Café																																			
Type of waste Streams & Bin Sizes	<p>Bin area (will be separated from the residence bin area.)</p> <ul style="list-style-type: none">Organics Bin – Not available in Murray BridgeGeneral Waste – For all Contaminated wet waste Hotel/café / Kitchens & Bottle Shop.<ul style="list-style-type: none">Wheelie bins as required for internal useGrease Arrestor – All grease trap liquid waste from Restaurant (located in outside car park & delivery area)Cardboard & paper – Wheelie bins as required for internal use <p>Bin Dimensions:</p> <p>Rear Lift Bins (Size Matrix)</p> <table><tr><th>Bin Size (Ltrs)</th><th>Wheel Diameter</th><th>Max weight in Bin</th><th>Bin Weight</th><th>Height (mm)</th><th>Width (mm)</th><th>Depth (mm)</th></tr><tr><td>140</td><td>200</td><td>48kg</td><td>11.4kg</td><td>920</td><td>535</td><td>640</td></tr><tr><td>240</td><td>200</td><td>96kg</td><td>15.5kg</td><td>1060</td><td>580</td><td>730</td></tr><tr><td>660</td><td>200</td><td>265kg</td><td>45kg</td><td>1200</td><td>1360</td><td>770</td></tr><tr><td>1100</td><td>200</td><td>440kg</td><td>58kg</td><td>1390</td><td>1360</td><td>1090</td></tr></table>	Bin Size (Ltrs)	Wheel Diameter	Max weight in Bin	Bin Weight	Height (mm)	Width (mm)	Depth (mm)	140	200	48kg	11.4kg	920	535	640	240	200	96kg	15.5kg	1060	580	730	660	200	265kg	45kg	1200	1360	770	1100	200	440kg	58kg	1390	1360	1090
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Service Frequency & Waste Volumes	<p>General Waste</p> <ul style="list-style-type: none">Volume – 7500L per week1 x 3m3 Front Lift binsService schedule 3 days x per week (as per existing collections). This can be ramped up to 5 days per week if required <p>Cardboard & Paper</p> <ul style="list-style-type: none">Volume – 4,600L per week1 x 3m3 Front Lift Bin.Serviced schedule 2 days per week <p>Grease Arrestor</p> <ul style="list-style-type: none">Volume 2400 ltr, serviced Quarterly (TBA volume) <p>Based on 7 days trading.</p>																																			

Bin Storage Locations & movement of bins	<ul style="list-style-type: none"> • Bins will be stored in the ground level bin Waste Compound near bottle shop. The bins will be clearly labelled with signage encouraging the restaurant staff to recycle as much as possible. • The bins will be managed by the Cleaners/Facility Manager– the bins will be serviced via South Terrace. • Bin storage area needs to be large enough to store bins allocated above.
Collection Points	<p>Waste & Recycling</p> <ul style="list-style-type: none"> • A Veolia truck will service the Waste Compound from South Terrace.
Specialised Facilities & Equipment	<ul style="list-style-type: none"> • There will be no specialised equipment for this project
Account Management & Customer Education	<ul style="list-style-type: none"> • If awarded the waste contract, Veolia will have a dedicated Account Manager to oversee the waste management services for the Bridgeport Hotel development. We can supply signage to help achieve improved recycling.

- Grease Arrestor servicing – Grease trap to be serviced early morning before 7am.

PROPOSED PERFORATED SCREEN IMAGES

Refer to Locker Group Pic Perf® brochure. Images displayed on perforated aluminium screens to be a Graphic artists' stylized representation of River Grasses, Reeds, and Bulrushes - Similar to, but not limited by the images below



River Grass



Reeds (& Dragonflies)



Bulrushes

Pic Perf[®]



RECEIVED
03/10/2017
SCAP

Pic Perf®

Stamp an indelible image on your next project.

Pic-Perf® is a unique technology allowing detailed images to be created in metal, for a variety of applications.

Pic-Perf® creates an image using hundreds or even thousands of holes, punched out of metal.

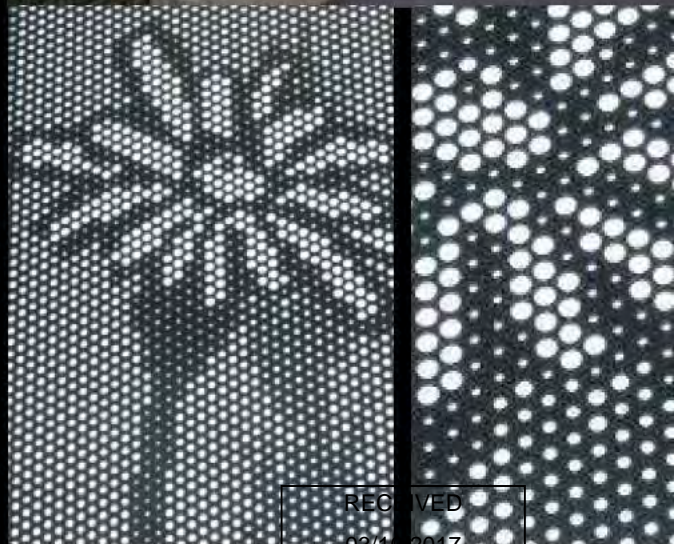


Image:

- Exceptional clarity is achievable in almost any size*
- Pic-Perf® can be an abstract artwork, logo or brand, a photograph and even a collage of images
- Pic-Perf® creates a statement without needing to use words, however they can be incorporated if required!
- Create an image that's effective and strong, yet subtle

Flexible:

- Pic -Perf® is custom designed for each application, therefore it's tailored to your requirements
- Pic Perf® can be detailed or abstract
- Created in different materials, Pic-Perf™ can be used in internal or external applications
- The image can be left untreated, anodised or powder-coated, depending on application requirements.
- Pic Perf® images are scaleable; small enough to frame or large enough to clad an entire building



REMOVED
03/11/2017



ORIGINAL SAMPLE



PIC PERF® IMAGE

Functional:

- Pic-Perf® can be used in a variety of applications including:
 - car park screens
 - balustrades
 - sunscreens
 - signage
 - partitions or
 - artwork/decoration
- Provides shade protection from the sun, reducing energy consumption
- Allows ventilation and airflow
- Provides security, allowing vision from inside to out
- Better wind resistance than traditional 'solid' signage
- Fixing holes can be incorporated into the design

Design:

- If you don't have an image or design in mind, Locker Group can create one for you

IMAGE IS CREATED USING
DIFFERENT SIZED HOLES



RECEIVED
03/10/2017
SCAP

Important things to consider when specifying Pic-Perf®:

- The most common viewing distance - each image will have an ultimate viewing distance, where it is clearest
- The background affects the final appearance. (A darker background requires a light, reflective surface)
- The application environment will affect the material chosen.
- A digital image in appropriate formats* or Locker Group requires / can arrange graphic design to suit your requirements
- The size of the finished panel, ensuring the aspect ratio* is the same as the original image
- The appropriate fixing* methods



* Please refer to our Technical Data Sheets for further information.



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Email: sales@lockernz.co.nz
Website: www.lockernz.co.nz

TOLL FREE 0800 285 837



Date: 21/06/2017 **Job Number:** 1606131

To: CED BUILDING DESIGN **Attention:** CRAIG EYLES **Email:** CEyles@hcs.on.net
cc: **Attention:** **Email:**
From: Adrian Ko
Project: BRIDGEPORT HOTEL - 2 BRIDGE STREET MURRAY BRIDGE SA 5253
Subject: Exterior Lighting modeling report

Dear Craig,

1.0 INTRODUCTION

TMK Consulting Engineers were engaged to provide documentation to address lighting requirement for the proposed Bridgeport hotel exterior lighting relating to the illumination (lux) levels on the car park and perimeter building.

The objective of this report is to:

- Provide certification/commentary on the light spill across the boundary line to adjoining commercial properties on the east, south and & west boundary during pre-curfew hours, for compliance to AS4282-1997 Table 2.1 criteria.
- Provide recommendation on the illumination level on the car park area, for compliance to AS1158.3.1 -2005 for P11c criteria.



The various steps undertaken in the investigation were:

- Computer modeling using readily available software & luminaire photometric (.IES) files received from lighting supplier.
- Cross referencing & examination of all relevant standards to ensure the requirement is achieved.

The following was excluded from the assessment:

- Site survey visual walk through to examine the condition around the site.

2.0 PROPOSED LIGHTING LAYOUT

Car Park Area Lighting

The lighting layout proposed is based on the use of:

- 54W LED pole mounted luminaries, mounted on 4.5m high poles/canopy mounted. (Light fitting type: Ligman Power mission 2 with Type 3 Optics). The light fittings shall be installed with 0 degree tilt.
- 54W LED pole mounted luminaries, mounted on 4.5m high poles/canopy mounted. (Light fitting type: Ligman Power mission 2 with Type 4 Optics). The light fittings shall be installed with 0 degree tilt.
- 40watt LED Flood light wall mounted (6 metres AFFL) above liquor store for transition light. (Light Fitting Type: Philips Tango G2 LED 40Watt Flood SWB)

Quantity of light fittings





Luminaire Schedule						
Symbol	Qty	Label	Total Lamp Lumens	LLF	Description	Lum. Watts
	1	A3	4000	0.800	PHILIPS TANGO G2 LED 40W FLOOD SWB	40
	4	A2	N.A.	0.800	PO29-LM059-T4-4 LIGMAN POWERMISSION 2 54W LED AREA LIGHT TYPE 4 OPTICS	59
	5	A1	N.A.	0.800	PO29-LM059-T3-4 LIGMAN POWERMISSION 2 54W LED AREA LIGHT TYPE 3 OPTICS	59
	48	WL	N.A.	0.800	Ligman GI-31601-W40 Gino 3 rectangular wall up-down light LED	16.5

Figure 1 provides an indication of the proposed lighting layout.

No lights assessment had been conducted under the alfresco area & outdoor playground, as these areas are being lit separately from the car park lighting.

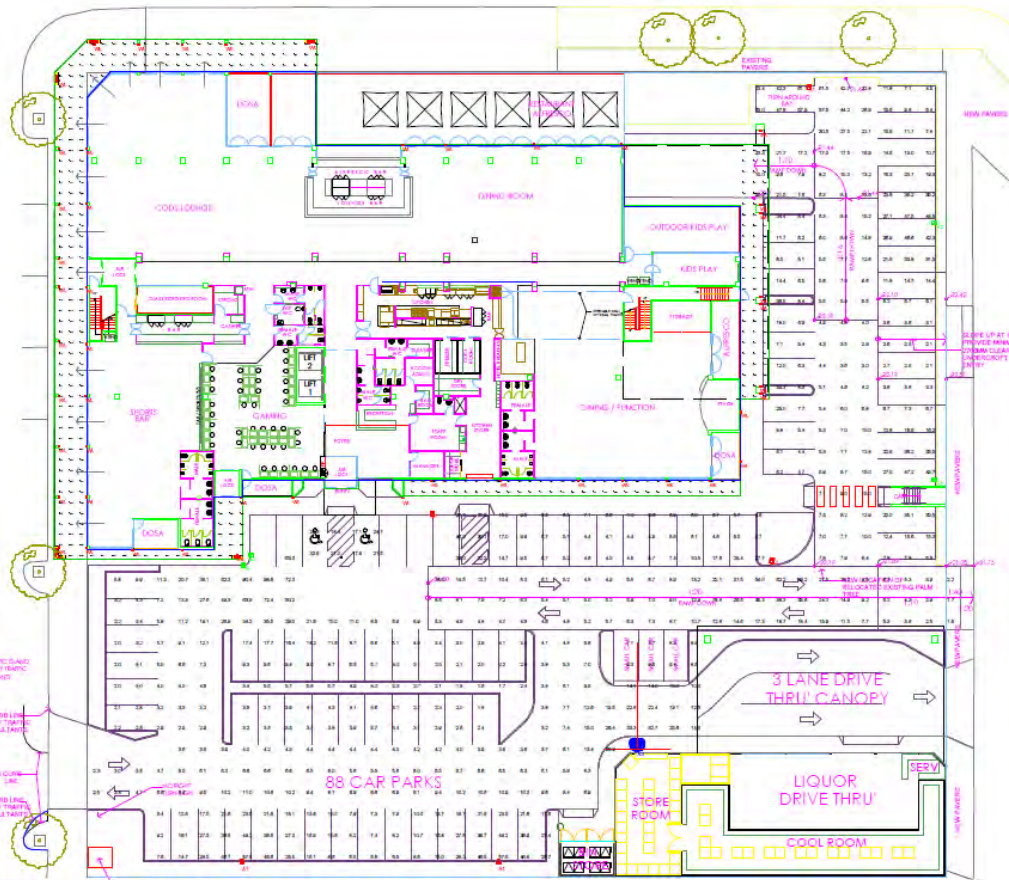


Figure 1: Proposed Car Park Area lighting layout



Luminaire Schedule						
Symbol	Qty	Label	Total Lamp Lumens	LLF	Description	Lum. Watts
	1	A3	4000	0.800	PHILIPS TANGO G2 LED 40W FLOOD SWB	40
	4	A2	N.A.	0.800	PO29-LM059-T4-4 LIGMAN POWERMISSION 2 54W LED AREA LIGHT TYPE 4 OPTICS	59
	5	A1	N.A.	0.800	PO29-LM059-T3-4 LIGMAN POWERMISSION 2 54W LED AREA LIGHT TYPE 3 OPTICS	59
	48	WL	N.A.	0.800	Ligman GI-31601-W40 Gino 3 rectangular wall up-down light LED	16.5

Calculation Summary						
Project: Project_1						
Label	CalcType	Units	Avg	Max	Min	Min/Avg
CAR PARK	Illuminance	Lux	13.20	98.8	1.5	0.11
Disabled Zone	Illuminance	Lux	22.64	32.8	17.1	0.76
Walkway_Workplane	Illuminance	Lux	30.83	142	1.8	0.06

Expanded Luminaire Location Summary			
Project: AGI-Import_2			
LumNo	Label	X	Y
71	A1	-8.933	38.551

Figure 2: Proposed Car Park Area lighting Type and Calculation Summary

Exterior Building Area Lighting

The lighting layout proposed is based on the use of:

- 36W LED wall mounted luminaries, mounted on 2.4m AFFL. (Light fitting type: Ligman Gino 3 Up /Down Wall Light)

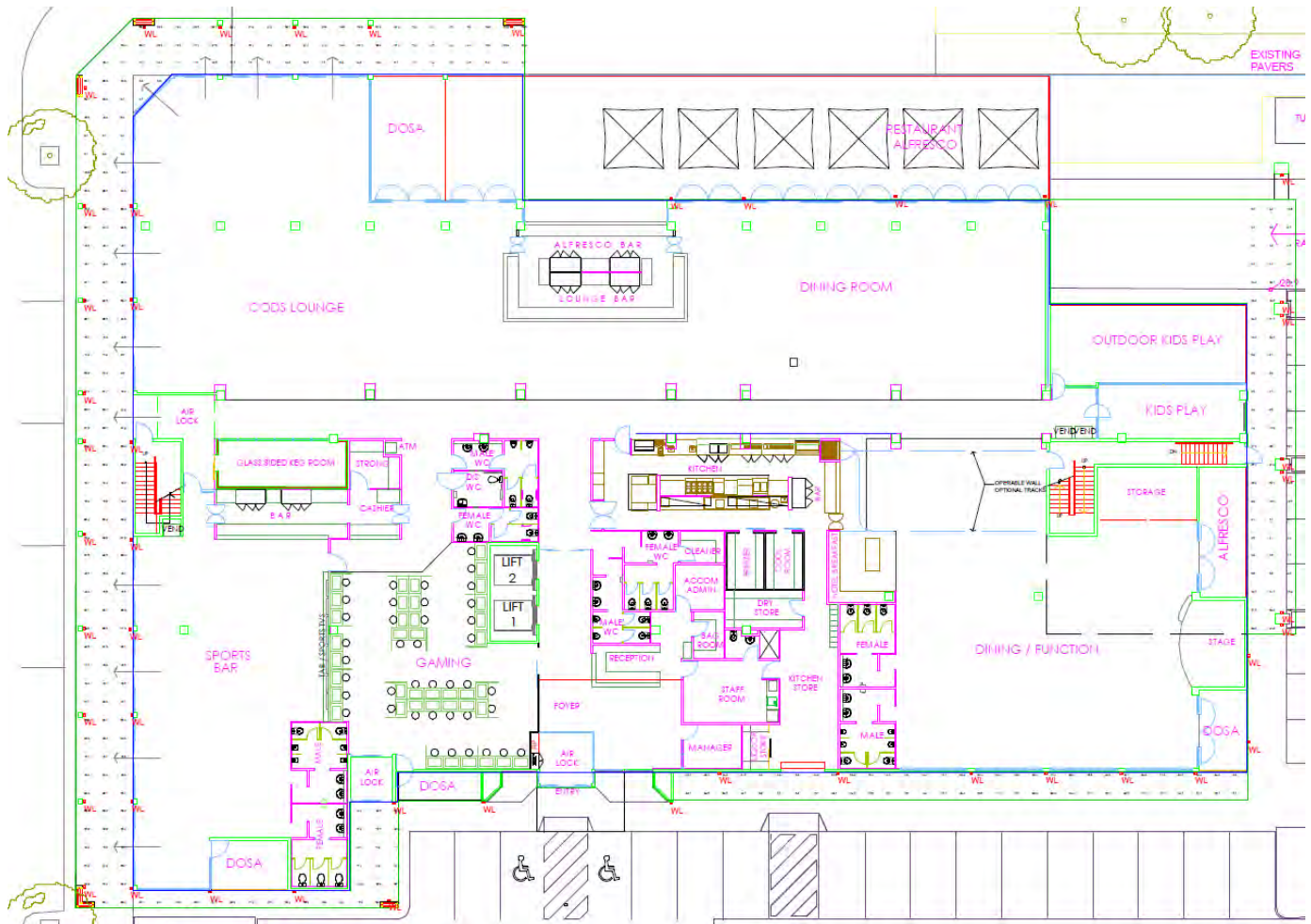


Figure 3: Proposed Northern and Western External Building Area lighting layout (Facing Road)



Luminaire Schedule						
Symbol	Qty	Label	Total Lamp Lumens	LLF	Description	Lum. Watts
+	48	WL	N.A.	0.800	GI-31601-W40 Rev_2	16.5

Calculation Summary						
Project: Project_1						
Label	CalcType	Units	Avg	Max	Min	Min/Avg
Walkway_Workplane	Illuminance	Lux	28.46	126	1.8	0.06

Calculation Summary			
Project: Spill Lighting			
Label	CalcType	Units	Max
East Tce_Cd_Seg1	Obtrusive Light - Cd	N.A.	5
East Tce_Ill_Seg1	Obtrusive Light - Ill	Lux	0.3
Murray Street_Cd_Seg1	Obtrusive Light - Cd	N.A.	2
Murray Street_Cd_Seg2	Obtrusive Light - Cd	N.A.	3
Murray Street_Cd_Seg3	Obtrusive Light - Cd	N.A.	5
Murray Street_Ill_Seg1	Obtrusive Light - Ill	Lux	0.1
Murray Street_Ill_Seg2	Obtrusive Light - Ill	Lux	0.4
Murray Street_Ill_Seg3	Obtrusive Light - Ill	Lux	0.4
South Terrace_Cd_Seg1	Obtrusive Light - Cd	N.A.	2
South Terrace_Ill_Seg1	Obtrusive Light - Ill	Lux	0.1

Figure 4: Proposed External Area lighting Type and Calculation Summary



3.0 LIGHT SPILL ASSESSMENT

The criteria for vertical light spills outline in AS 4282-1997 - Control of The obtrusive effects of outdoor lighting, Table 2.1:

- Pre-curfew hour (between 6am – 11pm) - maximum of 25 lux across the boundary line,
- Curfew hour (between 11pm – 6am) – maximum of 4 lux across the boundary line.

Assessment had been conducted based on the northern ,western, eastern and southern boundary line with no obstruction from trees or vegetation. Boundary vertical spill had been measured at ground level, and 4.5m into the adjoining northern eastern, western and southern boundary.

Table 1 & 2 below indicates the measured max points for light spills.

Car Park Area Boundary

The modelled maximum spill on the boundary east terrace perimeter,south terrace perimeter and bridge street is 0.4 lux.

Obtrusive Light - Compliance Report

AS 4282-1997, Post-Curfew, Commercial

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Illuminance

Maximum Allowable Value: 4 Lux

Calculations Tested (5):

Calculation Label	Test Results	Max. Illum.
East Tce_Ill_Seg1	PASS	0.4
Murray Street_Ill_Seg1	PASS	0.2
Murray Street_Ill_Seg2	PASS	0.4
Murray Street_Ill_Seg3	PASS	0.4
South Terrace_Ill_Seg1	PASS	0.1

Luminous Intensity (Cd) At Vertical Planes

Maximum Allowable Value: 2500 Cd

Calculations Tested (5):

Calculation Label	Test Results
East Tce_Cd_Seg1	PASS
Murray Street_Cd_Seg1	PASS
Murray Street_Cd_Seg2	PASS
Murray Street_Cd_Seg3	PASS
South Terrace_Cd_Seg1	PASS

Table 1: Obtrusive Light (Post Curfew) – Compliance Report from AGI



Obtrusive Light - Compliance Report

AS 4282-1997, Pre-Curfew, Commercial

Filename: Bridgeport-Copy new

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Illuminance

Maximum Allowable Value: 25 Lux

Calculations Tested (5):

Calculation Label	Test Results	Max. Illum.
East Tce_III_Seg1	PASS	0.4
Murray Street_III_Seg1	PASS	0.2
Murray Street_III_Seg2	PASS	0.4
Murray Street_III_Seg3	PASS	0.4
South Terrace_III_Seg1	PASS	0.1

Luminous Intensity (Cd) Per Luminaire

Maximum Allowable Value: 7500 Cd

Control Angle: 83 Degrees

Luminaire Locations Tested (58)

Test Results: **PASS**

Table 2: Obtrusive Light (Pre Curfew) – Compliance Report from AGI



External Building Boundary

The modelled maximum spill on the boundary is eastern boundary and southern boundary 0.4 lux.

Obtrusive Light - Compliance Report

AS 4282-1997, Post-Curfew, Commercial

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Illuminance

Maximum Allowable Value: 4 Lux

Calculations Tested (5):

Calculation Label	Test Results	Max. Illum.
East Tce_Ill_Seg1	PASS	0.3
Murray Street_Ill_Seg1	PASS	0.1
Murray Street_Ill_Seg2	PASS	0.4
Murray Street_Ill_Seg3	PASS	0.4
South Terrace_Ill_Seg1	PASS	0.1

Luminous Intensity (Cd) At Vertical Planes

Maximum Allowable Value: 2500 Cd

Calculations Tested (5):

Calculation Label	Test Results
East Tce_Cd_Seg1	PASS
Murray Street_Cd_Seg1	PASS
Murray Street_Cd_Seg2	PASS
Murray Street_Cd_Seg3	PASS
South Terrace_Cd_Seg1	PASS

Table 3 : Obtrusive Light – Compliance Report from AGI

4.0 ILLUMINATION LEVEL ASSESSMENT

The illumination level on the proposed car park had been conducted based on the recommendation of AS1158.3.1.

Table 2.5 of AS1158.3.1 recommendation for a public access outdoor car park lighting category:
Parking space – P11c, for night time vehicle movement in consideration of apartments above. e
Designated disabled park – P12

Table 2.9 of AS1158.3.1 recommendation for illumination level:
Parking space – 3.5 lux in average with point horizontal illuminance.
Designated Disabled Park – above 14 lux, and greater than the average of the overall car park lux level.

Based on the proposed lighting layout, the lighting level measured:
Parking space – highest average lux level recorded is 13.20 lux.
Designated Disabled Park – average 22.64 lux.

Figure 5 & 6 provides the light level for the car park & disable park area of the property. The reading indicates that the design complies with the requirement of AS1158.3.1.

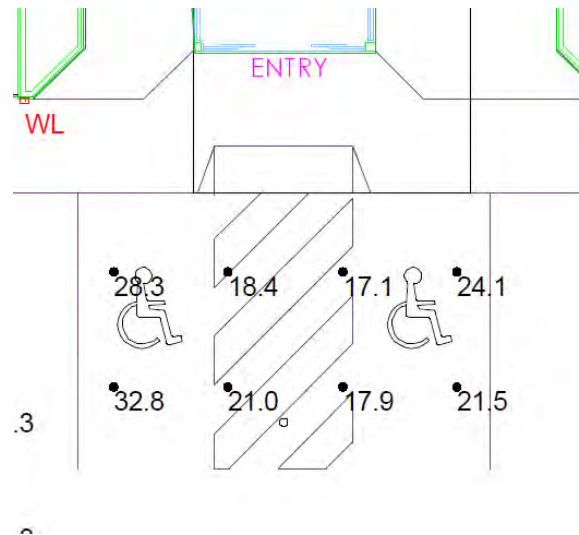






Fig 5: Designated Disable Park Lighting Level

Luminaire Schedule						
Symbol	Qty	Label	Total Lamp Lumens	LLF	Description	Lum. Watts
	1	A3	4000	0.800	PHILIPS TANGO G2 LED 40W FLOOD SWB	40
	4	A2	N.A.	0.800	PO29-LM059-T4-4 LIGMAN POWERMISSION 2 54W LED AREA LIGHT TYPE 4 OPTICS	59
	5	A1	N.A.	0.800	PO29-LM059-T3-4 LIGMAN POWERMISSION 2 54W LED AREA LIGHT TYPE 3 OPTICS	59
	48	WL	N.A.	0.800	Ligman GI-31601-W40 Gino 3 rectangular wall up-down light LED	16.5

Calculation Summary						
Project: Project_1						
Label	CalcType	Units	Avg	Max	Min	Min/Avg
CAR PARK	Illuminance	Lux	13.20	98.8	1.5	0.11
Disabled Zone	Illuminance	Lux	22.64	32.8	17.1	0.76
Walkway_Workplane	Illuminance	Lux	30.83	142	1.8	0.06

Expanded Luminaire Location Summary			
Project: AGI-Import 2			
LumNo	Label	X	Y
71	A1	-8.933	38.551

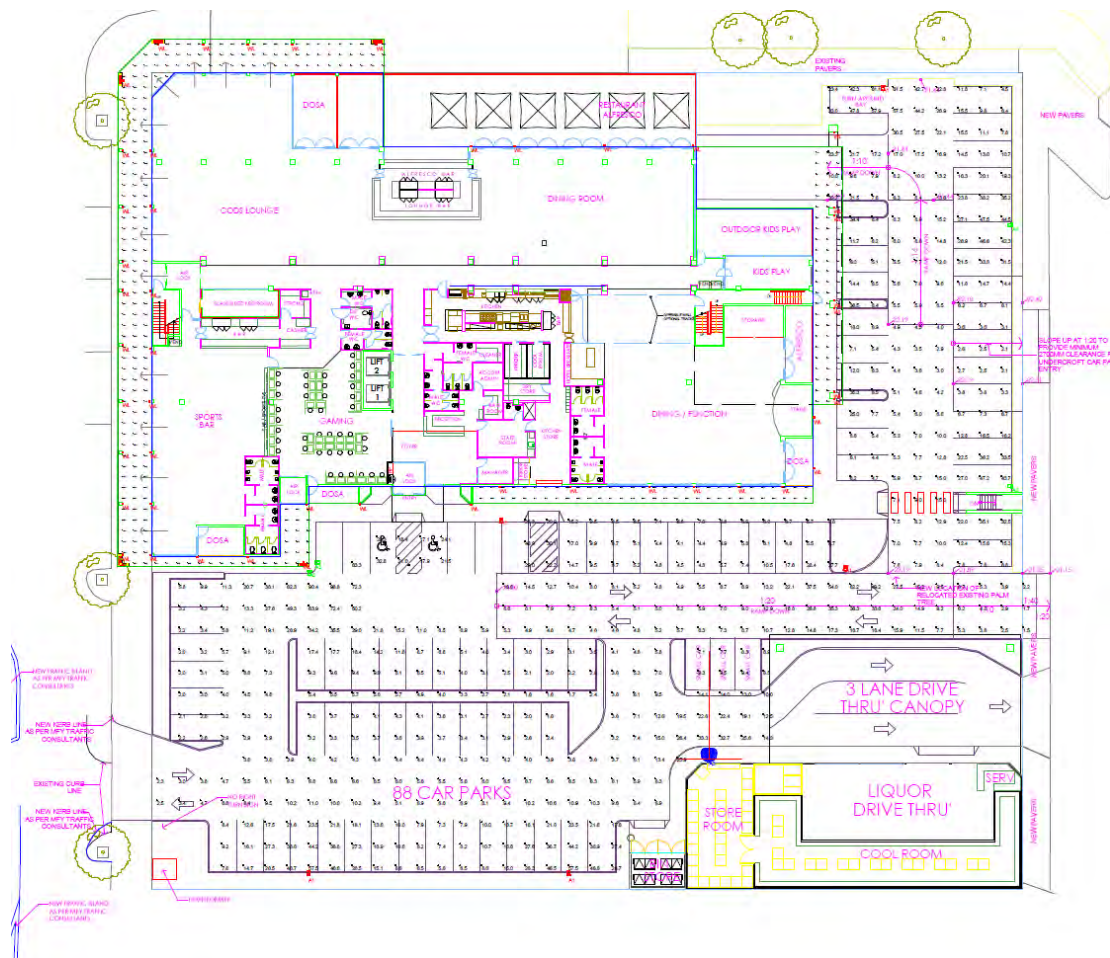


Fig 6: Car Park Lighting Level



5.0 CONCLUSION

The criteria for vertical light spills outline in AS 4282-1997 - Control of The obtrusive effects of outdoor lighting, Table 2.1:

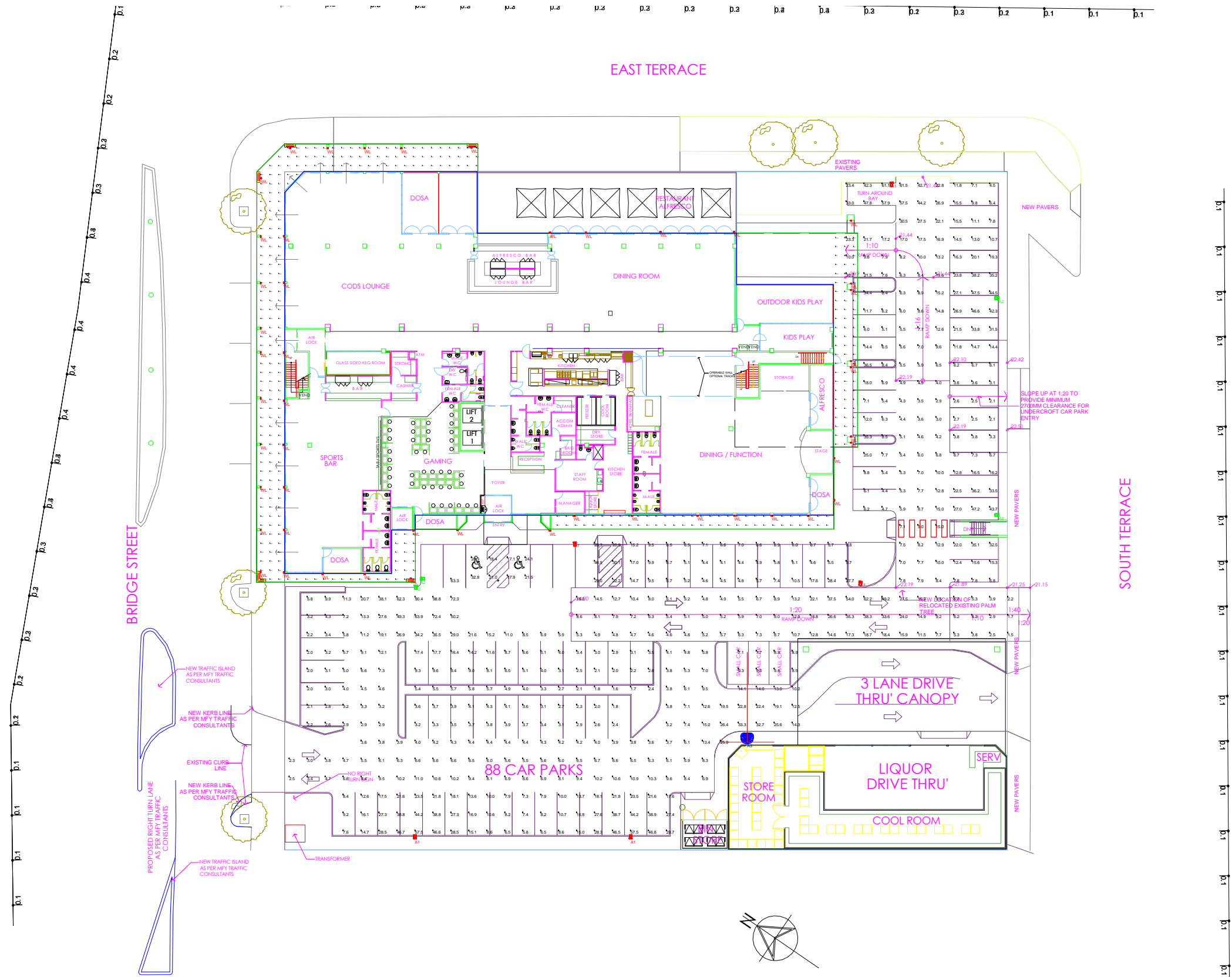
- Pre-curfew hour (between 6am – 11pm) - maximum of 25 lux across the boundary line,
- Curfew hour (between 11pm – 6am) – maximum of 4 lux across the boundary line.

The modelled maximum spill on the boundary for external lighting based on the above lighting layout and type complies with the criteria outlined in AS4282, AS 1158 P11c and AS 1158 P12 .

We trust the above is satisfactory. However, should there be any further clarifications/assistance please do not hesitate to contact the undersigned or Adrian Ko

For and on behalf of
TMK Consulting Engineers





Adrian Ko
SENIOR ELECTRICAL ENGINEER



CLIENT DETAIL TMK Consulting

PROJECT NAME Bridgeport Hotel
Spill Lighting from carpark & wall lights
03/10/2017
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DATE: 20/06/2017
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Luminaire Schedule						
Symbol	Qty	Label	Total Lamp Lumens	LLF	Description	Lum. Watts
	1	A3	4000	0.800	PHILIPS TANGO G2 LED 40W FLOOD SWB	40
	4	A2	N.A.	0.800	PO29-LM059-T4-4 LIGMAN POWERMISSION 2 54W LED AREA LIGHT TYPE 4 OPTICS	59
	5	A1	N.A.	0.800	PO29-LM059-T3-4 LIGMAN POWERMISSION 2 54W LED AREA LIGHT TYPE 3 OPTICS	59
	48	WL	N.A.	0.800	Ligman GI-31601-W40 Gino 3 rectangular wall up-down light LED	16.5

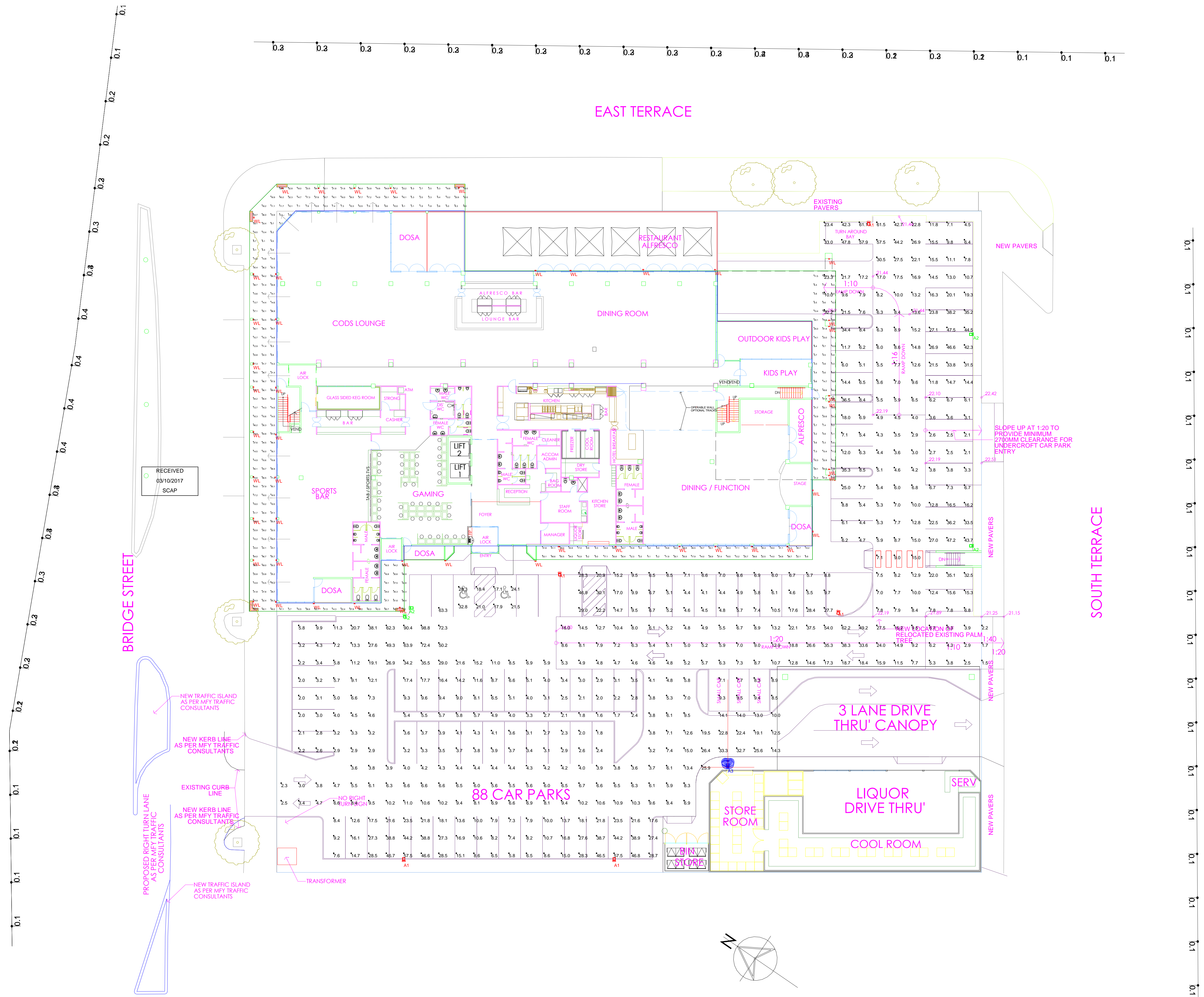
Calculation Summary						
Project: Project_1						
Label	CalcType	Units	Avg	Max	Min	Min/Avg
CAR PARK	Illuminance	Lux	13.20	98.8	1.5	0.11
Disabled Zone	Illuminance	Lux	22.64	32.8	17.1	0.76
Walkway_Workplane	Illuminance	Lux	30.83	142	1.8	0.06

Calculation Summary			
Project: Spill Lighting			
Label	CalcType	Units	Max
East Tce_Cd_Seg1	Obtrusive Light - Cd	N.A.	797
East Tce_Ill_Seg1	Obtrusive Light - Ill	Lux	0.4
Murray Street_Cd_Seg1	Obtrusive Light - Cd	N.A.	319
Murray Street_Cd_Seg2	Obtrusive Light - Cd	N.A.	367
Murray Street_Cd_Seg3	Obtrusive Light - Cd	N.A.	316
Murray Street_Ill_Seg1	Obtrusive Light - Ill	Lux	0.2
Murray Street_Ill_Seg2	Obtrusive Light - Ill	Lux	0.4
Murray Street_Ill_Seg3	Obtrusive Light - Ill	Lux	0.4
South Terrace_Cd_Seg1	Obtrusive Light - Cd	N.A.	205
South Terrace_Ill_Seg1	Obtrusive Light - Ill	Lux	0.1

Expanded Luminaire Location Summary					
Project: AGI-Import 2					
LumNo	Label	X	Y	Z	Tilt
71	A1	-8.933	38.551	4.5	0
72	A2	3.018	25.652	4.5	10
73	A2	3.018	1.515	4.5	10
74	A1	-62.006	-34.501	4.5	12
75	A1	-12.371	-5.867	4.5	10
76	A1	-44.202	-1.493	4.5	12
77	A1	-37.985	-34.501	4.5	12
78	A3	-25.006	-23.751	6	30
79	A2	-61.38	-5.565	4.5	5
80	A2	-61.909	-6.284	4.5	5

Rev B - 4.5m Mounting height to streetlights

<div><div>CLIENT DETAIL</div><div>TMK Consulting</div></div>	<div><div>PROJECT NAME</div><div>Bridgeport Hotel</div><div>Spill Lighting from carpark & wall lights</div></div>	<div><div>DATE:</div><div>20/06/2017</div></div> <div><div>SCALE:</div><div>NTS</div></div> <div><div>REV:</div><div>Page 2 of 2</div></div>	
<div><div>REVISION</div><div>03/10/2017</div><div>SCAP</div></div>			



Obtrusive Light - Compliance Report

AS 4282-1997, Post-Curfew, Commercial

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Illuminance

Maximum Allowable Value: 4 Lux

Calculations Tested (5):

<u>Calculation Label</u>	<u>Test Results</u>	<u>Max. Illum.</u>
East Tce_Ill_Seg1	PASS	0.4
Murray Street_Ill_Seg1	PASS	0.2
Murray Street_Ill_Seg2	PASS	0.4
Murray Street_Ill_Seg3	PASS	0.4
South Terrace_Ill_Seg1	PASS	0.1

Luminous Intensity (Cd) At Vertical Planes

Maximum Allowable Value: 2500 Cd

Calculations Tested (5):

<u>Calculation Label</u>	<u>Test Results</u>
East Tce_Cd_Seg1	PASS
Murray Street_Cd_Seg1	PASS
Murray Street_Cd_Seg2	PASS
Murray Street_Cd_Seg3	PASS
South Terrace_Cd_Seg1	PASS

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Obtrusive Light - Compliance Report

AS 4282-1997, Pre-Curfew, Commercial

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Illuminance

Maximum Allowable Value: 25 Lux

Calculations Tested (5):

Calculation Label	Test Results	Max. Illum.
East Tce_Ill_Seg1	PASS	0.4
Murray Street_Ill_Seg1	PASS	0.2
Murray Street_Ill_Seg2	PASS	0.4
Murray Street_Ill_Seg3	PASS	0.4
South Terrace_Ill_Seg1	PASS	0.1

Luminous Intensity (Cd) Per Luminaire

Maximum Allowable Value: 7500 Cd

Control Angle: 83 Degrees

Luminaire Locations Tested (58)

Test Results: PASS

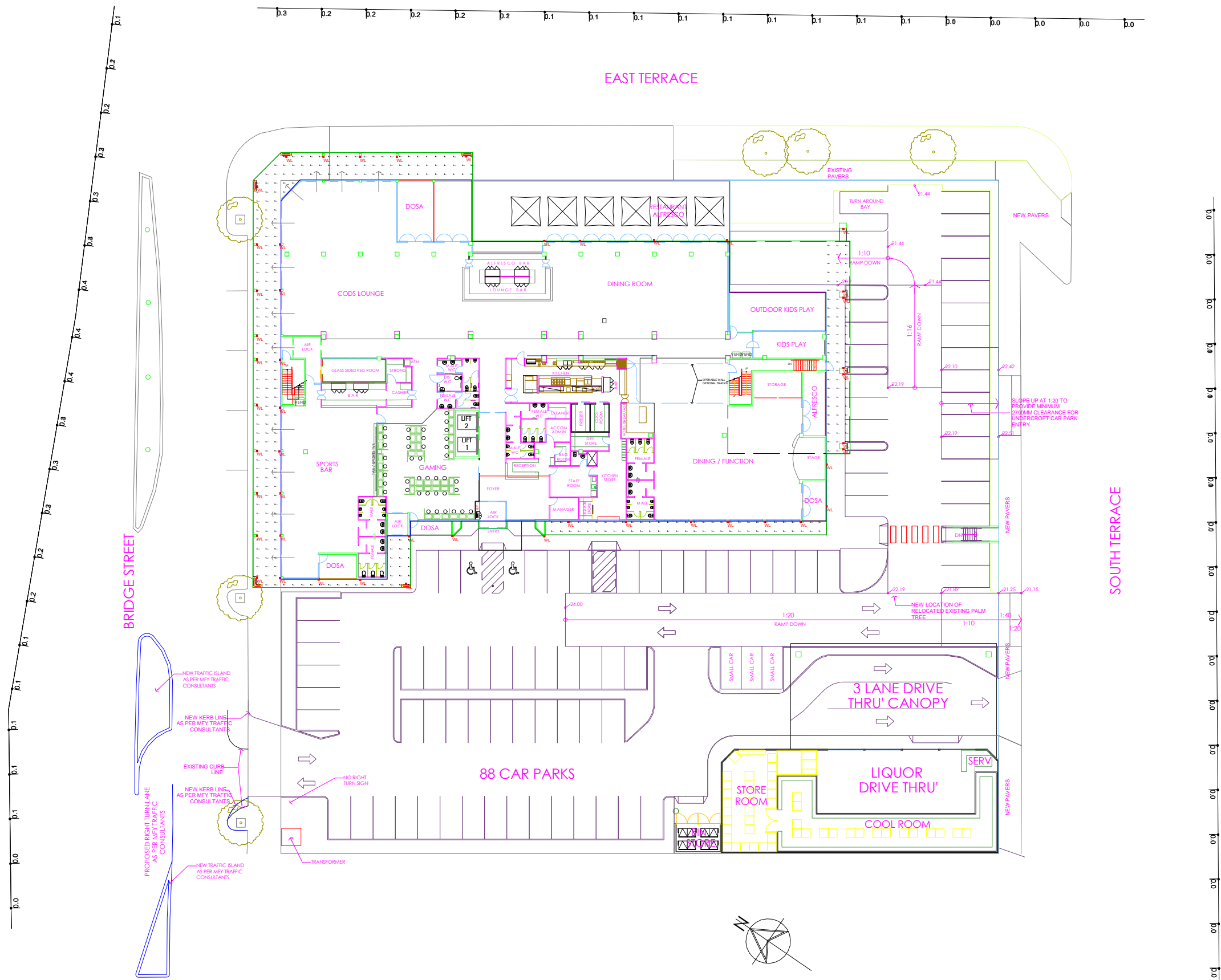
All Luminaire Locations (58):

Lum.No.	Label	Cd	Tilt	Roll	Spin
71	A1	66	0	0	0
75	A1	766	10	0	0
74	A1	1026	12	0	0
76	A1	1026	12	0	0
77	A1	1026	12	0	0
79	A2	702	5	0	0
80	A2	702	5	0	0
72	A2	1907	10	0	0
73	A2	1907	10	0	0
78	A3	36	5	0	0
1	WL	250	0	0	0
2	WL	250	0	0	0
3	WL	250	0	0	0
4	WL	250	0	0	0
5	WL	250	0	0	0
7	WL	250	0	0	0
10	WL	250	0	0	0
11	WL	250	0	0	0
12	WL	250	0	0	0
13	WL	250	0	0	0
14	WL	250	0	0	0
17	WL	250	0	0	0
18	WL	250	0	0	0
21	WL	250	0	0	0
22	WL	250	0	0	0
23	WL	250	0	0	0
32	WL	250	0	0	0
33	WL	250	0	0	0
37	WL	250	0	0	0
38	WL	250	0	0	0
39	WL	250	0	0	0
44	WL	250	0	0	0
45	WL	250	0	0	0
46	WL	250	0	0	0
47	WL	250	0	0	0
48	WL	250	0	0	0
49	WL	250	0	0	0
50	WL	250	0	0	0
51	WL	250	0	0	0
52	WL	250	0	0	0

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53	WL	250	0	0	0
54	WL	250	0	0	0
55	WL	250	0	0	0
56	WL	250	0	0	0
57	WL	250	0	0	0
58	WL	250	0	0	0
59	WL	250	0	0	0
60	WL	250	0	0	0
61	WL	250	0	0	0
62	WL	250	0	0	0
63	WL	250	0	0	0
64	WL	250	0	0	0
65	WL	250	0	0	0
66	WL	250	0	0	0
67	WL	250	0	0	0
68	WL	250	0	0	0
69	WL	250	0	0	0
70	WL	250	0	0	0

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PROJECT NAME

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DATE: 16/06/2017

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Luminaire Schedule						
Symbol	Qty	Label	Total Lamp Lumens	LLF	Description	Lum. Watts
-	48	WL	N.A.	0.800	GI-31601-W40 Rev_2	16.5

Calculation Summary						
Project: Project_1						
Label	CalcType	Units	Avg	Max	Min	Min/Avg
Walkway_Workplane	Illuminance	Lux	28.46	126	1.8	0.06

Calculation Summary			
Project: Spill Lighting			
Label	CalcType	Units	Max
East Tce_Cd_Seg1	Obtrusive Light - Cd	N.A.	5
East Tce_Ill_Seg1	Obtrusive Light - Ill	Lux	0.3
Murray Street_Cd_Seg1	Obtrusive Light - Cd	N.A.	2
Murray Street_Cd_Seg2	Obtrusive Light - Cd	N.A.	3
Murray Street_Cd_Seg3	Obtrusive Light - Cd	N.A.	5
Murray Street_Ill_Seg1	Obtrusive Light - Ill	Lux	0.1
Murray Street_Ill_Seg2	Obtrusive Light - Ill	Lux	0.4
Murray Street_Ill_Seg3	Obtrusive Light - Ill	Lux	0.4
South Terrace_Cd_Seg1	Obtrusive Light - Cd	N.A.	2
South Terrace_Ill_Seg1	Obtrusive Light - Ill	Lux	0.1

Obtrusive Light - Compliance Report

AS 4282-1997, Post-Curfew, Commercial
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Illuminance

Maximum Allowable Value: 4 Lux

Calculations Tested (5):

Calculation Label	Test Results	Max. Illum.
East Tce_Ill_Seg1	PASS	0.3
Murray Street_Ill_Seg1	PASS	0.1
Murray Street_Ill_Seg2	PASS	0.4
Murray Street_Ill_Seg3	PASS	0.4
South Terrace_Ill_Seg1	PASS	0.1

Luminous Intensity (Cd) At Vertical Planes

Maximum Allowable Value: 2500 Cd

Calculations Tested (5):

Calculation Label	Test Results
East Tce_Cd_Seg1	PASS
Murray Street_Cd_Seg1	PASS
Murray Street_Cd_Seg2	PASS
Murray Street_Cd_Seg3	PASS
South Terrace_Cd_Seg1	PASS

<u>CLIENT DETAIL</u>	TMK Consulting	<u>PROJECT NAME</u>	Bridgeport Hotel	DATE: 16/06/2017 SCALE: NTS REV: Page 2 of 2
			Spill Lighting from wall lights only	
		REVISION 03/10/2017 SCAP		

Obtrusive Light - Compliance Report

AS 4282-1997, Post-Curfew, Commercial

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Illuminance

Maximum Allowable Value: 4 Lux

Calculations Tested (5):

<u>Calculation Label</u>	<u>Test Results</u>	<u>Max. Illum.</u>
East Tce_Ill_Seg1	PASS	0.3
Murray Street_Ill_Seg1	PASS	0.1
Murray Street_Ill_Seg2	PASS	0.4
Murray Street_Ill_Seg3	PASS	0.4
South Terrace_Ill_Seg1	PASS	0.1

Luminous Intensity (Cd) At Vertical Planes

Maximum Allowable Value: 2500 Cd

Calculations Tested (5):

<u>Calculation Label</u>	<u>Test Results</u>
East Tce_Cd_Seg1	PASS
Murray Street_Cd_Seg1	PASS
Murray Street_Cd_Seg2	PASS
Murray Street_Cd_Seg3	PASS
South Terrace_Cd_Seg1	PASS

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TMK CONSULTING ENGINEERS

1606131_SMR/B
17th July 2017



STORMWATER MANAGEMENT REPORT

PROPOSED BRIDGEPORT HOTEL
2 BRIDGE STREET
MURRAY BRIDGE SA 5253

prepared for

BUILDWISE PTY LTD
c/o LIVING CHOICE



Principal:	BUILDWISE PTY LTD c/o LIVING CHOICE	Job Number:	1606131
Contractor:	T.B.A	Development No:	T.B.A
Project Title:	PROPOSED BRIDGEPORT HOTEL	Date:	17 th July 2017
Site:	2 BRIDGE STREET, MURRAY BRIDGE 5253		

This is a Stormwater Management Report giving specific recommendations for construction of the Proposed Hotel at the above site.

This report must be read in conjunction with all listed attachments. Changes to the design or construction must not be made without further written advice from the Engineer.

This report is valid for a period of 24 months, based on current standards and regulations, etc.

GENERAL NOTES:

1. These calculations are to be read in conjunction with the relevant associated Civil Drawings and / or details.
2. All work is to comply with relevant SAA Standards and Guides.

AS/NZS 3500.3: Part 3 Stormwater Drainage
Australian Rainfall and Run-off Volume 8: Urban Stormwater Management
Australian Runoff Quality – A Guide to Water Sensitive Urban Design
Storm drainage design in small urban catchments: A handbook for Australian practice
Water Sensitive Urban Design (WSUD) Engineering Procedure: Stormwater
Water Services Association of Australia Code (WSAA).

ATTACHMENTS: Stormwater Management Report
Civil Plan
Stormwater Calculations (SW1-SW6)

For and on behalf of
TMK Consulting Engineers

BENOIT HOPKINS
Engineer

A. General

This report details the stormwater management strategies for the proposed hotel located at 2 Bridge Street, Murray Bridge SA, 5253 (*Refer Figure 1 – Site Aerial View*). The objective of the report is to demonstrate how stormwater runoff would be captured and conveyed from the subject site safely to the receiving drainage network while considering stormwater quality management and the incorporation of Water Sensitive Urban Design (WSUD) elements.

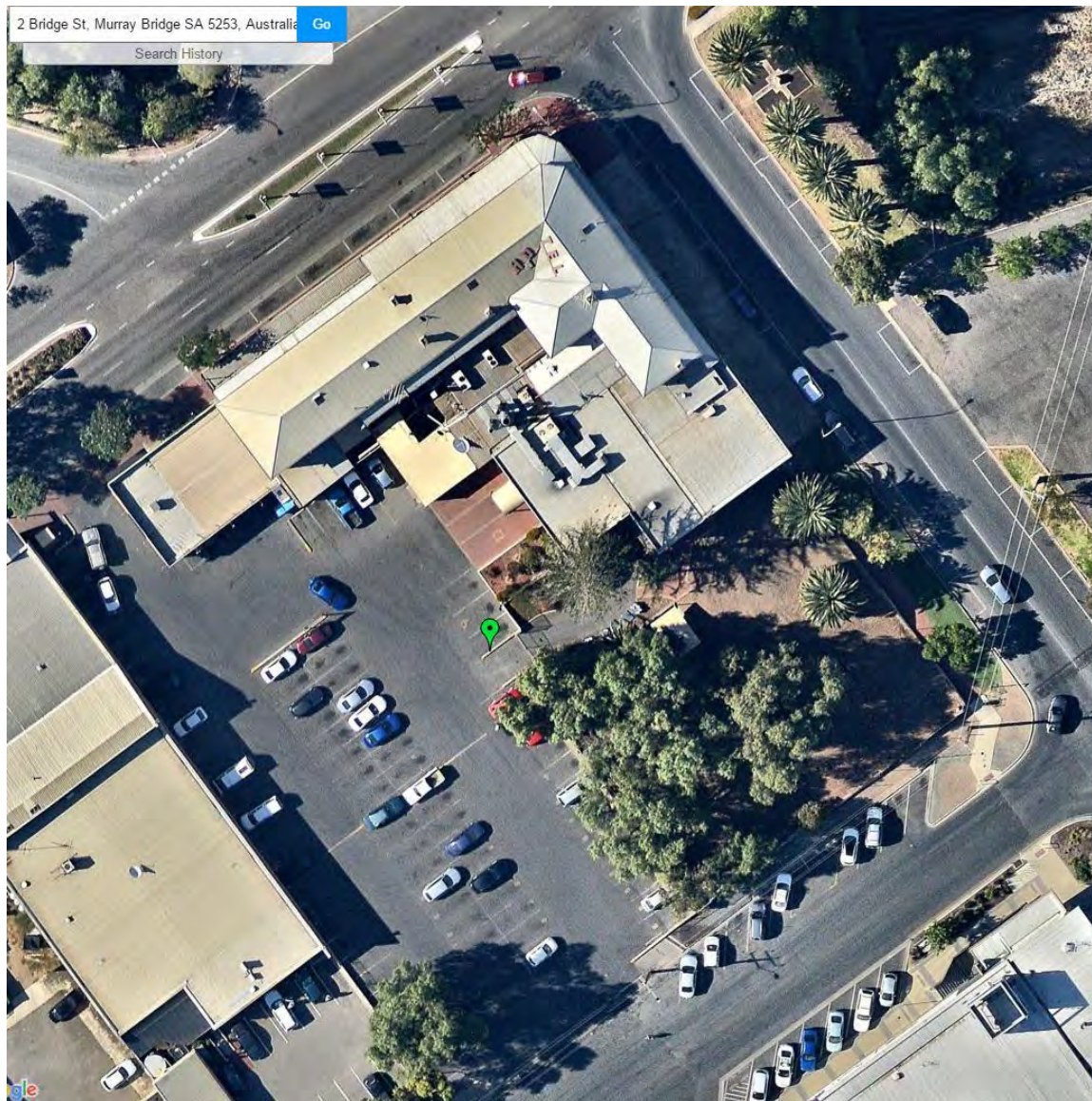


Figure 1 - Site Aerial View (Source: nearmaps.com)

B. Stormwater Drainage

OVERVIEW

The entire site covers an area of approximately 6700m² with an existing dwelling, paved areas and bitumen car park accounting for an impervious area of approximately 4820m². The site in its current state discharges to the existing Council stormwater network located on South Terrace. The proposed development would result in an increase of impervious area of approximately 15%.

Stormwater from the site would be routed through the proposed conventional underground drainage system comprising of Grated Sumps (GS), Side Entry Pits (SEP) and associated pipes, coupled with the use of WSUD elements including on-site detention storage, at-source pollution filters and a Gross Pollutant Trap (GPT) (refer 'Appendix A' for Civil plan).

The subject site has not been identified as being located within a flood plain in accordance with DEWNR Mapping.



The stormwater management report is prepared in accordance with the design criteria listed below:

- The stormwater drainage system is designed using Bureau of Meteorology (BoM) published rainfall Intensity Frequency Duration (IFD) data as a minor / major system to accommodate the 1 in 10 / 1 in 100 year ARI rainfall events.
- The flow rate of stormwater leaving the site shall be designed so that it does not exceed the pre-development flow rate for both the minor and major rainfall events.
- The total site discharges are modeled as described in *Storm Drainage Design in Small Urban Catchments*, a handbook for Australian practice by John Argue & *Australian Rainfall and Runoff (ARR87)*, Book Eight - Urban Stormwater Management.

STORMWATER QUANTITY MANAGEMENT

Stormwater from the proposed development will drain into a series of oversized Reinforced Concrete Pipes (RCP) designed to detain and reduce runoff to pre-development rates, before ultimately discharging to the existing drainage network located on South Terrace.

PRE-DEVELOPMENT FLOW CALCULATIONS

Pre-development flow rates were determined using the Rational Method to calculate the peak flow rates for both the minor 1 in 10 year and major 1 in 100 year ARI rainfall events. The results are summarized below (refer 'Appendix B' for calculations).

Pre-Developed Flow Rates

$Q_{10} = 86.3 \text{ L/sec}$

$Q_{100} = 144.4 \text{ L/sec}$

POST-DEVELOPMENT FLOW CALCULATIONS:

Post-development flow rates were determined using the Rational Method to calculate the stormwater discharge from the site and volume of detention required to limit peak flows to pre-developed rates for both the minor 1 in 10 year and major 1 in 100 year ARI rainfall events. The results are summarized below (refer to 'Appendix A' for calculations).

	Minor Storm (Q_{10})	Major Storm (Q_{100})
Maximum flow into storage tank	85.0 L/sec	146.1 L/sec
Un-detained flows	5.8 L/sec	9.9 L/sec
Maximum flow into existing stormwater network	80.4 L/sec	125.3 L/sec
Total post-development site discharge	86.2 L/sec	135.2 L/sec
Total detention volume (minimum)	9.1 m ³	21.6 m ³

The post-development stormwater calculations demonstrate that the proposed stormwater detention system will have sufficient capacity to restrict post-development stormwater discharge to the pre-development rates for both the minor and major rainfall events.

C. Stormwater Quality Management

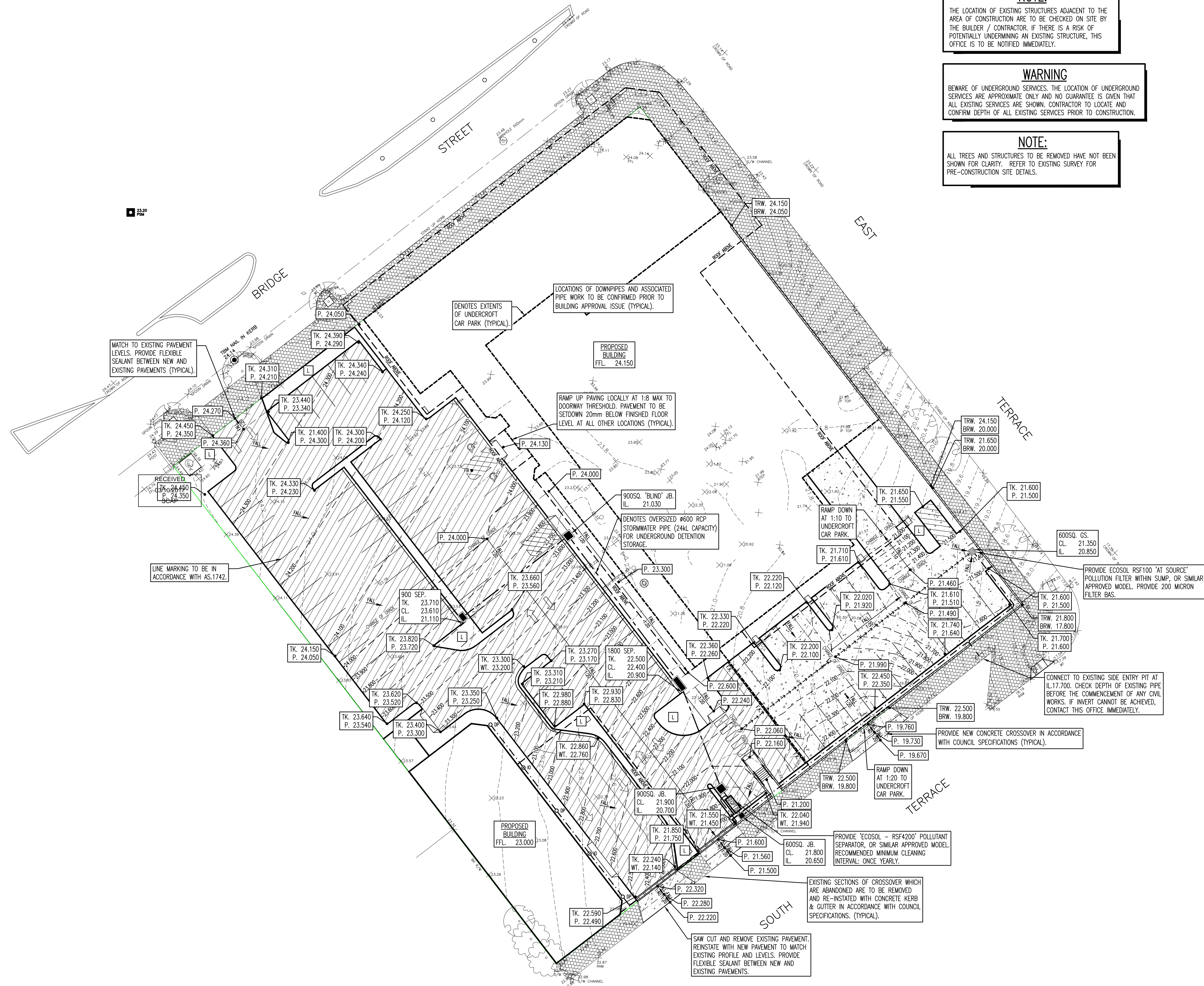
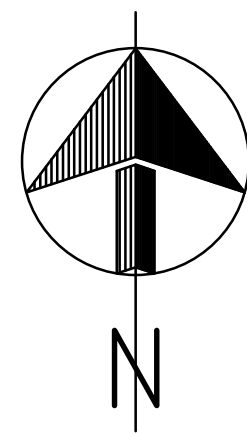
The key strategies planned to manage the quality of stormwater discharged from the proposed extension include the use of a number of WSUD elements including on-site detention storage to reduce peak flow rates and a GPT to ensure water quality improvement targets are achieved.

An Ecosol RSF 4200 GPT is proposed as the primary treatment method. The manufacturer specifications indicate a treatment effectiveness of 99% reduction of Gross Pollutants (GP), 80% reduction of Total Suspended Solids (TSS) and 45% reduction of Total Phosphorous (TP) and Total Nitrogen (TN) for the range of treatable flow.



APPENDIX A

Civil Plan



NOTE:

THE LOCATION OF EXISTING STRUCTURES ADJACENT TO THE AREA OF CONSTRUCTION ARE TO BE CHECKED ON SITE BY THE BUILDER / CONTRACTOR. IF THERE IS A RISK OF POTENTIALLY UNDERMINING AN EXISTING STRUCTURE, THIS OFFICE IS TO BE NOTIFIED IMMEDIATELY.

WARNING

BEWARE OF UNDERGROUND SERVICES. THE LOCATION OF UNDERGROUND SERVICES ARE APPROXIMATE ONLY AND NO GUARANTEE IS GIVEN THAT ALL EXISTING SERVICES ARE SHOWN. CONTRACTOR TO LOCATE AND CONFIRM DEPTH OF ALL EXISTING SERVICES PRIOR TO CONSTRUCTION.

NOTE:

ALL TREES AND STRUCTURES TO BE REMOVED HAVE NOT BEEN SHOWN FOR CLARITY. REFER TO EXISTING SURVEY FOR PRE-CONSTRUCTION SITE DETAILS.

LEGEND

- ASPHALT PAVEMENT
- SELECTED PEDESTRIAN BLOCK PAVING
- CONCRETE PAVEMENT
- SELECTED PAVEMENT (REFER ARCHITECTURAL)
- LANDSCAPING AREA
- EXISTING SPOT LEVEL
- EXISTING CONTOUR
- DESIGN CONTOUR
- 100 HIGH CONCRETE KERB & GUTTER
- CONCRETE FLUSH KERB
- STORMWATER PIPE @ 0.5% MIN U.N.O.
- STORMWATER PIPE @ 0.5% MIN (SEALED) U.N.O.
- FIXED BOLLARD
- SELECTED DOWNPIPE
- SURFACE INSPECTION OPENING
- (GS) 450SQ. GRATED SUMP (UNO)
- (JB) 450SQ. JUNCTION BOX (UNO)
- (GS) 600SQ. GRATED SUMP (UNO)
- (GS) 900SQ. GRATED SUMP (UNO)
- (JB) 900SQ. JUNCTION BOX (UNO)
- SIDE ENTRY PIT (S.E.P)
- SELECTED RETAINING WALL
- 300 WIDE GRATED TRENCH DRAIN
- 300 WIDE CONCRETE SPOON DRAIN (MIN. FALL 1:200)
- DESIGN LEVEL
- TK. TOP OF KERB
- WT. WATER TABLE
- P. PAVEMENT
- TRW. TOP OF RETAINING WALL
- B.R.W. BOTTOM OF RETAINING WALL
- C.L. COVER LEVEL
- IL. INVERT LEVEL
- F.F.L. FINISHED FLOOR LEVEL
- B.L. BENCH LEVEL
- N.S. NATURAL SURFACE LEVEL

NOTE:

THIS IS A PLANNING DRAWING ONLY, FURTHER ENGINEERING DESIGN INCLUDING SPECIFICATIONS, SIZING AND STORMWATER INVERTS TO BE PROVIDED PRIOR TO BUILDING RULES ASSESSMENT AND CONSTRUCTION.

B:	REVISED PLANNING APPROVAL ISSUE	BH	ASM	17.07.17
A:	PLANNING APPROVAL ISSUE	BH	ASM	19.07.16
No	REVISION	DRAWN	CHE'KD	DATE

PROJECT
PROPOSED BRIDGEPORT HOTEL
2 BRIDGE STREET
MURRAY BRIDGE SA 5253
FOR: BUILDWISE PTY LTD

DRAWING TITLE
CIVIL PLAN - GROUND LEVEL

Civil

Environmental

Mechanical

Fire

Lifts

Structural

Geotechnical

Electrical

Hydraulics

Green ESD

Level 6, 100 Pirie Street,
Adelaide SA 5000
Telephone 08 8238 4100
Facsimile 08 8410 1405

SCALES	AS SHOWN	© COPYRIGHT RESERVED
DRAWN	IH	DRAWING No.
DATE	JUNE 2016	1606131- C1/PB
ENGINEER	BH	
CHECKED	ASM	

SURVEYED: BY OTHERS

1:250 @ A1





APPENDIX B

Stormwater Calculations



Ref.: 1606131

Date: 06-Jul-16

Design: BH

Page: SW1

STORMWATER CALCULATIONS - DETERMINATION OF DETENTION TANK SIZES

Design Storm Intensity Table (mm/hr) - (from <http://www.bom.gov.au/cgi-bin/hydro/has/CDIRSWebBasic>)

STORM LOCATION

Adelaide

Latitude 34°59'S; Longitude 138°44'E

Adelaide

DURATION	AVERAGE RETURN INTERVAL (YEARS)						
	1	2	5	10	20	50	100
5 mins	52.60	69.00	89.40	104.00	123.00	151.00	175.00
6 mins	49.10	64.50	83.50	96.70	115.00	141.00	163.00
10 mins	39.80	52.00	66.90	77.10	91.30	112.00	129.00
20 mins	28.30	36.80	46.80	53.40	62.80	76.20	87.30
30 mins	22.70	29.40	37.10	42.30	49.50	59.80	68.30
1 hour	15.20	19.70	24.60	27.90	32.50	39.00	44.40
2 hours	10.10	13.00	16.20	18.30	21.20	25.40	28.90
3 hours	7.92	10.20	12.70	14.30	16.60	19.90	22.50
6 hours	5.24	6.75	8.37	9.44	11.00	13.10	14.90
12 hours	3.45	4.44	5.52	6.23	7.23	8.65	9.83
24 hours	2.22	2.86	3.58	4.05	4.72	5.66	6.44
48 hours	1.38	1.78	2.24	2.55	2.98	3.60	4.11
72 hours	1.02	1.32	1.67	1.90	2.23	2.70	3.10

PRE-DEVELOPMENT

Design ARI (yr) 10 yr
 Design Duration (min) 10 min
 Design Storm Intensity (mm/hr) 77.10 mm/hr

Design Parameters	C	Area (m ²)	% Area Detained
Roof	0.90	1765	0.00
Paving	0.75	3055	0.00
Grass / Landscaping	0.12	1250	0.00

Total Area 6070 m²

Weighted C (C_w)* 0.66

Max pre-development flow $Q_i = \Sigma(C_i A_i / 3600)$:

Roof	34.02 L/s
Paving	49.07 L/s
Grass / Landscaping	3.21 L/s

Pre-development flow = 86.30 L/s

Use Max Allowable Flow = 86.30 L/s

Note: The weighted average value of the runoff coefficient, C_w, includes roof, paving, grassed and landscaped areas of the site.

POST-DEVELOPMENT (*Determine Critical Storm)

Design ARI (yr) 10 yr
 Design Duration (min) 13 min
 Design Storm Intensity (mm/hr) 69.99 mm/hr

Design Parameters	C	Area (m ²)	% Area Detained
Roof	0.90	2065	100.00
Paving	0.75	3705	90.00
Grass / Landscaping	0.12	300	50.00

Total Area 6070 m²

Weighted C (C_w)* 0.77

Summary of Design Flows

Undetained flow, $Q_u = \Sigma(C_i A_i / 3600)$:

Roof	0.00 L/s
Paving	5.40 L/s
Grass / Landscaping	0.35 L/s

Design undetained flow = 5.75 L/s

Max. outflows from detention tanks:

Site water runoff	
Pumped	0.00 L/s
Piped	0.00 L/s
Roof water runoff	
Orifice-restricted	80.39 L/s

Total Max. Design Outflow = 86.15 L/s

< 86.30 L/s Allowed ∴ OK

Duration (mins)	Intensity (mm/hr)	Proposed Inflow Rate (L/s)	Proposed Inflow Rate x Duration (L)	Orifice Outflow Rate (L/s)	Orifice Outflow Volume (L)	Net Storage (L)
5	175.00	211.9	63,574	-134.24	-24,163	39,411
6	163.00	197.4	71,058	-134.24	-28,996	42,062
7	154.50	187.1	78,578	-134.24	-33,828	44,749
8	146.00	176.8	84,863	-134.24	-38,661	46,201
9	137.50	166.5	89,912	-134.24	-43,494	46,418
10	129.00	156.2	93,727	-134.24	-48,326	45,400
11	124.83	151.2	99,766	-134.24	-53,159	46,607
12	120.66	146.1	105,200	-134.24	-57,992	47,209
13	116.49	141.1	110,028	-134.24	-62,824	47,204
14	112.32	136.0	114,251	-134.24	-67,657	46,594
15	108.15	131.0	117,867	-134.24	-72,490	45,377
16	103.98	125.9	120,877	-134.24	-77,322	43,555
17	99.81	120.9	123,281	-134.24	-82,155	41,126
18	95.64	115.8	125,079	-134.24	-86,988	38,092
19	91.47	110.8	126,271	-134.24	-91,820	34,451
20	87.30	105.7	126,858	-134.24	-96,653	30,205
25	77.80	94.2	141,316	-134.24	-120,816	20,500
30	68.30	82.7	148,873	-134.24	-144,979	3,893
35	64.32	77.9	163,555	-134.24	-169,142	5,587
40	60.33	73.1	175,344	-134.24	-193,306	17,962
45	56.35	68.2	184,238	-134.24	-217,469	33,231
50	52.37	63.4	190,238	-134.24	-241,632	51,394
55	48.38	58.6	193,344	-134.24	-265,795	72,451
60	44.40	53.8	193,556	-134.24	-289,958	96,402
75	40.53	49.1	220,830	-134.24	-362,448	141,618
90	36.65	44.4	239,657	-134.24	-434,938	195,281



Ref.: 1606131
Date: 06-Jul-16
Design: BH
Page: SW6

CRITICAL STORM DURATION ORIFICE OUTFLOW

Critical Storm Duration = 12 mins
Max Storage Volume = 47,209 Litres

***Notes:** This calculation is used for determining the critical storm duration at the location shown on the main calculation sheet for the case where the maximum outflow rate from the tank orifice is as shown in the table.

The Net Storage Volume values shown in this table are approximate. The design uses results obtained from calculations where the effects of the change in head in the storage tank and the Time of Concentration have been included.

The critical storm results are shown highlighted in bold italics.



Ref.: 1606131
Date: 06-Jul-16
Design: BH
Page: SW2

STORMWATER CALCULATIONS - STORMWATER RUN-OFF - ORIFICE-RESTRICTED

Time of Concentration, T_C

Design Storm ARI (yr) 10 yr (from page SW1)
Design Storm Duration (mins) 13 mins (from page SW1)
Design Storm Intensity, i 69.99 mm/hr (from page SW1)

=> For run-off calculations, use $T_C =$

8.0 mins

<=Duration ∴ Use $T_C = 8$ mins.

Design Parameters	C	Area (m ²)	% Area Detained
Roof	0.90	2065	100.00
Paving	0.75	3705	90.00
Grass / Landscaping	0.12	300	0.00

Detained flow, $Q_d = \Sigma(CiA_d/3600)$:

Roof	36.13 L/s
Paving	48.62 L/s
Grass / Landscaping	0.00 L/s
Design detained flow =	84.75 L/s

=> Try the following retention tank design parameters:

Max Allowable outflow from storage =	80.55 L/s	(0.00 L/s is still available for outflow)
Number of orifice detention tanks	1	Each tank volume above orifice 24,000 L
Max. head allowed above orifice	1,450 mm	=> Each Tank Plan Area = 16.55 m ²
Outlet coefficient, C_o	0.6	
Orifice diameter, D_o	225 mm	=> Orifice area, $A_o = \pi(D_o/2)^2$ 39,761 mm ²

Graph Time v Flow:

Time (mins)	InFlow (L/s)	OutFlow (L/s)
0	0.00	0.00
8	84.75	60.41
13	84.75	79.81
21	0.00	0.00

Max. Calculated Outflow:

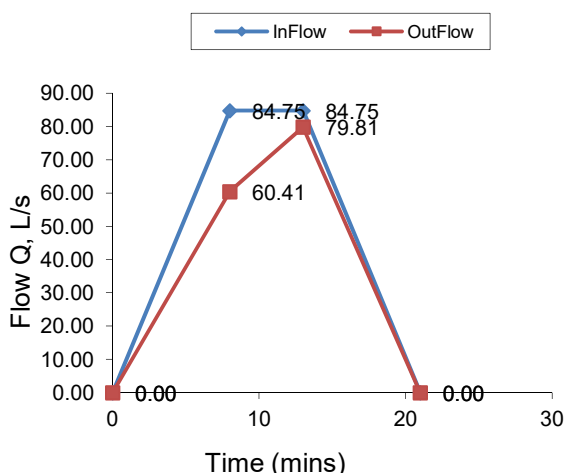
$Q_{max_{out}} =$ **80.39** L/s

< 80.55 L/s Max. Allowed ∴ OK

Max. Calculated Head of Water:

$H =$ **547** mm

< 1,450 mm Max. Allowed ∴ OK



=> Volume of Water To Be Detained:

$V =$ **9,058** L
i.e. $V =$ **9.06** m³

**=>USE 54m 600RCP = 24kL STORAGE VOLUME
225mm ORIFICE OUTLET PIPE**

Duration (mins)	Intensity (mm/hr)	Proposed Inflow Rate (L/s)	Proposed Inflow Rate x Duration (L)	Orifice Outflow Rate (L/s)	Orifice Outflow Volume (L)	Net Storage (L)
5	104.00	125.9	37,781	-80.55	-14,499	23,282
6	96.70	117.1	42,155	-80.55	-17,399	24,756
7	91.80	111.2	46,689	-80.55	-20,299	26,390
8	86.90	105.2	50,511	-80.55	-23,198	27,312
9	82.00	99.3	53,620	-80.55	-26,098	27,522
10	77.10	93.4	56,018	-80.55	-28,998	27,020
11	74.73	90.5	59,726	-80.55	-31,898	27,828
12	72.36	87.6	63,089	-80.55	-34,798	28,291
13	69.99	84.8	66,108	-80.55	-37,697	28,410
14	67.62	81.9	68,782	-80.55	-40,597	28,185
15	65.25	79.0	71,112	-80.55	-43,497	27,615
16	62.88	76.1	73,098	-80.55	-46,397	26,701
17	60.51	73.3	74,739	-80.55	-49,297	25,443
18	58.14	70.4	76,036	-80.55	-52,196	23,840
19	55.77	67.5	76,989	-80.55	-55,096	21,893
20	53.40	64.7	77,597	-80.55	-57,996	19,601
25	47.85	57.9	86,915	-80.55	-72,495	14,420
30	42.30	51.2	92,201	-80.55	-86,994	5,207
35	39.90	48.3	101,464	-80.55	-101,493	29
40	37.50	45.4	108,984	-80.55	-115,992	7,008
45	35.10	42.5	114,761	-80.55	-130,491	15,730
50	32.70	39.6	118,793	-80.55	-144,990	26,197
55	30.30	36.7	121,082	-80.55	-159,489	38,407
60	27.90	33.8	121,627	-80.55	-173,988	52,361
75	25.50	30.9	138,955	-80.55	-217,485	78,530
90	23.10	28.0	151,052	-80.55	-260,982	109,930



Ref.: 1606131
Date: 06-Jul-16
Design: BH
Page: SW3

CRITICAL STORM DURATION ORIFICE OUTFLOW

Critical Storm Duration = 13 mins
Max Storage Volume = 28,410 Litres

***Notes:** This calculation is used for determining the critical storm duration at the location shown on the main calculation sheet for the case where the maximum outflow rate from the tank orifice is as shown in the table.

The Net Storage Volume values shown in this table are approximate. The design uses results obtained from calculations where the effects of the change in head in the storage tank and the Time of Concentration have been included.

The critical storm results are shown highlighted in bold italics.



Ref.: 1606131

Date: 06-Jul-16

Design: BH

Page: SW4

STORMWATER CALCULATIONS - DETERMINATION OF DETENTION TANK SIZES

Design Storm Intensity Table (mm/hr) - (from <http://www.bom.gov.au/cgi-bin/hydro/has/CDIRSWebBasic>)

STORM LOCATION

Adelaide

Latitude 34°59'S; Longitude 138°44'E

Adelaide

DURATION	AVERAGE RETURN INTERVAL (YEARS)						
	1	2	5	10	20	50	100
5 mins	52.60	69.00	89.40	104.00	123.00	151.00	175.00
6 mins	49.10	64.50	83.50	96.70	115.00	141.00	163.00
10 mins	39.80	52.00	66.90	77.10	91.30	112.00	129.00
20 mins	28.30	36.80	46.80	53.40	62.80	76.20	87.30
30 mins	22.70	29.40	37.10	42.30	49.50	59.80	68.30
1 hour	15.20	19.70	24.60	27.90	32.50	39.00	44.40
2 hours	10.10	13.00	16.20	18.30	21.20	25.40	28.90
3 hours	7.92	10.20	12.70	14.30	16.60	19.90	22.50
6 hours	5.24	6.75	8.37	9.44	11.00	13.10	14.90
12 hours	3.45	4.44	5.52	6.23	7.23	8.65	9.83
24 hours	2.22	2.86	3.58	4.05	4.72	5.66	6.44
48 hours	1.38	1.78	2.24	2.55	2.98	3.60	4.11
72 hours	1.02	1.32	1.67	1.90	2.23	2.70	3.10

PRE-DEVELOPMENT

Design ARI (yr) 100 yr
 Design Duration (min) 10 min
 Design Storm Intensity (mm/hr) 129.00 mm/hr

Design Parameters	C	Area (m ²)	% Area Detained
Roof	0.90	1765	0.00
Paving	0.75	3055	0.00
Grass / Landscaping	0.12	1250	0.00
Total Area		6070 m²	
Weighted C (C _w)*	0.66		

Max pre-development flow $Q_i = \Sigma(C_i A_i / 3600)$:

Roof	56.92 L/s
Paving	82.10 L/s
Grass / Landscaping	5.38 L/s

Pre-development flow = 144.40 L/s

Use Max Allowable Flow = 144.40 L/s

Note: The weighted average value of the runoff coefficient, C_w, includes roof, paving, grassed and landscaped areas of the site.

POST-DEVELOPMENT (*Determine Critical Storm)

Design ARI (yr) 100 yr
 Design Duration (min) 12 min
 Design Storm Intensity (mm/hr) 120.66 mm/hr

Design Parameters	C	Area (m ²)	% Area Detained
Roof	0.90	2065	100.00
Paving	0.75	3705	90.00
Grass / Landscaping	0.12	300	50.00
Total Area		6070 m²	
Weighted C (C _w)*	0.77		

Summary of Design Flows

Undetained flow, $Q_u = \Sigma(C_i A_{iu} / 3600)$:

Roof	0.00 L/s
Paving	9.31 L/s
Grass / Landscaping	0.60 L/s

Design undetained flow = 9.92 L/s

Max. outflows from detention tanks:

Site water runoff	
Pumped	0.00 L/s
Piped	0.00 L/s
Roof water runoff	
Orifice-restricted	125.29 L/s

Total Max. Design Outflow = 135.21 L/s

< 144.40 L/s Allowed ∴ OK



Ref.: 1606131
Date: 06-Jul-16
Design: BH
Page: SW5

STORMWATER CALCULATIONS - STORMWATER RUN-OFF - ORIFICE-RESTRICTED

Time of Concentration, T_C

Design Storm ARI (yr) 100 yr (from page SW4)
Design Storm Duration (mins) 12 mins (from page SW4)
Design Storm Intensity, i 120.66 mm/hr (from page SW4)

=> For run-off calculations, use $T_C =$

8.0 mins

<=Duration ∴ Use $T_C = 8$ mins.

Design Parameters	C	Area (m ²)	% Area Detained
Roof	0.90	2065	100.00
Paving	0.75	3705	90.00
Grass / Landscaping	0.12	300	0.00

Detained flow, $Q_d = \Sigma(CiA_d/3600)$:

Roof	62.29 L/s
Paving	83.82 L/s
Grass / Landscaping	0.00 L/s

Design detained flow = 146.11 L/s

=> Try the following retention tank design parameters:

Max Allowable outflow from storage = **134.48** L/s

(0.00 L/s is still available for outflow)

Number of orifice detention tanks **1** Each tank volume above orifice **24,000** L

Max. head allowed above orifice **1,450** mm => **Each Tank Plan Area = 16.55** m²

Outlet coefficient, C_o **0.6**

Orifice diameter, D_o **225** mm => Orifice area, $A_o = \pi(D_o/2)^2$ **39,761** mm²

Graph Time v Flow:

Time (mins)	InFlow (L/s)	OutFlow (L/s)
0	0.00	0.00
8	146.11	90.77
12	146.11	121.51
20	0.00	67.43

Max. Calculated Outflow:

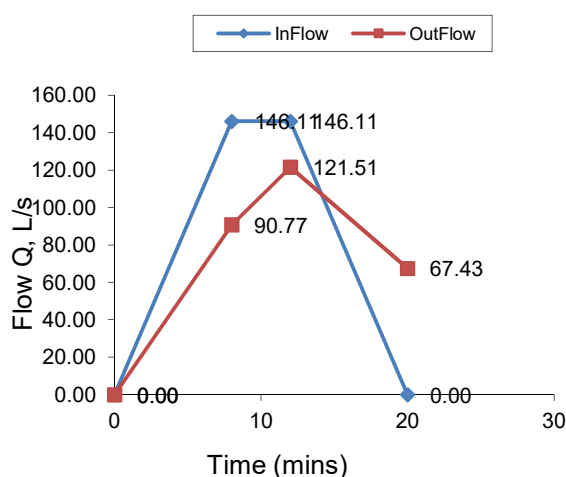
$Q_{max_{out}} = 125.29$ L/s

< 134.48 L/s Max. Allowed ∴ OK

Max. Calculated Head of Water:

$H = 1,303$ mm

< 1,450 mm Max. Allowed ∴ OK



=> Volume of Water To Be Detained:

$V = 21,570$ L

i.e. $V = 21.57$ m³

**=>USE 54m 600RCP = 24kL STORAGE VOLUME
225mm ORIFICE OUTLET PIPE**



Our Ref: 1606131_L3 ASM
14 September 2017

Kerin Bay Pty Ltd
C/- Eureka Group
540 Port Road
ALLENBY GARDENS SA 5009

ATTENTION: Mr Mike Lunniss

Email: mlunniss@hcs.on.net

Dear Mike,

**RE: PROPOSED BRIDGEPORT HOTEL REDEVLEOPMENT
NO. 2 BRIDGE STREET, MURRAY BRIDGE**

As requested by Botten Levinson Lawyers on your behalf TMK have been asked to comment on wind loading for the proposed development. We note that wind loading on a building is a normal engineering consideration when designing a building structure and is always considered in the engineering design & documentation of a project.

Proposed Redevelopment

From the plans provided the development consists of the demolition of the existing double storey building and construction of a new 6 level building. The proposed new building structure consists of the following;

- Double storey building structure (bars, lounges and terrace).
- Six storey building structure (accommodation tower 4 additional levels above double storey) setback in from the lower level double storey building line.

Wind Loading

Building structures such as the proposed are subject to forces from the wind caused by the impact and changes in the winds velocity as it approaches and flows around the building. The Australian / New Zealand Standard AS/NZS 1170.2 – Structural design actions, Part 2: Wind actions, sets out the requirements for the wind forces to be considered and designed for, for a Building such as the proposed. Consideration of wind loading when designing a building is standard practice and indeed a mandatory part of complying with the National Construction Code.

The proposed building will be subject to positive wind pressures on the windward face as wind strikes the building and suction or negative pressures to the side faces, leeward face & roof as the wind flows around the structure. The wind loads or forces from these pressures will be determined in the design documentation stage of the project as part of the normal design process.

Upon determination of the wind loading or forces a structural system will be designed and detailed to adequately resist these forces in accordance with code requirements – again this is part of the normal design process undertaken in the design documentation stage.



Civil - Structural - Environmental - Geotechnical - Mechanical - Electrical - Fire - Hydraulics - Lifts - Green ESD
Level 6, 100 Pirie Street, Adelaide SA 5000 Telephone (08) 8238 4100 Facsimile (08) 8410 1405
Berri Office: 25 Vaughan Terrace, Berri SA 5343

Email: tmksa@tmk.com.au



03/10/2017

SCAP

The proposed building is quite regular and typical of many buildings previously designed by TMK, it is not considered to present any unusual challenges with regards to wind loading, nor issue with or impediment to complying with the relevant Australian Standards.

Summary

The consideration of wind loading is normal practice when designing a building such as the proposed. The proposed building is typical of many thousands of structures built in Australian cities and presents no particular difficulties or challenges to comply with the requirements of AS/NZS 1170.2 and hence the National Construction Code for wind loading. Wind loading and the subsequent structure that is required to resist any design forces is an engineering issue that can readily be addressed, it is however an issue for the design documentation stage as the building structure is developed.

For and on behalf of
TMK Consulting Engineers

ANDREW MARTIN
Senior Associate



Civil - Structural - Environmental - Geotechnical - Mechanical - Electrical - Fire - Hydraulics - Lifts - Green ESD
Level 6, 100 Pirie Street, Adelaide SA 5000 Telephone (08) 8238 4100 Facsimile (08) 8410 1405
Berri Office: 25 Vaughan Terrace, Berri SA 5343

Email: tmksa@tmk.com.au



03/10/2017
SCAP

From: Benoit Hopkins [<mailto:bhopkins@tmkeng.com.au>]
Sent: Monday, 22 January 2018 11:27 AM
To: Craig Eyles <CEyles@hcs.on.net>
Cc: Andrew Martin <amartin@tmkeng.com.au>
Subject: RE: Bridgeport Hotel

Hi Craig,

Please see below comments in red.

Regards,

Benoit Hopkins

Engineer

D (08) 8238 4122 | F 08 8410 1405 | M

E bhopkins@tmkeng.com.au



Civil - Structural - Environmental - Geotechnical - Mechanical - Electrical - Fire - Hydraulics - Lifts - Green ESD

Level 6, 100 Pirie Street, Adelaide South Australia 5000

www.tmkeng.com.au

TMK Consulting Engineers have made car parking available while you are visiting us at Level 6, 100 Pirie Street, however this will require you to park in the UPark on Wyatt Street, 18 - 34 Wyatt Street, Enter/ Exit Wyatt St / Hyde St (between Grenfell and Pirie Streets) operated by the Adelaide City Council. When entering the car park, obtain your parking ticket as usual and proceed to a vacant car park. Prior to leaving our office, please ask one of the Administration staff at Reception to issue you with a UPark on Wyatt Street voucher to use upon returning to the car park pay station.

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From: Craig Eyles [<mailto:CEyles@hcs.on.net>]
Sent: Thursday, 18 January 2018 9:52 AM
To: Benoit Hopkins; Andrew Martin
Subject: Bridgeport Hotel

Hi Benoit and Andrew,

The Murray Bridge council have some queries regarding the Bridgeport Hotel documentation – see attached

Could you please respond to me about Item 11

11. *There are some anomalies in the stormwater report prepared by TMK :*

- In the stormwater calculator, it is estimated that the required stormwater detention would be 47.2kL; This value is the Net Storage Volume (pages SW3, SW6) and does not consider the effects of change in head and Time of Concentration for stormwater runoff. The data shown on these sheets is used by the calculator and further refined (pages SW2, SW5) to determine the actual detention volume. Refer to notes on pages SW3, SW6.

- However in the civil plan, there is a 24KL underground detention system; Refer above, actual detention volume is 21.57m³.

- The pre-development state of the site considered to be almost impervious; Pre-development approx 80% impervious, post-development approx 95% impervious, hence detention required to throttle the post-development runoff to pre-development rates.

- The total discharge as nominated by TMK, as a single outlet discharge, will deliver up to a total flow of 125.3L/s; Yes, as per pre-development conditions (however existing discharge location unknown).

- Further clarification is needed for the discharge during a minor storm where the allowable discharge is only 80.4L/s, where the orifice is sized to discharge 125.3L/s; This is a function of head within the detention system. Orifice is sized to accommodate flows from both minor/major events, as the head increases (during a major event) the discharge through the orifice increases.

- In addition to that, discharging a concentrated flow of 80.4L/s or 125.3L/s is deemed undesirable as it will cause damage to the infrastructure and also to pedestrian areas. The downstream pit(s) will not be able to handle such high flow; and

- Therefore the method of discharging the water will need to be reviewed. The total discharge rates are less than or equal to the pre-developed rates from the site, hence no increase of flows is proposed for the downstream stormwater network as a result of the development. A review of the discharge location and/or use of multiple discharge locations can be undertaken during detailed design. To assist with this review if Council could provide information about the location and size of nearby stormwater infrastructure that would be appreciated.

Regards Craig Eyles



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Bridgeport Hotel

Environmental Noise Assessment

S4988C3

September 2017

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Reviewer : Chris Turnbull, MAAS

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1 INTRODUCTION

An environmental noise assessment has been made for the proposed Bridgeport Hotel redevelopment at 2 - 6 Bridge Street, Murray Bridge

The development comprises a restaurant, sports lounge, gaming area, lounge area, function rooms, outdoor areas, accommodation rooms, liquor drive-through and car park facilities.

The assessment considers the environmental noise impact at the closest noise sensitive locations from:

- patrons in the outdoor areas;
- mechanical services plant;
- vehicle movements and activities associated with the use of the car park and liquor drive-through facilities; and
- music at the development.

The closest noise sensitive locations are the residences located north to northwest of the site, approximately 160m away (refer Figure 1). The existing acoustic environment of the area is dominated by traffic on Princess Highway / Bridge Street and the connecting roads (Railway Terrace and East Terrace).



Figure 1: Aerial image of the development site and the closest noise sensitive locations.

The assessment establishes appropriate criteria based on the relevant provisions of the Development Plan, *Environment Protection (Noise) Policy 2007* and the Environment Protection Authority (EPA) Music Noise Guideline. The criteria ensure that the noise from the proposal will not cause unreasonable interference or impact on the amenity at the residences.

The assessment has been based on:

- CED Building Design architectural drawings comprising:
 - “C1201 1.06/DA3”, dated 5 September 2017;
 - “C1201 1.03/DA1” through “C1201 1.05/DA1”, “C1201 1.07/DA1” through “C1201 1.12/DA1”, “C1201 2.01/DA1” through “C1201 2.04/DA1”, all dated 17 July 2017;
- MFY traffic and parking assessment reports:
 - “16-0112”, dated August 2016;
 - “MLM/16-0112”, dated 15 August 2017;
- continuous background noise measurements conducted between 29 July 2016 and 4 August 2016;
- an understanding that:
 - the proposed trading hours will be as per current Hotel Licence 50108345 (ie until midnight on Sunday and until 2am on other days); and,
 - only background¹ music will be played in the outdoor areas (alfresco) and will be restricted to between 9am and 9pm.

To preserve amenity and maintain the comfort of hotel guests, noise management measures will be implemented which will minimise the noise within the hotel and inherently, the noise emitted to the surroundings. Notwithstanding these measures, this assessment ensures the amenity of the locality is not unreasonably impacted upon by the proposal.

¹ The noise level below which voices do not need to be raised to be heard.

2 DEVELOPMENT PLAN

The subject site and the closest noise sensitive locations are located in the Regional Town Centre Zone within the Murray Bridge Council Development Plan². The Development Plan has been reviewed and particular regard has been given to the following Council Wide Interface between Land Uses provisions:

OBJECTIVES

Objective 1: Development located and designed to minimise adverse impact and conflict between land uses.

Objective 2: Protect community health and amenity from adverse impacts of development.

PRINCIPLES OF DEVELOPMENT CONTROL

1. *Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:*

...

(b) noise:

...

2. *Development should be site and designed to minimise negative impacts on existing and potential future land uses desired in the locality.*

7. *Development that emits noise (other than music noise) should include noise attenuation measures that achieve the relevant Environment Protection (Noise) Policy criteria when assessed at the nearest existing noise sensitive premises.*

9. *Outdoor areas (such as beer gardens or dining areas) associated with licensed premises should be designed or sited to minimise adverse noise impacts on adjacent existing or future noise sensitive development.*

10. *Development proposing music should include noise attenuation measures that achieve the following desired noise levels:*

Noise level assessment location	Desired noise level
Adjacent existing noise sensitive development property boundary	Less than 8 dB above the level of background noise ($L_{90,15min}$) in any octave band of the sound spectrum and Less than 5 dB(A) above the level of background noise ($L_{A90,15min}$) for the overall (sum of all octave bands) A-weighted level.

² Reference is made to the 11 August 2016 version of the Development Plan, which was in force at the time when the initial application was lodged.

3 ASSESSMENT CRITERIA

3.1 General Noise

Council Wide Interface between Land Uses Principle of Development Control 7 references the *Environment Protection (Noise) Policy 2007* (the Policy).

The Policy provides objective criteria for the assessment of environmental noise which are based on the World Health Organisation Guidelines to prevent annoyance, sleep disturbance and unreasonable interference on the amenity of an area. Therefore, compliance with the Policy is considered to be sufficient to satisfy all provisions of the Development Plan related to environmental noise.

The Policy does however exclude matters that may be the subject of proceedings under the *Liquor Licensing Act 1997*. This was done to ensure that the Environment Protection Authority (EPA) was not directly involved in liquor licensing matters through the Policies it enforces, as a specific and separate body and associated legislation³ existed to deal with those matters. Notwithstanding, the Policy still provides the most relevant indication of noise levels that will prevent adverse impact on the amenity of an area when assessing noise (other than music⁴) associated with a hotel development. The Policy is used to objectively assess the noise from patrons, mechanical services plant, and vehicle movements and activities associated with the use of the car park and drive-through facilities.

The Policy provides goal noise levels to be achieved at noise receivers (the residences), based on the Development Plan Zones in which the noise source (the development) and the noise receivers are located. For development and noise receivers within a Regional Town Centre Zone, the Policy recommends goal noise levels of 57 dB(A) during the day (7am to 10pm) and 50 dB(A) at night (10pm to 7am).

When measuring or predicting levels for comparison with the Policy, penalty adjustments are made for any dominant characteristic of tone, low frequency, modulation or impulsiveness. A single penalty of 5 dB(A) is applied if one characteristic is present, 8 dB(A) is added for two characteristics and 10 dB(A) is added for three of four characteristics. To apply a penalty, the characteristic must be dominant when considered within the context of the existing acoustic environment.

³ The Office of the Liquor and Gambling Commissioner and the Liquor Licensing Act 1997, respectively.

⁴ Assessed in accordance with the requirements of Council Wide Interface between Land Uses Principle of Development Control 10.

3.2 Music Noise

Council Wide Interface between Land Uses Principle of Development Control 10 provides the desired noise levels at the closest residences from music played at the hotel, based on the background noise levels. This approach is consistent with the EPA Music Noise Guideline⁵.

Continuous background noise monitoring has been conducted between 29 July 2016 and 4 August 2016 to determine the existing background noise levels in the area. The lowest measured background noise levels (L_{90}) during the period when music is envisaged at the hotel and the resultant music noise criteria⁶ (L_{10}) are provided in Table 1.

Table 1 – Music assessment criteria.

Noise Level	Noise Level (dB) by Octave Band Centre Frequency (Hz)							Overall Noise Level (dB(A))
	63	125	250	500	1000	2000	4000	
Until 9pm								
Background, L ₉₀ (dB)	50	51	46	38	38	35	21	44
Criteria, L ₁₀ (dB)	58	59	54	46	46	43	29	49
Until midnight on Sunday and until 2am on other days								
Background, L ₉₀ (dB)	45	47	42	31	27	27	18	38
Criteria, L ₁₀ (dB)	53	55	50	39	35	35	26	43

⁵ Music noise is assessed in accordance with the EPA Music Noise Guideline, entitled "Music noise from indoor venues and the South Australian Planning System", released July 2015.

⁶ Considered to be the L_{10} descriptor to ensure consistency with the EPA Music Noise Guideline.

4 ASSESSMENT

Predictions have been made of the noise levels at the closest residences from the different noise sources at the development. The predictions and comparison against the relevant assessment criteria are summarised below.

4.1 Noise from Patrons in Outdoor Areas

The noise from patrons has been measured at a number of licensed venues. The measurements, the architectural drawings, and the number of patrons in the outdoor areas provided below have been used to predict the noise from patrons at the development to the residences:

- 150 patrons within the restaurant alfresco;
- 20 patrons within the dining/function alfresco;
- 200 patrons within the terrace alfresco;
- 20 patrons within the CODS lounge DOSA;
- 10 patrons within the dining/function DOSA;
- 10 patrons within the sports bar DOSA; and,
- 10 patrons within the gaming DOSA.

Based on the prediction, the noise level at the residences will be no greater than 42 dB(A), and therefore achieves the 50 dB(A) night-time criterion of the Policy.

With a predicted overall noise level of 42 dB(A), a penalty is not considered to be warranted given that it is unlikely that individual voices will be heard amongst the overall noise from the ambient environment and the assumed concentration of patrons. Nevertheless, if a 5 dB(A) penalty is considered applicable, the resultant noise level of 47 dB(A) will still achieve the 50 dB(A) criterion.

4.2 Noise from Vehicle Movements and Activities

The noise from vehicle movements and activities within the car park (covered and uncovered) and the liquor drive-through facilities has been predicted based on:

- a range of noise measurements of car park activity which includes vehicle movements, the opening and closing of vehicle doors, and getting in and out of cars; and,
- the following number of vehicle movements and activity in any 15-minute period⁷, estimated based on MFY traffic and parking assessment report:
 - up to 75 vehicle movements within the car park;
 - up to 15 vehicle movements at drive-through facility; and,
 - car park activity in up to 90 car park bays.

⁷ Default assessment period of the Policy.

Based on the prediction, the noise level at the residences will be less than 30 dB(A) and therefore achieves the 50 dB(A) night-time criterion of the Policy. Given the nature of the noise and the existing influence of traffic on the surrounding roads, a penalty for character is not warranted.

4.3 Noise from Mechanical Services Plant

At the Development Application stage of a project, the mechanical services plant is generally not designed nor selected. Notwithstanding, given the designated location of the plant on the roof and within the Services Court on Level 1, and the significant separation distance between the development and the closest residences of no less than 160m, the noise from mechanical services plant is expected to easily achieve the Policy.

The ability of the mechanical services to achieve the Policy levels will be confirmed during the detailed design stage. It is noted that a penalty is considered not applicable given the continuous nature of noise and its expected low level in comparison with the existing ambient.

4.4 Cumulative Noise

Based on the predictions of noise levels from patrons, vehicle movements and car park activities, and the mechanical services plant being designed to be no greater than 42 dB(A) (which should be readily achieved), the cumulative noise level from all of these sources will achieve the 50 dB(A) night-time criterion of Policy at the residences.

4.5 Noise from Music

Noise from music at the development has been predicted based on music level measurements conducted at a range of similar licensed venues. The sections below summarise the predictions from music in different areas of the development.

4.5.1 Background Music in Outdoor Areas and Opened Adjoining Spaces

Background music in the outdoor areas (dining, function and terrace alfresco) will be played only until 9pm on any night. During this period, the external doors between the outdoor and adjoining indoor spaces (sports lounge, bar lounge, restaurant and function room) may be opened for ambience.

Noise from background music in the combined open space (alfresco and the adjacent indoor space with the doors opened) has been predicted based on previously measured music levels that are at the higher end of the range considered typical for "background", as provided in Table 2.

Table 2 – Higher end of the range of music levels typically considered as being background.

Background Music Level, L ₁₀ (dB) by Octave Band Centre Frequency (Hz)							Overall Music Level (dB(A))
63	125	250	500	1000	2000	4000	
74	83	79	76	74	71	68	79

The prediction is based on these music levels at the outer boundary of the outdoor areas, either from speakers located within the outdoor or indoor areas, or from any live music performances at the indoor stage. The prediction indicates that the noise levels at the residences will achieve the assessment criteria, as shown in Table 2.

Table 3 – Predicted noise levels from background music in the outdoor areas.

Noise Level	Noise Level, L ₁₀ (dB) by Octave Band Centre Frequency (Hz)							Overall Noise Level (dB(A))
	63	125	250	500	1000	2000	4000	
<i>Criteria - until 9pm on any night</i>	58	59	54	46	46	43	29	49
Predicted	39	47	41	40	39	36	29	44

4.5.2 Music from Inside the Sport Lounge, Lounge Bar and Restaurant

As indicated in Section 4.5.1, any music played in the sport lounge, lounge bar and restaurant whilst having the external doors and/or the bar serving window open will need to be limited to background music only as defined in Table 2 and not occur beyond 9pm on any night.

However, with all external doors and/or openings closed, except those that are used for access or service, music inside the spaces during any hour of trading is predicted to achieve the assessment criteria, provided that the internal music levels do not exceed the following:

Music Level, L ₁₀ (dB) by Octave Band Centre Frequency (Hz)							Overall Music Level (dB(A))
63	125	250	500	1000	2000	4000	
100	101	98	87	84	82	73	93

4.5.3 Music from Inside the Main Function Room

The noise from music inside the main function room on the first floor (ie Function Room 2) has been predicted based on the following internal levels:

Music Level, L ₁₀ (dB) by Octave Band Centre Frequency (Hz)							Overall Music Level (dB(A))
63	125	250	500	1000	2000	4000	
111	104	101	104	109	101	97	111

The music levels used above are higher than the levels that would be expected at the hotel, given that there are guest rooms at the hotel and management will ensure that hotel guests are not disturbed. This assessment therefore provides a worst case assessment of music in the function space in the interests of conservatism.

Based on the prediction, it is recommended that:

- all external openings/doors to the function room remain closed when music (other than background music as defined in Table 2) is played within the function room; and,
- access to the balcony (terrace alfresco) is via the airlock only (shown on the plan drawing adjacent to the staircase).

With the recommendation implemented, the predicted noise levels at the residences from music inside the function room easily achieve the assessment criteria, as shown in Table 4.

Table 4 – Predicted noise levels from music inside the function room.

Noise Level	Noise Level, L_{10} (dB) by Octave Band Centre Frequency (Hz)							Overall Noise Level (dB(A))
	63	125	250	500	1000	2000	4000	
<i>Criteria - until midnight Sunday, until 2am on other days</i>	53	55	50	39	35	35	26	43
Predicted	48	34	24	23	26	23	9	30

5 CONCLUSION

The environmental noise assessment has considered the noise impact at the closest residences from:

- patrons in outdoor areas;
- mechanical services;
- vehicle movements and activities associated with the car park and drive-through facilities; and,
- music at the development;

based on the following understanding:

- the proposed trading hours will be consistent with the existing approved hours being until midnight on Sunday and 2am on other days; and,
- only background music will be played in the outdoor areas and will be restricted to the hours between 9am and 9pm.

The assessment has established criteria based on:

- the *Environment Protection (Noise) Policy 2007* to objectively assess the cumulative noise from patrons in the outdoor areas (sports, lounge, restaurant, terrace alfresco); vehicle movements and activities associated with the car park and drive-through facilities; and mechanical services; and,
- the music noise criteria in the Development Plan to objectively assess music noise from the development. The criteria are consistent with the EPA Noise Guideline for "Music noise from indoor venues and the South Australian Planning System".

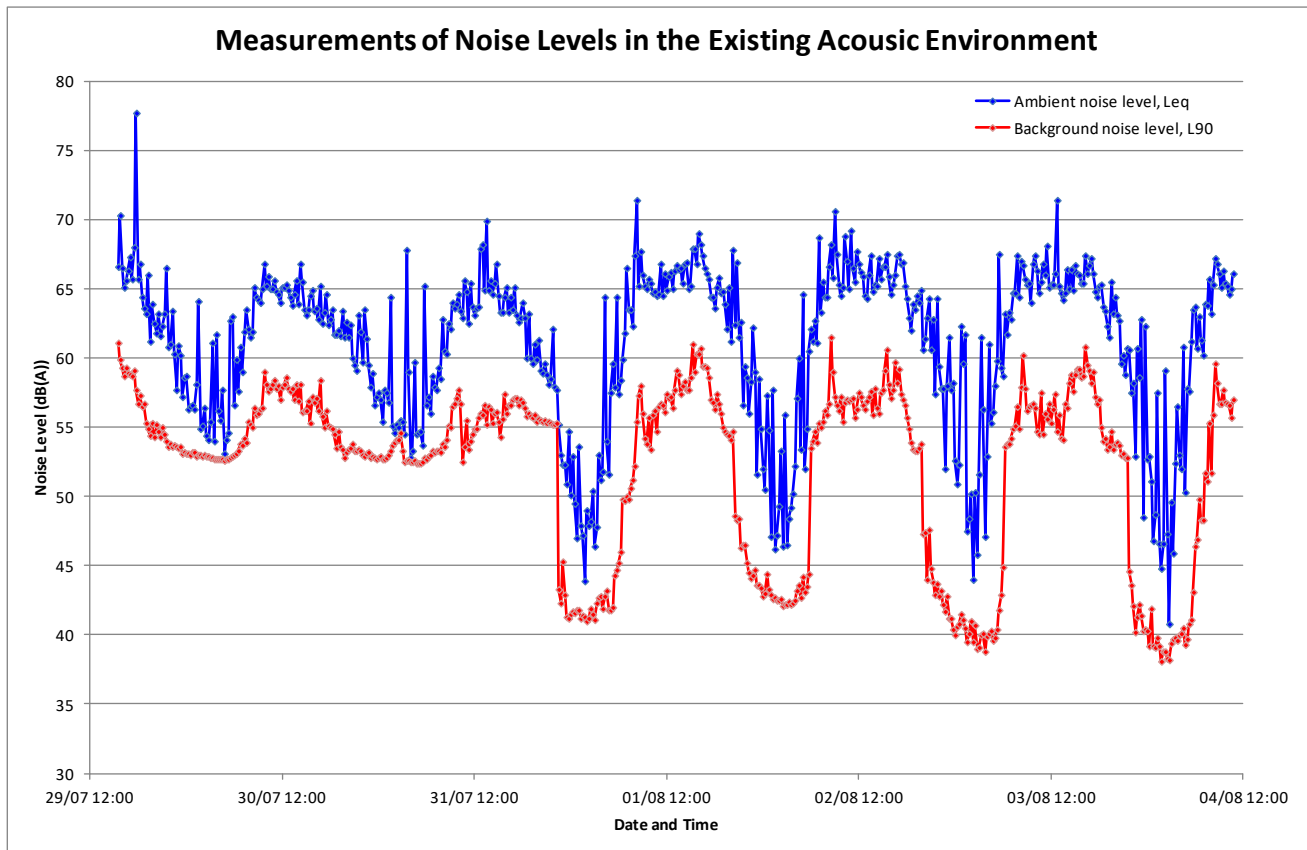
The assessment provides recommendations for acoustic measures in order to achieve the criteria, including:

- a recommendation for a detailed assessment of the mechanical services plant and equipment during the design stage of the project when equipment selection and design have been finalised;
- restriction of music in the outdoor areas to background music only, being, in subjective terms, a music level such that voices do not need to be raised to be heard or alternatively, in objective terms, those levels listed in Table 2;
- ensuring all external doors and openings to the main function room remain closed when music which is higher in level than background music is played inside the room;
- ensuring that access to the balcony (terrace alfresco) is via the airlock only when music which is higher in level than background music is played inside the room;
- restriction of the music levels inside the sports lounge, lounge bar and restaurant; and,
- ensuring all external doors and openings to the sports lounge, lounge bar and restaurant remain closed when music is played after 9pm on any day, except if required for access or service.

Based on the above, the proposed development will not cause unreasonable interference or impact on the amenity at the closest residences, and therefore, satisfies the environmental noise provisions of the Murray Bridge Council Development Plan.

APPENDIX

MEASUREMENTS OF NOISE LEVELS IN THE EXISTING ACOUSTIC ENVIRONMENT



14th August 2017

BRIDGEPORT HOTEL

2 BRIDGE STREET, MURRAY BRIDGE SA 5253

EXTERNAL COLOUR AND FINISHES SCHEDULE

• HOTEL:

EXTERNAL FINISHES	MATERIAL	COLOUR
Main roof	Colorbond metal	Surf mist
Car park Entry Canopy framing	Galvanised steel	Dulux Domino – SG6G8
Entry Canopy soffit lining	Expressed joint FRC sheet	Dulux Lexicon (Quarter) - PN2D1
Entry feature surround framing	Tiles	Vein cut beige Travertine with Ardex 677 Travertine grout
Ground floor walls generally	Conc' block & Hi-build paint	Dulux Lexicon (Quarter) - PN2D1
Upper floor main walls	Alucobond	Pure White 10 - 100
Staircase feature walls and feature panels	Alucobond	Black 326
Perforated decorative screens	Locker Group Pic-Perf - Galv. Steel	Galvanised Steel
Ground floor columns	Galvanised Steel	Dulux Ticking - SG6G7
Hotel Suite external walls to balconies	Kingwood Eternal Cladding 260x28	Horizontal - Spotted Gum
Hotel Suite balcony dividing walls	Expressed joint FRC sheet	Dulux Ticking - SG6G7
Battened screen fencing	Powder coated Aluminium	Ezislak Kwilla
Window glazing	Viridian Glass	SolTech Neutral
Window frames	Powder coated Aluminium	Anotec Natural Pearl matt 1009
Balustrade posts and framing	Powder coated Aluminium	Anotec Natural Pearl matt 1009
Umbrella posts	Powder coated Aluminium	Anotec Natural Pearl matt 1009
Umbrella membrane	Vinyl	White
Pool deck	Kingwood decking 140x28	Spotted Gum
Hotel Suite Passage end wall feature framing	Galvanised steel	Dulux Ticking - SG6G7
Lift foyer external framing	Alucobond Tiles	Alucobond Black 326 Vein cut beige Travertine with Ardex 677 Travertine grout

• Bottle SHOP:

EXTERNAL FINISHES	MATERIAL	COLOUR
Roof	Colorbond metal	Colorbond Surf mist
Drive-thru' canopy soffit lining	Painted Versilux - Flat acrylic	Dulux Lexicon (Quarter) - PN2D1
Walls generally	High-build paint	Dulux Colorbond Surf mist
Battened screen wall	Powder coated Aluminium	Dulux Precious Silver Pearl
Parapet capping	Colorbond steel	Surf mist
Window glazing	Viridian Glass	SolTech Neutral
Window frames	Powder coated Aluminium	Dulux Precious Silver Pearl
Auto roller shutter	Galvanised steel	Galvanised steel
Canopy posts	Stainless steel cladding	Brushed Stainless steel
Canopy edge capping	Stainless steel cladding	Brushed Stainless steel

PLAN ALTERATIONS from DAC plans issued 11/11/2016
to new plans dated 10/08/2017

Drawing No: 1.01 Site plan

- Dashed outline of proposed new Bridgeport Hotel building altered

Drawing No: 1.02 Landscaping plan

- Drawing removed from architectural set, and replaced by the following new Landscaping drawings created by Oxigen Pty. Ltd.:
 - o 1. Drawing No. 17.027.101 Revision A Landscape Planting Plan
 - o 2. Drawing No. 17.027.103 Revision A Landscape Planting Plan - Detail
 - o 3. Drawing No. 17.027.104 Revision A Landscape Planting Plan – Upper Level Terrace
 - o 4. Drawing No. 17.027.105 Revision A Landscape Species + Screen

Drawing No: 1.03 Basement Plan

- Commercial quality laundry added to west corner of basement to cater for all onsite linen requirements
- Basement car park enlarged to with parking increased from 69 to 71 parks
- Proposed ground floor building outline over altered to match updated floor plans

Drawing No: 1.04 Site Plan

- Ground floor plan revised to better address the Bridge Street and East Terrace frontages.
- Larger area of openable ground floor facade fronting site boundary to Bridge Street.
- Building extended to East Terrace boundary with return verandah extending over footpath.
- Sports bar relocated to north-west corner.
- Lounge bar relocated to north east site boundaries.
- Gaming lounge relocated further south east.
- East Terrace Designated Outdoor Smoking Area (DOSA) reduced
- East Tce raised restaurant alfresco terrace reduced in size
- East Tce onsite planting increased. Refer to Qxigen Landscape drawings.
- South west main entry driveway canopy removed and entry feature framing altered, with planting added.
- Columns to Drive through Bottle shop canopy shown
- Bridge Street car park frontage feature planting structure added. Refer to Qxigen Landscape drawings.
- Individual landscape planting removed. Refer to Qxigen Landscape drawings.
- Centre car park landscaping island added. Refer to Qxigen Landscape drawings.
- Outdoor Kids play area added
- Drive through Liquor store and canopy, remain as previously designed
- Car Park layout remains substantially as previously designed with Ground level parking increased from 83 to 88 parks

No: 1.05 Ground Plan

- Plan changes as per page 1.04
- Floor finishes removed
- Landscaping detail removed. Refer to Qxigen Landscape drawings.
- Area tabulation expanded

Drawing No: 1.06 1ST Floor plan

- Function room Terrace extended to over the East Terrace footpath.
- Alfresco terrace planters added. Refer to Oxigen landscape drawings.
- Function Store room added at the expense of lift foyer space.
- Function room Staging Kitchen added.
- Gym relocated, allowing extra hotel suite to be added.
- Function Bar & Toilets relocated allowing 2 x extra hotel suites to be added.
- Pool & pool deck reorientated
- Area tabulation expanded.

Drawing No: 1.07 2nd Floor plan

- Hotel Suite 217 Balcony enlarged to centralise lift & foyer structure over ground floor car park side entry
- Services void to 1st floor staging kitchen added

Drawing No: 1.08 3RD Floor plan

- Hotel Suite 317 Balcony enlarged to centralise lift & foyer structure over ground floor car park side entry.
- Services void to 1st floor staging kitchen added

Drawing No: 1.09 Floor Plan

- Hotel Suite 417 Balcony enlarged to centralise lift & foyer structure over ground floor car park side entry

Drawing No: 1.10 5th floor plan

- Hotel Suite 317 Balcony enlarged to centralise lift & foyer structure over ground floor car park side entry

Drawing No: 1.11 Roof Plan

- Roof plans revised to match current floor plan layouts

Drawing No: 1.12 Apartment Type Plans

- Minor changes to Bathroom, Kitchenette, and Robe layouts

Drawing No: 1.13 Standard apartment details

- Drawing removed from architectural set, deemed superfluous at this stage

Drawing No: 2.01 Proposed Elevations

- Elevations updated to match new floor plan layouts and new theming

Drawing No: 2.02 Proposed Elevations

- Elevations updated to match new floor plan layouts and new theming

Drawing No: 2.03 Bottle shop elevations

- Nil change to Previously submitted elevations

Drawing No: 2.04 Streetscape elevations

- Updated to match new elevations listed above

Drawing No: 2.05 Perspective views from East side of Murray River

- Updated to match new elevations listed above

Drawing No: (not numbered) Perspective views from Bridge Street

- Updated to match new elevations listed above

Bridgeport Hotel

STANDARD HOTEL SUITE MATERIALS & FINISHES SCHEDULE

BCA 2015 BUILDING CLASSIFICATION:	Class 3 Hotel suites, Class 6 Tavern, Class 7(a) Car Park
CLIMATE ZONE For thermal design:	6 - Murray Bridge
CONSTRUCTION FOR FIRE RESISTANCE:	Type A (6 storey + undercroft car park)
HOTEL SUITE TYPE :	Standard 1 room

1.0 FLOORING:	Material	Colour
Carpet	Broadloom carpet - Brintons	To be selected
Verandah / Balcony	300 x 300 Ceramic square laid R12 - National Tiles	Stratos Bianco
Internal wet area tiles	600 x 600 Ceramic square laid R10 - National Tiles	Veniza
2.0 FIRE RATINGS:	Material	Colour
Party walls	90/90/90 fire rated walls between suites	
Floors	90/90/90 Fire rated floors	
Doors to Entry & Party walls	Self Closing - /60/30 Fire Rated Doors	
3.0 WALLS:	Material	Colour
Party walls	To be confirmed	Dulux "Antique White U.S.A. (full strength)" Matt - PN1D1
Internal walls	90 x 35 MGP10 timber stud frame with 10mm flush jointed plasterboard	Dulux "Antique White U.S.A. (full strength)" Matt - PN1D1
Ceiling Height	2800 to main room & 2400 wet areas	
Wet areas	6mm Villaboard FRC, with tiles over (per drawings)	National tiles - Veniza 600 x 600 floor to ceiling
Skirtings	92 x 18mm Primed Bevelled Finger Jointed Pine	Dulux "Antique White U.S.A. (full strength)" Gloss - PN1D1
Architraves	66 x 18mm Primed Bevelled Finger Jointed Pine	Dulux "Antique White U.S.A. (full strength)" Gloss - PN1D1
4.0 INSULATION:	Material	Colour
Sound Insulation Floors	Rw+Ctr ≥ 50 & Ln,w +Ci ≤ 62	
Sound Insulation Party walls & common corridor walls	Rw+Ctr ≥ 50	
Sound insulation Doors to Party walls & common corridor walls	Rw+Ctr ≥ 30	
Sound insulation to wet area internal pipes & ducts	Rw+Ctr ≥ 25	
Sound insulation to main suite internal pipes & ducts	Rw+Ctr ≥ 40	
Internal non loadbearing walls:	Nil	
Thermal Insulation External walls (not party walls or common corridor walls)	R2.8 total (refer to BCA 2015 for further options)	
Thermal insulation Roof (top floor)	R3.2 total	
5.0 CEILINGS:	Material	Colour:
Linings:	10mm plasterboard flush jointed	Dulux Antique White U.S.A. Matt - PN1D1
Cornice:	Square set through out	Dulux Antique White U.S.A. Matt - PN1D1
Ceiling Access panels	Flush jointed 300 x 300 ceiling access panels with key lock	Dulux Antique White U.S.A. Matt - PN1D1
Air conditioning grilles	Aluminium Linear bar grille	Dulux Powder Coat Precious Silver Pearl Kinetic
Return Air Grille	Plastic Eggcrate Return Air Grille with filter	Dulux Powder Coat Precious Silver Pearl Kinetic
Exhaust air vents to Wet area	250mm Diameter ceiling exhaust grille to inline ducted fan	White
6.0 WINDOWS & SLIDING DOORS	Material	Colour:
Window Frames	AWS Powder coated aluminium	Dulux Powder Coat Precious Silver Pearl Kinetic
Sliding Door frames	AWS Powder coated aluminium	Dulux Powder Coat Precious Silver Pearl Kinetic
Stacking sliding doors	AWS Powder coated aluminium	Dulux Powder Coat Precious Silver Pearl Kinetic
Glass	Viridian Soltech	Grey No. 2
Sliding door handle	AWS ICON 316-grade stainless steel with internal turn snib only	brushed stainless steel
Flyscreens to windows	Black UV resistant fibreglass	Frames Powder coat Black
Sliding door flyscreens	Black UV resistant fibreglass	Frames Dulux Powder Coat Precious Silver Pearl Kinetic
Reveals	Ezy Reveals metal window reveals	Dulux Antique White U.S.A. Matt - PN1D1

7.0 FIRE DOORS & HARDWARE	Material	Colour
7.1 Entry Door	Pyropanel PFD060-38 Fire Rated Door, 2340x920x38mm thick, -/60/30 FRL, Ply face, with 300mm s/steel kick panels & Smoke seals	Internal = Dulux Antique White U.S.A. Gloss – PN1D1
Entry door frame	Fire rated Pyropanel Pressed Metal Door Frames	Dulux Antique White U.S.A. Gloss – PN1D1
Hinges (4 per door)	100 x 75 loose pin butt hinges	Brushed Stainless Steel
Door Closer	To be selected	Powder coated Silver
Lock & Lever	KABA Saflok RT RFID Contactless Electronic Lock	Satin Silver
Door seals - Jamb & Head	To be selected	To be selected
Door seals - Base	To be selected	To be selected
Door Stops	Half Moon Floor Stop	Satin Chrome with black rubber bumper
8.0 INTERNAL DOORS & HARDWARE	Material	Colour:
8.1 WC DOOR	Solid core prime coat flush panel door 2040x720x35	Internal = Dulux Antique White U.S.A. Gloss – PN1D1
Frame	112 x 32 mm MDF	Internal = Dulux Antique White U.S.A. Gloss – PN1D1
Lever	Lockwood Velocity Series Small Rose Door Handles "Accession" privacy set	Satin Chrome
Hinges (3 per door)	100 x 75 loose pin butt hinges	Satin Stainless Steel
Door Stop	Half Moon Floor Stop	Satin Chrome with black rubber bumper
8.2 SHOWER DOOR	Solid core prime coat flush panel door 2040x720x35	Dulux Antique White U.S.A. Gloss – PN1D1
Frame	112 x 32 mm MDF	Dulux Antique White U.S.A. Gloss – PN1D1
Lever	Lockwood Velocity Series Small Rose Door Handles "Accession" privacy set	Satin Chrome
Hinges (3 per door)	100 x 75 loose pin butt hinges	Satin Stainless Steel
Door Stop	Half Moon Floor Stop	Satin Chrome with black rubber bumper
8.3 Vanity Door	Corinthian MODA MODG 1 with Translucent glass 2040x920x35 (non-standard width)	Dulux Antique White U.S.A. Gloss – PN1D1
Frame	Cavity slide frame	Dulux Antique White U.S.A. Gloss – PN1D1
Lever	Lockwood Velocity Series Small Rose Door Handles "Accession" privacy set	Satin Chrome
Hinges (3 per door)	100 x 75 loose pin butt hinges	Satin Stainless Steel
Door Stop	Half Moon Floor Stop	Satin Chrome with black rubber bumper
Architraves	66 x 18mm Primed Bevelled Finger Jointed Pine	Dulux Antique White U.S.A. Gloss – PN1D1
Window reveals	Ezy Reveals metal window reveals	Dulux Antique White U.S.A. Gloss – PN1D1
10.0 ELECTRICAL	AS PER ELECTRICAL LAYOUTS	Colour:
Switches	Clipsal Slimline SC2000 Series	Black switches with brushed aluminium cover plates
Power points	Clipsal Slimline SC2000 Series	Black switches with brushed aluminium cover plates
Downlights	12W LED downlights (3000K warm white) IP54	White Bezel
External lights - Soffit mount	12W LED downlights (3000K warm white) IP54	White Bezel
TV Point	Clipsal Slimline SC2000 Series	Black switches with brushed aluminium cover plates
Phone Point	refer to layout	White
11.0 APPLIANCES	Material	Colour
Microwave Oven	Nil supplied	To be selected
Refrigerator	Nil supplied	To be selected
Television	48" LED LCD	To be selected
TV Wall bracket	300sq VESA fixed wall mounting bracket capable of supporting up to 45kg	Black

12.0 SANITARY & TAPWARE	Material	Colour:
12.1 Kitchen		
Sink	Mercer Endeavour ER036 single S/Steel bowl undermount	Stainless Steel
Kitchen tap	Grohe Eurosmart Cosmopolitan Gooseneck Kitchen Mixer	Chrome
12.2 Vanity		
Vanity unit / Basin	420mm diam x 150mm high Glass counter top vessel basin by "uniquesinks.com.au"	Silver Glass
Tap	Grohe BauEdge Basin Mixer with 130mm extension	Chrome
12.3 Shower		
Shower Rose	GROHE - Living Well Chrome shower set on polished s/steel grab bar	Chrome & S/stel
Shower Tap	Grohe Baumetric square Shower mixer	Chrome
Shower Screen	Nil	
12.4 Toilet		
Toilet suites	Argent Isis BTW Toilet Suite with Soft Closing Seat, Bottom Water Entry S-Trap 80-170mm - KO060101S4BDB	White
12.5 Floor Grates		
Vanity & Shower room floor grates	SMART TILE with cut tile insert (Iplex) 100mm D101STILESS100	Stainless steel with tile insert
Balcony	To be selected	To be selected
13.0 JOINERY	Material	Colour
13.1 Kitchen		
Benchtop	20mm Caesarstone with 40mm edge	Vanilla Noir 5100 (Black marble)
Splashback	600 x 600 Ceramic - National Tiles	Veniza
Exterior	Laminate -1mm edging	Polytec Gesso Lini Sheen
Drawers	Laminate -1mm edging	Polytec Gesso Lini Sheen
Kickboards	Laminate	Brushed Aluminium
Handles	Hafele 108.79.004	S/Steel coloured, brushed
Hinges	Hafele Soft Close	
Overhead Cupboards	Laminate -1mm edging	Polytec Gesso Lini Sheen
Overhead cupboard handles	Hafele 108.79.004	S/Steel coloured, brushed
Microwave Recess	Laminate -1mm edging	Polytec Gesso Lini Sheen
13.2 Built in Robes		
Hinged Doors	Laminex Diamond Gloss finish -1mm edging	Dark Wengeblock
Internal configuration	shelf at 1800 AFL and hanging rail below	White Melamine
Shelves	2mm impact edges	White Melamine
Hanging Rail	Metal	Satin Chrome
Handles	Hafele 108.79.004	S/Steel coloured, brushed
13.3 Vanity		
Benchtop	20mm Caesarstone with 200mm drop front	Vanilla Noir 5100 (Black marble)
Splashback	100mm high 20mm Caesarstone to 3 sides	Vanilla Noir 5100 (Black marble)
Exterior	Laminex Diamond Gloss finish -1mm edging	Dark Wengeblock
Drawers	Laminex Diamond Gloss finish -1mm edging	Dark Wengeblock
Kickboards	Laminate	Brushed Aluminium
Handles	Hafele 108.79.004	S/Steel coloured, brushed
Hinges	Hafele Soft Close	
13.4 Desk		
Benchtop	20mm Caesarstone with 100mm drop front / Lintel	Vanilla Noir 5100 (Black marble)
Splashback	100mm high 20mm Caesarstone to 2 sides	Vanilla Noir 5100 (Black marble)
Exterior	Laminex Diamond Gloss finish -1mm edging	Dark Wengeblock
Drawers	Laminex Diamond Gloss finish -1mm edging	Dark Wengeblock
Kickboards	Laminate	Brushed Aluminium
Handles	Hafele 108.79.004	S/Steel coloured, brushed
Hinges	Hafele Soft Close	

14.0 TILES	Material	Colour
14.1 Kitchen		
Wall	600 x 600 Ceramic square laid	Veniza
Wal Grout	Ardex	Misty Grey
Floor	600 x 600 Ceramic square laid	R10 Veniza
Floor Grout	Ardex	Misty Grey
14.2 Shower room		
Wall	600 x 600 Ceramic square laid - floor to ceiling	Veniza
Wal Grout	Ardex	Misty Grey
Floor	300 x 300 ceramic square laid	R10 Veniza
Floor Grout	Ardex	Misty Grey
14.3 Toilet		
Wall	600 x 300 Porcelain square laid - Skirting tile	Veniza
Wal Grout	Ardex	Misty Grey
Floor	300 x 300 ceramic square laid	R10 Veniza
Floor Grout	Ardex	Misty Grey
14.4 Entry/ Vanity		
Wall	600 x 600 Ceramic square laid - floor to ceiling	Veniza
Wal Grout	Ardex	Misty Grey
Floor	600 x 600 Ceramic square laid	R10 Veniza
Floor Grout	Ardex	Misty Grey
14.5 Balcony		
Floor	300 x 300 ceramic square laid	R12 Stratos Bianco
Floor Grout	Ardex	Misty Grey
15.0 BALUSTRADE	Material	Colour
Frames	Alusafe 503 Glazed Aluminium balustrade sytem	Dulux Powder Coat Precious Silver Pearl Kinetic
Glass	Toughened Glass	Neutral
Handrail	Alusafe 64 x 34 rectangular (curved top) aluminium	Dulux Powder Coat Precious Silver Pearl Kinetic

Some Observations about changes made to the Bridgeport Hotel since original construction, and comments on my findings arising out of my inspection of the Hotel on the 6th of March 2017

1.0 Changes made to the Ground floor East Terrace & Bridge Street facades:

- 1.1) 65% of the original ground floor street front walls no longer exist
- 1.2) Of the original walls that remain the upper 1/3 have been replaced with bricks and reinforcing
- 1.3) The remaining concealed walls were observed to be in an extremely poor condition and penetrated by steel and concrete beams that support the more recent balcony framing and walls above. (images 1.5a, 1.5b, 1.5c, & 1.5d)
- 1.4) Removal of balcony posts to the Bridge Street frontage and Balcony floor support shelf brackets to the East Tce frontage, are evidence of the change to the balcony floor structure. Cantilevered beams have been pocketed into the existing walls on East Tce, and the beams back-span under the 1st floor. I believe it's likely work was done to the walls to shore them up at the new loadbearing beam locations, and the patchwork was hidden by the brick façade that was completed shortly after. It appears that the cream brick façade was provided to cover and support the original walls
- 1.5) Water damage, bowing, and extensive deterioration is evident in most of the remaining original concealed walls.
- 1.6) The deterioration of these walls has been accelerated by over 50 years of concealment behind the cream brick walls, and the proximity of the railway tunnel (Fig. F) adjacent the loadbearing cellar walls
- 1.7) No remaining façade wall could be reinstated to original condition
- 1.8) None of the original timber framed doors or windows remain
- 1.9) Substantial concrete underpinning, visible in the cellar to East Terrace and Bridge Street frontages, is further evidence that any original street front walls remaining behind the later brick façade are not likely to be intact.

2.0 Changes made to the 1st floor street front Balcony & Verandah structure:

- 2.1) The absence of original ground floor balcony posts & wall support brackets indicates the balcony structure is not original. Structural beams revealed confirm this (images 1.5a, 1.5b, & 1.5d)
 - 2.2) No original wrought iron balustrade or post top corner friezes remain (images 1.6., 2.1, & 2.5)
 - 2.3) The original bull-nosed verandah no longer exists, it's been replaced by a later simpler structure
- Please also see attached historic & current photos of the hotel. (Fig.s A to F)

For further information relating to the changes made to the hotel, and the extent of remaining heritage fabric, refer to CED Building Design drawings nos. 3.01 & 3.02, and the attached Photographs marked Figs 1.1 to 2.10 (Cross referenced in the above drawings)

Bridgeport Hotel Murray Bridge

HISTORICAL IMAGERY & ONSITE INVESTIGATION

Fig. A - Early 1900s photo shows

- Bridge St side 1st floor verandahs and balconies were considerably smaller
- Earliest verandah roofs were convex to the 1st floor and OG shaped to the ground floor
- 1st floor balconies were supported off the lower stone walls by large balcony brackets, as well as a single post visible to the east end of the East terrace balcony

Fig. B - 1920s photo shows

- Bridge St ground floor verandah and corner 1st floor balcony had been removed, and replaced by a much larger rectangular timber post and beam verandah with a simple skillion roof, and a signage fascia panel facing Bridge street at the balcony floor level.

Fig. C - 1960 photo shows

- Ornate cast iron balustrades removed and replaced with more unadorned horizontal cast iron.
- The signage panel facing Bridge St has grown in size
- Decorative cast iron corner friezes to the 1st floor balcony posts have been removed

Fig. D - 1968 photo indicates substantial changes.

- Current cream brick facade with breeze holes to the toilet windows is visible.
- Bridge St & East Tce main entry corner changed to concrete columns supporting conc' lintel over
- The Corner balcony floor & roof has been curved
- New full length deep ribbed metal balcony floor fascia added
- All verandah posts & floor support shelf brackets have been removed, which would indicate substantial internal structural changes have also occurred to create the cantilevers
- Current window and door changes to the East Tce facade are visible
- Concrete footpath altered / added
- 3 chimneys have been removed

Fig. E – Current photo:

- The Bridge St / East Tce corner entry framing changed again to a recessed configuration over an artificial turf covered concrete slab above the old cellar
- Internal above ceiling investigations reveal the majority of the Bridge St ground floor façade has been removed, probably in the 1960s
- Substantial concrete beams & steel beams have been installed to support the 1st floor masonry walls and cantilevered balconies
- Existing 1st floors show evidence of a lot of movement, many rooms show a perceptible and often visible cross fall toward the internal passage parallel to Bridge St. Falls perceived to be in the order of 30mm to 60mm, suggesting settling or separation of support members related to the new floor / cantilevered balcony support beams.
- Maintenance staff member with an onsite history of 18 years, advises that the Bridge Street balcony frontage has dipped at the outer edge by approximately 75mm in the last 4 years
- Substantial portions of the internal ground floor walls have been removed, as evidenced by the large room dimensions that are inconsistent with the nature of a building of this age, and remaining engaged and isolated piers

Fig. F – Train tunnel

- Indicates close proximity of 1925 train tunnel to original building

Bridgeport Hotel Murray Bridge

Fig. A - Pre 1920



Fig. B – 1920s

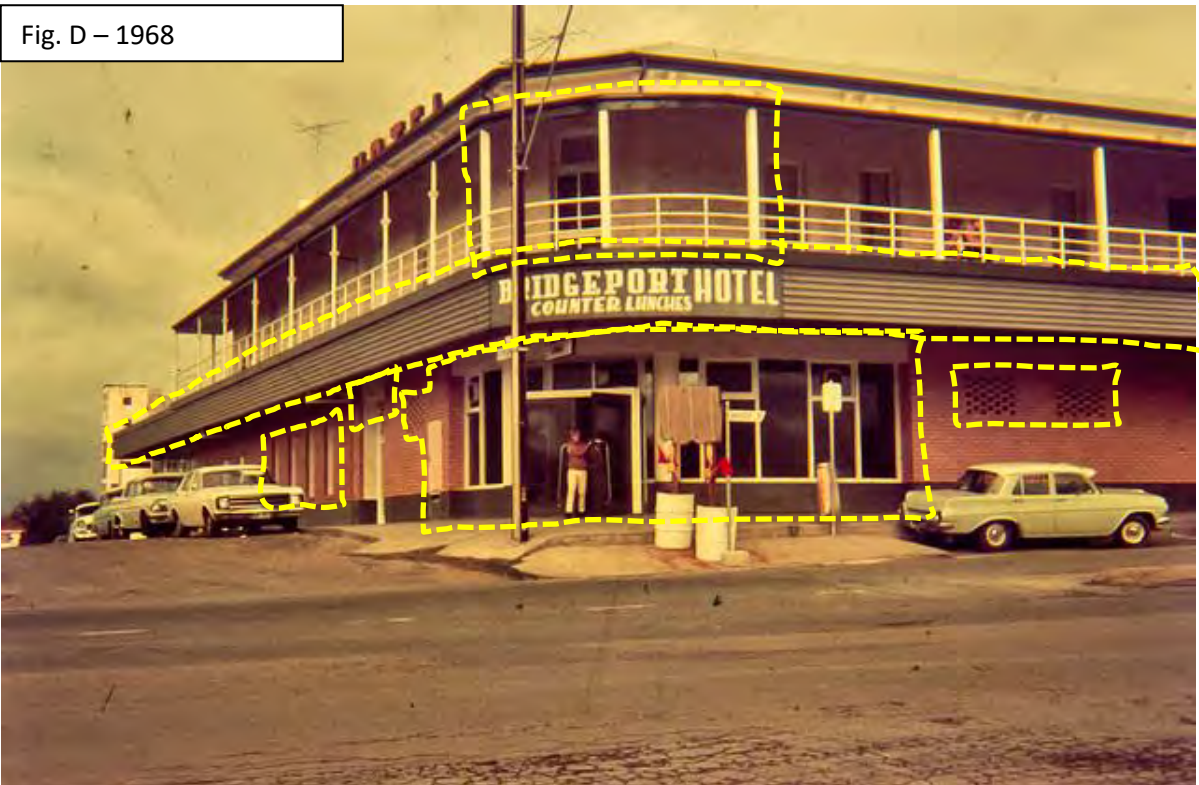


Bridgeport Hotel Murray Bridge

Fig. C – 1960



Fig. D – 1968



Bridgeport Hotel Murray Bridge

Fig. E – Current



Fig. F – 1925 Train tunnel



Bridgeport Hotel Murray Bridge

Fig 1.1



Fig 1.3



Bridgeport Hotel Murray Bridge

Fig 1.4



Fig 1.5a



Bridgeport Hotel Murray Bridge

Fig 1.5b



Fig 1.5c



Fig 1.5d



Bridgeport Hotel Murray Bridge

Fig 1.6a



Fig 1.6b



Bridgeport Hotel Murray Bridge

Fig 1.8a



Fig 1.8b



Bridgeport Hotel Murray Bridge

Fig 1.9



Fig 1.10

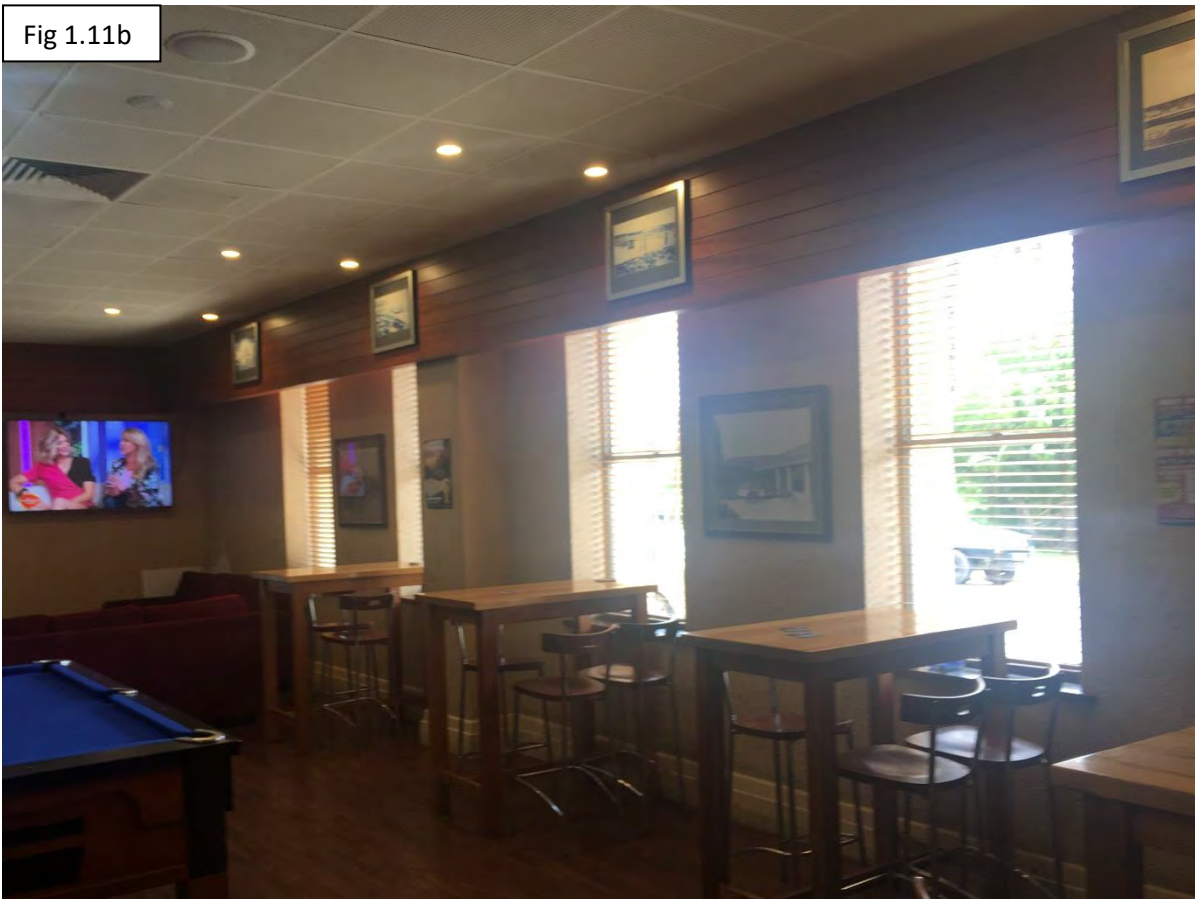


Bridgeport Hotel Murray Bridge

Fig 1.11a



Fig 1.11b



Bridgeport Hotel Murray Bridge

Fig 1.14



Fig 1.15



Bridgeport Hotel Murray Bridge

Fig 1.16



Fig 2.1

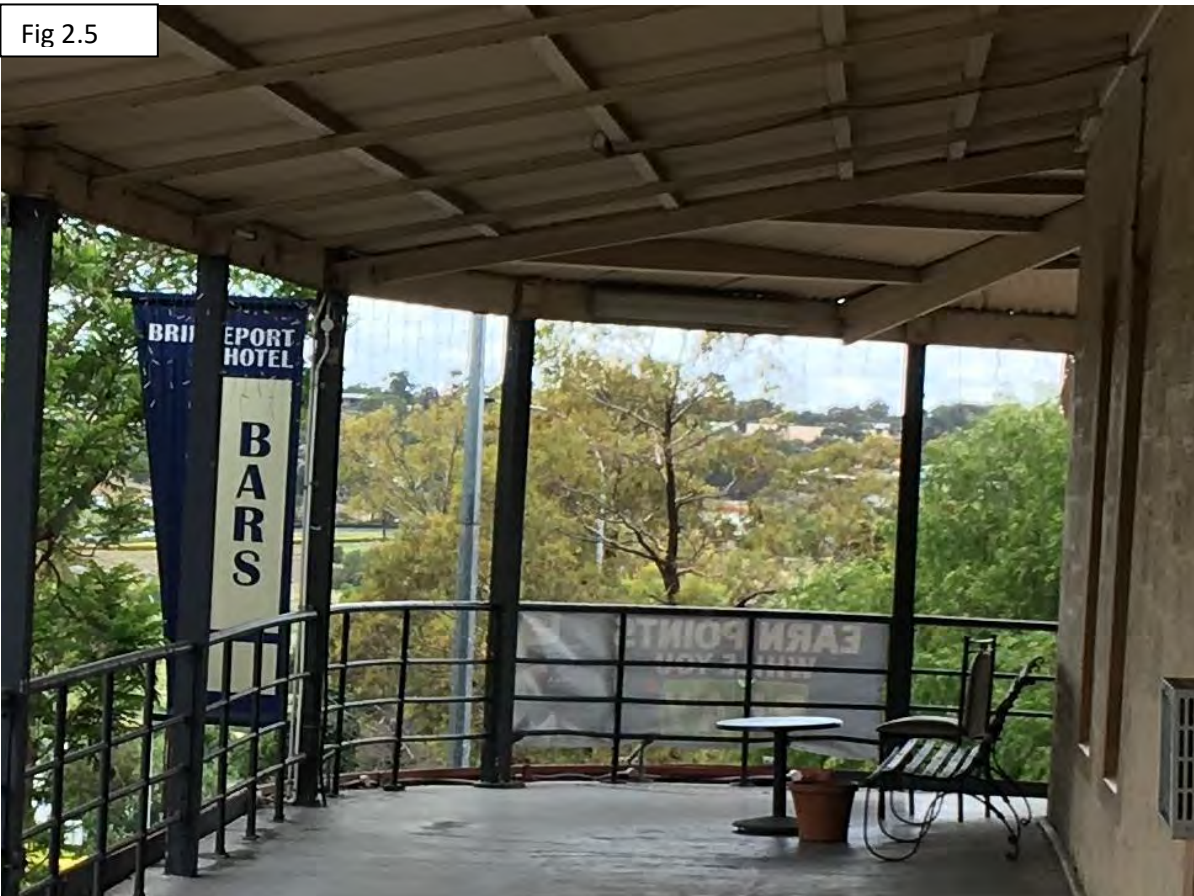


Bridgeport Hotel Murray Bridge

Fig 2.4



Fig 2.5



Bridgeport Hotel Murray Bridge

Fig 2.6



Fig 2.7



Bridgeport Hotel Murray Bridge

Fig 2.8



Fig 2.9



Bridgeport Hotel Murray Bridge

Fig 2.10



BUILDING ALTERATIONS:

- SUBSTANTIAL PARTS OF THE ORIGINAL LOADBEARING BUILDING FABRIC HAS BEEN REMOVED WITHIN THE HATCHED AREA, EVIDENCE OF ORIGINAL WALL LOCATIONS INCLUDES PIERS & NEWER SUPPORT BEAMS VISIBLE ABOVE THE ALUM' FRAMED PLASTERBOARD CEILING GRID
- 1.1

NEW CONCRETE COLUMNS & CONCRETE FIRST FLOOR SUPPORT BEAMS INSTALLED - 1960s
- 1.2

ORIGINAL STONE FACADE WALLS REMOVED - 1960s
- 1.3

CONCRETE FOOTINGS AND PIERS VISIBLE IN THE BASEMENT INSTALLED - PRESUME 1960s
- 1.4

ORIGINAL STONE FACADE WALLS AND TIMBER FRAMED WINDOWS & DOORS REMOVED & REPLACED WITH CONCRETE & MASONRY - 1960s
- 1.5

INTERNAL LOADBEARING MASONRY WALLS REMOVED - PRESUME 1960s
- 1.6

NON-ORIGINAL FIREPLACES
- 1.7

NEW SUPPORT COLUMN INSTALLED - PRESUME 1990s RELATED TO GAMING
- 1.8

DOOR OPENING & CHUTE TO CELLAR INSTALLED - 1960s
- 1.9

OLD WINDOW OPENING COVERED BY CREAM BRICK WITH BREEZE HOLES, NEW BATHROOM WINDOW SIZE CHANGED - 1960s
- 1.10

ORIGINAL ARCHED HEAD OPENING SQUARED OFF WITH CONCRETE LINTEL, NEW DOOR & WINDOW FRAMES INSTALLED - 1960s
- 1.11

ORIGINAL TIMBER FRAMED WINDOW REPLACED WITH ALUM' FRAME - 1960s
- 1.12

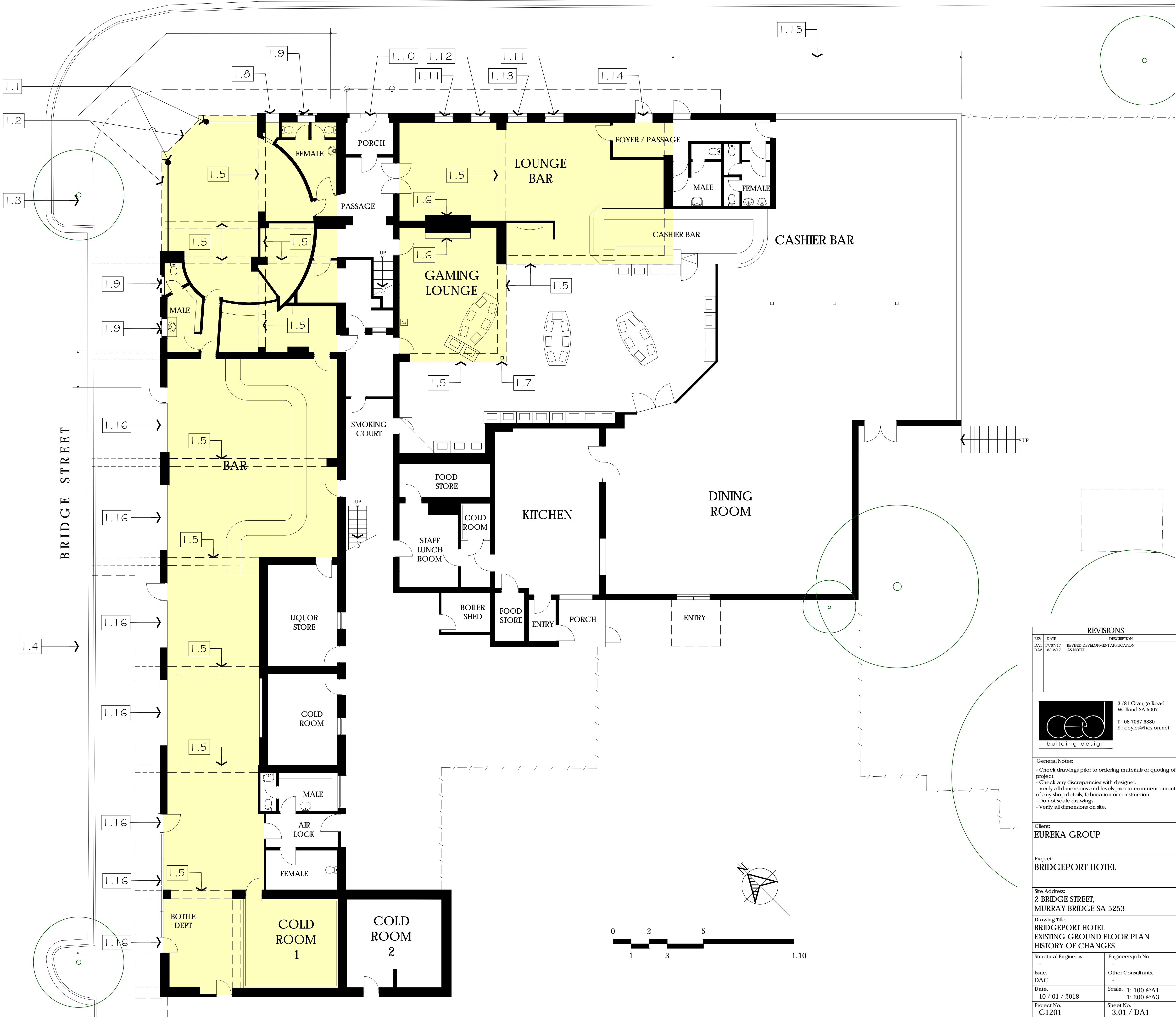
NEW WINDOW OPENING CUT INTO MASONRY - 1960s
- 1.13

LOWER DOOR FRAME CONVERTED TO HIGHER LEVEL WINDOW WITH WALL CUT AT HEAD AND INFILLED AT BASE - 1960s
- 1.14

ORIGINAL WINDOW OPENING CHANGED TO NEW DOOR WITH WALL CUT AT BASE - 1960s
- 1.15

LATER CONSTRUCTION SINGLE STOREY CREAM BRICK BUILDING WITH FLAT ROOF
- 1.16

ORIGINAL WINDOWS & DOORS REMOVED & REPLACED WITH LARGER OPENINGS TO NEW WALL - 1960s



REVISIONS	
REV	DATE
DA1	17/07/17
DA2	18/12/17

DESCRIPTION	
REVISED DEVELOPMENT APPLICATION AS NOTED.	

3 /81 Grange Road
Welland SA 5007
T: 08 7087 6880
E: ceyles@ced.on.net

General Notes:

- Check drawings prior to ordering materials or quoting of project.
- Check any discrepancies with designer.
- Verify all dimensions and levels prior to commencement of any shop details, fabrication or construction.
- Do not scale drawings.
- Verify all dimensions on site.

Client:

EUREKA GROUP

Project:

BRIDGEPORT HOTEL

Site Address:

2 BRIDGE STREET,
MURRAY BRIDGE SA 5253

Drawing Title:

BRIDGEPORT HOTEL
EXISTING GROUND FLOOR PLAN
HISTORY OF CHANGES


Structural Engineers:	Engineers Job No.
Issue:	Other Consultants:
DAC	-
Date:	Scale: 1: 100 @A1
10 / 01 / 2018	1: 200 @A3
Project No:	Sheet No.
C1201	3.01 / DA1

BUILDING ALTERATIONS:

- 2.1 CHIMNEY REMOVED 1960s
- 2.2 CHIMNEY REMOVED 1960s
- 2.3 CHIMNEY REMOVED 1960s
- 2.4 BATHROOMS WITH TERRAZZO FLOORS DATING FROM THE 1960s
- 2.5 BALCONY & VERANDAH CORNER SHAPE ALTERED 1960s WITH NEW ROOF & FLOOR FRAMING
- 2.6 BALCONY FLOOR SUBSTANTIALLY ALTERED 1960s & VERANDAH ROOF & FRAMING ALTERED 1960s
- 2.7 GROUND FLOOR VERANDAH ROOF ADDED 1960s
BALCONY FLOOR & ROOF FRAMING ALTERED 1960s
- 2.8 STAIRCASE REMOVED 1990s
- 2.9 BALCONY FLOOR COVERING UPDATED TO FIBRE CEMENT SHEET - DATE UNKNOWN
- 2.10 BALCONY FLOOR FACADE SUBSTANTIALLY CHANGED - 1960s



REVISIONS		
REV	DATE	DESCRIPTION
DA1	17/07/17	REVISED DEVELOPMENT APPLICATION
DA2	18/12/17	AS NOTED.



3 /81 Grange Road
Welland SA 5007
T: 08 7087 6880
E: ceyles@hcs.on.net

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Client:
EUREKA GROUP

Project:
BRIDGEPORT HOTEL

Site Address:
2 BRIDGE STREET,
MURRAY BRIDGE SA 5253

Drawing Title:
BRIDGEPORT HOTEL
EXISTING FIRST FLOOR PLAN
HISTORY OF CHANGES

Structural Engineers.	Engineers Job No.
Issue.	Other Consultants.
DAC	-
Date.	Scale.
10 / 01 / 2018	1: 100 @A1 1: 200 @A3
Project No.	Sheet No.
C1201	3.02 / DA1

In reply please quote 2017/01904/01, Process ID: 486356
Enquiries to Matthew Henderson
Telephone 0419 747 010
Facsimile (08) 8226 8330
E-mail dpti.luc@sa.gov.au



Government of South Australia

Department of Planning,
Transport and Infrastructure

31/10/2017

State Planning Commission
C/- Mr Troy Fountain
Department of Planning, Transport and Infrastructure
GPO Box 1815
ADELAIDE SA 5001

**SAFETY AND SERVICE –
Traffic Operations**

GPO Box 1533
Adelaide SA 5001

Telephone: 61 8 8226 8222
Facsimile: 61 8 8226 8330

ABN 92 366 288 135

Dear Troy,

SCHEDULE 8 - REFERRAL RESPONSE

Development No.	415/E014/16
Applicant	Kerin Bay Pty Ltd
Location	2-6 Bridge Street Murray Bridge
Proposal	Demolition of existing hotel and construction of a new hotel

I refer to the above development application forwarded to the Safety and Service Division of the Department of Planning, Transport and Infrastructure (DPTI) in accordance with Section 37 of the *Development Act 1993*. The proposed development involves development adjacent a main road as described above.

The following response is provided in accordance with Section 37(4)(b) of the *Development Act 1993* and Schedule 8 of the *Development Regulations 2008*.

CONSIDERATION

SSD has provided advice regarding a previous referral of this application in a letter dated 7 October 2016. SSD has reviewed the amended plans referred by the Commission and considers that the previously provided advice does not require amendment. A copy of the advice previously provided is appended to this letter.

SSD recommended a number of conditions to be applied to any approval granted in the previous advice. These are reproduced below for ease of reference. It should be noted that recommended condition 2 has been amended to reflect a staff change.

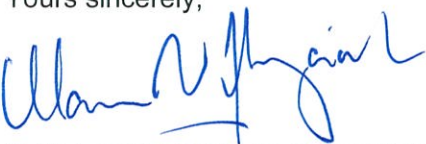
ADVICE

In view of the above, SSD does not object in-principle to the proposed development. The planning authority is advised to attach the following conditions to any approval:

1. The site shall be accessed via a single access point to/from Bridge Street, with the balance of access being via East Terrace. The Bridge Street access shall provide left-in, left-out and right-in movements only.
2. The Bridge Street access shall be provided with a channelised right turn treatment that is designed and constructed to DPTI and Council satisfaction. All costs (including design, construction, project management and any changes to road drainage, lighting etc. required) shall be borne by the applicant. Prior to undertaking detailed design, the applicant shall contact Mr Justin Henderson, Asset Enhancement Engineer, Road and marine Assets on telephone (08) 7223 6029 or via email Justin.henderson@sa.gov.au to progress this.

3. The applicant shall enter into a Developer Agreement with DPTI to undertake and complete the required road works.
4. All road works associated with the development shall be completed prior to the commencement of operation of the development.
5. All vehicles shall enter and exit the site in a forward direction.
6. Signage and line marking shall be utilised to reinforce the desired flow of traffic to, from and through the site.
7. The largest vehicle permitted on site shall be an 11 metre long rigid truck. This vehicle shall enter the site via the Bridge Street access and exit the site via the southern East Terrace access point.
8. All service vehicle movements associated with the development shall be undertaken outside of peak traffic hours on the adjacent roads and peak times of site operation.
9. All car parking and internal manoeuvring areas shall be in accordance with AS/NZS 2890.1:2004 and 2890.6:2009.
10. All commercial vehicle facilities shall be designed in accordance with AS 2890.2-2002.
11. All redundant crossovers to/from the site shall be reinstated to Council standard kerb and gutter at the applicant's expense prior to the operation of the development.
12. All materials and finishes shall not be permitted to result in glare or other effects that will result in the discomfort or impairment of road users.
13. Signage on this site that is viewable from the adjacent or nearby roads shall not utilise any element of LED/LCD display (the use of LED lighting for the internal illumination of a sign box is permissible).
14. Signage on this site shall not contain any element that flashes, scrolls, moves or changes.
15. Illuminated signage on this site shall be limited to a low level of illumination (≤ 200 cd/m²) so as to minimise distraction and discomfort to motorists.
16. Non-illuminated signage on this site shall be finished in a material of low reflectivity to minimise the risk of sun/headlamp glare that may dazzle or distract motorists.
17. The utilisation of Trailer Mounted Variable Message Displays for advertising purposes shall not occur on or adjacent to the subject land.
18. Stormwater run-off shall be collected on-site and discharged without jeopardising the safety and integrity of the adjoining arterial road. Any alterations to road drainage infrastructure required to accommodate the development shall be at the expense of the applicant.

Yours sincerely,



MANAGER, TRAFFIC OPERATIONS
For **COMMISSIONER OF HIGHWAYS**

A copy of the decision notification form should be forwarded to dpti.developmentapplications@sa.gov.au

In reply please quote 2016/00202/01, Process ID: 419157
Enquiries to Matthew Henderson
Telephone (08) 8226 8388
Facsimile (08) 8226 8330
E-mail dpti.luc@sa.gov.au



Government of South Australia
Department of Planning,
Transport and Infrastructure

07/10/2016

Development Assessment Commission
C/- Mr Troy Fountain
Department of Planning, Transport and Infrastructure
GPO Box 1815
ADELAIDE SA 5001

**SAFETY AND SERVICE –
Traffic Operations**

GPO Box 1533
Adelaide SA 5001

Telephone: 61 8 8226 8222
Facsimile: 61 8 8226 8330

ABN 92 366 288 135

Dear Mr Fountain,

SCHEDULE 8 - REFERRAL RESPONSE

Development No.	415/E014/16
Applicant	Kerin Bay Pty Ltd
Location	2-6 Bridge Street Murray Bridge
Proposal	Building work - demolition of existing hotel and construction of a new replacement hotel building.

I refer to the above development application forwarded to the Safety and Service Division (SSD) of the Department of Planning, Transport and Infrastructure (DPTI) in accordance with Section 37 of the *Development Act 1993*. The proposed development involves development adjacent a main road as described above.

The following response is provided in accordance with Section 37(4)(b) of the *Development Act 1993* and Schedule 8 of the *Development Regulations 2008*.

THE PROPOSAL

The application seeks approval for the demolition of an existing hotel and drive through liquor store and the construction of a new hotel that is 6 storeys in height and includes bar, restaurant and gaming facilities, along with accommodation (97 rooms), a gym and a pool, together with car parking and a detached drive through liquor store.

CONSIDERATION

Access and road safety

The site is bounded by Bridge Street, an arterial road under the care, control and management of DPTI, together with East Terrace and South Terrace, local roads under the care, control and management of the City of Murray Bridge.

Bridge Street has historically been the main street of Murray Bridge, and exemplifies the built form and road layout that is typical of the main streets of rural centres in South Australia. The construction of the South Eastern Freeway as a bypass of Murray Bridge has meant that since that time Bridge Street generally carries little through traffic (however, it does provide an alternate crossing of the River Murray). This is reflected in the relatively low traffic volumes (DPTI counts indicate approximately 11800 vehicles per day west of Railway Terrace and 8400 vehicles per day east of Railway Terrace) and low speed (50km/h posted speed limit) environment.

10876006

There are currently two vehicular access points to/from Bridge Street serving the site. One of these access points serves the drive through liquor store, which is located on the boundary, and is directly opposite the Bridge Street / Railway Terrace junction, creating a highly undesirable 4-way intersection arrangement. Anecdotal evidence also indicates that this access point experiences queues that extend onto the road at peak times of operation for the drive through liquor store due to the orientation of traffic flow being in from Bridge Street and out to the car park. SSD strongly supports the proposed closure of this access point. The crossover should be reinstated to Council standard kerb and gutter at the applicant's cost at the earliest possible stage.

The remaining Bridge Street access is approximately 6 metres east of the western property boundary. This access is proposed to be retained, and provided with a short channelised right turn treatment for ingress movements through alteration of the solid median scheme on Bridge Street. Although SSD would not generally support the provision of right turn movements such as this at a location where there are already a significant number of potentially conflicting vehicle movements, on review of the alternatives, (particularly the Bridge Street / East Terrace junction, which has limited sightlines and constrained road space available at the junction), the proposed access alteration represents the best possible outcome.

Following on from the above, the concept design provided in the Traffic and Parking Report by MFY, although falling short of the recommended design parameters from the Austroads Guide to Road Design, is generally acceptable to SSD. It is nonetheless recommended that the detailed design of this treatment include:

- Investigation of whether the pedestrian refuge can be moved further south west to provide additional storage for the right turn treatment;
- Refinement of the design to better prevent right turn egress movements through the installation of back-to-back kerbing on the opposite side of the channelised treatment;
- A 'no right turn sign' to be located generally opposite the access point.

The above road works are required to ensure safe right turn access into the development from Bridge Street and must be completed prior to operation of the development. It should be noted that road lighting will need to be reviewed during the detailed design process and may require upgrading. All costs will be borne by the applicant.

With respect to the design of the Bridge Street access point, SSD notes that the access proposed appears to be in the order of 6.0 metres in width, which will cater for simultaneous two-way movements of passenger vehicles. However, the turn paths produced in support of the application indicate that an 11.0 metre rigid truck (the nominated largest vehicle to service the site) would require the entirety of the access to complete a left-in manoeuvre. Consequently, it would be beneficial to widen the access to provide for simultaneous movements of an 11.0 metre truck with a B85 passenger vehicle.

Car parking and internal manoeuvring

The Traffic and Parking Report by MFY indicates that the largest vehicle expected on site for waste collection and deliveries is an 11 metre rigid truck. DPTI is satisfied that this vehicle will be able to enter the site via Bridge Street and exit the site to East Terrace in a forward direction. Deliveries and refuse collection should be undertaken outside of the peak times of operation for the site to reduce the likelihood of pedestrian / vehicular conflict and minimise the impact of these movements on traffic flow through the site.

The application proposes 151 on-site car parks. The Commission should be satisfied that sufficient off-street car parking consistent with AS/NZS 2890.1:2004 and AS/NZS 2890.6:2009 is provided to cater for the proposed land uses.

Signage and building finishes

The plans referred to SSD do not indicate the final format of signage on the site (i.e. whether illuminated or non-illuminated etc), albeit that the elevations provided indicate signage will be installed on both the hotel and drive through liquor store. SSD advises that any signage proposed should accord with DPTI's *Advertising Signs: Assessment Guidelines for Road Safety* and adhere to the following design principles:

- Signage upon the site should not contain any element of LED or LCD display that is viewable from the adjacent roads,
- Signage upon the site should be limited to a low level of illumination so as to minimise distraction to motorists,
- Signage upon the site should not contain any element that flashes, scrolls, moves or changes,
- Signage upon the site should be finished in a material of low reflectivity to minimise the risk of sun/headlamp glare that may affect motorists perception of the road,
- The utilisation of Trailer Mounted Variable Message Displays for advertising purposes should not be permitted on or adjacent to the subject land without development approval.

With respect to building materials and finishes, it will be necessary to ensure that all materials and finishes do not result in glare or other effects that will result in the discomfort or impairment of road users.

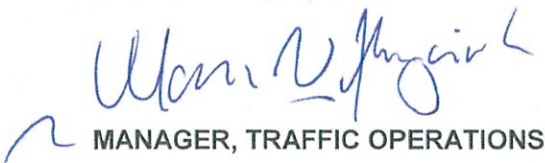
ADVICE

SSD does not object in-principle to the proposed development. The planning authority is advised to attach the following conditions to any approval:

1. The site shall be accessed via a single access point to/from Bridge Street, with the balance of access being via East Terrace. The Bridge Street access shall provide left-in, left-out and right-in movements only.
2. The Bridge Street access shall be provided with a channelised right turn treatment that is designed and constructed to DPTI and Council satisfaction. All costs (including design, construction, project management and any changes to road drainage, lighting etc. required) shall be borne by the applicant. Prior to undertaking detailed design, the applicant shall contact DPTI's Technical Officer, Ms Ali Allen on telephone (08) 7223 6022 or via email Alison.Allen@sa.gov.au to progress this.
3. The applicant shall enter into a Developer Agreement with DPTI to undertake and complete the required road works.
4. All road works associated with the development shall be completed prior to the commencement of operation of the development.
5. All vehicles shall enter and exit the site in a forward direction.
6. Signage and line marking shall be utilised to reinforce the desired flow of traffic to, from and through the site.

7. The largest vehicle permitted on site shall be an 11 metre long rigid truck. This vehicle shall enter the site via the Bridge Street access and exit the site via the southern East Terrace access point.
8. All service vehicle movements associated with the development shall be undertaken outside of peak traffic hours on the adjacent roads and peak times of site operation.
9. All car parking and internal manoeuvring areas shall be in accordance with AS/NZS 2890.1:2004 and 2890.6:2009.
10. All commercial vehicle facilities shall be designed in accordance with AS 2890.2-2002.
11. All redundant crossovers to/from the site shall be reinstated to Council standard kerb and gutter at the applicant's expense prior to the operation of the development.
12. All materials and finishes shall not be permitted to result in glare or other effects that will result in the discomfort or impairment of road users.
13. Signage on this site that is viewable from the adjacent or nearby roads shall not utilise any element of LED/LCD display (the use of LED lighting for the internal illumination of a sign box is permissible).
14. Signage on this site shall not contain any element that flashes, scrolls, moves or changes.
15. Illuminated signage on this site shall be limited to a low level of illumination (≤ 200 cd/m²) so as to minimise distraction and discomfort to motorists.
16. Non-illuminated signage on this site shall be finished in a material of low reflectivity to minimise the risk of sun/headlamp glare that may dazzle or distract motorists.
17. The utilisation of Trailer Mounted Variable Message Displays for advertising purposes shall not occur on or adjacent to the subject land.
18. Stormwater run-off shall be collected on-site and discharged without jeopardising the safety and integrity of the adjoining arterial road. Any alterations to road drainage infrastructure required to accommodate the development shall be at the expense of the applicant.

Yours sincerely,



MANAGER, TRAFFIC OPERATIONS

For COMMISSIONER OF HIGHWAYS

A copy of the decision notification form should be forwarded to dpti.developmentapplications@sa.gov.au

File No:
2014/20674/01

28 February 2018

Ref No:
12407763

Mr Troy Fountain
Planning Officer
Development Division
Department of Planning, Transport and Infrastructure
Level 5, 50 Flinders Street
Adelaide SA 5000

For the attention of the State Commission Assessment Panel

2-6 Bridge Street, Murray Bridge

Further to the revised drawings received 22 February 2018 in regards to the referral 415/E014/16, pertaining to the development application at the above address and in my capacity as a non-mandatory referral in the State Commission Assessment Panel, I would like to offer the following comments for your consideration. This letter supersedes the previous referral letter dated 15 November 2017.

The project team participated in one Design Workshop in 2015 and the proposal was presented to the Associate Government Architect in 2016. After the development application was lodged in August 2016, a site visit was carried out by the Government Architect on 28 March 2017. The project team attended two further design meetings with the Associate Government Architect in April and May 2017 regarding the building's expression and built form relationship with the streetscape context, over which period the project progressed considerably. Since the previous referral letter was issued, another design meeting was held in December 2017 to discuss outstanding concerns. A pre-lodgement agreement was not reached in advance of lodgement.

The proposed development is for the demolition of the existing Local heritage listed Bridgeport Hotel and construction of a six storey hotel building, separate drive-through liquor outlet and undercroft car park. I acknowledge the findings by the heritage consultant that over time the heritage value of the existing hotel has been significantly compromised. I support the potential of the development to contribute to the revitalisation of the area by increasing activity in the locality. I also support the continued use of this landmark corner as an iconic hotel, and the proposed intensity and diversity of uses. This proposal has the potential to offer a benchmark for development of this size and type on this landmark site in Murray Bridge and will be a defining structure at the entry to the township.

Located within the town centre, the site is the first on the approach over the bridge. The site is bound by Bridge Street to the north west and East Terrace to the north east. Bridge Street includes single and two storey commercial and institutional buildings, typical of a regional town main street. The topography of the site is relatively flat along Bridge Street, with a significant fall to the south east. East Terrace fronts onto a reserve that runs down to the river. The Murray Bridge Marketplace, a relatively recent shopping complex development, is located to the south over South Terrace. This development is singular in form, however the above

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Government
of South Australia

File No:
2014/20674/01

Ref No:
12407763

ground heights vary from approximately one to three storeys due to the site topography.

The height of the proposed development is six storeys (23.3 metres) along the Bridge Street frontage, which exceeds the maximum height of eight metres envisaged by the Development Plan. On balance, I support the proposed height as in my view, the podium and setback built form sufficiently mitigates the scale of the development. The scale and horizontal emphasis of the podium, together with the regularly spaced columns and canopy, respond to the established main street shopfront character. While in my opinion, a continuous built frontage to Bridge Street would strengthen the main street character, on balance I support the landscaped timber screen that assists in obscuring the car park. The existing heritage hotel building has a strong corner presence, which is characteristic of historic corner hotels. The proposed development intends to address the corner by building to the corner boundaries and introducing a corner splay.

The ground floor has been configured to include active uses along all four frontages, which I support. The main entrance, defined by a double height feature surround, is accessed from the car park. An entrance is also proposed on Bridge Street, which is configured to include an airlock with dual access to the lounge and sports bar. The egress stair exits out into the entry alcove. I acknowledge access to the hotel is likely to most occur via the at grade and undercroft car parks. I also acknowledge the change in materials to define the Bridge Street entry. While I am of the view that the Bridge Street entrance configuration can be improved, I acknowledge and support the provision of the entry gate on Bridge Street with the intent to define the entry point and improve arrival experience.

The ground level includes multiple bars and dining areas, an outdoor children's play area, alfresco bar and seating areas, a gaming area, foyer, kitchen and associated amenities and storage areas. The first floor includes a function room, bar, kitchen, terrace, 12 accommodation rooms, gymnasium and an outdoor pool and bar for hotel guests. Levels two to five are dedicated to hotel accommodation, with 22 rooms on each floor. I support the internal planning of the proposal, including the consideration given to light access to the shared corridors and lift lobbies and hotel room configurations.

The proposal incorporates 160 car parking spaces, both at grade and within the undercroft. According to the Development Plan the proposed development requires 387 spaces. I note that the proposal presents a significant shortfall in required car parking and I am concerned by the potential detrimental impact on local traffic conditions. However I defer comment on car parking numbers to the relevant authorities and specialists. Bicycle storage is located in the northern corner of the undercroft car park and 12 additional bicycle parking spaces are proposed around the southern entrance. I support the inclusion of conveniently located visitor bicycle parking spaces on the ground floor.

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On balance, I support the proposed architectural expression that attempts to break down the building mass and provide visual interest. The architectural expression of the building is characterised by a single storey podium clad with perforated metal screening. The northwest (Bridge Street) frontage includes an alfresco dining terrace with fixed umbrellas and raised planter beds. The hotel accommodation building above is characterised by vertically proportioned white metal panel cladding, continuous vertical glazing to the lift lobbies with expressed structure and a black square metal cladding element with a large 'B' that defines the entrance. A



File No:
2014/20674/01

Ref No:
12407763

similar expression is proposed to the south east (South Terrace) facade, with the exception of the vertical glazed and black elements. The north east (East Terrace) and south west (car park) frontages are horizontally proportioned with expressed floor plates, white/black metal panel cladding accents and timber facade cladding. Large 'BRIDGEPORT HOTEL' lettering is proposed at the top of the building on the East Terrace frontage. The submitted materials include indicative images for the perforated picture mesh. I anticipate further development of the screening details, including the scale and final images of the perforation, in the next phase of the design development, with the view to achieve a coherent overall architectural expression.

The landscape proposal includes shade trees within the car park that are intended to soften the sealed surface, provide scale to the proposed built form and provide amenity for site users. Low shrub and groundcover plantings are also proposed within garden beds and a timber batten screen with planting is located adjacent Bridge Street to visually soften and enclose the carpark when viewed from the street. The two existing palms on site are proposed to be relocated. The upper level terrace also includes a raised planter. I support the proposed landscape initiatives and consideration given to maintenance and species selections cognisant of the site conditions.

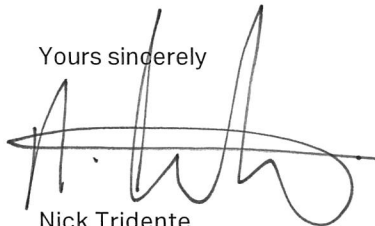
A new drive through liquor outlet is also proposed at the rear of the site accessed from South Terrace and includes a three lane drive through canopy. I support the location of the drive through and consider the simple design and material palette to be appropriate relative to the context.

The proposal does not present an Ecologically Sustainable Design (ESD) strategy. As a minimum, I urge further consideration of passive environmental principles as design development progresses.

To ensure the most successful design outcome is achieved, the State Commission Assessment Panel may like to consider particular aspects of the project, which would benefit from further development prior to approval:

- Provision of additional details on picture mesh elements to demonstrate the overall visual effect.

Yours sincerely



Nick Tridente
South Australian Associate Government Architect

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Ref: SH/26373D
Date: 12 December 2017

Secretary - Ms Alison Gill
State Commission Assessment Panel
GPO Box 1815
ADELAIDE SA 5001
Attention: Troy Fountain

Dear Mr Fountain

DESCRIPTION: DEMOLITION OF EXISTING LOCAL HERITAGE PLACE (HOTEL) AND CONSTRUCTION OF A 6 STOREY HOTEL BUILDING INCLUDING UNDERCROFT CARPARKING, OUTDOOR DINING, POOL, RESTAURANT, GAMING AREA, GYM AND DRIVE-THRU LIQUOR SHOP 2-6 BRIDGE STREET, MURRAY BRIDGE

Application number: 415/E014/16
Referral received: 20/10/2017
State heritage place: Murray Bridge Transport Precinct (designated place of archaeological significance). Place bordered by: Railway Tce/East Tce/Wharf Rd/Hume Res Rd/Bridge Rd MURRAY BRIDGE

The above application has been referred to the Minister for Sustainability, Environment and Conservation in accordance with Section 37 of the *Development Act 1993* as development that directly affects a State heritage place or, in the opinion of the relevant authority, materially affects the context within which a State heritage place is situated.

The proposed development is considered to be acceptable in relation to the above State heritage place for the following reason/s.

- The proposed development consists of a six storey hotel building, separate drive-through liquor outlet and undercroft carpark located on the site of an existing Local Heritage place. The site is separated by roads from the State heritage place, and does not physically affect the State heritage place. The archaeological significance of the State heritage place is not affected.
- The proposed development is restrained and contemporary in style and is not considered to visually detract from the State Heritage place opposite.
- The height of the development, whilst relatively tall in its immediate one and two storey built environment, is not without precedent as nearby industrial buildings are of a similar height and bulk.
- The setting, context and meaning of the State Heritage place would therefore not be diminished nor adversely affected by the proposed new development.

General notes

1. Any changes to the proposal for which planning consent is sought or granted may give rise to heritage impacts requiring further consultation with the Department of Environment, Water and Natural Resources, or an additional referral to the Minister for Sustainability, Environment and Conservation. Such changes would include for example (a) an application to vary the planning consent, or (b) Building Rules documentation that incorporates differences from the proposal as documented in the planning application.

2. In accordance with Regulation 43 of the *Development Regulations 2008*, please send the Department of Environment, Water and Natural Resources a copy of the Decision Notification.
3. Council is requested to inform the applicant of the following requirements of the *Heritage Places Act 1993*.
 - (a) If an archaeological artefact believed to be of heritage significance is encountered during excavation works, disturbance in the vicinity shall cease and the SA Heritage Council shall be notified.
 - (b) Where it is known in advance (or there is reasonable cause to suspect) that significant archaeological artefacts may be encountered, a permit is required prior to commencing excavation works.For further information, contact the Department of Environment, Water and Natural Resources.
4. Council is requested to inform the applicant of the following requirements of the *Aboriginal Heritage Act 1988*.
 - (a) If Aboriginal sites, objects or remains are discovered during excavation works, the Aboriginal Heritage Branch of the Aboriginal Affairs and Reconciliation Division of the Department of the Premier and Cabinet (as delegate of the Minister) should be notified under Section 20 of the *Aboriginal Heritage Act 1988*.

Any enquiries in relation to this application should be directed to Kevin O'Sullivan on telephone (08) 8124 4922 or e-mail DEWNR.StateHeritageDA@sa.gov.au.

Yours sincerely



Peter Wells

Principal Conservation Architect

Department of Environment, Water and Natural Resources

as delegate of the

MINISTER FOR SUSTAINABILITY, ENVIRONMENT AND CONSERVATION

Fielke, Matthew (DPTI)

From: Widdowson, Jane (AGD)
Sent: Monday, 20 November 2017 5:48 PM
To: Fountain, Troy (DPTI)
Cc: 'Ben Allen'
Subject: FW: Bridgeport Hotel- Referral - 415/E014/16 - Due 15/12/2017 (BLL 215074) [WALL-Matters.FID352297]

Good Afternoon Mr Fountain

Thank you for your time today and for your email advising of the Development Application for the Bridgeport Hotel in Murray Bridge.

I advise that the Liquor and Gambling Commissioner has no comment to make in relation to the Development Application.

As discussed an application for alterations will need to be made and approved by the Liquor and Gambling Commissioner prior to the commencement of any building works at the premises.

Should you have any further queries regarding this matter please contact me.

Regards

Jane

Jane Widdowson
Team Leader Licensing and Registration
Consumer and Business Services
95 Grenfell Street, Adelaide
Email: jane.widdowson@sa.gov.au
Tel: 08 8204 8557

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From: Ben Allen [<mailto:ben.allen@wallmans.com.au>]
Sent: Monday, 20 November 2017 11:21 AM
To: Inge, Darren (AGD); Widdowson, Jane (AGD)
Subject: FW: Bridgeport Hotel- Referral - 415/E014/16 - Due 15/12/2017 (BLL 215074) [WALL-Matters.FID352297]

Hi Darren/Jane-

Are you able to assist with this in John's absence please ?

Thanks



Ben Allen

Partner

Hospitality, Liquor Licensing and Gambling
Local Government, Planning and Environment
Wallmans Lawyers

Direct Tel (08) 8235 3018
Mobile 0411 705 078
Fax (08) 8232 0926
ben.allen@wallmans.com.au

Level 5, 400 King William St
Adelaide SA 5000
GPO BOX 1018
Adelaide SA 5001

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wallmans.com.au



From: Ben Allen

Sent: Friday, 17 November 2017 5:30 PM

To: 'Doran, John (AGD)'; 'Templeton, Robert (AGD)'

Subject: RE: Bridgeport Hotel- Referral - 415/E014/16 - Due 15/12/2017 (BLL 215074) [WALL-Matters.FID352297]

Hello-

Are you able to assist here pleased ?

Thanks



Ben Allen

Partner

Hospitality, Liquor Licensing and Gambling
Local Government, Planning and Environment
Wallmans Lawyers

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From: Ben Allen

Sent: Friday, 10 November 2017 5:32 PM

To: 'Doran, John (AGD)'; 'Templeton, Robert (AGD)'

Subject: Bridgeport Hotel- Referral - 415/E014/16 - Due 15/12/2017 (BLL 215074) [WALL-Matters.FID352297]

Hi John/Rob-

We act for the licensee of the above.

You will see correspondence below between DPTI and your office in connection with a referral under the Development Act regarding proposed changes to the gaming room.

I presume you will give your standard response, as per the attached.

We would be grateful if you could consider the request from DAC, and respond to them direct as you see fit, in order to allow the application to proceed without delay.

Could you please send me a copy of any response provided.

Thanks



Ben Allen

Partner

Hospitality, Liquor Licensing and Gambling
Local Government, Planning and Environment
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From: Leahey, Hannah (AGD)
Sent: Tuesday, 24 October 2017 10:24 AM
To: Fountain, Troy (DPTI) <Troy.Fountain@sa.gov.au>
Subject: RE: Referral - 415/E014/16 - Due 15/12/2017

Hi Troy

Thank you for advising.

We do not have any record on our system of this application for the premises known as the Bridgeport Hotel, therefore it appears that this may be an application in its early stages that has not been lodged with our Office yet.

The licensee will need to lodge an application with us prior to commencing any redefinitions or alterations to the licensed areas.

Kind regards,

Hannah Leahey
Senior Client Services Officer
Consumer and Business Services
95 Grenfell Street, Adelaide
Email: Hannah.Leahey@sa.gov.au
Tel: 08 8226 8905

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From: Fountain, Troy (DPTI)
Sent: Tuesday, 24 October 2017 9:51 AM
To: Leahey, Hannah (AGD)
Subject: RE: Referral - 415/E014/16 - Due 15/12/2017

Hi Hannah

In relation to the Bridgeport Hotel – 2-4 Bridge Street, Murray Bridge. The existing hotel which they are planning to demolish and reconstruct with a modern 6 storey building contains pokie machines and will include pokie machines in the future. I believe the numbers of machines and the size of the pokie machine area remains similar between the existing and proposed.

Any other questions, let me know.



Troy Fountain
Project Lead

Planning Reform | Planning and Development

Department of Planning, Transport and Infrastructure

T (08) 8343 2704 (22704) • E troy.fountain@sa.gov.au

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www.murraybridge.sa.gov.au

ABN 90 501 266 817

415/364/2016 – GLS

17 November 2017

Chairman
State Commission Assessment Panel
GPO Box 1815
ADELAIDE SA 5001

Dear Mr Chairman and Commission Members

RE: REFERRAL RESPONSE – DEVELOPMENT APPLICATION 415/E014/16 (REDEVELOPMENT OF THE BRIDGEPORT HOTEL)

Thank you for providing the Rural City of Murray Bridge with the opportunity to comment on Development Application 415/E014/16.

The Council has undertaken a thorough process to inform the content of this referral response including the consideration of the Application by staff and the Council Assessment Panel (CAP).

Enclosed in this referral response package are the following documents:

- Correspondence from the Council Assessment Panel;
- Minutes of the Council Assessment Panel meeting held (17 November 2017); and
- A copy of the Assessment Report, prepared by Council staff.

The Council would be happy to provide further clarification in regards to this referral response and to assist with the assessment of any amendments or further information that may be sought or provided.

Thank you in advance for your consideration of this matter.

Yours faithfully

GLENN SEARLE
SENIOR PLANNING OFFICER



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ABN 90 501 266 817

415/364/2016 – GLS

17 November 2017

Chairman
State Commission Assessment Panel
GPO Box 1815
ADELAIDE SA 5001

Dear Mr Chairman and Commission Members

RE: REFERRAL RESPONSE – DEVELOPMENT APPLICATION 415/E014/16 (REDEVELOPMENT OF THE BRIDGEPORT HOTEL)

The Council Assessment Panel of the Rural City of Murray Bridge, at its meeting held on Friday 17 November 2017, considered the amended design for the redevelopment of the Bridgeport Hotel as set out in Development Application 415/E014/16.

The Panel was provided with a number of documents including a detailed Assessment Report prepared by Council Officers, plans and details of the development and accompanying specialist reports commissioned by the Council and the developer.

In addition the Panel undertook an inspection of the land (from various vantage points) and held extensive deliberations regarding the proposal before reaching a resolution.

The Council Assessment Panel resolved to provide the following comments/amendments:

1. Support – The Council Assessment Panel is highly supportive of the concept of further development of the existing Bridgeport Hotel site. A substantial further development is likely to have significant economic and social benefits for Murray Bridge, including the provision of additional high end accommodation for the town and region.

The further development of the site is consistent with the established land use and the use of the land for a hotel is supported by the Development Plan.

2. Additional Information – Heritage - The Council Assessment Panel is not convinced that all efforts have been made to demonstrate that the ground floor heritage fabric has been removed. This failure to demonstrate that either:
 - o There is no heritage value remaining in the fabric of the ground floor; or
 - o There is a structural issue with the ground floor that the white brick is required to support the structure (i.e. the heritage fabric is structurally unsound)

impacts a critical matter in the assessment of the application. Without this information Council's Assessment Panel cannot recommend that this matter has been addressed.

Lerwin Nursing Home
67 Joyce Street
Murray Bridge
T 08 8539 1185

Murray Bridge Public Library
Cnr South and East Terrace
Murray Bridge
T 08 8539 1175

Regional Gallery and Town Hall
27 Sixth Street
Murray Bridge
T 08 8539 1420

Visitor Information Centre
3 South Terrace
Murray Bridge
T 08 8539 1142

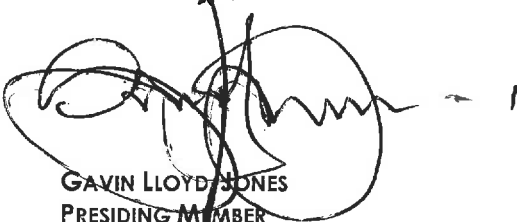
The Station
3 - 5 Railway Terrace
Murray Bridge
T 08 8539 1157

3. Additional Information – Car Parking – The Council Assessment Panel acknowledges that on-site car parking has been provided. However, there are concerns that the intensification of the hotel use will see a significant increase in the use of Council's on-street and Wharf Hill car parks. To reduce off-site impacts the Council Assessment Panel recommends that the proponent explore an agreement with the adjacent Marketplace to utilise a portion of their car park to supplement on-site car parking.
4. Additional Consideration – Pedestrian Linkage – Council has investigated (and budgeted) for a pedestrian linkage between the Marketplace and the Bridgeport Hotel across in South Terrace. To assist in the design of this important piece of infrastructure it is recommended that the proponent liaise with Council's engineering department. For this reason SCAP should consider an infrastructure agreement to contribute to the provision of a pedestrian linkage to support the abovementioned car park sharing arrangement. Alternatively in the absence of any agreement with the Marketplace the appropriate contribution should be made to Council's car parking fund.
5. Recommend Conditions - Should the SCAP, taking into consideration the comments contained within this report and the attachments, consider approving the proposal, the Council Assessment Panel has attached an additional document outlining recommended matters and conditions of consent that should be considered.

In summary, the Council Assessment Panel is supportive of the changes made with regards to the land use, visual amenity, stormwater, advertising, crime prevention, landscaping and access aspects of this application, which it now finds satisfactory. The Council Assessment Panel recommends that the SCAP seeks further information with regards to the heritage and car parking concerns as outlined in the Officer's Report and this letter, prior to making its determination.

The Council Assessment Panel acknowledges the role of the State Commission Assessment Panel as the relevant authority and expresses its thanks to SCAP for taking these comments into consideration as part of their determination.

Yours faithfully

A handwritten signature in black ink, appearing to read 'Gavin Lloyd Jones', written over a circular stamp or seal.

GAVIN LLOYD JONES
PRESIDING MEMBER
RCMB COUNCIL ASSESSMENT PANEL

Items that the Council considers need clarification:

1. Clarification of the maximum height of the building above natural ground level;
2. The plans show new pavers on East and South Terraces. A Section 221 (Local Government Act 1999) approval will be required to undertake this work. Furthermore the proponent should contact the Council's Engineers staff to discuss the specifications for the pavers and their installation;
3. A Section 221 (Local Government Act 1999) application is required to be lodged to occupy (overhang) the road reserve;
4. It appears from the plans that a street tree on Bridge Street is proposed to be removed. Details regarding the removal of the tree, including a replacement option, should be provided to Council;
5. The removal of the gas bullet appears to be proposed. Should this occur how will the development be connected to gas?
6. The plant and equipment located on the roof of the building should be incorporated into the design of the building or screened from public view;
7. The applicant should explore entering into an agreement with the adjacent shopping centre to utilise their car parks. If this is not feasible the applicant should be required to make the appropriate contribution to Council's car parking fund;
8. The applicant engage with Council in regards to access to South Terrace and re-design the access/egress arrangements such that the number of access points is reduced to two;
9. Consideration should be given to providing a vehicle turning path showing how a garbage truck can manoeuvre within the car parking area, as well as the undercroft car park;
10. The plans show that the existing pavers on Bridge Street and East Terrace will be retained. How will the existing structure be demolished and the new structure built ensuring that no damage is done to the existing pavers?
11. There are some anomalies in the stormwater report prepared by TMK;
 - o In the stormwater calculator, it is estimated that the required stormwater detention would be 47.2kL;
 - o However in the civil plan, there is a 24KL underground detention system;
 - o The pre-development state of the site considered to be almost impervious;
 - o The total discharge as nominated by TMK, as a single outlet discharge, will deliver up to a total flow of 125.3L/s;
 - o Further clarification is needed for the discharge during a minor storm where the allowable discharge is only 80.4L/s, where the orifice is sized to discharge 125.3L/s;
 - o In addition to that, discharging a concentrated flow of 80.4L/s or 125.3L/s is deemed undesirable as it will cause damage to the infrastructure and also to pedestrian areas. The downstream pit(s) will not be able to handle such high flow; and
 - o Therefore the method of discharging the water will need to be reviewed.

12. Additional water sensitive urban design (WSUD) measures (i.e. raingardens) should be incorporated in this area at the applicant's expense.

Suggested conditions, should SCAP consider approval:

1. A Waste Management Plan must be prepared and submitted to the satisfaction of the State Commission Assessment Panel prior to the occupation of the site which identifies, but is not limited to, how waste would be stored, transported and disposed of so as to minimise the potential for environmental harm.
2. The proposed car parking layout and vehicular entry points shall be designed and constructed to conform to the Australian Standard 2890.1:2004 (including clearance to columns and space requirements at the end of blind aisles) for Off Street Parking Facilities; Australian Standard 2890.6:2009 Parking facilities – Off street commercial vehicle facilities and designed to conform with Australian Standard 2890.6:2009 for Off Street Parking for people with disabilities.
3. All vehicles shall enter and exit the site in a forward direction.
4. The removal of existing line marked bays and the provision of new line marked on South Traffic shall be undertaken at the applicants expense;
5. The on-site Bicycle Parking facilities shall be designed in accordance with Australian Standard 2890.3-1993 and the AUSTROADS, Guide to Traffic Engineering Practice Part 14 – Bicycles.
6. All loading and unloading, parking and manoeuvring areas shall be designed and constructed to ensure that all vehicles can safely enter and exit the subject land in a forward direction.
7. Waste disposal vehicles and general delivery vehicles shall only service the development between the hours of 7.00am and 7.00pm Monday to Saturday inclusive, and shall only load or unload within the confines of the subject land.
8. That all external lighting of the site, including car parking areas and buildings, shall be designed and constructed to conform with Australian Standards and must be located, directed and shielded and of such limited intensity that no nuisance or loss of amenity is caused to any person beyond the site.
9. Mechanical plant or equipment shall be designed, sited and screened to minimise noise impact on adjacent premises or properties.
10. A Construction Environment Management Plan (CEMP) shall be prepared and implemented in accordance with current industry standards – including the EPA publication "Environmental Management of On-site Remediation" - to minimise environmental harm and disturbance during construction. The management plan must incorporate, without being limited to, the following matters:
 - a. air quality, including odour and dust
 - b. surface water including erosion and sediment control
 - c. soils, including fill importation, stockpile management and prevention of soil contamination

- d. groundwater, including prevention of groundwater contamination
- e. noise
- f. occupational health and safety.

For further information relating to what Site Contamination is, refer to the EPA Guideline: 'Site Contamination – what is site contamination?': www.epa.sa.gov.au/pdfs/guide_sc_what.pdf

A copy of the CEMP shall be provided to the State Commission Assessment Panel prior to the commencement of site works for both the hotel and car park.

11. An Encroachment Permit will be separately issued for the proposed encroachments into the public realm once Development Approval is granted. In particular, your attention is drawn to the following:
 - An annual fee may be charged in line with the Encroachment Policy
 - Permit renewals are issued on an annual basis for those encroachments that attract a fee.
 - Unauthorised encroachments will be required to be removed.
12. Exhaust from the kitchen must be dispersed in accordance with Australian Standards AS/NZS 1668.1 and AS 1688.2.
13. All Council, utility or state-agency maintained infrastructure (i.e. roads, kerbs, drains, crossovers, footpaths etc) that is demolished, altered, removed or damaged during the construction of the development shall be reinstated to Council, utility or state agency specifications. All costs associated with these works shall be met by the proponent.
14. All stormwater design and construction shall be in accordance with Australian Standards and recognised engineering best practices to ensure that stormwater does not adversely affect any adjoining property or public road. Final details of the stormwater design, including onsite retention and reuse, shall be provided to the satisfaction of the Commission prior to the approval of any building works upon the site.
15. To ensure compliance with the Food Safety Standards, plans and construction specifications detailing the design and fit-out of the kitchen spaces should be provided to the satisfaction of the Commission prior to the approval of any building works upon the land.
16. Specifications for the swimming pool should be provided to the satisfaction of the Commission prior to the approval of any building works upon the land.
17. Any damaged pavers on Bridge Street, East Terrace or South Terrace on account of the development shall be replaced (to the satisfaction of Council).

10.1	Date of Panel Meeting : 17 November 2017
DA NUMBER	415/E014/16
PROPOSAL	Request for comment regarding the redevelopment of the Bridgeport Hotel
APPLICANT	Kerin Bay Pty Ltd

LOCATION	2-6 (Lots 94-99) Adelaide Road, Murray Bridge
ASSESSING OFFICER	Glenn Searle
ZONE/POLICY AREA/PRECINCT	Bridge Street Policy Area 11 of the Regional Town Centre Zone
NATURE OF DEVELOPMENT	Merit
PUBLIC NOTIFICATION CATEGORY	1
REPRESENTATIONS	N/A
AGENCY CONSULTATIONS	Undertaken by DAC (DPTI Transport)
INTERNAL REFERRALS	Engineering, Health, Assets and Heritage Consultants
DEVELOPMENT PLAN	Consolidated 11 August 2016
RECOMMENDATION	To supply the Council's comments to the State Commission Assessment Panel

Background

The State Commission Assessment Panel (SCAP) as the 'relevant authority' for this application (as per Schedule 10 part 20 of the Development Regulations 2008) has requested that the Council use its professional expertise to assist in the assessment of the application by providing comprehensive comments.

The application has been previously considered by the Council's Development Assessment Panel (now the Council's Assessment Panel - CAP) on 9th December 2016. At that meeting the Panel considered that the concept for the redevelopment of the site for hotel purposes was supported by the Development Plan.

However, it was considered that the proposal failed to adequately address the following matters:

- Architectural design and visual interest
- Heritage concerns: Including the complete demolition of an item of local heritage significance
- Access / egress to South Terrace and car parking
- Advertising
- Crime Prevention

(see **TABLE 1 – ATTACHMENT 5** for a summary of these issues).

For these reasons the Panel considered that the proposal should not be supported.

This information was considered by Council and provided to the Development Assessment Commission (now the State Commission Assessment Panel – SCAP). It was then forwarded to the applicant. A copy of the original report along with the plans and reports can be found **ATTACHMENT 1**.

Since that time the applicant has amended the design of the hotel and provided additional reports regarding the five outstanding aspects.

Proposal

It is noted that the amended design has taken into account some of Council's previous comments and changes have been made to the following aspects of the design:

- The external façade;
- The internal layout to facilitate glazing to Bridge Street and East Terrace;
- The provision of more enhanced landscaping; and
- Slight increases to the amount of car parking in both the undercroft and the at grade car parks.

The proposal involves the redevelopment of the Bridgeport Hotel site comprising the following works:

- The demolition of the Local Heritage listed Bridgeport Hotel and all other structures on the land;
- The construction of a new 6 storey hotel building. The building will contain the following:
 - An undercroft level - with 71 car parking spaces and direct vehicle access to/from South Terrace;
 - Ground floor - a sports bar, lounge bar, alfresco seating areas (also extending within Council's road reserve), gaming area, restaurant, foyer, kitchen, outdoor children's play area and other associated amenities and storage areas;
 - First floor - function room, bar, alfresco terrace, 12 accommodation rooms, gym and outdoor pool and bar for hotel guests;
 - Second to fifth floors - 22 accommodation rooms on each level;
- A new drive-through bottle shop at the rear of the site facing South Terrace, comprising a store room, retail floor space, cool room and 3 lane vehicle drive-through with a canopy; and
- 89 at grade car parks.

The Hotel is to be constructed of pre-fabricated panels with a painted finish in white, bronze and dark grey colours. Roofs and verandahs of the hotel will be finished in Surfemist. The bottle shop building, to front South Terrace, is to be constructed of similar materials and finished in Surfemist.

The proponent has stated the proposal will be staged in the following manner:

- Stage 1 - Civil works
- Stage 2 - Demolition of the Hotel
- Stage 3 - Bottle shop
- Stage 4 - Hotel Building

The site plan, floor plans and elevations, along with additional reports are contained in **ATTACHMENTS 2 & 3**.

Referrals

The only referrals that Council can consider are internal referrals as the application is assessed by the SCAP who are required to undertake the necessary agency referrals.

Engineering:

The Council's engineers have raised concerns with the following matters:

- The dual access proposed to the undercroft carpark will remove all of the on-street car parks in this area (9 in total);
- To facilitate this, and to provide additional car parks, new line marking will be required – this will need to occur at the applicant's expense;
- Additional water sensitive urban design (WSUD) measures (i.e. raingardens) should be incorporated in this area at the applicants expense;
- The pedestrian path from Bridgeport Hotel will not be linked directly to the current entrance to Market Place;
- Consideration should be given to providing a vehicle turning path showing how a garbage truck can manoeuvre within the car parking area, as well as the undercroft

car park;

- The plans show that the existing pavers on Bridge Street and East Terrace will be retained. How will the existing structure be demolished and the new structure built ensuring that no damage is done to the existing pavers?
- There are some anomalies in the stormwater report prepared by TMK, viz:
 - In the stormwater calculator, it is estimated that the required stormwater detention would be 47.2kL;
 - However, in the civil plan, there is a 24KL underground detention system;
 - The pre-development state of the site considered to be almost impervious;
 - The total discharge as nominated by TMK, as a single outlet discharge, will deliver up to a total flow of 125.3L/s;
 - Further clarification is needed for the discharge during a minor storm where the allowable discharge is only 80.4L/s, where the orifice is sized to discharge 125.3L/s;
 - In addition to that, discharging a concentrated flow of 80.4L/s or 125.3L/s is deemed undesirable as it will cause damage to the infrastructure and also to pedestrian areas. The downstream pit(s) will not be able to handle such high flow; and
 - Therefore the method of discharging the water will need to be reviewed.

Council's engineers' main concerns with the proposal revolve around the Council's ability to provide safe pedestrian access across South Terrace (between the Bridgeport Hotel and the Marketplace). Council's engineers have requested that any approval take into account Council's design for the provision of a safe crossing place.

Assets:

An application to occupy the road reserve (Section 221 of the Local Government Act) will need to be lodged with the Council along with details regarding the new paving (on the footpath) and the removal of the street tree located on Bridge Street.

Health:

The Council's Environmental Health Officers generally have no concerns with the proposal. However, they have stated that the proponent should consider the requirements of the Food Safety Standards. Specifically Standard 3.2.2 Division 2(3)(b) regarding the design and adequacy of kitchen spaces. To ensure that this standard is met the Environmental Health Officers recommend that design, fit-out and constructions specifications be provided to Council prior to Building Rules Consent.

Furthermore, the specifications for the swimming pool should be provided to Council's Environmental Health Officers prior to Building Rules Consent.

External Heritage Advice:

Council engaged Flightpath Architects to provide a heritage advice relating to:

- The heritage value of the Bridgeport Hotel, including whether its listing as a local heritage place is warranted, taking into account the historical context and the condition of the building; and
- Provide an assessment of the proposed development against the relevant Heritage Guidelines and Provisions outlined in the Rural City of Murray Bridge Development Plan.

The report from Flightpath Architects (**ATTACHMENT 4**) provides a contrary position to the proponent's report (prepared by DASH Architects).

Discussion

This report will not enter into the same detailed analysis of the application as the previous report to the Panel as it is considered that the previous report sufficiently describes the proposal and its assessment against the Development Plan policies. Instead this report will consider the amended proposal against outstanding issues of concern that have been previously highlighted (please note Table 1).

It is considered that the amended design for the redeveloped Bridgeport Hotel adequately addresses Council's (previous) concerns with five points from Table 1 (being Land Use, Stormwater, Advertising, Crime Prevention and Landscaping). These five points are summarised in Table 1.

Table 1		
Issue	Inconsistencies with Development Plan	Suggestions / Options to Address Inconsistencies
1. Land Use	<ul style="list-style-type: none"> Not applicable – the proposed land use is consistent with the existing land use and the relevant provisions of the Development Plan. 	<ul style="list-style-type: none"> No further action required.
2. Stormwater	<ul style="list-style-type: none"> Council's Engineering Department has confirmed the proposal complies with the Council's requirements for stormwater management. 	<ul style="list-style-type: none"> No further amendments are required in relation to this matter, other than clarification of the issues raised by Council's engineers (see referral section)
3. Advertising	<ul style="list-style-type: none"> Council's previous comments have been taken into account with the redesign of the hotel with the large areas of blank wall being removed and replaced with glazing. 	<ul style="list-style-type: none"> No further action required.
4. Crime Prevention	<ul style="list-style-type: none"> Council's previous comments have been taken into account with the redesign of the hotel. Concerns regarding anti-social behaviour especially dropping items from the alfresco balcony area have been taken into account with the design of the alfresco balcony area. 	<ul style="list-style-type: none"> No further action required.
5. Landscaping	<ul style="list-style-type: none"> The Landscaping plan supplied is satisfactory. 	<ul style="list-style-type: none"> Request that SCAP impose conditions regarding the planting and maintenance of the landscaping.

This leaves three points which will be further discussed:

- Heritage
- Visual Amenity/Design
- Traffic/Car Parking

Heritage

	Regional Town Centre Zone	Bridge Street Policy Area	General Section
Heritage	Desired Future Character Statement para 8, para 9 PDC 12, PDC 13	Desired Future Character Statement para 1, 3, 5 PDC 8, PDC 9, PDC 14	Design and Appearance PDC 1 Heritage Places OBJ 1, OBJ 2, OBJ3, PDC 1, PDC 2, PDC 3, PDC 4, PDC 5, PDC 6, PDC 7 Orderly and Sustainable Development OBJ 4 Siting and Visibility PDC 1 Tourism Development OBJ 2, OBJ 3, PDC 1, PDC 2, PDC 3

Provisions contained within the Development Plan (listed above and below) seek, in the first instance, to retain heritage buildings in their current form. If there are changes proposed then the adaptive reuse of existing structures should be considered. The Development Plan essentially states that the demolition of heritage items should be the as a last resort and only in circumstances when a building is structurally unsound.

General Section – Heritage Places

OBJ 1: *The conservation of State and local heritage places.*

OBJ 2: *The continued use, or adaptive reuse of State and local heritage places that supports the conservation of their cultural significance.*

PDC 1: *A heritage place spatially located on Overlay Maps - Heritage and more specifically identified in Table MuBr/5 - State Heritage Places, Table MuBr/4 - Local Heritage Places, should not be demolished, destroyed or removed, in total or in part, unless either of the following apply:*

- (a) *that portion of the place to be demolished, destroyed or removed is excluded from the extent of the places identified in the Table(s)*
- (b) *the structural condition of the place represents an unacceptable risk to public or private safety.*

PDC 2: *Development of a State or local heritage place should retain those elements contributing to its heritage value, which may include (but not be limited to)*

- (a) *principal elevations*
- (b) *important vistas and views to and from the place*
- (c) *setting and setbacks*
- (d) *building materials*
- (e) *outbuildings and walls*
- (f) *trees and other landscaping elements*
- (g) *access conditions (driveway form/width/material)*
- (h) *architectural treatments*
- (i) *the use of the place.*

PDC 4: *Original unpainted plaster, brickwork, stonework, or other masonry of existing State or local heritage places should be preserved, unpainted.*

The previous Panel report highlighted the differences between the two heritage architects with one recommending that the Local Heritage building be demolished due to a lack of heritage fabric remaining meaning that the building should not be heritage listed. While the other raised concerns that heritage fabric (stonework) may remain behind the existing brick

frontage and therefore the building still had heritage value.

In its previous comments, the Council stated that:

- A comprehensive and accurate Heritage Impact Statement should be provided. This should include (but not be limited to) an assessment and grading of heritage value based on accurate floor plans of both levels and also all elevations;
- Incorporate the retention of the Bridgeport Hotel (in some manner or form appropriate to preserve the heritage value) – this could include the retention of some external surfaces / facades or portions of the building but would be dependent on maintaining the heritage value;
- Revise the external design of the building to reflect the historic nature of the site and the broader locality.

While the plans have been amended and additional heritage reports provided the differences of opinion between the two heritage architects remain largely unresolved. DASH Architects have provided two additional reports regarding Council's concerns in relation to Local Heritage impact and State Heritage impact. Council has again engaged Flightpath to provide a response/advice.

It appears that the argument still revolves around the same two issues:

- The heritage value of the building (and whether it would be listed in its current condition); and
- The extent of heritage fabric remaining.

Heritage Value of the Building:

It is considered that the DASH report still makes fundamental errors in considering the heritage value of the Bridgeport Hotel against other Victorian hotels and argues that the hotel, in its current state, is not be worthy of being listed on the Local Heritage Register. These arguments were considered in the original Panel Report and considered erroneous for the reasons outlined below.

Comparing the Bridgeport Hotel against other Victorian hotels that are three stories tall, located within the Adelaide Metropolitan area and (most importantly) are State Heritage Items does not assist in the assessment of the application. As stated in the original report:

"...it is an unfortunate oversight on the part of DASH Architects that they failed to compare the Bridgeport Hotel against other comparable Local Heritage Places".

The question of whether or not the Bridgeport Hotel would be listed, in its current state, has largely been answered in the previous report, where it was shown that no substantial building works have been undertaken to the building since the time of its listing as a Local Heritage Item (See Heritage Section of the Panel Report for a detailed breakdown). Essentially this argument was refuted thus:

"As such it is considered the additions and upgrades to the building, with the exception of the smoking compliance area, were taken into account by the Council when preparing the PAR documentation. Furthermore these works, the state of the building and the remaining heritage fabric would have been considered by the Minister for Planning when considering the PAR. As such, any argument that the building has changed state or lost heritage value since the PAR is unlikely to have a high level of relevance.

Furthermore the 2001 survey which formed part of the PAR investigations notes that the building "has undergone numerous stylistic and structural changes, mostly at Ground Floor level". While the data sheet states that "progress of building changes are clearly evident, the least original fabric being at ground level". In other words, despite the changes, the hotel was still considered to fulfil the Section 23(4) Criteria".

Extent of the Heritage Fabric Remaining:

In the original consideration of the proposal, Flightpath Architects raised concerns that original stonework may remain behind the existing cream brick façade.

"the external walls are very thick and there is every indication that the original walls still exist behind a more recent brick external skin. The encroachment of the brick veneer skin on to the public footpath could provide a reason for its removal, to reveal the original walling. This could be verified through a licensed boundary survey".

The updated DASH report provides the following comments in regarding this, stating:

"Having undertaken further investigations on site, including inspection within the ceiling spaces adjacent the Bridge Street and East Terrace facades, I note that:

Bridge Street Elevation

- It appears that the entire length of the ground floor section of the 'original' (c1879-1937) sections of stone wall has been removed and replaced with: (load bearing) brickwork, concrete columns and beams, and windows.*
- We were not able to establish if the brickwork encroaches onto the footpath (over the boundary).*
- Based on my 20 plus years of architectural consulting experience, I suggest that the removal of the brickwork and reinstatement of stonework (using new stone) would neither be readily achieved nor cost effective.*

Corner

- As per the DASH Report, the entire 'original' lower section of the corner has been removed.*

East Terrace Elevation

- Except at the corner where it has clearly been removed, there is evidence to suggest that there are sections of original stonework remaining behind the brickwork wall along this facade. We are not sure of the condition or extent of this stonework however we assume that it was only removed where required for the works at the corner and for new fenestration etc. I am also not sure whether the concealed stonework retains the capacity to be load bearing (this capacity may have been compromised either because of the design of the newer walls or due to its condition).*
- Again, I was not able to establish if the newer brickwork wall encroaches onto the footpath (over the boundary).*
- Minor concerns and comments (car parking, road treatment, garbage, solar panels, plant, etc)".*

There is no disagreement with the findings that the heritage fabric has been removed from the corner of Bridge Street and East Terrace. However, there remains disagreement on the extent of the heritage fabric remaining on the Bridge Street and East Terrace frontages, which is evident in the above extract (for East Terrace) from the DASH report. Clearly, the Council is not best placed to judge the varying opinions of the expert advice provided on this matter.

Flightpath Architects maintain that the evidence for the original walls remaining is still compelling (thickened external walls and the pattern of ground floor windows). In their opinion this could be verified through a licensed boundary survey. Alternatively, in Council's opinion, small scale work could be undertaken, under supervision, to remove some of the external cladding to reveal the presence of original stonework.

This lack of certainty regarding the extent of heritage fabric remaining is vexing as it goes to the heart of the assessment. On the one hand if, as the proponent claims there is limited heritage fabric remaining, then the reason for the removal of the heritage item is compelling. However, if the heritage fabric is simply covered by a façade of newer cream brick it would appear that the heritage item has been kept in closer to original condition. On this basis the provisions, and intent, of the Development Plan are clear that the heritage item should be preserved with any redevelopment taking into account and preserving those portions of the item.

Given this lack of certainty regarding the extent of heritage fabric remaining, not only between the heritage architects but also within the DASH Architects report, it is considered that more work should be undertaken to reveal the extent of heritage fabric that remains. This should include but not be limited to an accurate surveyed plan of the building to consider any encroachment onto the footpath and exploratory work to assess the extent of any stonework remaining behind the cream brick frontage. The Council suggests that the SCAP may wish to commission or receive the relevant evidence needed to make a determination as to which expert advice is correct prior to determining this matter.

Visual Amenity

	Regional Town Centre Zone	Bridge Street Policy Area	General Section
Visual Amenity	OBJ 3, OBJ 5 Desired Future Character Statement para 5, para 8, 9 PDC 4, PDC 11	OBJ 2 Desired Future Character Statement para 2, 3, 4, 5, 8 PDC 3, PDC 5, PDC 8, PDC 9, PDC 10, PDC 11, PDC 12, PDC 13, PDC 14, PDC 15	Centres and Retail Development PDC 2, PDC 3 (a & b) Crime Prevention PDC 2, PDC 3 Design and Appearance OBJ 1, PDC 1, PDC 2, PDC 3, PDC 4, PDC 5, PDC 7, PDC 8, PDC 9, PDC 11, PDC 12, PDC 13, PDC 14, PDC 16, PDC 18 Landscaping, Fences and Walls OBJ 1, PDC 1, PDC 4 Natural Resources OBJ 13 Orderly and Sustainable Development OBJ 1, OBJ 4 Siting and Visibility OBJ 1, PDC 1, PDC 2, PDC 5, PDC 6 Tourism Development OBJ 3, OBJ 4, PDC 1, PDC 2, PDC 3, PDC 5

As previously stated the internal design and the external façade of the proposed hotel building have been amended taking into account the Council's previous comments. This amended external design/façade is considered to be a significant improvement on the design previously presented to the Panel.

To briefly summarise the design of the proposed hotel building; it is a rectangular shaped structure constructed of pre-fabricated panels with a painted finish in white, bronze and dark grey colours. Solid panels appear blank with joint line articulations coinciding with floor levels. The building is to be located on the eastern portion of the site with its narrower width facing Bridge Street and its longer side facing east and west. The ground floor façade (to Bridge Street and East Terrace) has been completely redesigned reducing the amount of blank walls facing the street and replacing it with glazing and outdoor dining areas.

As with the previous report the main concern with the proposal is the height above natural ground level and the potential resultant impact on the visual amenity of the locality.

It is noted that Council and the proponent both agree that the site is a landmark location and as such special consideration should be given to allow for taller buildings. However, as previously considered if consideration is to be given to the special character of subject site (i.e. a landmark site) then special consideration must also be given to the architectural and design elements of the building which should reflect the unique and special character of the site and be conscious of the impact that any built form will have.

The proponent has stated that the height of the building is 23.3m from natural ground level at the Bridge Street frontage; however this is not the total height of the building above natural ground level. The total height of the building will be much higher than 23.3m.

The proponent has provided a plan showing the heights of the proposed building, the Marketplace and Ridleys Mill when viewed from East Terrace (see **ATTACHMENT 3**). This plan shows that the Marketplace building is approximately 20m above natural ground level. However, it should also be noted that the Marketplace is located in a different policy area which does not contain provisions regarding building height.

The prescriptive provisions of the Zone and Policy Area (listed later) speak of a maximum height of 8 metres (or two storeys) above ground level while the subjective provisions provide guidance in assessing this aspect of the proposal. These provisions speak to establishing a cohesive built form comprising historic and modern building designs whilst creating attractive buildings and places that enhance the image of the town.

Moreover the type of development and activity sought in Bridge Street *"needs to exhibit a high quality and image that reflects a prosperous and pleasant township"*, with an emphasis on the role of Bridge Street as the traditional 'main street' of Murray Bridge, reinforced by new development that creates a *"cohesive lineal streetscape appearance and where the scale of new development is compatible with the existing built-form"* with *"a high visual amenity and strong local character to create a sense of place and focal point within the town"*.

It is suggested that the building, being at least three times greater in height than the maximum height specified in the provisions, and much greater in height than any surrounding building fails to comply with these provisions. Furthermore the building has a limited relationship through its design, scale, bulk, proportions and compositions of design elements with the locality of the main street or the Murray River.

Whilst the proposal must be considered against the provisions of the Development Plan in place at the time of application, it is also clearly not pragmatic or in the best interests of the proponent, the Council or SCAP to seek a new application to be compared against current Development Plan policy. In this sense the current provisions are a very clear indication of Council's intent for this landmark site and are a material planning consideration.

As such consideration should be given to the amended design of the hotel building and whether this improved design (and the landmark nature of the site) offsets the potential of the building to visually dominate the vista on which it is proposed, potentially resulting in a negative impact upon the visual amenity and character of the locality.

Bridge Street Policy Area:

"PDC 10: New buildings should maintain the existing horizontal massing of built-form while incorporating vertical proportioning in the composition of facades.

PDC 13: The spatial character of Bridge Street, established by the width of the street relative to its generally one-to-two-storey building height, should predominate.

PDC 15: Buildings within Bridge Street should have a maximum height of 8 metres".

Regional Town Centre Zone

Desired Character Statement:

The Murray Bridge town centre reflects the growth of the town as a regional centre over the past 150 years. The current land use patterns reflect this evolution of the past, and show a range of building forms from historic to modern buildings. Development within the town centre is intended to generate a greater cohesion of these features and to create a more attractive and better functioning centre as it continues to grow.

Development will promote a vibrant, economically viable and attractive centre, which will best suit the needs of the community, increase the centre's historic role as a social meeting place, and enhance the image of the town and the level of comfort and convenience for residents and visitors.

Bridge Street Policy Area

OBJ 1: The reinforcement of the role of Bridge Street as the traditional 'main street' of Murray Bridge.

Desired Character Statement

Bridge Street is the primary focus and historical retail centre of Murray Bridge. It contains a diversity of built-form, from substantial historical buildings to buildings reflecting modern styles and construction techniques.

Its geographical prominence and the activities conducted in the street give it an important role in establishing an image for not only the town centre but also the whole of Murray Bridge. Development and activity in Bridge Street must therefore not only cater for the needs of the consumer, but it also needs to exhibit a high quality and image that reflects a prosperous and pleasant township.

The policy area will be developed as the principal retail area for specialised goods in the Council area. The role of Bridge Street as the traditional 'main street' of Murray Bridge will be reinforced by:

- (a) ensuring the economic viability and vitality of specialised retailing, entertainment, cultural and tourist services
- (b) encouraging new development to create a cohesive linear streetscape appearance and where the scale of new development is compatible with the existing built-form
- (c) encouraging development to make greater use of the street for pedestrian and social activities, creating an atmosphere where people want to spend time
- (d) establishing a high visual amenity and strong local character to create a sense of place and focal point within the town

Development in the policy area will respect the character and integrity of the locality in respect of its design, siting, scale, building and roof shape, bulk, height, building materials and colours, advertising signs and external illumination.
(my underlining)

Having set the context of new planning policy for this site, it is clear that height is less of a factor under current Development Plan policy. The current Development Plan states in the Regional Town Centre Zone Retail Core Policy Area, Bridge Street Precinct that:

"Buildings located along Bridge Street should have a maximum height of 12 metres although greater building heights are considered appropriate if a proposed development displays both significant design merit and adequately reflects the desired character."

Furthermore, the separation of the hotel element from the remainder of Bridge Street properties, creates a clear and unambiguous design statement. It allows for a gradual visual transition from the existing two storey premises (which can be three storey under the current Development Plan policies) to the hotel, which stands proud at the head of the street, creating a strong visual landmark drawing the eye upwards. The functional separation is also considered an appropriate reinforcement of the differentiation between retail and hospitality.

The Council considers that the design of the current proposal is a significant improvement on the previous version it considered and that the tenets of this policy are now met. No objections are raised from a design perspective.

Traffic / Car Parking and Access

	Regional Town Centre Zone	Bridge Street Policy Area	General Section
Visual Amenity	OBJ 3, OBJ 5 Desired Future Character Statement para 5, para 8, 9 PDC 4, PDC 11	OBJ 2 Desired Future Character Statement para 2, 3, 4, 5, 8 PDC 3, PDC 5, PDC 8, PDC 9, PDC 10, PDC 11, PDC 12, PDC 13, PDC 14, PDC 15	Centres and Retail Development PDC 2, PDC 3 (a & b) Crime Prevention PDC 2, PDC 3 Design and Appearance OBJ 1, PDC 1, PDC 2, PDC 3, PDC 4, PDC 5, PDC 7, PDC 8, PDC 9, PDC 11, PDC 12, PDC 13, PDC 14, PDC 16, PDC 18 Landscaping, Fences and Walls OBJ 1, PDC 1, PDC 4 Natural Resources OBJ 13 Orderly and Sustainable Development OBJ 1, OBJ 4 Siting and Visibility OBJ 1, PDC 1, PDC 2, PDC 5, PDC 6 Tourism Development OBJ 3, OBJ 4, PDC 1, PDC 2, PDC 3, PDC 5

The Council's Engineers have raised two principal concerns regarding traffic management; namely the impact on Council's ability to provide a pedestrian crossing on South Terrace and the amount of on-site car parking provided.

This design will adversely impact on the Council's ability to provide a pedestrian link across South Terrace between the Bridgeport Hotel and the Marketplace (entrance). Previously the Council's Engineers provided guidance to the proponent on how to redesign the vehicular access for South Terrace, removing the drive-through exit and redirecting the traffic through the main access/egress point.

Car Parking:

One of the issues that was previously considered was the amount and rate of car parking provided on-site for the proposal versus the need to utilise Council's on-street car parks.

It should be noted that the redesign of the proposal has increased the number of on-site car parks by 9 additional spaces, bringing the total to 160 car parks.

Table MuBr/2 [please see Tables 2(A) and 2(B)] of the Development Plan provides off street parking requirements for various developments. For a hotel, the table lists different rates depending on various uses within the building/facility. Similarly the applicant's Traffic Report references figures and sizes from the previous plan.

Table 2(A)			
Use	Car parking requirement	Approx. Floor area	Number of car parks required
Hotel room	1 per 3 guest rooms	97 rooms	32 spaces
Gaming lounge	1 per 2 square metres of gaming room	142m ²	71 spaces
Sports bar/lounge*	1 per 2 square metres or 1 per 6 square metres*	169m ²	28 (or 84.5)
Lounge bar	1 per 6 square metres of lounge or beer garden	236m ²	39
Dining area	1 per 3 seats	450 seats	150
Function room	1 per 3 seats	200 persons	67
Total number of car parks			387 (or 443*)

[NB *consideration needs to be given as to whether the sports bar/lounge should be considered a lounge bar or a public bar in the previous table].

The above table calculates the number of car parks required in accordance with Table MuBr/2. However, the above figures (as provided by the proponent) are only calculated for the interior portions of the building and do take into account exterior dining and seating areas such as the ground level sports designated outdoor smoking area (DOSA), gaming DOSA the lounge alfresco, the restaurant alfresco or the terrace level alfresco. Calculations for these areas are provided below:

Table 2(B)			
Use	Car parking requirement	Approx. Floor area	Number of car parks required
Gaming DOSA	1 per 6 square metres	22m ²	3.6 spaces
Sports DOSA	1 per 6 square metres	127m ²	21.1
Lounge/Dining alfresco	1 per 6 square metres	291m ²	48.5
Terrace alfresco	1 per 6 square metres	480m ²	80.1
Total number of car parks			153.3 Spaces

If these areas are calculated in the above manner then an extra 153 car parking spaces would be required (given that the number of seats in these areas was not provided). This would create a demand of over 500 car parks.

The proposal includes the provision for 160 car parks to be provided (89 at grade and 71 undercroft). The proponent's traffic engineer has put forward an argument for an alternative way of calculating car parking requirements (see the table below). In this method they have considered different peak periods for the hotel and added various weighting criteria to the car parking requirements. Using this method the consultant has calculated the maximum number of car parks required at 223 with a "regular evening period" of 143 car parks.

Use	Quantity	Rate	Period 1		Period 2		Period 3	
			Per cent occupancy	Peak evening dining period	Per cent occupancy	Peak bar occupancy period	Per cent occupancy	Peak lunch period
Hotel Room	100	1 per 3 rooms	80%	27	80%	27	50%	17
Gaming	40	1 per 2 gaming machines	15%	3	80%	16	85%	17
Lounge	machines							
Sports Lounge	177 m ²	1 per 6 m ²	65%	19	85%	25	15%	4
Lounge Bar	326 m ²	1 per 6 m ²	65%	35	85%	45	15%	8
Dining area	300 seats	1 per 3 seats	90%	90	65%	65	50%	50
Outdoor dining	150 seats	1 per 5 seats	90%	27	65%	20	50%	15
Function*	200 persons	1 per 3 seats	50%	33	10%	7	25%	17
Total				234		206		128

This shortfall of on-site car parking is considered to be problematic during peak periods such as the Australian International Pedal Prix, when V8 racing begins at "The Bend" racetrack, major horseracing or speedway events at the new racecourse or when any of these meet a major function (e.g. conference, wedding) and when occupancy of the hotel will also be subject to highest demand.

This will inevitably see the Council's on street and adjacent Wharf Hill car parks become utilised by hotel patrons. Council will have to introduce parking limits for South and East Terraces and the Wharf Hill car park as a result of indiscriminate parking, which would adversely impact the hotel. As such it is considered appropriate that the proponent enter into an agreement with the adjacent Marketplace shopping centre to utilise extra capacity. Alternatively the proponent can make a contribution to the Council's Town Centre Car Parking Fund.

Such an agreement with the Marketplace would deliver security and cooperation for both parties, together with convenience for patrons. The increase in pedestrian traffic in both South and East Terrace will necessitate the need for additional crossing points. The SCAP should ensure that the developer enters into an Infrastructure Agreement to ensure such works are completed at the applicant's expense prior to operation of any of the elements of the hotel building.

Subject to the resolution by the SCAP of these car parking and pedestrian safety concerns, the Council would have no objection on these grounds.

Summary:

As with the previous report to the Panel, it is considered that the concept of the redevelopment of the site for hotel purposes is supported by the Development Plan. Furthermore the concept of the subject land being a 'landmark site' is generally supported and it is accepted that development of a larger scale may be appropriate in this area if it displays a high degree of architectural merit and design.

However, as the site is a landmark, its re-development (if approved) will change the urban fabric of Murray Bridge and has the potential to set the scene as a catalyst for further development and change. It is therefore critically important that the development is a prime example of good planning and design.

Ultimately consideration of the proposal depends on the SCAP's view regarding heritage and visual amenity (height) matters. They must determine whether the existing Bridgeport Hotel continues to display sufficient merit to satisfy Local Heritage Listing or if they consider that the applicant has provided sufficient evidence to warrant the demolition of the building then the building should be demolished as there is no reasonable way that the remaining heritage portions could be reused. In that case, the height and design of the building, does not play as greater role in the assessment of the proposal.

For this reason it is suggested that Council write to the SCAP (draft letter can be found as **ATTACHMENT 6**) and respectfully request that they give careful consideration to the matters raised in this report and request that additional information is provided justifying the removal of Local Heritage Item and to resolve the traffic / car parking and access issues.

Should the SCAP prefer to approve the proposal without further referral, the Council has also included a list of recommended conditions (**ATTACHMENT 7**) that it would like to see attached to any approval.

RECOMMENDATION

1. *That the report and attachments (comprising comments from relevant Council Departments) be considered by the CAP*
2. *That the CAP endorse this report as well as attached letter to the State Commission Assessment Panel*

Attachments

1.	Attachment 1 - DAP Report and Assessment Report - 415-E014-2016	Attachment
2.	Attachment 2 - Amended Plans - 415-E014-2016	Attachment
3.	Attachment 3 - Applicants Report - 415-E014-2016	Attachment
4.	Attachment 4 - Flightpath Architects - Heritage Advice - 415-E014-2016	Attachment
5.	Attachment 5 - Table 1 - 415-E014-2016	Attachment
6.	Attachment 6 - Letter from CAP to SCAP	Attachment
7.	Attachment 7 - Conditions - 415-E014-2016	Attachment

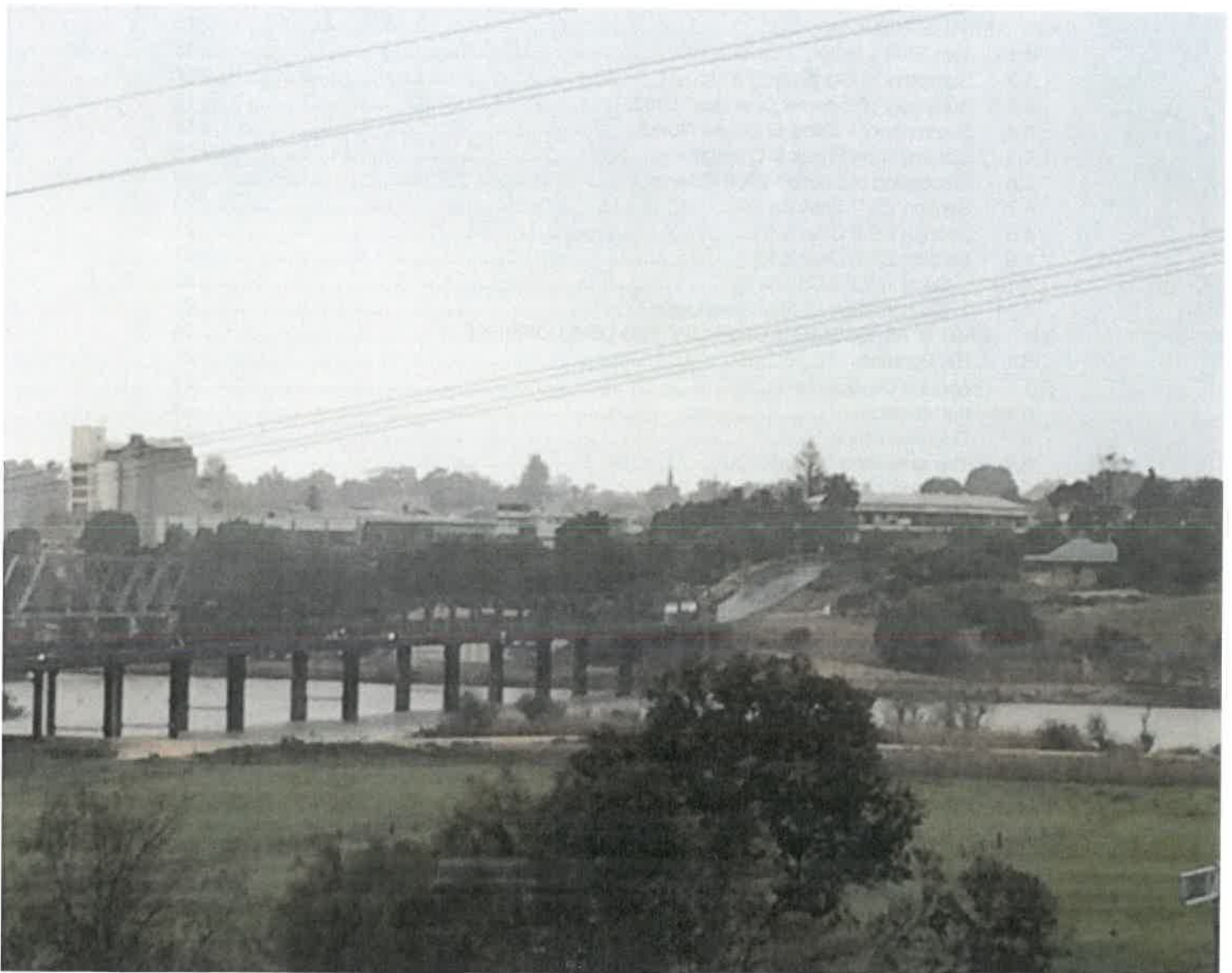
Heritage Advice

Bridgeport Hotel, Murray Bridge

for

Rural City of Murray Bridge

October 2017



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Acknowledgments

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The study team would like to acknowledge the assistance of the following people:

- **Geoff Parsons**
Manager Development & Regulation
- **Glenn Searle**
Senior Planning Officer

1.0 EXECUTIVE SUMMARY

1.1 Amended Application

Flightpath Architects have been engaged by the Rural City of Murray Bridge to review the amended documentation for the proposed development at 2-6 Bridge Street Murray Bridge.

The amended proposal includes the demolition of the Local Heritage Place and the construction of a five storey building.

Parts of this report refer to Flightpath Architects earlier report dated October 2016. Parts of the earlier report have been included here, as sections of advice remain unchanged. A historical outline has not been provided as it is contained in the earlier report. This said, we have included the historic photographs as we believe these are of importance.

As part of this review both DASH Architects reports: Bridgeport Hotel Redevelopment – Review of Proposed Demolition of Local Heritage Place (15.08.2017) and Bridgeport Hotel Redevelopment – Contextual Analysis Report (28.08.2017) have been considered.

The report below provides discussion, and response to their assessment of the review of the Local Heritage Listing.

Flightpath do not concur with the DASH Architects report that the Local Heritage Place does not meet the requirements for listing under the Development Act (1993) Section 23(4) criteria.

Of particular relevance is the opinion expressed regarding the construction of the external Ground Floor walling, the extent of removal and change. There is sufficient physical evidence on site to clearly demonstrate that the brick walling facing the street is a leaf of brickwork that has been applied to the original walls. Apart from the thickness of the walls, the pattern of original windows is still discernible. This view endorses the opinion of Bruce Harry and Associates, expressed at the time of Heritage Survey.

The Flightpath report also provides a response to DASH Architects comments regarding adaptive reuse of the Local Heritage Place.

Flightpath have considered the amended proposal for the new development but, as before, believe that the proposal needs to be considered as a whole within the context of the entire application including the demolition of a Local Heritage Place. These should not be considered separately given Flightpath's assessment that the inclusion of the place in the Development Plan is warranted.

Flightpath recommend refusal of the Application, with the demolition of the Local Heritage Place a crucial part of this decision.

2.0 PART 1: HERITAGE VALUE

2.1 Background

The Subject Site is 2-6 Bridge Street on the corner of East Terrace and South Terrace Murray Bridge. The Bridgeport Hotel occupies the North East portion of the Subject Site. It is a predominantly a two storey building with single storey portions to the west, east and south. It has access from South Terrace and Bridge Street.

Rural City of Murray Bridge has requested additional heritage advice relating to the Local Heritage Value of the hotel, in the context of the report provided by DASH Architects and also the amended proposed redevelopment which still involves the complete demolition of the hotel building.

Parts of this report contain information provided in the earlier October 2016 report but has been summarised as required.

2.2 Objectives of Report

The Scope of Work is as follows:

PART 1: HERITAGE VALUE

Provide a response to the recently updated DASH Architects report on the heritage value of the Bridgeport Hotel (including whether its listing as a local heritage place is warranted), taking into account the information provided and the condition of the building.

PART 2: HERITAGE ASSESSMENT

Subject to the heritage assessment confirming the listing, comment on the amended proposal including the design of the development.

The following tasks have been undertaken in order to develop an understanding of the heritage value of the place:

PART 1: HERITAGE VALUE

- Review of DASH Architects report Bridgeport Hotel Redevelopment – Review of Proposed Demolition of Local Heritage Place
- Response to DASH Architects conclusions and Assessment of Heritage Value.

PART 2: HERITAGE ASSESSMENT

- Review of DASH Architects report Bridgeport Hotel Redevelopment – Contextual Analysis Report
- Assessment of the revised proposal against the relevant Heritage Guidelines and Provisions outlined in the Murray Bridge Development Plan;
- Preparation of this Heritage Advice report.

2.3 Limitations of this Report

This report does not provide a full planning assessment and is limited to heritage assessment. The Council Wide, Zone and Policy area provisions have been reviewed for any heritage policy provisions or adjacency of heritage listed buildings. This report does not consider Aboriginal Heritage values or archaeology. The report does not replace a referral to the State Heritage Unit.

2.4 Current Use

The Subject Site and building is currently used as a hotel providing bar, dining, gaming, bottle

shop and accommodation facilities.

2.5 Location of Site

The property is located within the Rural City of Murray Bridge on the eastern escarpment of the township adjacent the descent to the River Murray and adjacent the Murray Bridge Road and Rail Bridges. Bridge Street is the traditional main street of Murray Bridge and therefore the hotel building is the last in a continuous run of buildings, constructed to the street boundary, typically with overhanging verandahs or balconies, in a west-east axis, before the river descent.



Figure 1 View look east along Bridge Street
Source: *Flightpath Architects 2016*



Figure 2 Location plan
Source: Location SA Map Viewer

2.6 Existing Setting and Surrounding Character

The hotel building on the Subject Land incorporates a two storey hotel building with Ground Floor bar, dining and gaming areas and a first floor providing accommodation, an open treed area to the south east, a modern single storey drive through bottle shop to the west and a single storey dining area to the south east, overlooking East Terrace, the railway reserve and slope towards the river.

2.7 Existing Heritage Listing

The Bridgeport Hotel at 2-6 Bridge Terrace Murray Bridge is listed as a Local Heritage Place and included in the Murray Bridge Development Plan TABLE MuBr/4: Schedule Of Local Heritage Places as follows:

2-6 Bridge Street MURRAY BRIDGE	Bridgeport Hotel; Elements to be retained - Original large two-storey Victorian Hotel section, including original rear single - storey storeroom and later two-storey Victorian extensions	99	CT 5187/857	a c d f	14209
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Figure 3 Part of Table Bur/2 Schedule of Local Heritage Places
Source: Murray Bridge Development Plan

The specific Section 23(4) criteria fulfilled following the 2001 Rural City of Murray Bridge Heritage Survey and included in the Development Plan are:

- (a) it displays historical, economic or social themes that are of importance to the local area;
- (c) it has played an important part in the lives of local residents;
- (d) it displays aesthetic merit, design characteristics or construction techniques of

significance to the local area;

(f) it is a notable landmark in the area.

2.8 Existing Adjacent Local Heritage Listings

The maps below in Figure 4 below, Local Heritage Places are designated in blue and State Heritage Places in red. The Subject Site is located adjacent a State Heritage Place described as the Murray Bridge Transport Precinct, shown on the map Figure 4 below.



Figure 4 Fig Bur HCPA/9 (Part)
Source: Location SA Map Viewer

Further detail on adjacent Heritage places is also provided on Map MuBr/21 and also Tables MuBr/2 and 3.

The first notable and adjacent Local Heritage Place, the row of Canary Island Palms, is opposite the Subject Site on the east side of East Terrace, lying between the Murray Bridge Transport Precinct and East Terrace.

The nearest Local Heritage Place on Bridge Street lies on the north side, west of the Subject Site, being the former Town Hall and Municipal Offices and on the opposite corner is Ruges Beehive Corner.

2.9 Existing Adjacent State Heritage Listings

The Murray Bridge Development Plan observes that Table MuBr/5 was last updated on 2 December 2010 and is an extract from the South Australian Heritage Register established under section 13 (1) of the Heritage Places Act 1993. In the event of a discrepancy between this extract and the South Australian Heritage Register, the South Australian Heritage Register shall prevail.

On 16 March 2016 Murray Bridge Transport Precinct (designated as a place of archaeological significance) confirmed as a State Heritage Place in the SA Heritage Register (State Heritage ID Number 26373). The precinct includes five already listed State Heritage Places: Murray Bridge Road Bridge (SHP: 10287), Murray Bridge Railway Bridge [Metal Truss] (SHP: 13826), Murray Bridge Wharf & Hand Crane (SHP: 13828), Murray Bridge Railway Station & Refreshment Rooms (SHP: 13820), Round House (former Murray Bridge Works Superintendent House) (SHP: 13577).

In terms of the Heritage Places Act Section 16 Criteria:

- a - it demonstrates important aspects of the evolution or pattern of the State's history
- b - it has rare, uncommon or endangered qualities that are of cultural significance
- c - it may yield information that will contribute to an understanding of the State's history

including its natural history.

Figures 9 and 10 following overleaf compare and demonstrate the proximity of the tunnel

alignment with the Bridgeport Hotel.



Figure 5 View of recently constructed Railway Tunnel c1925 from north side with Bridgeport Hotel to the above right.

Source: *State Library of South Australia*

During its time as a functioning railway and wharf complex, the Murray Bridge Transport Precinct helped shape the development of South Australia.



Figure 6 View of tunnel from above & Bridgeport Hotel from top of northern tunnel

Source: *Flightpath Architects September 2016*

The precinct is considered significant for its role in the development of river and rail transport in the colony of South Australia. The River Murray was an important economic artery for South Australia, and Murray Bridge played a special role as the site of the first road crossing point, as

well as the location of an important early wharf used to export goods to the Victorian goldfields. In the 1880s the original Murray Bridge was converted for use by rail, and the Overland railway route from Adelaide to Melbourne was opened. The railway station, depot and associated buildings and structures that were developed between the important rail-route and the wharf demonstrate a rare juxtaposition of rail and river transport. The Murray Bridge Transport Precinct also demonstrates some of the upgrades and efficiency measures instigated by the South Australian Railways under the leadership of William Alfred Webb (c1925). At that time, a second rail bridge was constructed to support larger gauge trains, and the transport depot was expanded. Webb's resignation from the South Australian Railways in 1930 marked the beginning of a slow decline, which saw the removal of some services and parts of the depot to Taillem Bend, and eventually the closure of the station building in the late 1970s. This decline mirrored the changing priorities for transport and travel during the mid-twentieth century.

In terms of Archaeology, the State Heritage register states that "historical research and physical survey indicate that the Murray Bridge Transport Precinct has many areas of archaeological potential. These areas mostly relate to places where buildings have been removed or demolished, some relate to sites of technological significance and others relate to excavations such as the railway cuttings and spur lines. Generally the archaeological sites can be expected to provide technological information or data on construction materials, practices and designs. Archaeological sites identified as having high archaeological potential include: * Site of removed Old Cottage (Detail Plan ref: 33) * Pumping Station footings and earthworks (Detail Plan ref: 36) * The site of the removed locomotive workshops (Detail Plan ref: 32) Archaeological sites identified as having moderate archaeological potential include: * Site of removed Depot and Motor Sheds (Detail Plan ref: 34) * Site of removed Cubicles (Detail Plan ref: 35) * Original cutting (Detail Plan ref: 10) * Site of removed goods shed (Detail Plan ref: 37) * Sites of other removed buildings in wharf precinct (Detail Plan ref: 38) * Site of South Australian Railways Institute (now relocated, Detail Plan ref: 39)."

3.0 Historical Outline

3.1 Refer to previous report dated October 2016 for history.

3.2 Historic photographs

The following historic photographs of the property are considered to be of relevance.



Figure 7 View of Bridgeport Hotel
Source: *Across the Mighty Murray* (excerpt provided by Murray Bridge Council)



Figure 8 View of Bridgeport Hotel
Source: *Mural inside Ground Floor Bar Area*



Figure 9 1937 View of Bridgeport Hotel
Source: *State Library of South Australia*



Figure 10 Railway Tunnel under construction looking west towards Bridgeport Hotel
Source: *State Library of South Australia*

4.0 Physical Evidence

This section is largely unchanged from our previous report.

4.1 Two Storey Hotel

A survey of extant fabric was undertaken on 28 September 2016. An additional visit has not been undertaken. The survey included:

- A perimeter inspection, viewing the hotel and grounds from the south, east, north and north west;
- Interior Ground Level;
- Interior First Floor level.

The early construction of the large two-storey Victorian Hotel is still evident externally and internally, while the Ground Floor has undergone change, original detailing and walls are still evident, particularly around the stair well. The First Floor layout, detailing, walls, ceilings and trims provide strong evidence of the size and stature of this early hotel.

For additional photos refer to the Flightpath Architects October 2016 report.



Figure 11 Bridgeport Hotel view from South Terrace
Source: Flightpath Architects 28 September 2016



Figure 12 Bridgeport Hotel view from Bridge Street
Source: *Flightpath Architects 28 September 2016*



Figure 13 Bridgeport Hotel view from East Terrace
Source: *Flightpath Architects 28 September 2016*



Figure 14 Bridgeport Hotel corner view from North East Terrace
Source: Flightpath Architects 28 September 2016



Figure 15 (Left) Detailed View showing build out of Ground Floor brick skin
Source: Flightpath Architects 28 September 2016

Figure 16 (Right) Detailed view of widened Ground floor wall due to brick skin
Source: Flightpath Architects 28 September 2016

4.2 Summary of Significant Fabric

From the inspection the following significant features have been noted:

- Ground Floor; external walls are very thick and there is every indication that the original walls still exist behind a more recent brick external skin; the pattern of original Ground Floor windows is also discernible providing further evidence to this opinion. This is a point of difference with DASH Architects. The encroachment of the brick veneer skin on to the

public footpath could provide a reason for its removal, to reveal the original walling. This could be verified through a licensed boundary survey;

- Ground Floor: original internal walls exist behind bar area and around the stair area;
- First floor internal layout including corridors and rooms, most ceilings and doors, architraves and skirtings and detailing;
- First floor sliding sash windows and access doors to balcony; stonework and brick quoins, (all painted);
- Chimneys;
- Roof form, cast iron posts.

4.3 Summary of External Changes Noted

It is evident from historic research and also the inspection of extant fabric that the Bridgeport Hotel has been evolving since its opening in 1885, becoming larger in size and accommodation provided particularly. Not unexpectedly the ground floor bar areas, typical of most hotel bar areas have evolved with changing economy, community needs, regulation and fashion.

Stages that are readily distinguishable on site include:

- Bridge Street: later two storey addition noted through vertical joint in stonework and quoins;
- East Terrace: later two storey addition noted through vertical joint in stonework and quoins;
- Bridge Street: single storey verandah removed to connect first floor cantilevered balcony and verandah on corner of East Terrace and Bridge Street, with cantilevered balcony at west end of hotel;
- Cast iron posts retained on East Terrace and narrow western stage facing Bridge Street;
- Widening of first floor cantilevered balcony to corner observed by wide skillion verandah roof, stepping to a curved verandah roof at west end and also change of posts from cast iron to timber around the widened area;
- Change in balustrading to horizontal round rods between round posts and deep metal fascia to first floor verandah floor area;
- Application of external brick veneer, alteration of Ground Floor Windows and removal of stone pier between original windows to allow for wider modern timber windows;
- Modern single storey additions to west (Bottle Shop) and south (Dining Room) sides;
- Opening up of North East corner involving concrete columns and beams.

4.4 Summary of Internal Changes Noted

- Modernisation of Ground Floor front bar area and gambling area;
- Internal arrangements to bar areas mainly on Bridge Street and corner bar; original internal Ground floor layout is still evident in part;
- Opening up of corner entrance and introduction of new partitions;
- Some suspended ceilings on both levels.

4.5 Comment on Physical Change

The physical changes noted demonstrate the ongoing process of modernisation of the hotel, which has been part of its history.

4.6 Discussion of Section 23(4) Criteria

Criteria for local heritage places – Section 23(4) Development Act has provided the opportunity for Rural City of Murray Bridge to consider places of local heritage value by specifying the criteria for assessing local heritage places. This has been undertaken through the Heritage Survey prepared in 2001 by Bruce Harry and Associates. The survey observed the extent of change that had occurred but still considered the Bridgeport Hotel to fulfil Criteria (a), (c), (d) and (f). The Planning Bulletin 2001 Appendix B provides Criteria and guidelines for State and local heritage places.

It is important to note that the Bridgeport Hotel is a Local Heritage Place, assessed under Section 23(4) Development Act. The Dash Architects report has used both the Newmarket Hotel and Botanic Hotels as comparative analysis. This is not considered relevant for the following reasons:

- The Newmarket and Botanic Hotels are listed as State Heritage Places and therefore have been assessed under the Heritage Places Act 1993, Section 16 Criteria;
- The hotels are considered to be of State significance;
- The hotels are three storeys and in urban locations at the North East and North West corners of North Terrace in the City of Adelaide.

Further, Jens Hotel in Mount Gambier is also used as a comparison. This too is a State Heritage Place.

Dash Architects have also focussed on the term 'display' from the criteria and confused historic character with the deeper meanings of Heritage Value, which go beyond physical appearance.

4.7 Section 23(4) Criterion (a)

a) It displays historical, economic or social themes that are of importance to the local area

The importance of the Bridgeport Hotel to the early and current local community of Murray Bridge has been demonstrated since its commencement in 1885 and through its ongoing response to changing needs that have occurred in the community over that time, evident through the physical changes to the building. The physical presence, close proximity to and commercial and cultural association with the river and road transport and trade and the nearby State Heritage listed wharf, railyards, and mill provide ample demonstration of the hotel's ability to fulfil and satisfy criterion (a).

The physical size of the hotel being originally 29 rooms with 14 bedrooms is significant for a local regional town, demonstrating the importance of the hotel in responding to the needs of river, road and rail transport.

The satisfaction of this criterion does not rely completely on physical integrity of the Local Heritage Place. The hotel continues to strongly demonstrate thematic associations and physical attributes of its presence as an early large, local hotel that has changed, mainly its bar areas, over its history to accommodate the changing economy, regulations and needs.

The opportunity still exists to remove the brick veneer at Ground Level to reveal the original walling, especially if it encroaches a public footpath. A public realm opportunity also exists to remove the dense planting and fencing and Palm trees to the railway tunnels to further reinforce this strong association.

Dash Architects have provided an opinion to the owner stating 'the degree to which the item meets the criteria for listing must be assessed on the basis of the current condition of the building, and not on what it was, or what it has the potential to be.'

Dash Architects have placed considerable importance on the integrity of the hotel, observing it would be 'likely that if the building was in 'original' (c1937) condition it would meet the requirements for listing under criterion (a).' Dash place emphasis on historic fabric being on display, suggesting the 'degree to which the Hotel displays the important period of development in the area between 1873-1886' has been reduced.

It should be observed the building was listed in near to its current state and therefore the fulfilment of criterion (a) must also be considered against deeper social and cultural values and the ongoing demonstration of change in response to economic or regulatory change. Heritage management is not necessarily about freezing places in time.

Criterion (a) is not assessed on condition or integrity alone and the physical size of the original hotel is still adequately demonstrated through its extant fabric, location and adjacency to State Heritage listed road and rail bridges and the railway precinct.

Therefore, the Bridgeport Hotel adequately fulfils Criterion (a).

Additional Advice

The above advice regarding Section 23(4) Criterion (a) remains unchanged from the Flightpath October 2016 report. DASH Architects argue that the changes to the ground floor of the Local Heritage Place have reduced its integrity to a point where it is no longer demonstrative of the historical and economic themes that are of importance to Murray Bridge. DASH Architects provide a notated photograph to demonstrate this. Flightpath disagree with this and believe that there may be original fabric still behind the brickwork on Bridge Street. Below are two photographs we believe demonstrate this. Also, entire replacement of a stone wall only at ground level while the stone portion remains above would have been cost prohibitive and we do not believe wholesale demolition of this part of the façade cannot be assumed without further investigations into the physical material.

In addition to this, the entirety of the structure needs to be taken into account. The first floor

retains much of the original structure, the ground floor changes only amount to a small percentage of the overall structure. Which includes the first floor, roofscape, and single storey additions to the rear.

4.8 Section 23(4) Criteria (c)

c) It has played an important part in the lives of local residents.

The Bridgeport Hotel continues to play an important and relevant part in the lives of local residents and visitors due to its key location in Bridge Street adjacent to modern retail developments to the south, the river, road and railyards.

The local (and broader) community has a special attachment to the Bridgeport Hotel due to its riverside, railway precinct and road and rail bridges location, historical context and sense of place arising.

It provides evidence of a prominent and surprisingly large, early regional hotel, constructed shortly after the rail bridge that has responded over its history to accommodate the changing economy, regulations and needs. Until the construction of the Swanport Bridge, this large hotel was on the main road and rail connection between Adelaide and Melbourne and its physical presence cannot be discounted as a source of community pride.

Dash Architects have placed considerable importance on 'the compromised integrity of the physical fabric of the place', which 'has undermined the ability of the place to demonstrate any importance it might have.'

It should be observed that the building was listed by Heritage Survey and assessed formally at State Government level once the appropriate statutory notifications had occurred, in close to its current state. Therefore the fulfilment of criterion (c) must also be valued against deeper social and cultural values, such as its original size in response to an economic and community need for a proper hotel and the ongoing demonstration of change in response to economic or regulatory change, the prominence of the hotel and the use of the hotel by the community, including church groups.

Criterion (c) also is not assessed on condition or integrity alone. The Bridgeport Hotel adequately fulfils Criterion (c).

Additional Advice

The above advice regarding Section 23(4) Criterion (c) remains unchanged from the Flightpath October 2016 report.

Refer to additional advice for the Criterion (a) for a response regarding DASH Architects emphasis on integrity.

4.9 Section 23(4) Criteria (d)

d) It displays aesthetic merit, design characteristics, or construction techniques of significance to the local area.

The Bridgeport Hotel, at the time of its construction, was a remarkably large and elaborate hotel for a regional township like Murray Bridge. The heritage survey identifies the need for a 'proper hotel' in 1883.

This surprisingly large two storey hotel, approximately 80km from Adelaide, was constructed less than 50 years after proclamation of the colony and yet incorporated two storey high stone walls that are embellished with quoined opening surrounds and Italianate bracket pairs to the eaves. This hotel also included cantilevered balconies supported by elaborate cast iron brackets, which may still be concealed beneath the deep first floor verandah above the metal soffit, and first floor decorated cast iron supports. This was a major construction, incorporating aesthetic embellishment, of significance to the local area.

The hotel continues to demonstrate aesthetic merit of significance to the local area; these considerable features are readily apparent through its high level extant original fabric and the original fabric that is concealed by a more recent but clearly brick veneer skin, on the outside face of the thicker Ground Floor Walls.

Dash Architects include Jens Hotel Mount Gambier as a comparison. This is a State Heritage listed place. The Pretoria Hotel at Mannum is currently being assessed as a Local Heritage Place for the Mid Murray Council. Therefore the Bridgeport Hotel adequately fulfils Criterion (d).

Additional Advice

The above advice regarding Section 23(4) Criterion (d) remains unchanged from the Flightpath October 2016 report.

Refer to additional advice for the Criterion (a) for a response regarding DASH Architects emphasis on integrity. In addition, it is important to emphasise again that there is a high probability that significant fabric remains behind the newer additions.

4.10 Section 23(4) Criteria (f)

(f) It is a notable landmark in the area.

The Bridgeport Hotel fulfils this criterion because it is undoubtedly visually prominent and a reference point for the whole community or a significant part of it. It is a large early building within a lower scale settlement that occurs on the alignment with the road and rail bridges and railway tunnels and is prominently placed in the town. The topography and in this case the escarpment to the river edge enhances the visual prominence, but there can be no disguising the surprising size and height of such an early building.

Criterion (f) also is not assessed on condition or integrity alone. Therefore the Bridgeport Hotel adequately fulfils Criterion (f).



Figure 17 (Left) View from road bridge looking west

Source: *Flightpath Architects 28 September 2016*

Figure 18 (Right) View of hotel from west side of River Murray

Source: *Flightpath Architects 28 September 2016*

Additional Advice

The above advice regarding Section 23(4) Criterion (f) remains unchanged from the Flightpath October 2016 report.

DASH Architects assert that the Local Heritage Place does not meet this Criterion due to the fact that it relies on its location and the topography of the subject site. Flightpath disagree with this assessment and believe that the setting and siting of this Local Heritage Place is part of its significance. DASH refer to the Planning Bulletin – Heritage which includes an example of 'Memorials at road intersections or prominently placed in town centres'. This links the siting and location of a Place with its ability to meet Criterion (f).

4.11 Section 23(4) Criteria Conclusion

It can be concluded the Bridgeport Hotel fulfils the following Section 23(4) Criteria:

- a) It displays historical, economic or social themes that are of importance to the local area, through its extant external form and detail, presence, size, proximity to river, road and rail transport and demonstration of ongoing change to meet changing economic, community, regulatory and other needs. The hotel was constructed shortly after the construction of the first bridge and therefore has an undeniable association with the early growth of Murray Bridge ;
- c) It has played an important part in the lives of local residents, through its past, present and future social and cultural value to the local community, its original size in response to an economic and community need and the ongoing demonstration of change in response to economic or regulatory change, the prominence of the hotel and the use of the hotel by the community, including church groups. Also the hotel is associated with the River Murray crossing point at the two bridges and the railway tunnel;
- d) It displays aesthetic merit, design characteristics, or construction techniques of significance to the local area, through its surprising size and stages as a large two storey hotel, approximately 80km from Adelaide, constructed less than 50 years after proclamation of the colony and yet incorporating two storey high stone walls that are embellished with quoined opening surrounds and Italianate bracket pairs to the eaves. This hotel also included cantilevered balconies supported by elaborate cast iron brackets, which may still be concealed beneath the deep first floor verandah above the metal soffit, and first floor decorated cast iron supports;
- (f) It is a notable landmark in the area because it is a large early building within a lower scale settlement that occurs on the alignment with the road and rail bridges and railway tunnels and is prominently placed in the town. The topography and in this case the escarpment to the river edge enhances the visual prominence, but there can be no disguising the cumulative effect of locality and scale.

The concept of heritage value assists in identifying and assessing the qualities that make a place or object of value to the community. An understanding of the nature and degree of significance indicates where constraint is required with future work, and conversely, introduces flexibility by identifying aspects that can be developed with greater freedom.

Places likely to be considered of significance are those that provide an understanding of the past or enrich the present, and will be of value to present, or future, generations. Heritage Value encompasses all the values or meanings that a place may have to people beyond its utilitarian value, and can refer to the aesthetic, historical, scientific, or social value of a place.

Additional Advice

The above Conclusion regarding Section 23(4) remains unchanged from the Flightpath October 2016 report.

5.0 PART 2: ASSESSMENT OF PROPOSED DEVELOPMENT

5.1 Background

Subject to the heritage assessment confirming the listing, comment on the proposed re-development of the site, including the amended design of the development follows.

6.0 Proposed Development

6.1 Introduction

The amended proposed development consists of:

- Demolition of the Bridgeport Hotel, a Local Heritage Place;
- Construction of a new undercroft carpark for 71 cars accessed from South Terrace;
- Construction of a Ground Floor consisting bar and kitchen areas, site car parking and separate bottle shop;
- First floor consisting part function area, elevated alfresco terrace, pool and part accommodation;
- Four additional upper levels above ground level providing accommodation.

6.2 Documentation

This assessment is based upon the following amended drawings:

Drawing Title	Date	Project No:	Drawing No:	Issue
Existing Site Plan	17 July 2017	C1201	1.01/DA1	DAC
Basement Plan	10 August 2017	C1201	1.03/DA2	DAC
Site Plan	10 August 2017	C1201	1.04/DA2	DAC
Ground Floor Plan	10 August 2017	C1201	1.05/DA2	DAC
First Floor Plan	4 September 2017	C1201	1.06/DA3	DAC
Second Floor Plan	17 July 2017	C1201	1.07/DA1	DAC
Third Floor Plan	17 July 2017	C1201	1.08/DA1	DAC
Fourth Floor Plan	17 July 2017	C1201	1.09/DA1	DAC
Fifth Floor Plan	17 July 2017	C1201	1.10/DA1	DAC
Part Roof Drainage Plan	10 August 2017	C1201	1.11/DA2	DAC
Apartments Type	10 August 2017	C1201	1.12/DA2	DAC
Proposed Elevations	17 July 2017	C1201	2.01/DA1	DAC
Proposed Elevations	17 July 2017	C1201	2.02/DA1	DAC
Bottle Shop Proposed Elevations & Cross Section	17 July 2017	C1201	2.03/DA1	DAC
Streetscape Elevations	17 July 2017	C1201	2.04/DA1	DAC
Existing and Proposed Perspectives	17 July 2017	C1201	2.05/DA1	DAC

3D renders				
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6.3 The Amended Design

The new hotel building and bottle shop will replace an existing Local Heritage Place, a two storey hotel building.

Drawing 1.03 indicates the outline of the existing building and the proposed. The existing building is constructed to the Bridge Street and East Terrace corner and street boundaries. The early frontage to Bridge Street is greater than the early frontage to East Terrace. The later single storey addition facing East Terraces results in almost two equal faces addressing the street boundary.

The proposed building has its narrower width facing Bridge and its longer side facing east and west. The proposed building will occupy the eastern portion of the site and is set back from the East Terrace street boundary to accommodate Alfresco areas. The frontage at Ground Floor facing Bridge Street is built to the street boundary and includes an escape stair.

Whereas the existing modern bottle shop is close to Bridge Street and attached to the main hotel building, the proposed bottle shop is located in the South West corner to South Terrace. The width of the crossover to Bridge Street is increased from the current arrangement and the west elevation features a broad entrance canopy and foyer. The design of the bottle shop is unchanged from the original proposal.

The upper floor of the existing building faces Bridge Street and the main internal corridor is parallel to Bridge Street. The upper levels of the proposed building incorporate a main corridor that is perpendicular to Bridge Street.

The alfresco areas to the first floor of the new hotel building provide coverage to the Ground Floor areas. The supports at Ground Floor to Bridge Street are located towards the far edge of the footpath. Those to East Terrace occur at approximately the middle of the footpath. Uncovered verandah areas will have umbrella structures with white membrane and silver coloured aluminium.

The second floor does not incorporate a fixed structure to provide cover to the elevated alfresco terrace below. Umbrellas are shown in the perspectives and on Drawing 1.06.

The main hotel foyer is a two storey high volume, with a canopy extension providing a covered drop off. It is located away from the street frontages and addresses the car park.

The elevation to Bridge Street incorporates mainly glazing to Ground and First Floor. A vertical element, which appears tiled, continues from ground level to the roof. This coincides with the central corridor and stairwell for the upper floors. The solid panels appear blank with joint line articulations coinciding with floor levels. Vertically proportioned windows which are offset from each other are located within solid panels from the second to fifth floors.

The east elevation is similar to the Bridge Street elevation and incorporates the stepped windows and sections of glazing and solid walling. The undercroft appears to be concrete which is articulated at a regular spacing.

The west elevations incorporate glazed walling between exposed regularly spaced columns. The main roof to the building is low pitched, with box gutters set behind a parapet. Details of air conditioning plant and screening is not readily apparent.

The Bottle shop is a single storey structure with a high level signage fascia and a skillion covered area to the three lanes of drive through service lanes.

6.4 Proposed External Finishes

Ground floor walls are mostly concrete block painted white. Decorative perforated screens to the podium are to have a grasses and bulrushes pattern on them. They are proposed in a galvanised finish.

Walls above ground level are generally Alucobond in either Black or Pure White. Between glazing for Levels 2 – 5 there is a composite timber cladding in a spotted gum finish. Glazing framing and balustrade posts and framing is powdercoated in Natural Pearl matt.

The dividing partitions between balconies will have an FRC sheet painted a dark grey.

Verandah posts will be manor red to match the main canopy framing and capping. The underside of the main canopy is proposed as pre-coated composite timber.

Umbrellas to the terrace are to be white.

The Bottle shop walls will be surfmist colour.

6.5 Heritage Places Provisions

The following provisions are considered to be of relevance:

Heritage Places

Objectives: 1, 2, 3

PRINCIPLES OF DEVELOPMENT CONTROL: 1,2,3,4,5,6

6.6 Zoning of Site

The Subject Land is located within the Bridge Street Policy Area 11 of the Regional Town Centre. The following provisions are considered to be of relevance:

Bridge Street Policy Area 11

Objectives: 1, 2, 3

Desired Character

PRINCIPLES OF DEVELOPMENT CONTROL: 1,2,3,4,5,6,7,8,9,10,11,12,13,14,15.

Regional Town Centre

Objectives: 3, 4 and 5

Desired Character

PRINCIPLES OF DEVELOPMENT CONTROL: 4,11, 12.

7.0 Discussion and Assessment

7.1 Introduction

The following reflects advice on the amended proposal.

7.2 Heritage Places Assessment

Heritage Places Objective 1 is not satisfied by the proposed development because it involves the complete discarding of a Local Heritage Place, the Bridgeport Hotel, previously assessed through a Heritage Survey followed by proper public consultation and statutory listing process as having Heritage Value.

The opportunity for continued use or adaptive reuse of the local heritage place, the Bridgeport Hotel, supporting its conservation of its cultural significance is lost through demolition and therefore upsetting Heritage Places Objective 2.

Further the setting of the Bridgeport Hotel will be significantly disturbed through its demolition and therefore Objective 3 is also not satisfied.

Principle of Development Control 1, offers demolition, destruction or removed, in total or in part, under two key circumstances, which include the exclusion of the portion to be demolished from the description or if the structural condition of the place represents an unacceptable risk to public or private safety.

In the first instance, while the exclusions are only implied through the description, the bottle shop addition or the single storey dining room facing East Terrace may be considered as likely possibilities, with little disturbance of Heritage Value.

Principle of Development Control 2 contemplates development of a local heritage place that retains 'those elements contributing to its heritage value' and therefore intends to retain:

- '(a) principal elevations
- (b) important vistas and views to and from the place
- (c) setting and setbacks
- (d) building materials
- (e) outbuildings and walls
- (f) trees and other landscaping elements
- (g) access conditions (driveway form/width/material)
- (h) architectural treatments
- (i) the use of the place'.

Clearly the proposal ignores and abruptly challenges this provision.

Principle of Development Control 3 also assumes retention of elements of Heritage Value by seeking compatible new development to occur, when developing a Local Heritage Place. Demolition in this instance is not contemplated. Principle of Development Control 4 also assumes 'Original unpainted plaster, brickwork, stonework, or other masonry of existing State or local heritage places' would be preserved, unpainted' and therefore not demolished.

Principle of Development Control 5 also infers retention of a Local Heritage Place by seeking to avoid new construction between the front street boundary and the façade of existing local heritage place. This provision provides significant guidance that a new development may be

located to the rear of the Local Heritage Place.

Finally, Principle of Development Control 6 also contemplates retention by guiding that development that materially affects 'the context within which the heritage place is situated should be compatible with the heritage place.' This provision is of particular relevance in considering the replacement building, because it would materially affect the context of adjacent remaining Local Heritage Places, being the Palm Trees to the opposite side of East Terrace and also the State Heritage listed Railway Reserve.

The new building, through its design, layout, scale and bulk, size and proportion and composition of design elements such as roof lines and openings could not be considered as compatible to those adjacent places, were the demolition to occur.

This provision also seeks information on proposed colour and texture of external materials, which appear quite modern and unsuitable for an historic main street.

7.3 Bridge Street Policy Area 11 Heritage Assessment

Objective 1, which contemplates the reinforcement of the role of Bridge Street as the traditional 'main street' of Murray Bridge, is not satisfied by removing the Bridgeport Hotel. The amended proposal does provide some reference to the scale and of the main street, with the first floor terrace which also provides some covered areas for pedestrians below. The columns for the verandah are located on the public footpaths. The amended proposal does not satisfy Objective 2, which seeks improvement of the existing environment and the surrounding townscape of South Terrace. The Bridgeport Hotel is part of this existing environment which should be retained, the amended proposal still includes complete demolition of this Local Heritage Place.

Objective 3 in considering the satisfaction of desired character of the policy area acknowledges Bridge Street as the primary focus and historical retail centre of Murray Bridge, referring to it containing 'a diversity of built-form, from substantial historical buildings to buildings reflecting modern styles and construction techniques.' This reference does not give license to the removal of one of Bridge Streets primary Local Heritage Places, but clearly desires retention of substantial historical buildings, of which the Bridgeport Hotel is a significantly sized example in a prominent location.

Desired Character also acknowledges geographical prominence and the 'important role in establishing an image for not only the town centre but also the whole of Murray Bridge'.

The type of development and activity sought in Bridge Street 'needs to exhibit a high quality and image that reflects a prosperous and pleasant township', with an emphasis on the role of Bridge Street as the traditional 'main street' of Murray Bridge, reinforced by new development that creates a 'cohesive lineal streetscape appearance and where the scale of new development is compatible with the existing built-form' with 'a high visual amenity and strong local character to create a sense of place and focal point within the town'.

The removal of the Bridgeport Hotel fails to satisfy the Desired Character for Bridge Street. It's replacement, while providing a podium at street level is also of a scale which does not satisfy this Desired Character.

While the amended proposal has a verandah of sorts provided by the Terrace podium it does not feature 'continuous retail frontages' that 'comprise small retail tenancies opening onto the street'.

The proposal, rather than respecting the character and integrity of the locality in respect of its design, siting, scale, building and roof shape, bulk, height, building materials and colours, advertising signs and external illumination, proposes a building that is of shiny, modern composite metal panels and large glazed areas at ground level. It is also of a scale much larger than that described in the Desired Character.

Principle of Development Control 3 speaks against development that is inconsistent with the

desired character for the policy area, stating it should not be undertaken. Principle of Development Control 5 is ignored because it seeks development abutting all road frontages where there is more than one frontage and no direct vehicular access to car parking or service areas of the development from Bridge Street (PDC 6). The amended application still provides access to the car park from Bridge Street.

The amended application has taken steps to address Principle of Development Control 8 in seeking visual cohesion with local features by providing the terrace podium, but the use of metal screening as the main material for this is at odds with the details found in Bridge Street. Principle of Development Control 9, which seeks buildings that 'exhibit a high proportion of solid-to-void construction in the composition of facades and veranda design' is not addressed, large sections of glazing face Bridge Street in the amended proposal. Principle of Development Control 10 which seeks new buildings that maintain the existing horizontal massing of built-form while incorporating vertical proportioning in the composition of facades is disappointed.

Turning to the spatial character of Bridge Street, Principle of Development Control 13 encourages 'the width of the street relative to its generally one-to -two -storey building height to be the predominant element. The amended proposal presents a five storey volume at the Bridge Street boundary, with the podium element over the footpath. The desire for a cohesive townscape appearance along Bridge Street through the blending of new development compatible with the traditional architecture (PDC 14) is simply not addressed by the proposal given the scale of the development and the height of the proposal at the Bridge Street boundary.

Finally, the maximum height of eight metres contemplated in Principle of Development Control 15 is considerably exceeded.

7.4 Regional Town Centre Heritage Assessment

Objective 3 in seeking a visually cohesive and integrated town centre is somewhat disturbed by the proposed intrusion of an out of scale and bulky development that includes demolition of a Local Heritage Place and one that fails to contribute to the desired character of the zone (Objective 5).

Desired Character is cognisant of Murray Bridge as a significant historic regional town centre, the town centre of which 'is located on a ridge overlooking the River Murray, and the two bridges crossing the river'. The proximity of the Bridgeport Hotel to the escarpment, give great meaning to this sense of place.

The statement acknowledges the significant role in the commercial activities of rural South Australia, referencing the major commercial link between the River Murray boating industry and the national rail system of early last century and as a 'key centre in South Australia on the national highway' and its evolution and sustained growth over the past 150 years that 'will continue to provide goods and services for residents and travellers in the Murray Mallee Region of South Australia'. The statement seeks compact orderly development that provides for a greater cohesion between the range of building forms from historic to modern buildings and development that has 'regard to policy contained within the individual policy areas'.

Objective 5 and PDC 4 are therefore not satisfied through this proposal. Principle of Development Control 11 also seeks a cohesive townscape character and suggests it be achieved through:

- '(i) ensuring that buildings maintain or establish a relatively continuous edge of built-form abutting or situated close to those major street frontages;
- (ii) emphasizing the townscape importance of development at the intersections of major streets, with corner buildings of a scale and form appropriate to their location and situated close to or abutting both street frontages;
- (iii) ensuring that buildings fronting on to such streets are of a shape, scale and orientation which

relate to and reinforce the rectilinear grid pattern of the town centre;

(b) ensuring that the design, external materials, colours and finishes of buildings have regard to their townscape context as appropriate to the desired future character of the relevant policy area;

(c) avoiding the use of large expanses of highly reflective materials;

(d) ensuring the height of any building is compatible with the desired streetscape character.'

Therefore, Principle of Development Control 11 is not satisfied in terms of the failure of this development to deliver:

- an appropriate scale and form appropriate to the location;
- contextual design;
- appropriate height;
- appropriate, non reflective materials.

Satisfaction of this and many other provisions could have been achieved through retention of the Local Heritage Place and development of the adjacent rear and side portions of the site as encouraged finally by Principle of Development Control 15, which guides that 'development of items recognised within each policy area as having historic or cultural value should be conserved.'

7.5 Effect on Section 23(4) Criteria

The complete demolition of the Bridgeport Hotel, a Local Heritage Place, will result in the failure to fulfil all of the following Section 23(4) Criteria:

a) It will no longer display historical, economic or social themes that are of importance to the local area. Its complete removal is a dramatic intervention that provides no evidence or connection with past themes;

c) The loss of the hotel will remove any evidence of its connection with the establishment of the town and the important part it has played in the lives of local residents;

d) The loss of the hotel will remove the solid remaining evidence of large, early, two storey hotel constructed less than 50 years after proclamation of the colony;

(f) While the replacement building is considerably taller, the removal of the early notable landmark in the area and its strong association with the Murray Bridge Transport Precinct and adjacent Local Heritage Places will be a significant loss.

7.6 Effect on State Heritage Places Act 1993

The proposed development is considered to materially affect the context within which State Heritage Place and therefore a referral to the Minister of Department of Environment Water and Natural Resources, responsible for State Heritage Places, is recommended.

The proposal is considered to have an adverse impact on the Murray Bridge Transport Precinct for the following reasons:

- The appearance and form of the new development will remove an important local reminder of the early growth of Murray Bridge and the strong association with river, road and rail transport;
- The size, height and location of the proposed development will visually dominate the adjacent State Heritage Place.

8.0 Conclusion and Recommendation

8.1 Part 1: Heritage Value

As indicated in our earlier report, following a Review of the Development Act Section 23(4) Criteria it is concluded that the Bridgeport Hotel is worthy of its listing as a Local Heritage Place, taking into account the information provided, researched and the condition of the building.

8.2 Part 2: Heritage Assessment

As per our previous advice, the proposed re-development of the site is not appropriate because it involves the unexplained and complete removal of the Local Heritage Place.

The design of the replacement development fails to satisfy the provisions of the Bridge Street Policy Area of the Regional Town Centre Zone and has adverse heritage impacts on the adjacent Local and State Heritage Places.

8.3 Part 2: Recommendation

As per our previous advice, it is recommended the development in its current form, involving the removal of a Local Heritage Place, be reconsidered to incorporate the retention of the Bridgeport Hotel and a revised, contextual design response, redesigned to a high architectural standard, with heritage design advice provided.

This should include but not be limited to an assessment and grading of Heritage Value based on accurate floor plans of both levels and also all elevations. If this does not occur it is recommended the application be refused in its current form.

Minutes of the **MEETING** of the Council Assessment Panel held on **FRIDAY 17 NOVEMBER, 2017**, at 10:00 am at the Local Government Centre, Council Chamber, 2 Seventh Street, Murray Bridge

PRESENT: Mr Gavin Lloyd-Jones – Presiding Member
Cr Jerry Wilson
Mr Tony Huppatz
Mr Myles Somers
Mr Marc Voortman

IN ATTENDANCE: Mr Andrew Meddle - General Manager Sustainable Communities
Mr Geoff Parsons – Manager Development & Regulation
Ms Cherry Getsom – Team Leader Development
Mrs Sue Bell – Minutes Secretary

OBSERVERS: Mrs Giulia Mason
Ms Belinda Ludewigs
Mr Michael Shillabeer
Mr Michael Sedgeman (entered the meeting at 10:22 am)

1. APOLOGIES
Nil

2. CONFIRMATION OF MINUTES

Recommendation:

That the minutes of the Meeting of Development Assessment Panel held on the 20 October, 2017 as circulated to Members, be taken as read.

Cr Jerry Wilson moved

That the minutes of the Meeting of Development Assessment Panel held on the 20 October, 2017 as circulated to Members, be taken as read.

Seconded by Marc Voortman and CARRIED

3. PRESIDING MEMBER'S REPORT
Nil

4. BUSINESS ARISING FROM PREVIOUS MINUTES
Nil

5. CONFLICT OF INTEREST DECLARATIONS
Nil

6. DEVELOPMENT ASSESSMENT MATTERS

6.1	Date of Panel Meeting : 17 November 2017
DA NUMBER	415/346/2017
PROPOSAL	Supported Accommodation Facility
APPLICANT	Murray Mallee Aged Care

LOCATION	14-22 Warner Rd, Murray Bridge
ASSESSING OFFICER	Cherry Getsom
ZONE/POLICY AREA/PRECINCT	Residential
NATURE OF DEVELOPMENT	Merit
PUBLIC NOTIFICATION CATEGORY	Three (3)
REPRESENTATIONS	Four (4)
AGENCY CONSULTATIONS	None required
INTERNAL REFERRALS	Engineering
DEVELOPMENT PLAN	Murray Bridge Development Plan Consolidated 2 May 2017
RECOMMENDATION	Approval subject to conditions

1. Background

The Murray Mallee Aged Care Group is an organisation which seeks to provide low cost supported accommodation for residents of Murray Bridge and surrounding areas, who are aged 55 and over. The proposed facility is to be managed under the SA Residential Tenancies Act 1995, via non-means tested rental agreements and there will be no requirement for a refundable accommodation deposit. The facility does anticipate that tenants will be eligible for Commonwealth Assistance Allowance.

The facility aims to follow the "Village life" concept, which is designed to cater for elderly residents who are capable of living alone but seek social support and assistance in maintaining their home. The concept is centered on the creation of quality serviced accommodation while incorporating a traditional community lifestyle. The facility seeks to provide a significant degree of independent living whilst providing additional support in the form of assistance with shopping, companionship and organised activities.

In this sense, the Council considers that the proposal is in accordance with the Commonwealth Government's *Living Longer: Living Better* reforms.

2. Proposal

Built form:

The proposal seeks to establish a supported accommodation facility incorporating:

- 8 single storey residential flat buildings accommodating a total of 50 units
- One (1) two storey community centre and associated manager's residence
- Provision of 44 car parks and 1 ambulance bay
- Landscaping throughout the facility.
- 2.4m high covered walkways throughout the facility

The eight residential flat buildings are to be clad in brick with aluminium window frames and colorbond roofing. They are generally located around the perimeter of the site with the exception of:

- Building 5, which is more centrally located to allow for an existing shed; and
- Building 8 which is perpendicular to the western boundary rather than parallel to it.

Each residential flat building provides between five and eight individual accommodation units. All are inward facing with frontage toward the centre of the site.

Seven of the accommodation units allow for double occupancy, three have been specifically designed to provide disabled facilities and the balance are single occupancy. All units follow a flow through design which incorporates a living area at the entrance to the unit, a centrally located galley kitchen and bathroom with a single bedroom at the rear having direct access to a rear courtyard.

The proposed Community Centre is located in the centre of the site, it has a maximum height of 7.17m to the top of the roof. However, the building consists of two levels:

- A single storey component containing the common room and library; and
- A two storey component where the common room extends into a kitchen and laundry area and where the main office is located. A manager's residence forms part of the second level above, which comprises three bedrooms, a living / dining area and kitchen and bathroom facilities. The manager's residence is accessed via a staircase adjacent the main office on the ground floor. The main office and stairwell can be closed off from the common facilities and a separate rear access is provided. A small balcony is located on the north eastern corner of the upper level manager's residence.

Vehicular access to the site is via Warner Road with a central driveway running through the site; car parks 1 and 3 straddle the Community Centre to the north of the driveway. Car park 1 allows for 16 vehicles and car park 3 provides for 12 vehicles, including three disabled and an ambulance bay. Car park 2 is located south of the central driveway between accommodation units 5 and 8 and provides 16 parking spaces.

A pedestrian only access is available via the northern boundary where it adjoins Sol Avenue and a number of covered walkways are provided across the site to provide protected pedestrian access between buildings. Significant landscaping is also to be provided throughout the site, separating each building and open space area and along the driveway.

An existing large shed located in the south eastern corner of the allotment is to be retained and immediately west of this a stormwater basin is to be provided.

The bulk of the site is to be separated from Warner Road by four residential allotments, ranging in depth from 30 -35m, proposed allotment 1 in the south western corner is to retain an existing dwelling. A distance of 12m separates proposed lots 2 and 3 in order to provide for the main access driveway. The division of the four allotments at the front of the site does not form part of this application.

Social provisions:

The facility will employ two full time staff who will act as on site manager and chef. They will be provided accommodation in the form of a manager's residence located above the community centre. It is expected the permanent site managers will be a couple. Other staff such as gardeners will visit the site on an as needs basis.

Residents will have full use of the community building and meals are provided as part of the weekly rental charge, breakfast and dinner will be provided to the individual accommodation units, whilst lunch is provided in the communal dining hall. The community building incorporates a laundry, a library, tea and coffee facilities and a large television.

Security screens are to be fitted to all doors and windows of each accommodation unit with the option of an emergency call pendant available. Each unit is also to be provided with an

air conditioner and fresh linen is to be provided weekly.

The Community Centre has been centrally located specifically to act as an activity hub, with organised social events and activities such as cards, movies and exercise classes. The facility will also provide organised visits from a pharmacist and weekly hairdresser visits.

An eleven seater bus owned by the Murray Mallee Aged Care Group will be made available to residents who do not drive or own their own vehicle for visits to the local shops or the occasional outing.

Copies of application documents can be found in **Attachment 1**.

3. Site and Locality

The subject land is described as Certificate of Title Volume 5619, Folio 704, Allotment 151 Deposited Plan 6457 in the area named Murray Bridge, Hundred of Mobilong. It is more commonly known as 14 Warner Road.

The current title has an area of 11,510m² however the site of the facility has an area of approximately 9,032m². The balance of the land is proposed to be excised to provide for four residential allotments with direct frontage to Warner Road. One of the titles to be excised is to contain an existing dwelling and its associated outbuildings. A large shed located on the south eastern corner of the existing allotment is to remain and be utilised as part of the supported accommodation facility. The balance of the land is undeveloped.

The locality is characterised by relatively low density residential allotments immediately south of the site, whilst land immediately north has recently been sub divided to allow for residential development. The broader locality however includes a number of larger allotments reflecting the overall location of the site on the edge of the town. A large parcel of land, on the north eastern corner contains a number of glass houses, reflective of the historical land uses associated with the area. The broader locality also includes land zoned Light Industry and Deferred Urban and as such includes a number of more commercial land uses on larger allotments, or larger allotments within the Deferred Urban zone which display more rural land uses, such as glasshouses.

4. Summary of Representations and Applicants Response

Pursuant to Section 38(5) of the Development Act the proposal underwent Category 3 Public Notification. A notification was placed in the Murray Valley Standard on 21st September 2017 with representations to be received by 6th October 2017. Letters were also sent to affected landowners.

From this public notification period four (4) representations were received. The table below identifies the representations received and the applicant's response. Full copies of the representations received and the applicant's response can be found in **Attachment 2**.

Representor	Issue	Applicant's response	To be heard
Fred & Helen McInerney	<p>Stormwater Mitigation</p> <p>Their property adjoins the site and they have problems with stormwater runoff. Currently utilise a drain to manage this however heavy rains lead to the shed being flooded. Are concerned that the development will increase stormwater impact.</p> <p>Are willing to sell property to assist in stormwater management.</p>	<p>Engineering professionals Epic Projects and Consulting were engaged to address infrastructure and service requirements, including stormwater and sewerage. They undertook this work in consultation with Council Engineers. The applicant appreciates the offer to purchase the adjoining property however it does not form part of the application process.</p>	Yes
Robert Sherrah	<p>Supports the development however has concerns regarding vehicle headlights from residents/visitors/staff shining into his lounge room/ bedroom.</p> <p>Suggests relocation of driveway</p>	<p>Are open to suggestions to address the matter of head- lights, possibly through a combination of options possibly through advisory signage requiring people to dim their headlights and the use of additional landscape screening. Suggest this be managed by way of a condition.</p>	Yes
Kym Docherty and Yvonne Thomas	<p>Raises concerns regarding the time frame for public consultation. Are away until after the consultation ends so will be unable to view plans. Query the clientele for the facility, whether or not it is solely for aged residents or for other clientele</p> <p>Are concerned regarding the location of the Community Centre, and should it not be located centrally the potential impact upon their property.</p> <p>Raise concerns regarding the potential density of the development and it's appropriateness within a residential zone.</p>	<p>Notes the public notification period is defined by legislation and plans can be made available electronically if requested. Are happy to meet and discuss the proposal with the representers. Confirms the facility seeks to provide supported accommodation to people aged 55 years and over, in a response to growing demands for affordable secure and innovative housing for the elderly.</p> <p>Confirms the two storey structure is to be centrally located within the site. They also note the representers property is immediately adjacent one of the newly created residential allotments.</p> <p>With reference to the scale of the development the applicant notes that a supported accommodation facility is envisaged within the provisions of the Residential Zone and throughout the Development Plan.</p>	Yes

Representor	Issue	Applicant's response	To be heard
		The proposal incorporates significant amounts of green space and the proposal has been specifically designed to meet the provisions associated with 'Village Life' projects.	
Than On	Is in support of the proposal and has no concerns with the construction and implications of the proposed development	Noted.	No

5. Referrals

An internal referral to Council's Engineering section was undertaken with comments incorporated in the relevant *Development Assessment* sections that follow.

As the facility has access to SA Water utilities, no Environmental Health referral was required.

6. Development Assessment

The proposal is neither a complying nor non-complying form of development and must be considered on its merits against the relevant provisions of the Development Plan. The *Development Act 1993* provides that a Planning Authority is to have regard to the relevant provisions of the Development Plan in assessing development proposals.

The site is located within the Residential Zone of the Murray Bridge Council Development Plan, consolidated 2nd May 2017. The land is depicted in map series MuBr/26 and MuBr/27 within the Development Plan.

6.1 Land Use

The Residential Zone seeks to provide a range of dwelling types throughout the zone, with specific reference to increased densities in close proximity to centres, public transport and public open space. Principle of Development Control (PDC) 1 identifies supported accommodation as an envisaged form of land use within the zone. The *General Section, Residential Development, Supported Accommodation and Housing for Seniors*, PDC 1 also identifies that such facilities be located within walking distance of essential facilities. The proposal is considered consistent with these provisions in that it specifically provides residential accommodation for elderly residents of Murray Bridge and surrounding areas. The site is just over a one kilometre walking distance from the local IGA supermarket located on Adelaide Road, and the provision of a resident's bus allows for easy access into the town centre for those who do not drive.

This being the case the proposed land use is considered appropriate for the zone.

6.2 Siting, Design and Appearance

The *General Section, Residential Development, Supported Accommodation and Housing for Seniors* within Council's Development Plan assists in providing guidance as to the form and appearance of any supported accommodation facility, more specifically;

PDC 1 Supported accommodation and housing for seniors (including nursing homes, hostels, retirement homes, retirement villages, residential care facilities and special accommodation houses) should be:

- (a) located within walking distance of essential facilities such as convenience shops, health and community services and public and community transport.
- (b) located where on-site movement of residents is not unduly restricted by the slope of the land
- (c) sited and designed to promote interaction with other sections of the community, without compromising privacy
- (d) of a scale and appearance that reflects the residential style and character of the locality
- (e) provided with public and private open space and landscaping.

PDC 2 Supported accommodation and housing for seniors should be designed to provide safe, secure, attractive, convenient and comfortable living conditions for residents that include:

- (a) internal communal areas and private spaces
- (b) useable recreation areas for residents and visitors, including visiting children
- (c) spaces to accommodate social needs and activities, including social gatherings, internet use, gardening, keeping pets, preparing meals and doing personal laundry
- (d) storage areas for items such as boats, trailers and caravans
- (e) mail boxes and waste disposal areas within easy walking distance of all units.

The proposal is generally consistent with these provisions in that the facility has been specifically designed to promote interaction within the facility, through the provision of a central Community Centre and covered walkways between buildings. The Community Centre is intended to assist in promoting the social needs of residents through activities, social events and a communal eating area.

Each accommodation unit has a small rear courtyard to allow for individual gardening opportunities if required and the site itself provides suitable landscaping throughout the facility in order to assist in the visual appearance and provision of open space and to provide opportunities for socialising.

The site is relatively flat and ground level access is available to all units, with three units specifically identified as being for disabled use and being suitable for wheelchair access. The existing shed is to be retained and allow for storage of larger items in a common area.

6.3 Car Parking and Access

The General Section, Residential Development, Supported Accommodation and Housing for Seniors within Council's Development Plan also provides guidance regarding car parking provision and access:

PDC 3 Access roads within supported accommodation and housing for seniors developments should:

- (a) not have steep gradients
- (b) provide convenient access for emergency vehicles, visitors and residents
- (c) provide space for manoeuvring cars and community buses
- (d) include kerb ramps at pedestrian crossing points
- (e) have level-surface passenger loading areas.

PDC 4 Car parking associated with supported accommodation and housing for seniors should:

- (a) be conveniently located on site within easy walking distance of resident units
- (b) be adequate for residents, service providers and visitors
- (c) include covered and secure parking for residents' vehicles
- (d) have slip-resistant surfaces with gradients not steeper than 1-in-40
- (e) allow ease of vehicle manoeuvrability
- (f) be designed to allow the full opening of all vehicle doors
- (g) minimise the impact of car parking on adjacent residences owing to visual intrusion and noise
- (h) be appropriately lit to enable safe and easy movement to and from vehicles.

The proposal involves the provision of 44 car parks of a size and design considered acceptable by Council's engineering staff, three of these car parks are for people with disabilities. The car parks are separated into three distinct locations so as to be appropriately located near available accommodation units. The facility is to be accessed via a central 7m wide driveway with provision at the end which allows smaller vehicles to turn around if necessary. An ambulance bay is to be centrally located near the Community Centre and provision is made for two staff car parks and four visitor car parks.

Table MuBr/2 – Off Street Vehicle Parking Requirements identifies that for an "Aged Care retirement home" car parking should be provided at a rate of one space per residential unit, given that the proposal involves the provision of 50 accommodation units, plus a managers residence, there is a short fall of some six car parks. The applicant has argued that knowledge of the supported accommodation industry across South Australia indicated that there is limited ownership/ reliance on cars in such facilities and residents are more likely to rely on community transport services, such as the 11 seater bus which is to be provided by the facility. Additionally, as a managed facility the development will be able to self-regulate in terms of provision of car parks in relation to accommodation unit rental.

One representor has raised concerns regarding the potential for light glare associated with traffic leaving the site during the evening. The applicant has responded to this concern and whilst it is considered that it is unlikely there will be excessive night time vehicle movements, a condition has been imposed requesting a sign be provided for vehicles leaving the site to dim their lights.

As such the proposed car park provision and configuration is considered acceptable.

6.4 Landscaping

The applicant has provided a landscaping plan which indicates the types and location of species to be planted. The species list includes both natives and more ornamental plants such as Standard Roses and Agapanthus, given that the facility intends to provide recreational gardening and socialising opportunities this mix is considered appropriate.

It is also noted that there was some discussion regarding the provision of a Community Garden for the growing of herbs and vegetables, which whilst not forming part of the current landscaping plan has the potential to assist with the Village feel of the proposal. The proposal is considered consistent with *General Section, Landscaping Fences and Walls, Objective 1* which seeks to ensure the amenity of the land and development is enhanced with appropriate planting and other landscaping works.

6.5 Environmental Assessment

An earlier land division application associated with this site identified that the site may have previously contained glass houses and associated horticultural uses. As such uses have the potential to give rise to site contamination issues, further investigation was requested. The results of these investigations have been presented as part of this proposal and will also be transferred to the current land division proposal.

An "Environmental Site History" investigation was undertaken in June 2017, this was undertaken in accordance with the *National Environmental Protection (assessment of Site Contamination Measure 1999 (NEPM))*. This report indicated that there was low to moderate potential risk presented to the identified human health and environment receptors associated with the site. However, it did recommend that a preliminary soil investigation, specifically targeting the identified areas of interest during the Environmental Site History be undertaken.

In July 2017 this limited soil investigation was undertaken and this determined that there was no evidence of contamination within the upper 1m of the soil profile locations tested that would present an unacceptable risk to human health and/or the environment in a sensitive land use setting. All of these reports were undertaken by FMG Engineering and copies can be found in **Attachment 3**.

The proposal is therefore considered to meet the *General Section, Hazards, Site Contamination, Site Contamination*, Objectives 8 and 9 and PDC 23 of the Development Plan.

6.6 Stormwater, waste and Infrastructure provision

A stormwater management plan has been prepared by Epic Projects and Consulting and reviewed by the Council's engineering staff. The stormwater management plan involves the provision of an easement through a downstream property on Doecke Road, which will remove the need for excess fill on the site.

In essence the stormwater management plan includes utilisation of the three car parking areas as surface detention combined with the main detention basin located on the southern boundary. In the event of storms greater than the 1 in 10 year ARI, the site will drain via the main basin and its overflow through the site and out to Doecke Road. The applicant is in the process of ensuring the easement has been registered. Three rainwater tanks are to be provided adjacent the existing shed.

The site has access to SA Water sewer reticulation and will be serviceable via gravity drains. An existing SA Water 100mm water main is located in Warner Road, which is suitable to service the site.

The applicant has indicated that individual waste/garbage from the site is to be managed via the use of 20 standard size wheelie bins which will be periodically emptied into three skip bins located near the entrance to the facility. These skip bins will then be emptied by a private waste contractor on an as needs basis. No kerbside collection will be required.

The proposal is considered to meet *General Section, Infrastructure* PDC 1 in terms of provision of adequate utilities and services and PDC 2 regarding the provision of easement

7. Assessment Summary

The application has been assessed against the Murray Bridge Development Plan consolidated 2 May 2017. The proposal has also been reviewed by Council's Engineer who considers the proposal to be generally acceptable. The land use is entirely appropriate, and envisaged, within the Zone. The design and appearance of the proposal is consistent with Objectives and Principles contained within the Development Plan.

It is therefore considered that the proposed development is not seriously at variance with the relevant provisions of Development Plan and on this basis displays sufficient merit to warrant support.

RECOMMENDATION

Following a detailed assessment of the proposal against the relevant provisions of the Rural City of Murray Bridge Development Plan consolidated 2 May 2017, referrals and consideration of written & verbal representations the Development Assessment Panel resolves that:

- A. *That pursuant to Section 35 (2) of the Development Act, 1993, the proposal is not seriously at variance with the relevant provisions of the Murray Bridge Development Plan.*
- B. *That pursuant to Section 33 of the Development Act, 1993, Development Application Number 415/346/2017 be GRANTED Development Plan Consent subject to the following conditions*

DEVELOPMENT PLAN CONSENT CONDITIONS:

1. That except where minor amendments may be required by other relevant Acts, or by conditions imposed by this application, the development shall be established in strict accordance with the details and plans submitted in development application number 415/346/2017

Plan numbers listed here:

- Site Plan – Drawing No DA-01 Revision F, prepared by Salvatore Marzullo Building Design ,received 28 September 2017
- Typical Floorplans - Drawing No DA-03, Revision F, prepared by Salvatore Marzullo Building Design received 21 August 2017
- Community Centre Floor plan Drawing No DA-04, Revision F, prepared by Salvatore Marzullo Building Design received 21 August 2017
- Elevations 1,2 and 3 – Drawing No DA-05, Revision F, prepared by Salvatore Marzullo Building Design received 21 August 2017
- Elevation 4 – Drawing No DA-06, Revision F, prepared by Salvatore Marzullo Building Design received 21 August 2017
- Floor Plan Building 1 Drawing No DA-7, Revision F, prepared by Salvatore Marzullo Building Design received 21 August 2017
- Elevations Building 1 Drawing No DA-08, Revision F, prepared by Salvatore Marzullo Building Design received 21 August 2017
- Floor Plan Building 2 Drawing No DA-09, Revision F, prepared by Salvatore Marzullo Building Design received 21 August 2017
- Elevations Building 2 Drawing No DA-10, Revision F, prepared by Salvatore Marzullo Building Design received 21 August 2017

- Floor Plan Building 3 Drawing No DA-11, Revision F, prepared by Salvatore Marzullo Building Design received 21 August 2017
- Elevations Building 3 Drawing No DA-12, Revision F, prepared by Salvatore Marzullo Building Design received 21 August 2017
- Floor Plan Building 4, Drawing No DA-13, Revision F, prepared by Salvatore Marzullo Building Design received 21 August 2017
- Elevations Building 4, Drawing No DA-14, Revision F, prepared by Salvatore Marzullo Building Design received 21 August 2017
- Floor Plan Building 5, Drawing No DA-15, Revision F, prepared by Salvatore Marzullo Building Design received 21 August 2017
- Elevations Building 5, Drawing No DA-16, Revision F, prepared by Salvatore Marzullo Building Design received 21 August 2017
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- Floor Plan Building 7, Drawing No DA-19, Revision F, prepared by Salvatore Marzullo Building Design received 21 August 2017
- Elevations Building 7, Drawing No DA-20, Revision F, prepared by Salvatore Marzullo Building Design received 21 August 2017
- Floor Plan Building 8, Drawing No DA-21, Revision F, prepared by Salvatore Marzullo Building Design received 21 August 2017
- Elevations Building 8, Drawing No DA-22, Revision F, prepared by Salvatore Marzullo Building Design received 21 August 2017
- Project Report Supported Accommodation Facility, Lot 151 Warner Rd Murray Bridge, August 2017
- Epic Projects and Consulting, Proposed supported living residential development at Murray Bridge, Stormwater Management Plan and Services Report, Rev B, received 21 August 2017
- Site Plan prepared by Salvatore Marzullo Building Design, dated by hand 16 August 2017 and providing details of proposed landscaping.
- Hand written note from Misty Creek Garden Centre dated 12 August 2017 providing details of proposed landscaping

Reason: To ensure the development proceeds in an orderly manner.

2. All external lighting of the site, including car parking areas, driveways and buildings must be designed and constructed to conform with Australian Standards and must be located, directed and shielded and be of such limited intensity that no nuisance or loss of amenity is caused to any person beyond the site.

Reason: To preserve the amenity of the locality.

3. The landscaping as approved by Council must be established upon the subject land within twelve (12) months of the date of this consent or the completion of construction of the buildings approved by this application (whichever occurs first). The landscaping shall be maintained and nurtured at all times to the satisfaction of Council.

Reason: To preserve the amenity of the locality

4. The proposed 'Skip Location' as identified on the Site Plan – Drawing No DA-01 Revision F, prepared by Salvatore Marzullo Building Design, received 28 September 2017, shall be appropriately screened by either plantings or structural screening within twelve (12) months of the date of this consent or the completion of construction of the buildings approved by this application (whichever occurs first).

Reason: To preserve the amenity of the locality

5. The car parking area must be line marked in accordance with the approved plans and in accordance with the Australian Standard 2890.1-2004 Off Street Parking, Car Parking Facilities prior to the occupation of the proposed development. The line marking must be maintained to the satisfaction of Council.

Reason: To ensure the safe and orderly movement of vehicles

6. All car parking spaces, driveways and associated manoeuvring areas shall be sealed in bitumen, concrete or brick pavers prior to the occupation of the proposed development.

Reason: To ensure the safe and orderly movement of vehicles

7. A sign, having an area not exceeding 0.2 square metres, requiring vehicles leaving the site to dim their lights, shall be erected at the area identified as the skip location on the Site Plan – Drawing No DA-01 Revision F, prepared by Salvatore Marzullo Building Design, received 28 September 2017. This sign shall be visible to those exiting the site.

Reason: To preserve the amenity of the locality

8. Access to buildings and designated disabled car parking spaces shall be designed and provided in accordance with the provisions contained in "Guidelines for the provision of parking for people with disabilities in South Australia" (March 1993) and AS1428, Parts 1, 2 and 4.

Reason: To ensure the development proceeds in an orderly manner

9. All stormwater from buildings and paved area shall be disposed of in accordance with the Stormwater Management Plan provided and such a manner that it does not result in the entry of water into any building, affect the stability of any building or impact any adjoining property.

Reason: To ensure stormwater is disposed of in a controlled manner.

10. That an appropriate Construction Environment Management Plan (CEMP) which addresses the mitigation or minimisation of impacts (especially from noise and dust) during the construction phase shall be prepared and implemented. Dust generated by machinery and vehicular movement during site works, and any open stockpiling of soil or building materials at the site, must be suppressed to ensure that dust generation does not become a nuisance off site. Site development machinery should generally not be operated outside the hours of 7am to 6pm daily

Reason: To preserve the amenity of the locality

NOTES:

1. The development must be substantially commenced within 12 months of the date of this Notification, unless this period has been extended in writing by Council.
2. Before proceeding with this proposal, you are required to seek Building Rules Consent pursuant to the provisions of the Development Act, 1993.
3. Building work must not be commenced until Building Rules Consent and Development Approval is granted.
4. You have a right of appeal against the conditions which have been imposed on this Provisional Development Plan Consent or Development Approval
5. Such an appeal must be lodged at the Environment, Resources and Development Court within two months of the day on which you receive this notice or such longer time as the Court may allow.
6. Please contact the Court if you wish to appeal. The Court is located in the Sir Samuel Way Building, Victoria Square, Adelaide. (Telephone number 8204 0300).
7. The approval for this development does NOT imply approval to alter, shift or remove any street tree, side entry pit, stobie pole, bus stop, fire hydrant or other gas, electricity, water, telecommunications or other similar infrastructure. Approval to alter any of these or similar infrastructure needs to be obtained from the relevant government department, private organisation or Council. Any costs associated with such alteration are the responsibility of the applicant.
8. As your proposed development includes the construction of a driveway(s) over the footpath area, you are advised that a Permit to undertake works that impact on Council's infrastructure is required. The owner/applicant is required to complete an 'Application to Construct Crossing Place' prior to commencing work on the driveway associated with this development in order for Council to issue the permit.
9. The applicant is advised to seek information from 'Dial before you dig' to ensure that no services will be affected prior to commencing work.
10. Directional signs indicating the location of car parking and disabled car parking spaces must be provided on the subject land and maintained in a clear and legible condition at all times. Further approval is required if the sign is greater than 0.2 square metres and visible from outside the site.
11. You are advised that the proposal should be designed to meet the requirements of the Disability Discrimination Act in relation to the provision of facilities and access for people with disabilities to the development.
12. You are advised that under the Fences Act you are legally required to give notice for the removal of a fence on the common boundary. Please refer to the Fences Act for the correct procedural requirements.

DISCUSSION

Representors Fred & Helen McInerney addressed the Panel in relation to their concerns on the proposed development. The applicant's representative Mr David Altmann, Ms Anna Howard & Mr Robert McDonald responded to the representors concerns and questions from Panel Members. It is noted that representor Mr Robert Sherrah was not present at the meeting.

Representors Kym Docherty and Yvonne Thomas withdrew their representation just prior to the meeting, after speaking with Murray Mallee Aged Care they are satisfied the proposed development will not impact their property.

The Panel discussed the application and reached the following decision.

DECISION**Cr Jerry Wilson moved**

Following a detailed assessment of the proposal against the relevant provisions of the Rural City of Murray Bridge Development Plan consolidated 2 May 2017, referrals and consideration of written & verbal representations the Development Assessment Panel resolves that:

- A. *That pursuant to Section 35 (2) of the Development Act, 1993, the proposal is not seriously at variance with the relevant provisions of the Murray Bridge Development Plan.*
- B. *That pursuant to Section 33 of the Development Act, 1993, Development Application Number 415/346/2017 be GRANTED Development Plan Consent subject to the following conditions*

DEVELOPMENT PLAN CONSENT CONDITIONS:

1. That except where minor amendments may be required by other relevant Acts, or by conditions imposed by this application, the development shall be established in strict accordance with the details and plans submitted in development application number 415/346/2017

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- Site Plan prepared by Salvatore Marzullo Building Design, dated by hand 16 August 2017 and providing details of proposed landscaping.
- Hand written note from Misty Creek Garden Centre dated 12 August 2017 providing details of proposed landscaping

Reason: To ensure the development proceeds in an orderly manner.

2. All external lighting of the site, including car parking areas, driveways and buildings must be designed and constructed to conform with Australian Standards and must be located, directed and shielded and be of such limited intensity that no nuisance or loss of amenity is caused to any person beyond the site.

Reason: To preserve the amenity of the locality.

3. The landscaping as approved by Council must be established upon the subject land within twelve (12) months of the date of this consent or the completion of construction of the buildings approved by this application (whichever occurs first). The landscaping shall be maintained and nurtured at all times to the satisfaction of Council.

Reason: To preserve the amenity of the locality

4. The proposed 'Skip Location' as identified on the Site Plan – Drawing No DA-01 Revision F, prepared by Salvatore Marzullo Building Design, received 28 September 2017, shall be appropriately screened by either plantings or structural screening within twelve (12) months of the date of this consent or the completion of construction of the buildings approved by this application (whichever occurs first).

Reason: To preserve the amenity of the locality

5. The car parking area must be line marked in accordance with the approved plans and in accordance with the Australian Standard 2890.1-2004 Off Street Parking, Car Parking Facilities prior to the occupation of the proposed development. The line marking must be maintained to the satisfaction of Council. One (1) designated car parking space shall be identified for use by doctors only.

Reason: To ensure the safe and orderly movement of vehicles

6. All car parking spaces, driveways and associated manoeuvring areas shall be sealed in bitumen, concrete or brick pavers prior to the occupation of the proposed development.

Reason: To ensure the safe and orderly movement of vehicles

7. A sign, having an area not exceeding 0.2 square metres, requiring vehicles leaving the site to dim their lights, shall be erected at the area identified as the skip location on the Site Plan – Drawing No DA-01 Revision F, prepared by Salvatore Marzullo Building Design, received 28 September 2017, This sign shall be visible to those exiting the site.

Reason: To preserve the amenity of the locality

8. Access to buildings and designated disabled car parking spaces shall be designed and provided in accordance with the provisions contained in "Guidelines for the provision of parking for people with disabilities in South Australia" (March 1993) and AS1428, Parts 1, 2 and 4.

Reason: To ensure the development proceeds in an orderly manner

9. All stormwater from buildings and paved area shall be disposed of in accordance with the Stormwater Management Plan provided and such a manner that it does not result in the entry of water into any building, affect the stability of any building or impact any adjoining property.

Reason: To ensure stormwater is disposed of in a controlled manner.

10. That an appropriate Construction Environment Management Plan (CEMP) which addresses the mitigation or minimisation of impacts (especially from noise and dust) during the construction phase shall be prepared and implemented. Dust generated by machinery and vehicular movement during site works, and any open stockpiling of soil or building materials at the site, must be suppressed to ensure that dust generation does not become a nuisance off site. Site development machinery should generally not be operated outside the hours of 7am to 6pm daily

Reason: To preserve the amenity of the locality

NOTES:

1. The development must be substantially commenced within 12 months of the date of this Notification, unless this period has been extended in writing by Council.
2. Before proceeding with this proposal, you are required to seek Building Rules Consent pursuant to the provisions of the Development Act, 1993.
3. Building work must not be commenced until Building Rules Consent and Development Approval is granted.
4. You have a right of appeal against the conditions which have been imposed on this Provisional Development Plan Consent or Development Approval
5. Such an appeal must be lodged at the Environment, Resources and Development Court within two months of the day on which you receive this notice or such longer time as the Court may allow.
6. Please contact the Court if you wish to appeal. The Court is located in the Sir Samuel Way Building, Victoria Square, Adelaide. (Telephone number 8204 0300).
7. The approval for this development does NOT imply approval to alter, shift or remove any street tree, side entry pit, stobie pole, bus stop, fire hydrant or other gas, electricity, water, telecommunications or other similar infrastructure. Approval to alter any of these or similar infrastructure needs to be obtained from the relevant government department, private organisation or Council. Any costs associated with such alteration are the responsibility of the applicant.
8. As your proposed development includes the construction of a driveway(s) over the footpath area, you are advised that a Permit to undertake works that impact on Council's infrastructure is required. The owner/applicant is required to complete an 'Application to Construct Crossing Place' prior to commencing work on the driveway associated with this development in order for Council to issue the permit.
9. The applicant is advised to seek information from 'Dial before you dig' to ensure that no services will be affected prior to commencing work.
10. Directional signs indicating the location of car parking and disabled car parking spaces must be provided on the subject land and maintained in a clear and legible condition at all times. Further approval is required if the sign is greater than 0.2 square metres and visible from outside the site.
11. You are advised that the proposal should be designed to meet the requirements of the Disability Discrimination Act in relation to the provision of facilities and access for people with disabilities to the development.
12. You are advised that under the Fences Act you are legally required to give notice for the removal of a fence on the common boundary. Please refer to the Fences Act for the correct procedural requirements.

Seconded by Myles Somers and CARRIED

Attachments

1.	Attachment 1 - Application Documents - 415-346-2017	Attachment
2.	Attachment 2 - Representations and Applicant Response - 415-346-2017	Attachment
3.	Attachment 3 - Site Investigation Report - 415-346-2017	Attachment



7. EXECUTIVE OFFICERS REPORT

Nil

8. CORRESPONDENCE

Nil

9. FUTURE APPLICATIONS

Nil

10. GENERAL BUSINESS

10.1	Date of Panel Meeting : 17 November 2017
DA NUMBER	415/E014/16
PROPOSAL	Request for comment regarding the redevelopment of the Bridgeport Hotel
APPLICANT	Kerin Bay Pty Ltd

LOCATION	2-6 (Lots 94-99) Adelaide Road, Murray Bridge
ASSESSING OFFICER	Glenn Searle
ZONE/POLICY AREA/PRECINCT	Bridge Street Policy Area 11 of the Regional Town Centre Zone
NATURE OF DEVELOPMENT	Merit
PUBLIC NOTIFICATION CATEGORY	1
REPRESENTATIONS	N/A
AGENCY CONSULTATIONS	Undertaken by DAC (DPTI Transport)
INTERNAL REFERRALS	Engineering, Health, Assets and Heritage Consultants
DEVELOPMENT PLAN	Consolidated 11 August 2016
RECOMMENDATION	To supply the Council's comments to the State Commission Assessment Panel

Background

The State Commission Assessment Panel (SCAP) as the 'relevant authority' for this application (as per Schedule 10 part 20 of the Development Regulations 2008) has requested that the Council use its professional expertise to assist in the assessment of the application by providing comprehensive comments.

The application has been previously considered by the Council's Development Assessment Panel (now the Council's Assessment Panel - CAP) on 9th December 2016. At that meeting the Panel considered that the concept for the redevelopment of the site for hotel purposes was supported by the Development Plan.

However, it was considered that the proposal failed to adequately address the following matters:

- Architectural design and visual interest
- Heritage concerns: Including the complete demolition of an item of local heritage significance
- Access / egress to South Terrace and car parking
- Advertising
- Crime Prevention

(see **TABLE 1 – ATTACHMENT 5** for a summary of these issues).

For these reasons the Panel considered that the proposal should not be supported.

This information was considered by Council and provided to the Development Assessment Commission (now the State Commission Assessment Panel – SCAP). It was then forwarded to the applicant. A copy of the original report along with the plans and reports can be found **ATTACHMENT 1**.

Since that time the applicant has amended the design of the hotel and provided additional reports regarding the five outstanding aspects.

Proposal

It is noted that the amended design has taken into account some of Council's previous comments and changes have been made to the following aspects of the design:

- The external façade;
- The internal layout to facilitate glazing to Bridge Street and East Terrace;

- The provision of more enhanced landscaping; and
- Slight increases to the amount of car parking in both the undercroft and the at grade car parks.

The proposal involves the redevelopment of the Bridgeport Hotel site comprising the following works:

- The demolition of the Local Heritage listed Bridgeport Hotel and all other structures on the land;
- The construction of a new 6 storey hotel building. The building will contain the following:
 - An undercroft level - with 71 car parking spaces and direct vehicle access to/from South Terrace;
 - Ground floor - a sports bar, lounge bar, alfresco seating areas (also extending within Council's road reserve), gaming area, restaurant, foyer, kitchen, outdoor children's play area and other associated amenities and storage areas;
 - First floor - function room, bar, alfresco terrace, 12 accommodation rooms, gym and outdoor pool and bar for hotel guests;
 - Second to fifth floors - 22 accommodation rooms on each level;
- A new drive-through bottle shop at the rear of the site facing South Terrace, comprising a store room, retail floor space, cool room and 3 lane vehicle drive-through with a canopy; and
- 89 at grade car parks.

The Hotel is to be constructed of pre-fabricated panels with a painted finish in white, bronze and dark grey colours. Roofs and verandahs of the hotel will be finished in Surfsmist. The bottle shop building, to front South Terrace, is to be constructed of similar materials and finished in Surfsmist.

The proponent has stated the proposal will be staged in the following manner:

- Stage 1 - Civil works
- Stage 2 - Demolition of the Hotel
- Stage 3 - Bottle shop
- Stage 4 - Hotel Building

The site plan, floor plans and elevations, along with additional reports are contained in **ATTACHMENTS 2 & 3.**

Referrals

The only referrals that Council can consider are internal referrals as the application is assessed by the SCAP who are required to undertake the necessary agency referrals.

Engineering:

The Council's engineers have raised concerns with the following matters:

- The dual access proposed to the undercroft carpark will remove all of the on-street car parks in this area (9 in total);
- To facilitate this, and to provide additional car parks, new line marking will be required – this will need to occur at the applicant's expense;
- Additional water sensitive urban design (WSUD) measures (i.e. raingardens) should be incorporated in this area at the applicants expense;
- The pedestrian path from Bridgeport Hotel will not be linked directly to the current entrance to Market Place;
- Consideration should be given to providing a vehicle turning path showing how a garbage truck can manoeuvre within the car parking area, as well as the undercroft car park;
- The plans show that the existing pavers on Bridge Street and East Terrace will be retained. How will the existing structure be demolished and the new structure built ensuring that no damage is done to the existing pavers?

- There are some anomalies in the stormwater report prepared by TMK, viz:
 - In the stormwater calculator, it is estimated that the required stormwater detention would be 47.2kL;
 - However, in the civil plan, there is a 24KL underground detention system;
 - The pre-development state of the site considered to be almost impervious;
 - The total discharge as nominated by TMK, as a single outlet discharge, will deliver up to a total flow of 125.3L/s;
 - Further clarification is needed for the discharge during a minor storm where the allowable discharge is only 80.4L/s, where the orifice is sized to discharge 125.3L/s;
 - In addition to that, discharging a concentrated flow of 80.4L/s or 125.3L/s is deemed undesirable as it will cause damage to the infrastructure and also to pedestrian areas. The downstream pit(s) will not be able to handle such high flow; and
 - Therefore the method of discharging the water will need to be reviewed.

Council's engineers' main concerns with the proposal revolve around the Council's ability to provide safe pedestrian access across South Terrace (between the Bridgeport Hotel and the Marketplace). Council's engineers have requested that any approval take into account Council's design for the provision of a safe crossing place.

Assets:

An application to occupy the road reserve (Section 221 of the Local Government Act) will need to be lodged with the Council along with details regarding the new paving (on the footpath) and the removal of the street tree located on Bridge Street.

Health:

The Council's Environmental Health Officers generally have no concerns with the proposal. However, they have stated that the proponent should consider the requirements of the Food Safety Standards. Specifically Standard 3.2.2 Division 2(3)(b) regarding the design and adequacy of kitchen spaces. To ensure that this standard is met the Environmental Health Officers recommend that design, fit-out and constructions specifications be provided to Council prior to Building Rules Consent.

Furthermore, the specifications for the swimming pool should be provided to Council's Environmental Health Officers prior to Building Rules Consent.

External Heritage Advice:

Council engaged Flightpath Architects to provide a heritage advice relating to:

- The heritage value of the Bridgeport Hotel, including whether its listing as a local heritage place is warranted, taking into account the historical context and the condition of the building; and
- Provide an assessment of the proposed development against the relevant Heritage Guidelines and Provisions outlined in the Rural City of Murray Bridge Development Plan.

The report from Flightpath Architects (**ATTACHMENT 4**) provides a contrary position to the proponent's report (prepared by DASH Architects).

Discussion

This report will not enter into the same detailed analysis of the application as the previous report to the Panel as it is considered that the previous report sufficiently describes the proposal and its assessment against the Development Plan policies. Instead this report will consider the amended proposal against outstanding issues of concern that have been previously highlighted (please note Table 1).

It is considered that the amended design for the redeveloped Bridgeport Hotel adequately addresses Council's (previous) concerns with five points from Table 1 (being Land Use, Stormwater, Advertising, Crime Prevention and Landscaping). These five points are summarised in Table 1.

Table 1		
Issue	Inconsistencies with Development Plan	Suggestions / Options to Address Inconsistencies
1. Land Use	<ul style="list-style-type: none"> Not applicable – the proposed land use is consistent with the existing land use and the relevant provisions of the Development Plan. 	<ul style="list-style-type: none"> No further action required.
2. Stormwater	<ul style="list-style-type: none"> Council's Engineering Department has confirmed the proposal complies with the Council's requirements for stormwater management. 	<ul style="list-style-type: none"> No further amendments are required in relation to this matter, other than clarification of the issues raised by Council's engineers (see referral section)
3. Advertising	<ul style="list-style-type: none"> Council's previous comments have been taken into account with the redesign of the hotel with the large areas of blank wall being removed and replaced with glazing. 	<ul style="list-style-type: none"> No further action required.
4. Crime Prevention	<ul style="list-style-type: none"> Council's previous comments have been taken into account with the redesign of the hotel. Concerns regarding anti-social behaviour especially dropping items from the alfresco balcony area have been taken into account with the design of the alfresco balcony area. 	<ul style="list-style-type: none"> No further action required.
5. Landscaping	<ul style="list-style-type: none"> The Landscaping plan supplied is satisfactory. 	<ul style="list-style-type: none"> Request that SCAP impose conditions regarding the planting and maintenance of the landscaping.

This leaves three points which will be further discussed:

- Heritage
- Visual Amenity/Design
- Traffic/Car Parking

Heritage

	Regional Town Centre Zone	Bridge Street Policy Area	General Section
Heritage	Desired Future Character Statement para 8, para 9 PDC 12, PDC 13	Desired Future Character Statement para 1, 3, 5 PDC 8, PDC 9, PDC 14	Design and Appearance PDC 1 Heritage Places OBJ 1, OBJ 2, OBJ3, PDC 1, PDC 2, PDC 3, PDC 4, PDC 5, PDC 6, PDC 7 Orderly and Sustainable Development OBJ 4 Siting and Visibility PDC 1 Tourism Development OBJ 2, OBJ 3, PDC 1, PDC 2, PDC 3

Provisions contained within the Development Plan (listed above and below) seek, in the first instance, to retain heritage buildings in their current form. If there are changes proposed then the adaptive reuse of existing structures should be considered. The Development Plan essentially states that the demolition of heritage items should be the as a last resort and only in circumstances when a building is structurally unsound.

General Section – Heritage Places

OBJ 1: *The conservation of State and local heritage places.*

OBJ 2: *The continued use, or adaptive reuse of State and local heritage places that supports the conservation of their cultural significance.*

PDC 1: *A heritage place spatially located on Overlay Maps - Heritage and more specifically identified in Table MuBr/5 - State Heritage Places, Table MuBr/4 - Local Heritage Places, should not be demolished, destroyed or removed, in total or in part, unless either of the following apply:*

- (a) *that portion of the place to be demolished, destroyed or removed is excluded from the extent of the places identified in the Table(s)*
- (b) *the structural condition of the place represents an unacceptable risk to public or private safety.*

PDC 2: *Development of a State or local heritage place should retain those elements contributing to its heritage value, which may include (but not be limited to)*

- (a) *principal elevations*
- (b) *important vistas and views to and from the place*
- (c) *setting and setbacks*
- (d) *building materials*
- (e) *outbuildings and walls*
- (f) *trees and other landscaping elements*
- (g) *access conditions (driveway form/width/material)*
- (h) *architectural treatments*
- (i) *the use of the place.*

PDC 4: *Original unpainted plaster, brickwork, stonework, or other masonry of existing State or local heritage places should be preserved, unpainted.*

The previous Panel report highlighted the differences between the two heritage architects with one recommending that the Local Heritage building be demolished due to a lack of heritage fabric remaining meaning that the building should not be heritage listed. While the other raised concerns that heritage fabric (stonework) may remain behind the existing brick frontage and therefore the building still had heritage value.

In its previous comments, the Council stated that:

- A comprehensive and accurate Heritage Impact Statement should be provided. This should include (but not be limited to) an assessment and grading of heritage value based on accurate floor plans of both levels and also all elevations;
- Incorporate the retention of the Bridgeport Hotel (in some manner or form appropriate to preserve the heritage value) – this could include the retention of some external surfaces / facades or portions of the building but would be dependent on maintaining the heritage value;
- Revise the external design of the building to reflect the historic nature of the site and the broader locality.

While the plans have been amended and additional heritage reports provided the differences of opinion between the two heritage architects remain largely unresolved. DASH Architects have provided two additional reports regarding Council's concerns in relation to Local Heritage impact and State Heritage impact. Council has again engaged Flightpath to provide a response/advice.

It appears that the argument still revolves around the same two issues:

- The heritage value of the building (and whether it would be listed in its current condition); and
- The extent of heritage fabric remaining.

Heritage Value of the Building:

It is considered that the DASH report still makes fundamental errors in considering the heritage value of the Bridgeport Hotel against other Victorian hotels and argues that the hotel, in its current state, is not be worthy of being listed on the Local Heritage Register. These arguments were considered in the original Panel Report and considered erroneous for the reasons outlined below.

Comparing the Bridgeport Hotel against other Victorian hotels that are three stories tall, located within the Adelaide Metropolitan area and (most importantly) are State Heritage Items does not assist in the assessment of the application. As stated in the original report:

"...it is an unfortunate oversight on the part of DASH Architects that they failed to compare the Bridgeport Hotel against other comparable Local Heritage Places".

The question of whether or not the Bridgeport Hotel would be listed, in its current state, has largely been answered in the previous report, where it was shown that no substantial building works have been undertaken to the building since the time of its listing as a Local Heritage Item (See Heritage Section of the Panel Report for a detailed breakdown). Essentially this argument was refuted thus:

"As such it is considered the additions and upgrades to the building, with the exception of the smoking compliance area, were taken into account by the Council when preparing the PAR documentation. Furthermore these works, the state of the building and the remaining heritage fabric would have been considered by the Minister for Planning when considering the PAR. As such, any argument that the building has changed state or lost heritage value since the PAR is unlikely to have a high level of relevance.

Furthermore the 2001 survey which formed part of the PAR investigations notes that the building "has undergone numerous stylistic and structural changes, mostly at Ground Floor level". While the data sheet states that "progress of building changes are clearly evident, the least original fabric being at ground level". In other words, despite the changes, the hotel was still considered to fulfil the Section 23(4) Criteria".

Extent of the Heritage Fabric Remaining:

In the original consideration of the proposal, Flightpath Architects raised concerns that original stonework may remain behind the existing cream brick façade.

"the external walls are very thick and there is every indication that the original walls still exist behind a more recent brick external skin. The encroachment of the brick veneer skin on to the public footpath could provide a reason for its removal, to reveal the original walling. This could be verified through a licensed boundary survey".

The updated DASH report provides the following comments in regarding this, stating:

"Having undertaken further investigations on site, including inspection within the ceiling spaces adjacent the Bridge Street and East Terrace facades, I note that:

Bridge Street Elevation

- *It appears that the entire length of the ground floor section of the 'original' (c1879-1937) sections of stone wall has been removed and replaced with: (load bearing) brickwork, concrete columns and beams, and windows.*
- *We were not able to establish if the brickwork encroaches onto the footpath (over the boundary).*
- *Based on my 20 plus years of architectural consulting experience, I suggest that the removal of the brickwork and reinstatement of stonework (using new stone) would neither be readily achieved nor cost effective.*

Corner

- *As per the DASH Report, the entire 'original' lower section of the corner has been removed.*

East Terrace Elevation

- *Except at the corner where it has clearly been removed, there is evidence to suggest that there are sections of original stonework remaining behind the brickwork wall along this facade. We are not sure of the condition or extent of this stonework however we assume that it was only removed where required for the works at the corner and for new fenestration etc. I am also not sure whether the concealed stonework retains the capacity to be load bearing (this capacity may have been compromised either because of the design of the newer walls or due to its condition).*
- *Again, I was not able to establish if the newer brickwork wall encroaches onto the footpath (over the boundary).*
- *Minor concerns and comments (car parking, road treatment, garbage, solar panels, plant, etc)".*

There is no disagreement with the findings that the heritage fabric has been removed from the corner of Bridge Street and East Terrace. However, there remains disagreement on the extent of the heritage fabric remaining on the Bridge Street and East Terrace frontages, which is evident in the above extract (for East Terrace) from the DASH report. Clearly, the Council is not best placed to judge the varying opinions of the expert advice provided on this matter.

Flightpath Architects maintain that the evidence for the original walls remaining is still compelling (thickened external walls and the pattern of ground floor windows). In their opinion this could be verified through a licensed boundary survey. Alternatively, in Council's opinion, small scale work could be undertaken, under supervision, to remove some of the external cladding to reveal the presence of original stonework.

This lack of certainty regarding the extent of heritage fabric remaining is vexing as it goes to the heart of the assessment. On the one hand if, as the proponent claims there is limited heritage fabric remaining, then the reason for the removal of the heritage item is compelling. However, if the heritage fabric is simply covered by a façade of newer cream brick it would appear that the heritage item has been kept in closer to original condition. On this basis the provisions, and intent, of the Development Plan are clear that the heritage item should be preserved with any redevelopment taking into account and preserving those portions of the item.

Given this lack of certainty regarding the extent of heritage fabric remaining, not only between the heritage architects but also within the DASH Architects report, it is considered that more work should be undertaken to reveal the extent of heritage fabric that remains. This should include but not be limited to an accurate surveyed plan of the building to consider any encroachment onto the footpath and exploratory work to assess the extent of any stonework remaining behind the cream brick frontage. The Council suggests that the SCAP may wish to commission or receive the relevant evidence needed to make a determination as to which expert advice is correct prior to determining this matter.

Visual Amenity

	Regional Town Centre Zone	Bridge Street Policy Area	General Section
Visual Amenity	OBJ 3, OBJ 5 Desired Future Character Statement para 5, para 8, 9 PDC 4, PDC 11	OBJ 2 Desired Future Character Statement para 2, 3, 4, 5, 8 PDC 3, PDC 5, PDC 8, PDC 9, PDC 10, PDC 11, PDC 12, PDC 13, PDC 14, PDC 15	Centres and Retail Development PDC 2, PDC 3 (a & b) Crime Prevention PDC 2, PDC 3 Design and Appearance OBJ 1, PDC 1, PDC 2, PDC 3, PDC 4, PDC 5, PDC 7, PDC 8, PDC 9, PDC 11, PDC 12, PDC 13, PDC 14, PDC 16, PDC 18 Landscaping, Fences and Walls OBJ 1, PDC 1, PDC 4 Natural Resources OBJ 13 Orderly and Sustainable Development OBJ 1, OBJ 4 Siting and Visibility OBJ 1, PDC 1, PDC 2, PDC 5, PDC 6 Tourism Development OBJ 3, OBJ 4, PDC 1, PDC 2, PDC 3, PDC 5

As previously stated the internal design and the external façade of the proposed hotel building have been amended taking into account the Council's previous comments. This amended external design/façade is considered to be a significant improvement on the design previously presented to the Panel.

To briefly summarise the design of the proposed hotel building; it is a rectangular shaped structure constructed of pre-fabricated panels with a painted finish in white, bronze and dark grey colours. Solid panels appear blank with joint line articulations coinciding with floor levels. The building is to be located on the eastern portion of the site with its narrower width facing Bridge Street and its longer side facing east and west. The ground floor façade (to Bridge Street and East Terrace) has been completely redesigned reducing the amount of blank walls facing the street and replacing it with glazing and outdoor dining areas.

As with the previous report the main concern with the proposal is the height above natural ground level and the potential resultant impact on the visual amenity of the locality.

It is noted that Council and the proponent both agree that the site is a landmark location and as such special consideration should be given to allow for taller buildings. However, as previously considered if consideration is to be given to the special character of subject site (i.e. a landmark site) then special consideration must also be given to the architectural and design elements of the building which should reflect the unique and special character of the site and be conscious of the impact that any built form will have.

The proponent has stated that the height of the building is 23.3m from natural ground level at the Bridge Street frontage; however this is not the total height of the building above natural ground level. The total height of the building will be much higher than 23.3m.

The proponent has provided a plan showing the heights of the proposed building, the Marketplace and Ridleys Mill when viewed from East Terrace (see **ATTACHMENT 3**). This plan shows that the Marketplace building is approximately 20m above natural ground level. However, it should also be noted that the Marketplace is located in a different policy area which does not contain provisions regarding building height.

The prescriptive provisions of the Zone and Policy Area (listed later) speak of a maximum height of 8 metres (or two storeys) above ground level while the subjective provisions provide guidance in assessing this aspect of the proposal. These provisions speak to establishing a cohesive built form comprising historic and modern building designs whilst creating attractive buildings and places that enhance the image of the town.

Moreover the type of development and activity sought in Bridge Street "needs to exhibit a high quality and image that reflects a prosperous and pleasant township", with an emphasis on the role of Bridge Street as the traditional 'main street' of Murray Bridge, reinforced by new development that creates a "cohesive lineal streetscape appearance and where the scale of new development is compatible with the existing built-form" with "a high visual amenity and strong local character to create a sense of place and focal point within the town".

It is suggested that the building, being at least three times greater in height than the maximum height specified in the provisions, and much greater in height than any surrounding building fails to comply with these provisions. Furthermore the building has a limited relationship through its design, scale, bulk, proportions and compositions of design elements with the locality of the main street or the Murray River.

Whilst the proposal must be considered against the provisions of the Development Plan in place at the time of application, it is also clearly not pragmatic or in the best interests of the proponent, the Council or SCAP to seek a new application to be compared against current Development Plan policy. In this sense the current provisions are a very clear indication of Council's intent for this landmark site and are a material planning consideration.

As such consideration should be given to the amended design of the hotel building and whether this improved design (and the landmark nature of the site) offsets the potential of the building to visually dominate the vista on which it is proposed, potentially resulting in a negative impact upon the visual amenity and character of the locality.

Bridge Street Policy Area:

"PDC 10: New buildings should maintain the existing horizontal massing of built-form while incorporating vertical proportioning in the composition of facades.

PDC 13: The spatial character of Bridge Street, established by the width of the street relative to its generally one-to-two-storey building height, should predominate.

PDC 15: Buildings within Bridge Street should have a maximum height of 8 metres".

Regional Town Centre Zone

Desired Character Statement:

The Murray Bridge town centre reflects the growth of the town as a regional centre over the past 150 years. The current land use patterns reflect this evolution of the past, and show a range of building forms from historic to modern buildings. Development within the town centre is intended to generate a greater cohesion of these features and to create a more attractive and better functioning centre as it continues to grow.

Development will promote a vibrant, economically viable and attractive centre, which will best suit the needs of the community, increase the centre's historic role as a social meeting place, and enhance the image of the town and the level of comfort and convenience for residents and visitors.

Bridge Street Policy Area

OBJ 1: The reinforcement of the role of Bridge Street as the traditional 'main street' of Murray Bridge.

Desired Character Statement

Bridge Street is the primary focus and historical retail centre of Murray Bridge. It contains a diversity of built-form, from substantial historical buildings to buildings reflecting modern styles and construction techniques.

Its geographical prominence and the activities conducted in the street give it an important role in establishing an image for not only the town centre but also the whole of Murray Bridge. Development and activity in Bridge Street must therefore not only cater for the needs of the consumer, but it also needs to exhibit a high quality and image that reflects a prosperous and pleasant township.

The policy area will be developed as the principal retail area for specialised goods in the Council area. The role of Bridge Street as the traditional 'main street' of Murray Bridge will be reinforced by:

- (a) ensuring the economic viability and vitality of specialised retailing, entertainment, cultural and tourist services
- (b) encouraging new development to create a cohesive lineal streetscape appearance and where the scale of new development is compatible with the existing built-form
- (c) encouraging development to make greater use of the street for pedestrian and social activities, creating an atmosphere where people want to spend time
- (d) establishing a high visual amenity and strong local character to create a sense of place and focal point within the town

Development in the policy area will respect the character and integrity of the locality in respect of its design, siting, scale, building and roof shape, bulk, height, building materials and colours, advertising signs and external illumination.

(my underlining)

Having set the context of new planning policy for this site, it is clear that height is less of a factor under current Development Plan policy. The current Development Plan states in the Regional Town Centre Zone Retail Core Policy Area, Bridge Street Precinct that:

"Buildings located along Bridge Street should have a maximum height of 12 metres although greater building heights are considered appropriate if a proposed development displays both significant design merit and adequately reflects the desired character."

Furthermore, the separation of the hotel element from the remainder of Bridge Street properties, creates a clear and unambiguous design statement. It allows for a gradual visual transition from the existing two storey premises (which can be three storey under the current Development Plan policies) to the hotel, which stands proud at the head of the street, creating a strong visual landmark drawing the eye upwards. The functional separation is also considered an appropriate reinforcement of the differentiation between retail and hospitality.

The Council considers that the design of the current proposal is a significant improvement on the previous version it considered and that the tenets of this policy are now met. No objections are raised from a design perspective.

Traffic / Car Parking and Access

	Regional Town Centre Zone	Bridge Street Policy Area	General Section
Visual Amenity	OBJ 3, OBJ 5 Desired Future Character Statement para 5, para 8, 9 PDC 4, PDC 11	OBJ 2 Desired Future Character Statement para 2, 3, 4, 5, 8 PDC 3, PDC 5, PDC 8, PDC 9, PDC 10, PDC 11, PDC 12, PDC 13, PDC 14, PDC 15	Centres and Retail Development PDC 2, PDC 3 (a & b) Crime Prevention PDC 2, PDC 3 Design and Appearance OBJ 1, PDC 1, PDC 2, PDC 3, PDC 4, PDC 5, PDC 7, PDC 8, PDC 9, PDC 11, PDC 12, PDC 13, PDC 14, PDC 16, PDC 18 Landscaping, Fences and Walls OBJ 1, PDC 1, PDC 4 Natural Resources OBJ 13 Orderly and Sustainable Development OBJ 1, OBJ 4 Siting and Visibility OBJ 1, PDC 1, PDC 2, PDC 5, PDC 6 Tourism Development OBJ 3, OBJ 4, PDC 1, PDC 2, PDC 3, PDC 5

The Council's Engineers have raised two principal concerns regarding traffic management; namely the impact on Council's ability to provide a pedestrian crossing on South Terrace and the amount of on-site car parking provided.

This design will adversely impact on the Council's ability to provide a pedestrian link across South Terrace between the Bridgeport Hotel and the Marketplace (entrance). Previously the Council's Engineers provided guidance to the proponent on how to redesign the vehicular access for South Terrace, removing the drive-through exit and redirecting the traffic through the main access/egress point.

Car Parking:

One of the issues that was previously considered was the amount and rate of car parking provided on-site for the proposal versus the need to utilise Council's on-street car parks.

It should be noted that the redesign of the proposal has increased the number of on-site car parks by 9 additional spaces, bringing the total to 160 car parks.

Table MuBr/2 [please see Tables 2(A) and 2(B)] of the Development Plan provides off street parking requirements for various developments. For a hotel, the table lists different rates depending on various uses within the building/facility. Similarly the applicant's Traffic Report references figures and sizes from the previous plan.

Table 2(A)			
Use	Car parking requirement	Approx. Floor area	Number of car parks required
Hotel room	1 per 3 guest rooms	97 rooms	32 spaces
Gaming lounge	1 per 2 square metres of gaming room	142m ²	71 spaces
Sports bar/lounge*	1 per 2 square metres or 1 per 6 square metres*	169m ²	28 (or 84.5)
Lounge bar	1 per 6 square metres of lounge or beer garden	236m ²	39
Dining area	1 per 3 seats	450 seats	150
Function room	1 per 3 seats	200 persons	67
Total number of car parks			387 (or 443*)

[NB *consideration needs to be given as to whether the sports bar/lounge should be considered a lounge bar or a public bar in the previous table].

The above table calculates the number of car parks required in accordance with Table MuBr/2. However, the above figures (as provided by the proponent) are only calculated for the interior portions of the building and do take into account exterior dining and seating areas such as the ground level sports designated outdoor smoking area (DOSA), gaming DOSA the lounge alfresco, the restaurant alfresco or the terrace level alfresco. Calculations for these areas are provided below:

Table 2(B)			
Use	Car parking requirement	Approx. Floor area	Number of car parks required
Gaming DOSA	1 per 6 square metres	22m ²	3.6 spaces
Sports DOSA	1 per 6 square metres	127m ²	21.1
Lounge/Dining alfresco	1 per 6 square metres	291m ²	48.5
Terrace alfresco	1 per 6 square metres	480m ²	80.1
Total number of car parks			153.3 Spaces

If these areas are calculated in the above manner then an extra 153 car parking spaces would be required (given that the number of seats in these areas was not provided). This would create a demand of over 500 car parks.

The proposal includes the provision for 160 car parks to be provided (89 at grade and 71 undercroft). The proponent's traffic engineer has put forward an argument for an alternative way of calculating car parking requirements (see the table below). In this method they have considered different peak periods for the hotel and added various weighting criteria to the car parking requirements. Using this method the consultant has calculated the maximum number of car parks required at 223 with a "regular evening period" of 143 car parks.

Use	Quantity	Rate	Period 1		Period 2		Period 3	
			Per cent occupancy	Peak evening dining period	Per cent occupancy	Peak bar occupancy period	Per cent occupancy	Peak lunch period
Hotel Room	100	1 per 3 rooms	80%	27	80%	27	50%	17
Gaming Lounge	40 machines	1 per 2 gaming machines	15%	3	80%	16	85%	17
Sports Lounge	177 m ²	1 per 6 m ²	65%	19	85%	25	15%	4
Lounge Bar	326 m ²	1 per 6 m ²	65%	35	85%	46	15%	8
Dining area	300 seats	1 per 3 seats	90%	90	65%	65	50%	50
Outdoor dining	150 seats	1 per 5 seats	90%	27	65%	20	50%	15
Function*	200 persons	1 per 3 seats	50%	33	10%	7	25%	17
Total				234		206		128

This shortfall of on-site car parking is considered to be problematic during peak periods such as the Australian International Pedal Prix, when V8 racing begins at "The Bend" racetrack, major horseracing or speedway events at the new racecourse or when any of these meet a major function (e.g. conference, wedding) and when occupancy of the hotel will also be subject to highest demand.

This will inevitably see the Council's on street and adjacent Wharf Hill car parks become utilised by hotel patrons. Council will have to introduce parking limits for South and East Terraces and the Wharf Hill car park as a result of indiscriminate parking, which would adversely impact the hotel. As such it is considered appropriate that the proponent enter into an agreement with the adjacent Marketplace shopping centre to utilise extra capacity. Alternatively the proponent can make a contribution to the Council's Town Centre Car Parking Fund.

Such an agreement with the Marketplace would deliver security and cooperation for both parties, together with convenience for patrons. The increase in pedestrian traffic in both South and East Terrace will necessitate the need for additional crossing points. The SCAP should ensure that the developer enters into an Infrastructure Agreement to ensure such works are completed at the applicant's expense prior to operation of any of the elements of the hotel building.

Subject to the resolution by the SCAP of these car parking and pedestrian safety concerns, the Council would have no objection on these grounds.

Summary:

As with the previous report to the Panel, it is considered that the concept of the redevelopment of the site for hotel purposes is supported by the Development Plan. Furthermore the concept of the subject land being a 'landmark site' is generally supported and it is accepted that development of a larger scale may be appropriate in this area if it displays a high degree of architectural merit and design.

However, as the site is a landmark, its re-development (if approved) will change the urban fabric of Murray Bridge and has the potential to set the scene as a catalyst for further development and change. It is therefore critically important that the development is a prime example of good planning and design.

Ultimately consideration of the proposal depends on the SCAP's view regarding heritage and visual amenity (height) matters. They must determine whether the existing Bridgeport Hotel continues to display sufficient merit to satisfy Local Heritage Listing or if they consider that the applicant has provided sufficient evidence to warrant the demolition of the building then the building should be demolished as there is no reasonable way that the remaining heritage portions could be reused. In that case, the height and design of the building, does not play as greater role in the assessment of the proposal.

For this reason it is suggested that Council write to the SCAP (draft letter can be found as **ATTACHMENT 6**) and respectfully request that they give careful consideration to the matters raised in this report and request that additional information is provided justifying the removal of Local Heritage Item and to resolve the traffic / car parking and access issues.

Should the SCAP prefer to approve the proposal without further referral, the Council has also included a list of recommended conditions (**ATTACHMENT 7**) that it would like to see attached to any approval.

RECOMMENDATION

1. *That the report and attachments (comprising comments from relevant Council Departments) be considered by the CAP*
2. *That the CAP endorse this report as well as attached letter to the State Commission Assessment Panel*

DISCUSSION

The Panel discussed the request for comments by SCAP in relation to the redevelopment of the Bridgeport Hotel and reached the following decision.

It is noted a representative from the Eureka Group, Mr Michael Lunniss, was present at the meeting during the discussion.

DECISION

Cr Jerry Wilson moved

1. *That the report and attachments (comprising comments from relevant Council Departments) be considered by the CAP*
2. *That the CAP endorse this report as well as attached letter to the State Commission Assessment Panel*

Seconded by Marc Voortman and CARRIED

Attachments

1.	Attachment 1 - DAP Report and Assessment Report - 415-E014-2016	Attachment
2.	Attachment 2 - Amended Plans - 415-E014-2016	Attachment
3.	Attachment 3 - Applicants Report - 415-E014-2016	Attachment
4.	Attachment 4 - Flightpath Architects - Heritage Advice - 415-E014-2016	Attachment
5.	Attachment 5 - Table 1 - 415-E014-2016	Attachment
6.	Attachment 6 - Letter from CAP to SCAP	Attachment
7.	Attachment 7 - Conditions - 415-E014-2016	Attachment



10. GENERAL BUSINESS *continued ...*

10.2 COUNCIL ASSESSMENT PANEL MEETING PROCEDURES

An amended version of the Council Assessment Panel Meeting Procedures will be presented to CAP for discussion at a future meeting.

10.3 APPOINTMENT OF DEPUTY MEMBER TO THE COUNCIL ASSESSMENT PANEL

Council appointed Cr Karen Eckermann to serve as the Deputy Member on the Council Assessment Panel, should current sitting Council Member Cr Jerry Wilson be unable to attend a meeting.

11. NEXT MEETING

The next meeting of the Council Assessment Panel is to be held on **FRIDAY 15 DECEMBER, 2017**, in the **MOBILONG SUITE, LOCAL GOVERNMENT CENTRE, 2 SEVENTH STREET, MURRAY BRIDGE**

Meeting closed at 10:32 am.

Minutes taken as read and confirmed this day of 2017.

.....
PRESIDING MEMBER



The Rural City of
**MURRAY
BRIDGE**

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ABN 90 501 266 817

In response please quote AM/AM

28 February 2018

Mr Troy Fountain
DPTI
BY EMAIL TO: troy.fountain@sa.gov.au

Dear Mr Fountain

RE: BRIDGEPORT HOTEL REDEVELOPMENT - DA415/EO14/16

I write further to your correspondence dated 27th February 2018, regarding the above matter. The Council is pleased that this matter is moving forwards towards a determination. Whilst we have little in terms of volume that we wish to add to our previous comments, the Council would like to make the following points for clarification:

LOCAL HERITAGE

The Eyles Site inspection Report dated 6th March 2017 was not shared with the Council prior to this latest set of correspondence and may have affected the Council's response with regards to Local Heritage. The Council has no further information to add and relies on its previous submission.

CAR PARKING

The Council believes that parking stress will be created as the development does not provide for the relevant parking on site. The Council put forward a suggested solution of using an adjacent car park very early in the development process, but accepts that this may not be the proponent's preferred solution. The Council is prepared accept a significant contribution to its approved Car Parking Fund in light of the significant shortfall. The availability of on-street parking is not something that will necessarily be available going forward.

Currently, the scheme provides 71 underground car parking spaces, which does not satisfactorily cater for all the rooms, if occupied. At times full occupancy will be at the same time as major events nearby, e.g. the Australian International Pedal Prix and the availability of surface car parking within the development cannot easily be controlled. There will be a shortfall in provision against demand and the Council will undoubtedly see parking displacement and will have to reduce parking time periods in response, preventing overnight car parking in on- and off-street public car parking spaces.

The Council is preparing to implement a PLEC scheme with streetscape improvements which may also reduce public on- and off-street car parking capacity and which may also restrict the allowed parking time periods. The proponent will either need to provide appropriate levels of car parking on site or contribute to the Council's Car Parking Fund.

PEDESTRIAN CROSSING

If the proponent was intending to use the spare private car parking capacity nearby under the Marketplace, then the additional foot traffic between the development and the car parking would suggest a need for an additional formalised crossing point, which would not be necessary given the current levels of pedestrian flows. Rather than such works being a requirement, it was intended that the developer would voluntarily enter into an undertaking with the Council to jointly deliver such works, as is best practice in many planning regimes.

COUNCIL'S ASSESSMENT

For the avoidance of doubt, the Council does not accept the comments made by Mr Botten in his letter dated 13th February 2018, that its assessment was flawed.

PROPONENT'S CLARIFICATION

The Council notes the clarification provided in Mr Botten's aforementioned letter and thanks the proponent for these.

POTENTIAL CONDITIONS

The proponent's comments are noted.

If you need any further clarification on this matter, please contact me.

Yours sincerely

A handwritten signature in black ink, appearing to read 'A Meddle', with a stylized flourish at the end.

ANDREW MEDDLE

GENERAL MANAGER – SUSTAINABLE COMMUNITIES

cc Planning File
 Chief Executive Officer

Our ref: JRB/215074

13 February 2018

Mr Troy Fountain
Senior Planning Officer
Department of Planning, Transport and Infrastructure
GPO Box 1815
ADELAIDE SA 5001

By delivery and email: troy.fountain@sa.gov.au

Dear Troy

DA 415/EO14/16 - Kerin Bay Pty Ltd – Bridgeport Hotel redevelopment, 2-6 Bridge Street, Murray Bridge

I refer to my letter to you dated 7 February 2018 attaching a set of amended proposal plans.

This letter provides the balance of information relating to the abovementioned development application (**the DA**) to enable the State Commission Assessment Panel (**SCAP**) to determine the DA.

1. Balance of further material sought by ODASA

1.1 Bicycle parking

ODASA, in their referral response letter dated 15 November 2017, expressed concern regarding the location of the undercroft bicycle storage, given its distance from the car park entry and potential conflict with vehicles.

ODASA also recommended a further review of the location and capacity of the bicycle storage area, together with the provision of bicycle parking at ground level.

Following consideration of these comments, our client has made some minor amendments to the proposed bicycle parking arrangements. As is evident from the amended proposal plans¹, the proposed basement 'Bike Store' has been altered to ensure compliance with AS 2890.3-2015. Further, two additional six space (i.e. 12 in total) bicycle storage areas have been included in the proposal, adjacent the south-west (car park) hotel entrance to ensure the provision of safe and convenient bicycle access and storage at ground level.

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¹ Refer specifically to sheets 1.03/DA3, 1.04/DA3 & 1.05/DA3
BL Lawyers Pty Ltd trading as Botten Levinson Lawyers ABN 36 611 397 285 ACN 611 397 285
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1.2 Waste storage capacity and collection strategy

ODASA, in their abovementioned referral response letter to DPTI dated 15 November 2017, recommended that capacity calculations and a collection strategy be provided to confirm waste storage requirements

The first point to make in relation to this matter is that my client has already provided you with a Waste Management Plan. It did so in the form of a detailed Waste Management Plan prepared in July 2017 by CED Building Design (**the July 2017 Waste Management Plan**). A copy of the July 2017 Waste Management Plan was provided to the DPTI under cover of a letter from our firm dated 22 September 2017.

The July 2017 Waste Management Plan is comprehensive, and details how waste generated by the proposed development will be managed.

My client is satisfied on the basis of the July 2017 Waste Management Plan that the volume of waste likely to be generated by the proposed development will be appropriately managed in a fashion that is, safe, convenient and does not adversely impact on users of the proposal

I question whether ODASA, when it prepared its referral response letter of 15 November 2017 perhaps had not read the July 2017 Waste Management Plan?

Nonetheless, given the comments made by ODASA in their abovementioned referral response letter my client has recently engaged Veolia Environmental Services to, amongst other things, in effect, update the July 2017 Management Plan. I hope to receive a copy of that updated Waste Management Plan shortly. When I receive it, I will obviously forward it on to you.

1.3 Additional details on picture mesh podium fascia

ODASA further requested, in its updated referral response, the provision of additional details on the picture mesh podium fascia. In answer to that question, our client has updated its document titled 'Proposed Perforated Screen Images', to include detail regarding the 'first floor terrace balcony edge feature (i.e. podium fascia) ventilated screens'.

Importantly, with respect to the picture mesh podium fascia, the images displayed will be in the form of a stylised organic image without an obvious repeating pattern. An example of such stylised organic image is provided on page 2 of the **attached** updated 'Proposed Perforated Screen Images' report dated 30 January 2018.

1.4 Additional information on signage intent and materiality

As outlined in my letter dated 7 February 2018 our client has amended the proposal plans to provide for additional signage, and to clarify the detail of all signage.²

² Refer specifically to sheets numbered 1.04/DA3, 1.05/DA3, 2.01/DA2, 2.02/DA2, 2.03/DA2, 2.04/DA2 and the 3D Perspective Views from Bridge Street (x3).
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Notably, a new Bridge Street entry feature has been included into the proposal to strengthen the sense the address and hotel identity on its main street frontage.

Further, a new “B” sign has been added to the Bridge Street and East Terrace balcony fascia to reinforce the corner hotel idiom.

Additional detail of the proposed signage has been noted on the amended proposal plans as follows:

- the south east elevations “B” sign;
- the north west elevations “B” sign;
- the Bridge Street entrance sign;
- the Bridge Street and East Terrace corner balcony fascia “B”; and
- that the north east elevation “Bridgeport Hotel” sign will be LED backlit, with opal diffusers and powder-coated black aluminium surrounds.

With respect to the bottle-shop signage, additional signage detail has been provided which details that the signage will be backlit only.

1.5 Materials sample board and schedule

Please find **attached** with the hand delivered copy of this letter, a materials sample board and schedule.

1.6 The Bridge Street railway tunnel - ARTC

Late last year you provided me with an email from the Australian Rail Track with Corporation (**ARTC**) to you in the form of an email from ARTC to you, containing further commentary concerning the proposed development. The abovementioned email was further to the information supplied by my client via its consulting engineers, TMK concerning an assessment of the impact, if any, of the construction of the proposed new hotel building on the nearby Bridge Street railway tunnel.

In the abovementioned email from ARTC to you ARTC advised that they were seeking an assurance that the proposed development works will have no adverse effect on the Bridge Street railway tunnel.

Subsequent to the receipt of a copy of the email from ARTC, Mr Andrew Martin of TMK liaised with ARTC concerning the matter. Arising out of those discussions, and further consideration of the matter by TMK, TMK have provided a further report on the topic to my client.

That report takes the form of a letter dated 9 January 2018 from TMK to Kerin Bay. A copy of that report is **attached**.

You will note from a perusal of the attached report that TMK continue to hold the view that:

- the construction of the proposed new hotel building can be designed so that does **not** have an adverse impact on the railway tunnel; and
- those construction activities can be managed so that they do not have an adverse impact on the railway tunnel.

As I understand matters, ARTC agreed with TMK that to take it to the next level (i.e. seeking an assurance of the kind referred to in ARTC latest email to you) would require detailed design. It was recognised that at this stage of the matter (i.e. the development plan consent application stage) detailed design was not justified. It was recognised however that such detailed design would be a necessary part of the application for Building Rules consent. ARTC indicated that they would be satisfied with the matter progressing further if my client provided an assurance that at an appropriate time during the detailed design phase (when the engineering design is sufficiently complete), and prior to obtaining building rules consent, they (i.e. Kerin Bay) would make contact with ARTC concerning the detailed design phase. In particular ARTC sought an undertaking that Kerin Bay would submit to ARTC, prior to obtaining Building Rules consent, the detailed design plans of the relevant works, and an engineering report relating to those works in terms of their relationship with the tunnel for consideration by, and feedback from, ARTC.

I can advise that my client has, through TMK, provided ARTC with such an undertaking.

Clearly, in my submission, the opinion reached by TMK concerning the impact, if any, of the proposed works on the railway tunnel's more than sufficient for development plan consent assessment purposes i.e., that the building can be designed to not have an adverse impact on the railway tunnel, and that the construction activities can be appropriately managed. In the circumstances, it would, in my view, be inappropriate for the planning authority, as part of the development plan assessment process, to require my client to prepare detailed designs. That is a matter appropriate for the Building Rules consent assessment phase of the matter.

2. Referral response from the Rural City of Murray Bridge's Council Assessment Panel

2.1 General

The referral response of the Rural City of Murray Bridge's Council Assessment Panel (**the Council**) is set out in a letter dated 17 November 2017 to SCAP.

Relevantly the Council advised in that letter (amongst other things) that:

- the CAP is **highly supportive** of the concept of further development of the existing Bridgeport Hotel site;
- further development is likely to have significant economic and social benefits for Murray Bridge, including the provision of additional high-end accommodation for the town and region;
- the further development of the site is **consistent** with the established land use and the use of the land for a hotel is **supported by the Development Plan**;
- the Council is supportive of the changes made concerning the land-use, visual amenity, stormwater, advertising, crime prevention, landscaping and aspect and access aspects of the DA.

The Council however, in their referral response, recommended that further information be provided concerning heritage and car parking issues outlined in the relevant agenda report to the relevant Council meeting (being a meeting held on 17 November 2017).

Although my client is not obliged to respond to the Council's referral response it has nonetheless chosen to do so in this letter.

I respond on behalf of my client below to certain specific matters raised by the Council. In that regard it is noted that the Council on pages 2 and 3 of its referral response letter summarised, in the paragraphs numbered 2 – 4, the matters it believed required further consideration (viz, the adequacy of car parking provided, a pedestrian linkage to the Marketplace, a pedestrian linkage to the Marketplace, and the extent of remaining heritage fabric), and then, in an attachment to that letter, listed items that the Council considered needed clarification, and suggested conditions of consent should SCAP consider approval.

In this letter I will, in sub-paragraphs numbered 2.2 – 2.4 below, deal with the abovementioned matters referred to on pages 2 – 3 of the Council's letter, and then in sub-paragraphs numbered 2.5 – 2.6, of this letter deals with certain matters raised in the abovementioned attachment to the Council's letter.

2.2 Car parking and access

In the referral response letter of the Council dated 17 November 2017 the Council expressed concern that the intensification of the proposed hotel will increase the use of on-street parking and the Wharf Hill car park. To address what the Council consider to be "off-site impacts" the Council has recommended that my client "... *explore an agreement with the adjacent marketplace to utilise a portion of their car park to supplement on-site car parking*".

The abovementioned views of the Council regarding the adequacy of car parking and off-site impacts are essentially the same views that the Council expressed in their first referral response regarding the DA.

My client **strongly** disagrees with the Council's contention that there will be unacceptable off-site car park impacts caused by what the Council considers to be inadequate on-site car parking associated with the proposed development.

My client's views in this regard are based on the expert advice it has sought, and obtained, from Ms Melissa Mellen of MFY, traffic consultants.

Ms Mellen's views have been thoroughly set out by Ms Mellen in a number of reports prepared for this DA and submitted to SCAP via DPTI. In particular, you are referred to:

- the traffic and parking report prepared by Ms Mellen dated August 2016 (**the first MFY report**);
- the report from MFY in the form of a letter dated 1 November 2016 to our firm (**the second MFY report**). This report (i.e., the second MFY report) responds to an alternative car park design and traffic flow arrangement for the proposed development prepared by officers from the Council. That particular alternative car park design and traffic flow arrangement contemplated the provision of a pedestrian crossing on South Terrace, linking the proposed development to the Marketplace. Ms Mellen, in the second MFY report, also commented on the proposed pedestrian crossing; and
- a report dated 15 August 2017 from Ms Mellen to our firm (**the third MFY report**);

In the circumstances, my client does not intend responding specifically to the above contentions made by the Council. My client refers to the views expressed by Ms Mellen, and submits that her views are to be preferred. That said, for completeness, I particularly draw to your attention, from the abovementioned reports of Ms Mellen, to the following conclusions reached by Ms Mellen:

- in the first MFY report Ms Mellen concludes that the parking and access provision *"...will comply with **all** relevant Australian standards and has been designed to accommodate **all** vehicles entering and exiting the site in a forward direction"*.
- in the first MFY report this Mellen concludes that the proposed parking provision will *"....adequately cater for the peak lunch trading period, when parking on the adjacent street network as used by customers of the adjacent land uses"*. In that report she added that she anticipated that the parking provision would also cater for *"... the regular evening parking demand"*. She added that in the event that a large hotel function was to coincide with the dining and accommodation facilities (and both being occupied at or near capacity) there will be a demand for parking on the adjacent street network. Ms Mellen concluded that in that scenario there would be **adequate** parking available on street to satisfy the demand. Given the infrequent nature of such events all occurring, it is **appropriate** that additional parking demand be accommodated on the street.

Ms Mellen expressed the view that the adjacent road network can “...**readily** accommodate such volumes with minimal impact”.

- in the first MFY report Ms Mellen also concluded that: “*importantly the proposal will **ameliorate** existing safety issues on Bridge Street and will not adversely impact on the operation of the adjacent road network*”. (my emphasis)

2.3 Pedestrian linkage

In the referral response from the Council it is stated that the Council has investigated (and budgeted) for a pedestrian linkage between the Marketplace and the Bridgeport Hotel across South Terrace. In that referral response the Council recommends that my client liaise with the Council's engineering department to assist in the design of the pedestrian linkage. The Council go on to provide that for that reason “...*the SCAP should consider an infrastructure agreement to contribute to the provision of pedestrian linkage to support the abovementioned carpark sharing arrangement*”.

Alternatively, in the absence of any agreement with the Marketplace the Council suggests that an appropriate contribution should be made to the council's car parking fund.

The Council in the abovementioned referral response do not explain how SCAP could achieve or secure such an infrastructure agreement (presumably being an agreement between my client and the Council), let alone whether if SCAP was inclined to promote securing such an agreement (or a contribution into the Council's car parking fund presumably by my client), it could **lawfully** do so as part of the DA.

I have a number of comments to make concerning the Council's comments regarding the pedestrian linkage.

Firstly, quite apart from how the Council expect SCAP to secure an infrastructure agreement or a contribution by my client into the Council's car parking fund, the pedestrian linkage is not, in my submission, a matter relevant to the assessment of the DA. My client is **not** proposing a pedestrian linkage as part of the proposed development. Nor does my client consider the proposed development generates the need for a pedestrian linkage. That said, based on the advice provided by Ms Mellen in the second MFY report, my client is satisfied that the installation of pedestrian linkage would not compromise the operation of the proposed development, and would complement the pedestrian linkage within the site.

The proposed pedestrian linkage, it would seem from what the Council has stated in their referral response of 17 November 2017, is a Council initiative, and is thus, in my client's view, a proposal for Council to pursue if it wishes, design and fund. That said, my client is more than happy to liaise, as requested, with Council's engineering department to assist in the design of the pedestrian linkage.

Secondly, and respectfully, any attempt by SCAP to condition any approval granted for the proposed development in such a way that it obliges my client to enter into an infrastructure agreement with a third party such as the Council (or to contribute monies into the council's car

parking fund) would for that reason alone be **invalid** i.e., in my submission it is not lawful for a planning authority to oblige or to purport to oblige an applicant to enter into an agreement with anyone, let alone a third party.

In my submission such a condition would also be invalid on the grounds that it is unrelated to the proposed development, and in any event, unreasonable in the circumstances.

2.4 Remaining heritage fabric and demolition proposal

2.4.1 Background

In the Council's referral response letter to SCAP the Council contended that further information was required to demonstrate that either:

- that there is no heritage value remaining in the fabric of the ground floor of the existing Bridgeport hotel; or
- there is a structural issue with the ground floor that the white brick is required to support the structure (i.e., the heritage fabric is structurally unsound).

In the circumstances the Council contends that further information is required concerning the ground floor heritage fabric. That particular position is founded on the report presented to the Council meeting held on 17 November 2017 by the assessing officer, Mr Glenn Searle, which report was, I understand, endorsed by the Council.

In the abovementioned assessment report Mr Searle contended that my client should engage surveyors to undertake a survey of the ground floor walls of the hotel or alternatively commission contractors to remove some of the external brick cladding to determine the presence, if any, of original stonework.

2.4.2 My client's position

My client is **not** prepared to undertake further investigatory work in relation to the remaining heritage fabric of the existing hotel. It considers that **more than enough** investigatory work and expert analysis has been undertaken concerning the extent and condition of any remaining ground floor heritage fabric. In addition, of course, there has also been considerable investigatory work and expert analysis undertaken regarding the heritage value of that portion of the existing hotel that is designated as a local heritage place.

The investigatory work and expert analysis undertaken to date in relation to the extent and condition of the remaining ground floor heritage fabric, in my submission, demonstrates that there is **little left**, and that what is left is **not particularly sound**.

As a corollary, the compromised integrity of that portion of the existing Bridgeport Hotel that is designated as a local heritage place (coupled with its location) means that an adaptive reuse approach to the proposed development is **not** warranted.

I will below summarise some of the material provided to date on behalf of my client that demonstrates the above position, and indeed, add to that material with the provision of further information prepared by Mr Craig Eyles of CED Building Design.

My client considers that the Council has had ample opportunity to consider its position and to have produced, had it elected to do so, further information concerning the heritage value of the hotel.

The development application was, after all, lodged as far back as **August 2016**, (with my client participating in an informal pre-lodgement process that began in **October 2015**). It is my understanding that DPTI formally referred the development application to the Council for comment on **13 September 2016**, remembering that my client had informally consulted with the Council well **before** that date.

In short, my client considers that more than enough information has been presented to it DPTI for the SCAP to make an informed decision on extent and condition of the remaining, ground floor heritage fabric.

To be complete, my client submits that:

- the extent of the remaining ground floor heritage fabric is **not extensive**, and is in a **compromised** condition. For example Mr Holland in the 2017 DASH report expressed the view that it appears “...*the entire length of the ground floor wall of the ‘original’ (c1879 – 1937) sections of the stone wall has been removed and replaced with: (local bearing) brickwork, concrete columns and beams, and windows.*”
- Further, Craig Eyles in his **attached** report (which is referred to in greater detail in sub-paragraph 2.4.3 of this letter) expresses the opinion, based on his inspections, that 65% of the ground floor original walls (facing Bridge Street and East Terrace) no longer exist and that of those walls that do remain the upper third have been replaced with bricks and reinforcing;
- even if the extent of and condition of the remaining ground floor heritage fabric was greater and in better condition than my client’s consultants believe it to be, it is still, nonetheless, relative to the overall size of the hotel, a very small portion of the hotel; and

- in any event the compromised integrity of that portion of the Bridgeport Hotel that is designated as a local heritage place, and the location and nature of the remaining fabric, means that an adaptive reuse approach to the proposed development is **not** warranted.

For additional completeness I will also make some comments below)in sub-paragraph 2.4.4) on the approach of the Council to the assessment of the demolition component of the proposed development.

2.4.3 Information supplied to date by my client

i) Existing information

My client has already supplied extensive information concerning the extent and condition of any remaining ground for heritage fabric of the Hotel.

That information includes the following:

- (a) early in 2016 (as part of the development application process), my client engaged David Holland of DASH Architects to assess the heritage value of the local heritage place listed portion of the Bridgeport Hotel. For the purposes of considering his views, Mr Holland inspected the hotel during that process. In due course Mr Holland produced a written report titled “Bridgeport Hotel, Murray Bridge Review of local Heritage listing (Issue A) dated February 2016 **(the 2016 DASH report)**;
- (b) in early March 2017 Mr Holland at the request of my client, reviewed his position as documented in the 2016 DASH report;
- (c) my client arranged for representatives of DPTI and ODASA (including the South Australia Government Architect, Ms Kirsteen McKay) to inspect the existing hotel and to, in particular, inspect the extent of the remaining ground floor heritage fabric of the hotel. Representatives of my client, including David Holland, were present during that meeting to facilitate the inspection. That inspection took place on 15 March 2017; and

- (d) my client commissioned Mr Holland to provide a further report relating to the proposed demolition component of the development application (i.e., DA 2014/20674/01), including therefore a further review of the local ground floor heritage place value (if any) of the existing hotel, and the extent and condition of the ground floor heritage fabric remaining of the existing hotel. As part of that process Mr Holland was also requested to review and comment on the report prepared for the Council by Flightpath Architects dated October 2016, titled “Heritage Advice Bridgeport Hotel”;
- (e) Mr Holland duly prepared a further report, being a report titled “Bridgeport Hotel redevelopment - Review of Proposed Demolition of Local Heritage Place (DA 2014/20674/01)” Issue B dated 15 August 2017 (**the 2017 DASH Report**).
- (f) In the course of preparing the 2017 DASH report Mr Holland took into account, amongst other things, the results of his earlier inspections of the hotel and, in particular, his inspection on 15 March 2017 during which he specifically inspected areas within the ceiling space, between the ground and first floors, adjacent to the external walls of the existing hotel.

In the 2017 DASH report Mr Holland detailed, amongst other things, the results of his inspections of the remaining heritage fabric of the hotel and, as you know, concluded, based on the “...*compromised integrity of the place, and the location and nature of the fabric that is left, it is my view that (an adaptive reuse approach) is **not** warranted in this instance*”. (my emphasis)

- (g) As noted above, on 15 March 2017 Mr David Holland, together with representatives from DPTI and ODASA inspected the existing hotel and, in the course of that inspection, inspected the extent of some of the remaining ground floor heritage fabric of the Hotel.

ii) Further information

It is also the case that on 6 March 2017 Mr Craig Eyles of CED Building Design inspected the existing hotel for the purposes of making observations about the extent and condition of any remaining ground floor fabric. He has, at my client’s request, recorded the observations he made at that site inspection on 6 March 2017 into the form of the abovementioned attached report (which also comprises the two **attached** plans/drawings prepared by CED Building Design referred to in the report (viz, CED Building Design Drawings numbered 3.01 & 3.02)) (**the Eyles report**).

The Eyles report not only details those particular observations, but also outlines in another section (viz, section 2.0), conclusions and observations Mr Eyles has made regarding the first-floor street front balcony and verandah structure.

It also, in a further section (viz, the section titled “Historical Imagery and Onsite Investigation” includes photographs of the Hotel, plus its alterations overtime, and photographs relating to the extent of the remaining historic fabric).

Without exhaustively drawing from the attached Eyles report particular attention is drawn to the following observations and views reached by Mr Eyles:

- 65% of the original ground for street front walls no longer exist;
- of the original walls remain the upper third have been replaced with bricks and reinforcing;
- the remaining concealed walls were observed to be in poor condition and penetrated by steel and concrete beams that support the more recent balcony framing an walls above;
- substantial concrete underpinning, visible in the cellar to East Terrace and Bridge Street frontages, further evidences that any original street front walls remaining behind the latter brick facade are not likely to be intact.
- substantial concrete and steel beams have been installed re support the first floor masonry walls and cantilevered balconies; and
- substantial portions of the ground floor walls have been removed, as evidenced by the large room dimensions that are inconsistent with the nature of a building of the age of the hotel, and remaining engaged and isolated piers.

2.4.4 The Council's approach to the assessment of the demolition component of the proposed development is flawed

In Mr Searle's abovementioned report to the Council meeting (which, as noted earlier, was endorsed by the Council) he contended that if the extent of the ground floor heritage fabric is more than suggested by my client's consultants, and structurally sound, the intent of the Development Plan is clear and that is that the heritage item should be preserved with any redevelopment taken in to account in preserving those portions of the item.

Also relevantly, Mr Searle earlier on in his report asserted that the “... *the Development Plan essentially states that the demolition of heritage items should be the as a **last resort** (sic) and **only** in circumstances when a building are structurally unsound.*” (my emphasis)

Respectfully, Mr Searle’s approach generally to the assessment of the merits of my client’s proposal, and, in particular the demolition component, of that proposal, is flawed.

The extent and condition of the remaining heritage fabric of the existing Bridgeport Hotel is a relevant consideration. For this reason my client sought, and obtained, advice from David Holland and Craig Eyles on that topic.

You are well aware of their conclusions some of which have been outlined above. **However**, in my submission the **threshold question** with respect to the matter of the proposed demolition is what is the relative heritage value of that portion of the existing Bridgeport Hotel that is designated as a local heritage item?

To approach the application of the provisions of the Development Plan relevant to the assessment of the proposed demolition on the basis that Mr Searle and the Council did (i.e., that demolition should only be considered as a “last resort” and only “if the item is structurally unsound”) is an approach that is **far too rigid**.

That approach fails to recognise that, as is often the case with the assessment of development proposals, there can be tension between different provisions and objectives of the relevant Development Plan.

In this matter, there is clear tension between the objectives and principles applicable to the Regional Town Centre. On the one hand, provisions designate the area as the primary focus for business and commercial services for the region, that a hotel as an “envisaged” form of development, and that economic viability, vibrancy and vitality is sought. On the other hand there are provisions dealing with heritage places that seek, where possible, conservation and preservation of those places.

Where there is such tension the findings and directions of the Full Court of the Supreme Court in the matter of *Lakshmanan v City of Norwood Payneham and St Peters* are instructive³.

In the *Lakshmanan* case the Full Court had occasion to consider a proposal for the demolition of a local heritage listed dwelling that was located within a floodplain, and thus prone to flooding.

³ (2010) 174 LGERA 428
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There were competing provisions within the Development Plan relating to, on the one hand, the conservation and preservation of local heritage places and, on the other hand, provisions relating to flood protection and safety.

Justice Kourakis (as he then was) delivered the leading judgment. In his judgement he observed that:

*"On occasions, perhaps even commonly, developments will advance the objects of some parts of the development plan but be **inconsistent** with others. In that case, a **planning judgment** must be made as to the merits of the proposed development."* (my emphasis)

Later in his judgement, he further found as follows:

*"Once it is accepted that the flooding risk is a material consideration, it necessarily follows that the **relative significance** of the residence as a Local Heritage Place becomes **relevant**. Even though the flooding risk and the local heritage value are hardly commensurable, a planning judgment is called for as to which consideration should, as a matter of planning judgment, predominate.*

The degree of flooding risk which will constitute good reason to approve demolition will necessarily be higher the greater the heritage value of the place which is the subject of the development application".

Kourakis J went on to determine that:

*"An enquiry into the heritage value of a local heritage place is not conducted by way of a collateral challenge to the designation of the place by the development plan. ... The inquiry is undertaken for the purpose of determining the **weight** to be given to that listing... In the case of a local heritage place, an assessment of its **relative heritage importance** is necessary to determine whether the to depart from the principles which protected it".* (my emphasis)

It follows from the approach outlined by the Full Court that Mr Searle's approach (and thus the Council's approach) to the assessment of the demolition component of the proposed development is **flawed**. Demolition must **not** be considered **only** as a last resort, nor **only** when the building is structurally unsound.

If there is tension between different provisions of the Development Plan, (as is the case in this matter), then an assessment of its of the relative value of the local heritage place is called for, and ultimately, a planning judgment.

David Holland has done just that in his reports, and concluded that:

- the portion of the Bridgeport hotel that is designated as a local heritage place, if measured today against the relevant statutory criteria, would **not** amount to a local heritage place; and
- in any event, the compromised integrity of that portion of the Bridgeport Hotel that is designated as a local heritage place means that an adaptive reuse approach to the proposed development is not warranted.

In short, David Holland has approached his assessment of the demolition component of the proposed development in a manner that is in accord with the directions set out by the Full Court in the *Lakshmanan* case. On the other hand, the Council's assessment has departed from that approach. It follows that Mr Holland's approach is to be preferred.

2.5 Items that the Council considers need clarification

As noted earlier, in the Council's referral response letter, it set out a number of items that it considered needed clarification. I turn now to deal with those matters individually.

2.5.1 Maximum building height from finished ground floor level

The Council has sought clarification regarding the maximum height of the building above natural ground level.

In our submission, the detail provided to date is sufficient for the purposes of determining the height of the proposed building, especially considering the fall across the site, and that site works have obviously occurred over time and thereby altered the natural ground level. However, in order to assist SCAP in considering the height of the proposed development, I provide further detail below.

In early 2013, our client engaged Mattson & Martyn Surveying and Planning Consultants to prepare a survey of the site. I understand that no changes to the site levels have occurred since that date. Please find **attached** for your consideration a copy of that survey plan, dated 3 April 2013 (**the survey plan**).

As the survey plan shows, the ground floor level of the existing hotel is marked as FFL 24.15.

As is evident on the proposed elevation drawings, being sheets no. 2.01/DA2 and 2.02/DA24, the existing ground floor level will be maintained in the proposed development (i.e. the proposed ground floor's finished floor level will be 24.15).

In light of that fact, the proposed maximum building height from existing (and to be maintained) finished ground floor level, will be 23.53m.

2.5.2 East & South Terrace paving

The Council indicated that approval will be required to undertake the new paving proposed along East and South Terrace.

Our client is aware of its obligation to obtain Council's approval under the *Local Government Act, 1999*⁵ and will do so prior to undertaking this work.

2.5.3 Occupation of the road reserve

The Council has sought clarification regarding our client's intention to obtain Council approval for part of the proposed development to overhang the road reserve. Again, our client is aware this obligation and will obtain a section 221 approval prior to occupying (overhanging) the road reserve.

2.5.4 Street tree

The Council sought clarification as to our client's intention with respect to the existing street tree situated to the west of the existing Bridge Street bottle-shop cross-over. Our client has amended the proposed site plan⁶ to indicate the proposed removal of this tree.

Further, our client proposes to plant a replacement tree and will liaise with the Council, in accordance with the provisions of the *Local Government Act*, as to its preferred species in the event that approval is granted.

2.5.5 Gas bullet

The Council also sought clarification as to the removal and replacement of the existing gas bullet. I can confirm that the existing gas bullet will be removed from the site, and that the proposed development will be connected to a gas source. The precise location of a replacement gas bullet has not, at this point in time, been determined. That will occur in the detailed design phase. My client, however, is satisfied that a replacement gas bullet can be appropriately accommodated on the land.

2.5.6 Rooftop plant & equipment

The Council raised concern regarding the siting and visibility of the rooftop plans and equipment, shown on the proposed 'Part Roof Drainage Plan'.⁷

⁵ Section 221

⁶ Being Sheet No. 1.04/DA3 prepared by CED Building Design dated 18 December 2017.

⁷ Being Sheet No. 1.11/DA2 prepared by CED Building Design and dated 10 August 2017.

I understand that the proposed rooftop construction includes parapets which will suitably conceal from vision all rooftop plant and equipment from public view.

2.5.7 Car parking agreement

The Council again suggested that our client explore entering into an agreement with the adjacent shopping centre to utilise its car parks, or, if such an agreement was not feasible, the applicant make the appropriate contribution to the Council's car parking fund.

I have already dealt with this issue raised by the Council and comment on this point only to reiterate the submissions made in paragraph 2.2 above, including the submission that the proposed development, in the circumstances, makes appropriate provision for car parking.

2.5.8 South Terrace access & egress

The Council raised further traffic and access concern by inviting our client to engage with it regarding access to South Terrace. Particularly, the Council sought a redesign of the South Terrace access to reduce the number of access points to one. As noted earlier, This suggestion has previously been made by the Council, where it put to our client an alternative access design. Ms Mellen responded to this alternative access design in the second MFY report.

Importantly, in the second MFY report, Ms Mellen states in relation to the Council's suggestion regarding South Terrace:

- *"While I appreciate that the Council's intent may have been to reduce the number of crossovers, I have a number of concerns with the suggestion..."*

...

- *"In summary, therefore, while the Council's option would remove the proposed egress from the bottleshop to South Terrace, it would create a number of **safety and operational issues** on the subject site.*
- *Of particular relevance is that it would transfer the existing collision potential which currently occurs on Bridge Street to South Terrace and create a **potential crash risk** on this road. These issues are **far more significant** that the addition of a crossover on the road, particularly when the proposed egress with comply with AS/NZ2890.1:2004." (my emphasis)*

In addition to the conclusions drawn by Ms Mellen, the proposed development has been referred to the Department of Planning, Transport and Infrastructure's Safety and Service Division (**the SSD**) now on two occasions.

The SSD's referral responses raise no objection to the proposed development.

2.5.9 Vehicle turn path

The Council has also sought clarification regarding the accessibility of the site to a garbage truck. Particularly, the Council has sought the provision of a vehicle turning path to show that a garbage truck can access and manoeuvre the site.

As outlined above, our client has obtained numerous reports from Melissa Mellen in relation to the traffic and accessibility implications of the proposed development. In short, our client considers that it has provided sufficient information to enable the SCAP to assess those traffic and accessibility implications. Thus, our client will not be providing an additional vehicle turning path.

I do however refer you to the first MFY report, in particular I refer you to page 6 of that report. Ms Mellen states on page 6 of the first MFY report:

*“Deliveries and refuse collection for the site will occur at the rear of the hotel bottleshop. Commercial vehicles, up to 11.0m in length will enter the site via Bridge Street and exit through the hotel bottleshop to South Terrace. Adequate head height clearance will be provided for such vehicles within the drive-through lanes of the bottleshop. **Figure 2 illustrates the swept path of an 11.0m truck accessing the site.***

All such vehicles will enter and exit the site in a forward direction.” (my emphasis)

Clearly there has been sufficient information provided to address any concerns regarding the accessibility of the site for delivery vehicles and garbage trucks.

2.5.10 Damage to existing paving

Further concern has been raised by the Council that the existing paving on Bridge Street and East Terrace may be damaged during the demolition and construction of the proposed development.

Whilst our client understands this concern and considers it easily dealt with, it is not a relevant town planning consideration. That said I am instructed that my client will endeavour to undertake the proposed development, should it be approved, in a manner which will result in as little damage as possible to Council pavers. However, should existing Council paving be damaged in the demolition and construction process, my client undertakes to remediate such paving to its prior condition, to the reasonable satisfaction of the Council.

2.5.11 TMK Stormwater Management Plan

In relation to the Stormwater Management Plan⁸, the Council contends that there are a number of anomalies in that report. My client requested TMK to review the Stormwater Management Plan. They have done so and set out their response to the matters raised by the Council in an email to CED Building Design dated 18 January 2018. A copy of that email is **attached**. It is submitted that TMK's response satisfactorily answers the queries raised by the Council.

In particular, in TMK's view, the total discharge rates will be less than, or equal to, the pre-development rates from the site, hence no increase of flows are proposed for the downstream stormwater network as a result of the development.

2.6 The Council's suggested conditions should SCAP consider approval

As noted earlier, the Council, in its referral response, put forward 17 conditions which it submitted should be imposed, should SCAP grant development plan consent to the proposed development.

I comment below on each of the proposed conditions.

2.6.1 Waste Management Plan

As you know, my client provided SCAP with a Waste Management Plan last year (i.e., the July 2017 Waste Management Plan).

That being so, it would appear that the Council is unaware of that fact, or perhaps not read that Plan.

Further, as stated earlier, my client intends to shortly provide you with an updated Waste Management Plan.

In short, it is submitted that there is no need for the imposition of the condition suggested by the Council.

2.6.2 Car parking layout

You are referred to the reports prepared by Ms Mellen. In my submission, based on those reports, the suggested condition is superfluous.

2.6.3 Access and egress

Our client is willing to abide by a condition requiring all vehicles to enter and exit the site in a forward direction.

⁸ Prepared by TMK Consulting Engineers dated 17 July 2017.
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2.6.4 South Terrace line marked bays

Our client is willing to abide by a condition requiring it to remove all existing line marked bays and the provision of new line marked bays on South Terrace at its own expense.

2.6.5 Bicycle parking facilities

You are referred back to the further information supplied in this letter (and in the amended plans) concerning bicycle parking. That information, in my submission, makes the Council's suggested condition unnecessary.

2.6.6 Forward access and egress

Our client is willing to abide by a condition of consent requiring all loading and unloading, parking and manoeuvring areas to be designed and constructed to ensure that all vehicles can enter and exit the subject land in a forward direction.

2.6.7 Waste and delivery vehicles

Council has suggested a condition requiring waste disposal vehicles and general delivery vehicles to service the development only between the hours of 7:00am and 7:00pm Monday to Saturday inclusive, and further, that loading and unloading of such vehicles to be limited to within the confines of the subject land.

Our client is willing to abide by a suitably worded condition to that effect.

2.6.8 External lighting

Our client is willing to abide by a condition requiring all external lighting of the site, including the car parking areas and buildings, to be designed and constructed to conform with Australian Standards, and which condition requires the external lighting to be located, directed and shielded and of such limited intensity that no nuisance or loss of amenity is caused to any person beyond the site.

2.6.9 Mechanical plant or equipment

Again, our client is willing to abide by a suitably worded condition which requires all mechanical plant and equipment to be designed, sited and screened to minimise noise impact on adjacent premises or properties.

2.6.10 Construction Environment Management Plan

Council has suggested a condition requiring the preparation and implementation of a Construction Environment Management Plan to minimise environmental harm and disturbance during construction.

Specifically, the CEMP sought by such a condition should, in the Council's view, incorporate matters including, but not limited to:

- a) air quality, including odour and dust;
- b) surface water, including erosion and sediment control;
- c) soils, including fill importation, stockpile management and prevention of soil contamination;
- d) groundwater, including prevention of groundwater contamination;
- e) noise; and
- f) occupational health and safety.

A suitably worded condition to that effect is acceptable to our client.

2.6.11 Encroachment permit

The Council's comments in this respect are noted.

2.6.12 Kitchen exhaust

Our client is willing to abide by a condition requiring the exhaust from the kitchen to be dispersed with in accordance with Australian Standards AS/NZ 1668.1 and 1688.2.

2.6.13 Infrastructure

Council has requested a condition requiring all Council, utility or state-agency maintained infrastructure (i.e. roads, kerbs, drains, crossovers, footpaths, etc) that is demolished, altered, removed or damaged during the construction of the development to be reinstated to Council, utility, or stage-agency specifications, with the associated reinstatement costs being met by the proponent.

Our client is happy that such a condition be imposed.

2.6.14 Stormwater

A condition requiring all stormwater design and constructions to be in accordance with Australian Standards and recognised engineering best practice to ensure that stormwater does not adversely affect any adjoining property or public road is acceptable to our client.

Further, a condition requiring the provision of final details of the stormwater design, including onsite retention and reuse, to the reasonable satisfaction of the SCAP prior to the approval of any building work upon the site is also acceptable.

2.6.15 Food Safety Standards

Our client is content with a condition requiring the plans and constructions specifications, detailing the design and fit-out of the kitchen spaces to be provided to the satisfaction of the Commission, prior to the approval of any building work upon the site.

2.6.16 Swimming pool

Our client is willing to abide by a condition requiring specification for the swimming pool to be provided to the satisfaction of the Commission, prior to the approval of any building work on the site.

2.6.17 Damaged pavers

Council has requested a condition requiring any damaged pavers on Bridge Street, East Terrace or South Terrace on account of the development to be replaced to the satisfaction of the Council.

I refer you to our comments concerning Council pavers etc in sub-paragraph 2.5.2 and 2.5.10 of this letter, i.e., this matter is a *Local Government Act* matter. That said, my client understands that if it damages paving it will need to remediate the paving.

Yours faithfully



Jamie Botten
BOTTEN LEVINSON
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Enc: As described above

Our ref: JRB/215074

7 February 2018

Mr Troy Fountain
Senior Planning Officer
Department of Planning, Transport and Infrastructure
GPO Box 1815
ADELAIDE SA 5001

By email: troy.fountain@sa.gov.au

Dear Troy

DA 415/EO14/16 - Kerin Bay Pty Ltd – Bridgeport Hotel redevelopment, 2-6 Bridge Street, Murray Bridge

I refer to our previous communications concerning this development application (**the DA**).

1. ODASA referral report

I also refer to our meeting at your offices on 7 December 2017 concerning the referral report letter from the Office for Design and Architecture (**ODASA**) to you dated 15 November 2017 concerning the DA. That referral report letter was based on the revised drawings received by ODASA on 20 October 2017, and as noted in that letter, the comments in that letter superseded the previous ODASA referral response letter dated 13 April 2017.

The author of the ODASA referral response letter dated 15 November 2017 (namely Mr Nick Tridente - South Australian Associate Government Architect), together with you and a number of representatives from my client, (including myself and Mr Ian Tregoning) attended the abovementioned meeting.

The meeting was a productive one and it is my understanding that at that meeting it was agreed that:

1. my client, Kerin Bay, would consider whether it was prepared to make further amendments to the proposal plans relating to “... *the corner expression to reinforce the corner hotel idiom*”;
2. Kerin Bay would also consider whether it is prepared to make further amendments to the proposal plans to strengthen the sense of address and hotel identity of the main street frontage;
3. Kerin Bay would confirm the waste storage capacity requirements and collection strategy for the proposed development;
4. Kerin Bay would consider providing additional information on signage intent and materiality;

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5. Kerin Bay would consider providing additional details on picture mesh podium fascia and presentation materials to demonstrate the overall visual effect of the proposed development; and
6. Kerin Bay would consider providing a materials sample board and schedule to demonstrate selections.

It was also, I believe, at that meeting acknowledged by ODASA that matters relating to adequacy of car parking and bicycle numbers etc are matters more appropriately dealt with by other participants in the process i.e., that those matters were outside of ODASA's remit.

It was also agreed that upon receipt of Kerin Bay's response to the abovementioned matters (and after ODASA had had an opportunity of considering such further information) ODASA would further review the proposed development, and then provide a fresh referral response report letter to you concerning the DA.

2. Amended plans

I can report that since the abovementioned meeting my client has made a number of amendments to the current proposal plans in response to the ODASA referral response letter of 15 November 2017. It is also in the process of assembling further information relating to the other matters referred to above, such as preparing a materials sample board and schedule.

To progress the processing of the DA I provide to you now a set of the latest amended plans (which plans are to be substituted for the amended plans received by ODASA on 20 October 2017 (and provided to you on 22 September 2017)) so that you may forward them on to ODASA for its consideration.

For the sake of completeness the **attached** amended plans comprise the following plans prepared by CED:

- 3D perspective – Internal 1 bedroom suite, dated 17 July 2017;
- 3D perspective – Internal 2 bedroom suite, dated 17 July 2017;
- 3D perspective – Bridge Street Entry Blow-Up;
- 3D perspective – Bridge Street North (with umbrellas);
- 3D perspective – Bridge Street South (with umbrellas);
- existing site plan, being sheet No. 1.01/DA1 (Project No. C1201) dated 17 July 2017;
- proposed basement plan, being sheet No. 1.03/DA3 (Project No. C1201) dated 18 December 2017;
- proposed ground floor site plan, being sheet No. 1.04/DA3 (Project No. C1201) dated 18 December 2017;
- proposed ground floor plan, being sheet No. 1.05/DA3 (Project No. C1201) dated 18 December 2017;

- proposed first floor plan, being sheet No. 1.06/DA3 (Project No. C1201) dated 4 September 2017;
- proposed second floor plan, being sheet No. 1.07/DA1 (Project No. C1201) dated 17 July 2017;
- proposed third floor plan, being sheet No. 1.08/DA1 (Project No. C1201) dated 17 July 2017;
- proposed fourth floor plan, being sheet No. 1.09/DA1 (Project No. C1201) dated 17 July 2017;
- proposed fifth floor plan, being sheet No. 1.10/DA1 (Project No. C1201) dated 17 July 2017;
- proposed part roof drainage plan, being sheet No. 1.11/DA2 (Project No. C1201) dated 10 August 2017;
- proposed apartment type plan, being sheet No. 1.12/DA2 (Project No. C1201) dated 10 August 2017;
- proposed elevations plan, being sheet No. 2.01/DA2 (Project No. C1201) dated 18 December 2017;
- proposed elevations plan, being sheet No. 2.02/DA2 (Project No. C1201) dated 18 December 2017;
- proposed bottleshop elevations & cross section plan, being sheet No. 2.03/DA2 dated 18 December 2017; and
- streetscape elevations plan, being sheet No. 2.04/DA2 dated 18 December 2017.

The latest amendments, when compared to the amended plans received by ODASA on 20 October 2017, comprise the following:

- Sheet 1.01/DA1 – existing site plan has been amended to include a note relating to the proposed removal of the existing street tree situated on the western side of the existing Bridge Street cross-over.
- Sheet 1.03/DA3 – proposed basement plan has revised the bicycle parking area to ensure compliance with AS 2890.3-2015. Further detail regarding provision of bicycle parking will be provided under the cover of a separate letter which will include the remaining items requested by ODASA.
- Sheet 1.04/DA3 – proposed site plan makes provision for additional on-site bicycle parking adjacent the south west hotel entrance, and the inclusion of an entry statement on the Bridge Street ground floor entrance in order to strengthen the sense of address and hotel identity.
- Sheet 1.05/DA3 – proposed ground floor plan has been amended to provide amendments to reflect the additional on-site bicycle car parking, and the entry statement included on Sheet 1.04/DA3 (outlined above).

- Sheet 2.01/DA2 – proposed elevations plans provide for the Bridge Street entry statement, the addition of the “B” signage on the Bridge Street and East Terrace balcony fascia, and the notation of the new signage details in order to strengthen the sense of address and hotel identity and describe the signage intent and materiality.
- Sheet 2.02/DA3 – proposed elevations plan incorporate a note relating to the proposed signage details.
- Sheet 2.02/DA2 – proposed elevations plan include a note relating to the proposed signage details (thereby describing the signage intent and materiality).
- Sheet 2.03/DA2 – proposed bottleshop elevations & cross section plan include note relating to the proposed signage.
- Sheet 2.04/DA2 – streetscape elevation plan has been amended to reflect the signage, entry statement and additional details outlined above; and
- 3D perspective – Bridge Street Entry Blow-Up, 3D perspective – Bridge Street North (with umbrellas) and 3D perspective – Bridge Street South (with umbrellas) have been amended to reflect the signage details outlined above, and to include a close up of the new Bridge Street entry statement, which has been added to strengthen the sense of address and hotel identity.

In terms of the amendments I advise that my client, subsequent to the abovementioned meeting with Mr Tridente, gave consideration to replacing the black panelling variously spaced on the Bridge Street elevation (i.e., the north west elevation) of the proposed hotel building with the hotel apartment windows. It decided that such an amendment was not warranted for a variety of reasons, including:

- the design of the building including the Bridge Street elevation is of high quality and appropriately articulated (and in that form, supported by David Holland);
- that the views that would be enjoyed from those windows by occupants of the apartments is not particularly noteworthy; and
- the windows, given that they would be facing west, would significantly increase the heat loads into those rooms.

I hope to have the remaining items requested by ODASA (and a response to certain matters raised in the referral response letter from the Rural City of Murray Bridge’s Assessment Panel) to you either later this week or next week.

3. Heritage SA’s referral response

It is noted that Heritage South Australia as delegate of the Minister for Sustainability, Environment and Conservation have, by letter to State Commission Assessment Panel dated 12 December 2017, provided its referral response on the DA.

My client is pleased to note that Heritage SA consider the proposed development to be acceptable in relation to relevant State heritage places in the area for a number of reasons including that the proposal is “...*restrained and contemporary in style and is not considered to visually detract from the State Heritage Place opposite*”.

I trust that this information is of assistance.

Yours faithfully



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Enc: As described above



7 December
Mr Nick Tridente
S.A. Associate Government Architect

RE: Bridgeport Hotel, Murray Bridge

Thankyou Nick for meeting with us today to discuss the points raised in your letter of 15th November 2017 to Mr Troy Fountain.

We wish to confirm our responses as follows:

- Further review of the corner expression to reinforce the corner hotel idiom.
New 3D and plan to be provided – agreed
- Further review of the Bridge Street entrance to strengthen the sense of address and hotel identity on the main street frontage.
New 3D and plan to be provided – agreed
- Further review of car parking numbers to ensure a safe and convenient arrival experience for hotel patrons with minimal impact on the local traffic conditions
Refer report by Ms. Melissa Mellen MFY Traffic Engineers
- Further consideration of bicycle parking, with a view to providing safe and convenient access within the under croft and provision of bicycle parking at ground level.
Plans to be updated - agreed
- Confirmation of waste storage capacity requirements and collection strategy.
Further information to be provided - agreed
- Provision of additional details on picture mesh podium fascia and presentation materials that demonstrate of the overall visual effect.
Samples to be submitted - agreed
- Provision of additional information on signage intent and materiality.
Further information to be provided – agreed
- Provision of a materials sample board and schedule to demonstrate selections.
Samples to be submitted – agreed

In summary we were delighted today you accepted participation in what has been an extensive review process as per the chronology provided. Finally we would ask you to provide a new letter to Mr Troy Fountain as discussed.

Thankyou,
Yours Sincerely

Ian Tregoning
Managing Director

A handwritten signature in black ink, appearing to read "Ian Tregoning", is written over the typed name and title.

Our ref: JRB/215074

22 September 2017

Mr Troy Fountain
Senior Planning Officer
Department of Planning, Transport and Infrastructure
GPO Box 1815
ADELAIDE SA 5001

By email: troy.fountain@sa.gov.au

Dear Troy

DA 415/EO14/16 - Kerin Bay Pty Ltd – Bridgeport Hotel, 2-6 Bridge Street, Murray Bridge

I refer to our previous communications concerning this development application (**the DA**).

You will recall that when we last spoke I advised that :

- my client was finalising various amendments to the proposal plans;
- my client was considering whether any upgrade to the landscaping proposal is required;
- my client was arranging with the various experts, for them, where necessary, to update their respective reports in view of any amendments made to the proposal plans etc;
- my client had sought an expert Contextual Analysis Report regarding the proposal plans (and as they may be amended); and
- my client was reviewing the waste management strategy details lodged with the DA.

In addition, for completeness, my client has sought, and obtained, expert engineering advice on:

- the impact, if any, of the proposed development on the railway tunnel running under Bridge Street, in the general vicinity of the subject site;

- on wind loading for the proposed development; and
- a review, by TMK Engineering, of its earlier exterior lighting report (as submitted to you).

Please find **attached** in pdf form the following documents :

- a) a set of amended proposal plans comprising the following plans prepared by CED, namely:
- existing site plan, being sheet No. 1.01/DA1 (Project No. C1201) dated 17 July 2017;
 - proposed basement plan, being sheet No. 1.03/DA2 (Project No. C1202) dated 10 August 2017;
 - proposed site plan, being sheet No. 1.04/DA2 (Project No. C1202) dated 10 August 2017;
 - proposed ground floor plan, being sheet No. 1.05/DA2 (Project No. C1202) dated 10 August 2017;
 - proposed first floor plan, being sheet No. 1.06/DA3 (Project No. C1202) dated 4 September 2017;
 - proposed second floor plan, being sheet No. 1.07/DA1 (Project No. C1202) dated 17 July 2017;
 - proposed third floor plan, being sheet No. 1.08/DA1 (Project No. C1202) dated 17 July 2017;
 - proposed fourth floor plan, being sheet No. 1.09/DA1 (Project No. C1202) dated 17 July 2017;
 - proposed fifth floor plan, being sheet No. 1.10/DA1 (Project No. C1202) dated 17 July 2017;
 - proposed part roof drainage plan, being sheet No 1.11/DA2 (Project No. C1202) dated 10 August 2017;
 - proposed apartments type plan, being sheet No. 1.12/DA2 (Project No. C1202) dated 10 August 2017;
 - proposed elevations plan, being sheet No. 2.01/DA1 (Project No. C1202) dated 17 July 2017;
 - proposed elevations plan, being sheet No. 2.02/DA1 (Project No. C1202) dated 17 July 2017;
 - proposed bottleshop elevations & cross section plan, being sheet No. 2.03/DA1 (Project No. C1202) dated 17 July 2017;

- streetscape elevations plan, being sheet No. 2.04/DA1 (Project No. C1202) dated 17 July 2017;
 - existing and proposed perspective, being sheet No. 2.05/DA1 (Project No. C1202) dated 17 July 2017; and
 - colour photographs of indicative apartment.
- b) a landscape plan report from Mr James Hayter of Oxygen in the form of a letter from Mr Hayter to Botten Levinson Lawyers dated 29 June 2017 including a landscaping plan in the form of four sheets, being:
- a Landscape Planting Plan, being Drawing No. 17.027.101 Revision A;
 - a Landscape Planting Plan Detail, being Drawing No. 17.027.103 Revision A;
 - a Landscape Planting Plan Upper Level Terrace, being Drawing No. 17.027.104 Revision A; and
 - a Landscape Species + Screen plan, being Drawing No. 17.027.105 Revision A.
- c) an updated Review of proposed demolition of Local Heritage Place (relating to the Bridgeport Hotel) in the form of a report from Mr David Holland of DASH Architects dated 15 August 2017;
- d) a report from Mr David Holland of DASH Architects titled “Contextual Analysis Report (relating to the proposed plans as amended)” dated 28 August 2017;
- e) a supplementary Traffic Engineering Assessment report from Ms Melissa Mellen of MFY in the form of a report letter from Ms Mellen to Botten Levinson Lawyers dated 15 August 2017;
- f) an updated Planning Report dated 15 September 2017 relating to the DA prepared by Mr Marcus Rolfe of URPS;
- g) a report from Mr Andrew Martin of TMK Consulting Engineers relating to the impact, if any, of the proposed development on the nearby railway tunnel, in the form of a letter from Mr Martin to Kerin Bay Pty Ltd dated 22 August 2017;
- h) an updated Waste Management Plan relating to the proposed development prepared by CED and dated 5 July 2017;
- i) proposed Perforated Screen Images prepared by CED and dated 17 July 2017;
- j) an updated Exterior Lighting Modelling Report prepared by TMK Consulting Engineers and dated 21 June 2017;

- k) a Stormwater Management Report prepared by TMK Consulting Engineers and dated 17 July 2017;
- l) a wind loading impact report letter from TMK to Kerin Bay Pty Ltd dated 14 September 2017;
- m) an Environmental Noise Assessment Report prepared by Sonus and dated 11 September 2017;
- n) an External Colour and Finishes Schedule prepared by CED and dated 14 August 2017;
- o) a report, prepared by CED dated 22 September 2017, detailing amendments made to plans issued on 11 November 2016; and
- p) a Standard Hotel Suite materials & Finishes Schedule prepared by CED and dated 17 July 2017.

Below I shall make some particular comments on a number of the attachments.

1. Amended plans

The amended plans, which my client hereby formally lodges with the Development Assessment Commission pursuant to section s39 (4) of the *Development Act 1993*, largely speak for themselves in terms of the changes made. The proposal remains unchanged in terms of the nature of the development. The amendments relate to matters of design primarily, and have essentially been made to ensure in a contextual sense, the proposed development is compatible with built form and the public realm located within the locality.

As noted above, attached, for your convenience, is a schedule prepared by CED titled “Plan Alterations” that details the amendments to the various drawings lodged on 15 November 2016.

2. The demolition

The attached report from Mr David Holland, relating to the review of the proposed demolition of those portions of the existing Bridgeport Hotel that are on the local heritage list, has supplemented or expanded on his original report as lodged with the DA. The attached report adds further detail regarding:

- the structural condition of that portion of the existing hotel that is on the local heritage list;
- the potential for the adaptive re-use of those portions of the hotel on the local heritage list;
- and in providing extra detail concerning the above mentioned two matters, responding to a number of the comments made by Flightpath Architects.

Without exhaustively setting out all of the observations made and conclusions reached by Mr Holland, in his attached report, I particularly draw your attention

to the following observations and conclusions:

“Bridge Street Elevation

- *It appears that the entire length of the ground floor section of the “original” (c1879-1937) sections of stone wall has been removed and replaced with : (load bearing) brickwork, concrete columns and beams and windows.*
- *Based on my 20 plus years of architectural consulting experience, I suggest that the removal of the brickwork and reinstatement of stonework (using new stone) would neither be readily achieved nor cost effective.*

Corner

- *As per the DASH Report, the entire “original” lower section of the corner has been removed.*

East Terrace Elevation

I am also not sure whether the concealed stonework retains the capacity to be load bearing (this capacity may have been compromised either because of the design of the newer walls or due to its condition).

Adaptive Reuse

As an extension to the above discussions about integrity, while there is often potential for the adaptive reuse of a building, there are several factors in this instance that would likely affect the viability of this approach and incur time, cost and/or risk penalties. These include :

- *Much of the remaining “original” fabric is at first floor level (potentially limiting the ability of new work to be built “over and through” the existing building);*
- *The layout of the upper floor does not lend itself to reuse (it compromises multiple small compartments);*
- *There is no setback to the main street frontages (meaning that new work would likely need to be positioned behind this section of building);*
- *There is a need to replace sections of the lower floor facades (adding cost and risk); and*
- *The need to upgrade the existing building to meet current construction codes (again, adding cost and likely requiring physical intervention in remaining fabric).*

*Based on the compromised integrity of the place, and the location and nature of the fabric that is left, it is my view that this approach is **not** warranted in this instance”. (my emphasis)*

3. Contextual Analysis Report

You will note from reading the attached Contextual Analysis Report prepared by Mr Holland that he has, for the purposes of preparing his report (amongst other things) attended meetings with the Government Architect on site and with the Associate Government Architect at DPTI's office, liaised with the designer (Craig Eyles of CED), and reviewed the final amended design proposal.

You will also note that Mr Holland has (amongst other things) in preparing his report considered the impact of the proposed development on the various heritage places in the general vicinity of the site, including State heritage places and local heritage places. He has concluded that the proposed development is, in effect, one that is compatible with those heritage places. In particular, he has noted the proposed development does not directly affect any heritage place, nor materially affect the context within which any heritage place is situated.

Mr Holland has also concluded that the proposal plans (as amended) result in a proposal that relates, in a contextual sense, satisfactorily with the adjacent public domain and addresses the feedback provided by ODASA.

4. Landscaping plan

Is to be noted from the landscape report prepared by Mr Hayter (attached) that Mr Hayter has prepared his plan having regard to a variety of considerations. In particular, it is to be noted that he has concluded that :

- the species proposed in the landscape plans are ones that are likely to be successful horticulturally; and
- are ones that will contribute towards the overall development and precinct in a positive way.

5. Traffic and parking assessment

Our client provided Ms Melissa Mellen of MFY with copies of the amended plans so that she could review the changes and update her earlier traffic and parking advice of August 2016 to reflect the current proposal. Her review report is **attached**.

I will not set out in detail the observations and conclusions made by Ms Mellen. I do however note the following comments made by Ms Mellen in her attached report:

- *"The current plans have only included limited change in relation to the traffic operation of the site..."*
- *"In regard to the car park design, access, delivery and refuse vehicle requirements and bicycle provision, the proposal is consistent with the earlier design, as detailed in the July 2016 report."*

- *“It can be seen from the above assessment that the current proposal is forecast to generate a **marginally** greater peak parking demand when compared with the earlier forecast. (my emphasis)*

*However, the proposal also includes **nine additional spaces** on the subject site. Accordingly, there will only be a small increase in on-street parking at night when compared with the previous assessment. As such, it is anticipated that the proposed car park will **adequately cater for the forecast demand** during the day and that there will be **adequate on-street parking to accommodate any additional parking requirements during peak evening periods.**” (my emphasis)*

6. The railway tunnel

For completeness my client has also sought engineering advice from Mr Andrew Martin of TMK Consulting Engineers on the impact in a structural sense, if any, of the construction of the proposed development on the nearby railway tunnel. As you know, the railway tunnel passes near to the northern corner of the subject site beneath the intersection of Bridge Street and East Terrace.

In the attached report from Mr Andrew Martin, Mr Martin has concluded that the proposed development is sufficiently setback from the existing railway tunnel to not have an adverse impact on the tunnel, nor any affect on its structural integrity. Relevantly, Mr Martin has also noted that even if the tunnel was located within a zone of influence from the proposed development (which they have concluded it is not) it would be normal or “... *common engineering practice to design a deep footing system such as piles which would direct loads from the building below the tunnel and prevent surcharge loading.*”

Further, Mr Martin has concluded that “*whilst there will be vibration created from normal construction activities undertaken at the site in TMK’s experience these **are not likely** to have an adverse impact on the tunnel structure*”, and that “*current vibration levels on the railway tunnel from passing traffic such as trucks is possibly greater than any site induced construction activity.*”

Mr Martin concludes that “*construction activities at the site can be effectively managed to limit vibration levels to considered acceptable limits by the preparation of a vibration management plan.*”

7. Wind loading

You are referred to the attached report letter from Mr Martin, TMK Consulting Engineers to Kerin Bay dated 14 September 2017 concerning wind loading for the proposed building. You will note that Mr Martin advises that wind loading impact on the proposed development will, as per normal, be considered during the detailed design phase of the proposal ie, post development plan consent assessment. He further advises that wind loading, or forces placed on the proposed development, will be dealt with by the incorporation, in the detailed design phase, of a structural system that satisfies the relevant code requirements.

Relevantly, for Development Plan assessment purposes, Mr Martin has advised

that the proposed development is typical of many buildings they have, or are involved in, and presents no unusual challenges.

In view of the above, it is strongly contended that for Development Plan assessment purposes it is evident, based on the advice of TMK, that the proposed development will more than satisfactorily deal with wind loading forces and that the precise structural means of doing so can, and will, be dealt with during the building rules consent phase of the process.

8. Exterior lighting

The Exterior Lighting Modelling Report has also been updated. The changes have included the slight revision of the aiming of the car park lighting and an increase of 500mm to the light pole heights. The amendments will result in more even carpark lighting to meet Australian Standard 1158. The external up and down wall lighting to the perimeter of the hotel has also been reviewed to provide greater lighting for access and security.

TMK have indicated that both of these changes will **not** affect any light spill compliance.

9. Stormwater management

The attached Stormwater Management Report has not been materially amended. The date of the report has been updated to 17 July 2017, simply to reflect the inclusion of the updated Civil Plan. I understand that the stormwater calculations remain unchanged.

10. Environmental Noise Assessment report

I refer to the **attached** Environmental Noise Assessment report dated September 2017 prepared by Mr Jason Turner of Sonus Pty Ltd (and reviewed by Mr Chris Turnbull of Sonus Pty Ltd).

The Environmental Noise Assessment report essentially speaks for itself so I will not, in this letter, refer extensively to its contents.

I do however highlight the following matters from the attached:

- Mr Turner has considered the noise impact at the closest residences from patrons and outdoor areas, mechanical services, vehicle movements and activities associated with the car park and drive-through facilities, and music at the proposed development;
- the assessment has been based on criteria contained in the *Environment Protection (Noise) Policy 2007* and the music noise criteria in the relevant Development Plan (which criteria is consistent with the EPA Noise Guideline for “*music noise from indoor venues and the South Australian Planning System*”;
- the recommendations, detailed report, for acoustic measures to assist in achieving the abovementioned criteria;

- Mr Turner's conclusion that the proposed development will **not** cause unreasonable interference or impact on the amenity at the closest residences, and therefore, satisfies the environmental noise provisions of the Murray Bridge Council Development Plan.

11. Revised planning report

You are also referred to the attached revised planning report prepared by Mr Marcus Rolfe of URPS. That report is dated 15 September 2017.

You will note, from a perusal of that report, that URPS have reviewed the amended plans and associated documentation relating to the amended plans, and again considered the proposal against the relevant provisions of the Development Plan.

Relevantly, Mr Rolfe has concluded that the proposed development, for all the reasons detailed in his attached report, warrants a grant of development plan consent.

I trust that the above information is of assistance to you. Needless to say, if you have any queries please do not hesitate to contact me.

I understand that you plan now to refer the amended plans, and the associated material to the Rural City of Murray Bridge for comment. Then, after receipt of any feedback from the Council, you will arrange for the matter to be referred to the Development Assessment Commission for determination.

I would be grateful if you could keep me closely informed as to progress and timing.

Yours faithfully



Jamie Botten
BOTTEN LEVINSON
Mob: 0419 816 598
Email: jrb@bllawyers.com.au

Enc: As described throughout

Fielke, Matthew (DPTI)

From: Fountain, Troy (DPTI)
Sent: Thursday, 5 April 2018 4:07 PM
To: Fielke, Matthew (DPTI)
Subject: FW: DA 415/E014/16 - Bridgeport Hotel (BLL 215074)
Attachments: 206 DA1 Bridgeport BIN STORE_A3 040418.pdf

Troy Fountain

Project Lead

Planning Reform | Planning and Development

Department of Planning, Transport and Infrastructure

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From: Jamie Botten [mailto:jrb@bllawyers.com.au]
Sent: Thursday, 5 April 2018 3:33 PM
To: Fountain, Troy (DPTI) <Troy.Fountain@sa.gov.au>
Cc: James Jordan <jrj@bllawyers.com.au>
Subject: DA 415/E014/16 - Bridgeport Hotel (BLL 215074)

Hi Troy,

I refer to your email of 28 March in which you raised a number of queries regarding the Bridgeport Hotel redevelopment. Those queries related to the screening and treatment of the ground level bin storage area, the two waste management plans, and a site plan demonstrating the bin types and locations as defined in the Veolia waste management plan.

I confirm what James Jordan advised you of earlier today, i.e., that our client wishes to proceed only on the basis of the Veolia Waste Management Plan. In that regard the Management Plan prepared by Craig Eyles can be disregarded.

In response to your query about collection frequency, I note that the Veolia Waste Management Plan sets out the "Service Frequency & Waste Volumes" for both the waste from the hotel suites, hotel and café area, and also the kitchen and restaurant refuse area. The service schedule for general waste from all areas is 3 days per week, which can be increased to 5 days per week should the need arise. The service schedule for cardboard and paper from all areas is 2 days per week.

Further, please find **attached** a 'Bin Storage Details' plan prepared by CED Building Design, being drawing number 2.06/DA 1 dated 4 April 2018. This plan includes a 'Bin Store North East Elevation' showing the screening treatment to the ground level bin storage area and notes the detail of its construction.

Further, this plan includes a 'Bin Store Plan' which details the bin types and locations as defined in the Veolia Waste Management Plan. As you will see, the attached plan also includes a note outlining that the bin store has been sized in accordance with recommendations of the waste management plan prepared by Veolia.

I trust this information clarifies your queries regarding waste disposal on the site.

If you have any queries, please do not hesitate to contact me or James.

Cheers,

Jamie



Jamie Botten
Principal

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Regional Town Centre Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A centre representing the primary focus for business and commercial services for the region providing a full range of shopping, administrative, cultural, community, entertainment, education, religious and recreational facilities, and public and private office development.
- 2 A town centre providing a focus for public transport interchanges and networks.
- 3 Development of a visually and functionally cohesive and integrated town centre.
- 4 A centre accommodating medium density residential development in conjunction with non-residential development in appropriate locations.
- 5 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

This zone comprises the regional town centre of Murray Bridge. The primary and traditional focus of the town centre for specialty retail is Bridge Street, however the zone includes other retail, business and recreational policy areas along Mannum and Swanport Roads and the encompassing parklands. The town centre is located on a ridge overlooking the River Murray, and the two bridges crossing the river.

The town centre has played a significant role in the commercial activities of rural South Australia, being the major commercial link between the River Murray boating industry and the national rail system early last century, and now as a key centre in South Australia on the national highway.

The development of the town over time, along with changes in the transportation industries, has seen the town evolve into a major urban centre, catering at a regional scale for the rural areas from Lake Alexandrina, to Keith, Pinnaroo and Swan Reach. This regional role, and the geographical position of Murray Bridge and its location on the Adelaide to Melbourne to Sydney highways, ensures that the town centre will continue to provide goods and services for residents and travellers in the Murray Mallee Region of South Australia.

The policy areas within the town centre are aimed at reinforcing the town centre as the major commercial centre in the region and as a place for local residents and visitors to shop, work, meet, entertain and relax in an attractive, comfortable and safe environment.

It is desirable that the retail core of the town centre develops in a compact yet orderly manner within an attractive environment for the convenience and safety of pedestrians. As redevelopment occurs, off-street parking and pedestrian areas will be constructed to create a vibrant and attractive centre and establish an efficient and safe pedestrian and vehicle movement system to, from, and within the town centre and Sturt Reserve by:

- (a) adopting an integrated road hierarchy to efficiently distribute road traffic
- (b) establish adequate levels of staff and visitor car parking areas at appropriate locations
- (c) co-ordinate vehicle movements with car parking areas and pedestrian access requirements
- (d) devise directory signage strategies to assist in the efficient distribution of vehicle and pedestrian traffic

- (e) promote pedestrian network routes to facilitate movement within and around the town centre, by defining the pedestrian links using surfaces identifying pedestrian access
- (f) devise traffic calming and control strategies through Bridge Street to improve pedestrian and bicycle amenity.

The establishment of three retail nodes acknowledges the existing retail land use patterns and promotes further opportunities for expansion. It is necessary, however, to encourage improved pedestrian linkages between all the policy areas, especially the retail nodes, to promote a more functional centre. In particular, pedestrian linkages to the **South Terrace Policy Area 15** and the **Adelaide/Swanport Road Retail Policy Area 10** are envisaged.

Given the functional framework provided by the structure plan, together with the linkage designated, particularly for pedestrians, the town centre will be able to consolidate and evolve as an integrated centre with the desired mix and distribution of activities and linkages.

The Murray Bridge town centre reflects the growth of the town as a regional centre over the past 150 years. The current land use patterns reflect this evolution of the past, and show a range of building forms from historic to modern buildings. Development within the town centre is intended to generate a greater cohesion of these features and to create a more attractive and better functioning centre as it continues to grow. Development will therefore have regard to policy contained within the individual policy areas.

Development will promote a vibrant, economically viable and attractive centre, which will best suit the needs of the community, increase the centre's historic role as a social meeting place, and enhance the image of the town and the level of comfort and convenience for residents and visitors.

The efficiency and safety of movement and parking within the centre is of key importance to the health of the town centre. The provision of designated movement routes, off-street parking areas and pedestrian linkages will facilitate increased vitality and user comfort. This will be coupled with appropriate development, an effective land use strategy and environmental improvement programs, to assist the town centre of Murray Bridge in assuring its future as a prominent regional centre.

Provision of pedestrian shelters in the form of awnings, canopies, balconies, or verandas will occur to encourage pedestrian movement within the town centre and maintain pedestrian comfort.

Service and light industry will only be undertaken within the **Adelaide Road Policy Area 9** and at a scale and intensity that will not adversely detract from the desired amenity of the policy area and town centre environment.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

1 The following forms of development are envisaged in the zone:

- affordable housing
- bank
- child care centre
- civic centre
- consulting room
- department store
- dwelling in conjunction with non-residential development
- educational establishment
- emergency services facility
- entertainment facility
- health facility
- hospital
- hotel
- indoor games centre

- library
 - motel
 - motor repair station
 - office
 - place of worship
 - playing field
 - pre-school
 - primary school
 - residential flat building in conjunction with non-residential development
 - restaurant
 - service industry within Adelaide Road Policy Area 9
 - shop
 - supermarket
 - swimming pool.
- 2 Development listed as non-complying is generally inappropriate.
- 3 Medium density residential development, including affordable housing, and development comprising a variety of residential and non-residential uses should only be developed:
- (a) if it does not prejudice the operation of existing or future retail activity within the zone
 - (b) within any of the following policy areas:
 - (i) **Business Policy Areas 12**
 - (ii) **South Terrace Policy Area 15.**

Form and Character

- 4 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 5 Development should be designed and sited to promote linkages between the various developments within the centre and adjoining main roads.
- 6 Facilities within the centre should be sited and designed with a view to promoting after-hours use to reinforce the centre as the focus of social activity in the region.
- 7 Dwellings should be located only behind or above non-residential uses on the same allotment.
- 8 Where possible, new developments should provide car parking in an integrated manner between developments that encourages use and pedestrian linkages between adjoining developments and policy areas.
- 9 Under-croft car parking should occur where practicable.
- 10 Access to car parking areas should be obtained from minor streets and lanes wherever possible.
- 11 Development should establish a cohesive townscape character by:
- (a) defining the major streets as important linear public spaces by:
 - (i) ensuring that buildings maintain or establish a relatively continuous edge of built-form abutting or situated close to those major street frontages
 - (ii) emphasizing the townscape importance of development at the intersections of major streets, with corner buildings of a scale and form appropriate to their location and situated close to or abutting both street frontages

- (iii) ensuring that buildings fronting on to such streets are of a shape, scale and orientation which relate to and reinforce the rectilinear grid pattern of the town centre
 - (b) ensuring that the design, external materials, colours and finishes of buildings have regard to their townscape context as appropriate to the desired future character of the relevant policy area
 - (c) avoiding the use of large expanses of highly reflective materials
 - (d) ensuring the height of any building is compatible with the desired streetscape character.
- 12 Development of items recognised within each policy area as having historic or cultural value should be conserved.
- 13 Development should be designed and sited to retain and protect an existing tree of substantial size and merit and cultural importance.
- 14 Advertisements (excluding its supports) should not exceed 2 metres in height and 3 metres in width.

Land Division

- 15 Land division in the zone is appropriate provided new allotments are of a size and configuration to ensure the objectives of the zone can be achieved.

Bridge Street Policy Area 11

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 The reinforcement of the role of Bridge Street as the traditional 'main street' of Murray Bridge.
- 2 The establishment of an attractive local centre which improves the existing environment and the surrounding townscape of South Terrace and any adjacent residential development.
- 3 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

Bridge Street is the primary focus and historical retail centre of Murray Bridge. It contains a diversity of built-form, from substantial historical buildings to buildings reflecting modern styles and construction techniques.

Its geographical prominence and the activities conducted in the street give it an important role in establishing an image for not only the town centre but also the whole of Murray Bridge. Development and activity in Bridge Street must therefore not only cater for the needs of the consumer, but it also needs to exhibit a high quality and image that reflects a prosperous and pleasant township.

The policy area will be developed as the principal retail area for specialised goods in the Council area. The role of Bridge Street as the traditional 'main street' of Murray Bridge will be reinforced by:

- (a) ensuring the economic viability and vitality of specialised retailing, entertainment, cultural and tourist services
- (b) encouraging new development to create a cohesive lineal streetscape appearance and where the scale of new development is compatible with the existing built-form
- (c) encouraging development to make greater use of the street for pedestrian and social activities, creating an atmosphere where people want to spend time
- (d) establishing a high visual amenity and strong local character to create a sense of place and focal point within the town

Continuous retail frontages including eating places such as cafes, restaurants and entertainment facilities will be characteristic of Bridge Street. These frontages will comprise small retail tenancies opening onto the street.

Development in the policy area will respect the character and integrity of the locality in respect of its design, siting, scale, building and roof shape, bulk, height, building materials and colours, advertising signs and external illumination.

Car parking will not disrupt the continual flow of retail frontages along Bridge Street and will be located at the rear of buildings or off adjoining side streets, wherever possible. Pedestrian access to the car parking areas will be designed with safety and convenience for the pedestrian in mind.

The greening of Bridge Street and provision for readily identifiable pedestrian cross-overs will contribute to greater pedestrian safety and comfort. The streetscape improvement of Bridge Street will also improve safe vehicular movement, whilst retaining a pedestrian theme.

Development will contribute to the creation of a comfortable pedestrian shopping environment, in which people will want to spend time, which may incorporate the provision of verandas and awnings in front of developments to provide a pleasant streetscape. Verandas and awnings will be sited in such a way as to not impede pedestrian movement along the footpath and to provide shade.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the policy area:
 - café
 - entertainment facility eg. movie cinema
 - information centre
 - office
 - restaurant
 - small-scale specialty shops
 - tourist facilities.
- 2 Development in Bridge Street should provide the principal focus for cultural and tourist activity within the policy area and more broadly the whole zone.

Form and Character

- 3 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 4 Development located within the side streets off Bridge Street should provide interpretative frontages to the street that promote pedestrian movement between the policy area and adjoining policy areas.
- 5 All buildings developed within the policy area should abut all road frontages where there is more than one frontage.
- 6 Direct vehicular access to car parking or service areas of the development should not be provided from Bridge Street.
- 7 Developments should not encourage large vehicles and trucks to travel through Bridge Street.
- 8 Buildings should incorporate design elements such as verandas and balconies and decoration and ornamentation that are consistent with local features.
- 9 Buildings should exhibit a high proportion of solid-to-void construction in the composition of facades and veranda design.
- 10 New buildings should maintain the existing horizontal massing of built-form while incorporating vertical proportioning in the composition of facades.
- 11 The frontages to Bridge Street at street level should be composed predominantly of display windows, avoiding bland surfaces and solid infills.
- 12 The use of large expanses of very bright colours should be avoided.
- 13 The spatial character of Bridge Street, established by the width of the street relative to its generally one-to-two-storey building height, should predominate.
- 14 Development should create a cohesive townscape appearance along Bridge Street through the blending of new development compatible with the traditional architecture.
- 15 Buildings within Bridge Street should have a maximum height of 8 metres.
- 16 Advertisements and/or advertising hoardings should use simple graphics and be restrained in size, design and colour to help create a unified character along Bridge Street, particularly for pedestrian interpretive signs.

Council Wide Principles of Development Control

Murray Bridge (RC) Development Plan – Consolidated 11 August 2016

Key Policies

Advertisements

PDC 1 – The location, siting, design, materials, size, and shape of advertisements and/or advertising hoardings should be:

- a) Consistent with the predominant character of the urban or rural landscape
- b) In harmony with any buildings or sites of historic significance or heritage value in the area
- c) Coordinated with and complement the architectural form and design of the building they are to be located on.

PDC 7 – Advertisements and/or advertising hoardings attached to buildings should not be sited on the roof or higher than the walls of a building, unless the advertisement or advertising hoarding is appropriately designed to form an integrated and complementary extension of the existing building.

Centres and Retail Development

PDC 2 – Development within centres should be designed to be compatible with adjoining areas. This should be promoted through landscaping, screen walls, centre orientation, location of access ways, buffer strips and transitional use areas.

PDC 5 – The design of under-croft or semi-basement car parking areas should not detract from the visual quality and amenity of adjacent pedestrian paths, streets or public areas.

PDC 6: Under-croft or semi-basement car parking areas should not project above natural or finished ground level by more than one metre.

Crime Prevention

PDC 1 – Development should be designed to maximise surveillance of public spaces through the incorporation of clear lines of sight, appropriate lighting and the use of visible permeable barriers wherever practicable.

PDC 2 – Buildings should be designed to overlook public and communal streets and public open space to allow casual surveillance.

PDC 5 – Development, including car parking facilities should incorporate signage and lighting that indicate the entrances and pathways to, from and within sites.

Design and Appearance

PDC 1 – The design of a building may be of a contemporary nature and exhibit an innovative style provided the overall form is sympathetic to the scale of development in the locality and with the context of its setting with regard to shape, size, materials and colour.

PDC 3 – Buildings should be designed to reduce their visual bulk and provide visual interest through design elements such as:

- a) Articulation
- b) Colour and detailing
- c) Small vertical and horizontal components
- d) Design and placing of windows
- e) Variations to facades.

PDC 7 – The external walls and roofs of buildings should not incorporate highly reflective materials which will result in glare.

PDC 8 – Structures located on the roofs of buildings to house plant and equipment should form an integral part of the building design in relation to external finishes, shaping and colours.

PDC 9 – Building design should emphasise pedestrian entry points to provide perceptible and direct access from public street frontages and vehicle parking areas.

PDC 13 – Where applicable, development should incorporate verandahs over footpaths to enhance the quality of the pedestrian environment.

PDC 14 – Development should be designed and sited so that outdoor storage, loading and service areas are screened from public view by an appropriate combination of built form, solid fencing and/or landscaping.

PDC 16 – Balconies should:

- a) Be integrated with the overall architectural form and detail of the building
- b) Be sited to face predominantly north, east or west to provide solar access
- c) Have a minimum area of 2 square metres.

PDC 18 – The setback of buildings from public roads should:

- a) Be similar to, or compatible with, setbacks of buildings on adjoining land and other buildings in the locality
- b) Contribute positively to the streetscape character of the locality
- c) Not result in or contribute to a detrimental impact upon the function, appearance or character of the locality.

Heritage Places

PDC 1 – A heritage place spatially located on *Overlay Maps – Heritage* and more specifically identified in... *Table MuBr/4 – Local Heritage Places*, should not be demolished, destroyed or removed, in total or in part, unless either of the following apply:

- a) That portion of the place to be demolished, destroyed or removed is excluded from the extent of the place identified in the *Table(s)*
- b) The structural condition of the place represents an unacceptable risk to public or private safety.

PDC 2 – Development of a State or local heritage place should retain those elements contributing to its heritage value, which may include (but not be limited to)

- a) Principal elevations

- b) Important vistas and views to and from the place
- c) Setting and setbacks
- d) Building materials
- e) Outbuildings and walls
- f) Trees and other landscaping elements
- g) Access conditions (driveway form/width/material)
- h) Architectural treatments
- i) The use of the place.

Infrastructure

PDC 4 – Development should not take place until adequate and coordinated drainage of the land is assured.

Interface between Land Uses

PDC 1 – Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:

- a) The emission of effluent, odour, smoke, fumes, dust or other airborne pollutants
- b) Noise
- c) Vibration
- d) Electrical interference
- e) Light spill
- f) Glare
- g) Hours of operation
- h) Traffic impacts.

PDC 9 – Outdoor areas (such as beer gardens or dining areas) associated with licensed premises should be designed or sited to minimise adverse noise impacts on adjacent existing or future noise sensitive development

PDC 10 – Development proposing music should include noise attenuation measures that achieve the following desired noise levels:

Noise level assessment location	Desired noise level
<i>Adjacent existing noise sensitive development property boundary</i>	<p>Less than 8 dB above the level of background noise ($L_{90,15min}$) in any octave band of the sound spectrum</p> <p>and</p> <p>Less than 5 dB(A) above the level of background noise ($LA_{90,15min}$) for the overall (sum of all octave bands) A-weighted level</p>
<i>Adjacent land property boundary</i>	<p>Less than 65dB(Lin) at 63Hz and 70dB(Lin) in all other octave bands of the sound spectrum</p> <p>or</p> <p>Less than 8 dB above the level of background noise ($L_{90,15min}$) in any octave band of the sound spectrum and 5 dB(A) overall (sum of all octave bands) A-weighted level</p>

PDC 12 – Chimneys or exhaust flues associated with commercial development (including cafes, restaurants and fast food outlets) should be designed to ensure they do not cause a nuisance or health concerns to nearby sensitive receivers by:

- a) Incorporating appropriate treatment technology before exhaust emissions are released to the atmosphere
- b) Ensuring that the location and design of chimneys or exhaust flues maximises dispersion and takes into account the location of nearby sensitive uses.

Landscaping, Fences and Walls

PDC 1 – Development should incorporate open space and landscaping and minimise hard paved surfaces in order to:

- a) Complement built form and reduce the visual impact of larger buildings (eg taller and broader plantings against taller and bulkier building components)
- b) Enhance the appearance of road frontages
- c) Screen service yards, loading areas and outdoor storage areas
- d) Minimise maintenance and watering requirements
- e) Enhance and define outdoor spaces, including car parking areas
- f) Maximise shade and shelter
- g) Assist in climate control within and around buildings
- h) Minimise heat absorption and reflection
- i) Maintain privacy
- j) Maximise stormwater reuse
- k) Complement existing vegetation, including native vegetation
- l) Contribute to the viability of ecosystems and species
- m) Promote water and biodiversity conservation.

PDC 4 – Fences and walls, including retaining walls, should:

- a) Not result in damage to neighbouring trees
- b) Be compatible with the associated development and with existing predominant, attractive fences and walls in the locality
- c) Enable some visibility of buildings from and to the street to enhance safety and allow casual surveillance
- d) Incorporate articulation or other detailing where there is a large expanse of wall facing the street
- e) Assist in highlighting building entrances
- f) Be sited and limited in height, to ensure adequate sight lines for motorists and pedestrians especially on corner sites
- g) In the case of side and rear boundaries, be of sufficient height to maintain privacy and/or security without adversely affecting the visual amenity or access to sunlight of adjoining land
- h) Be constructed of non-flammable materials.

Natural Resources

PDC 5 – Development should be designed to maximise conservation, minimise consumption and encourage reuse of water resources.

PDC 7 – Development should be sited and designed to:

- a) Capture and reuse stormwater, where practical
- b) Minimise surface water runoff
- c) Prevent soil erosion and water pollution
- d) Protect and enhance natural water flows
- e) Protect water quality by providing adequate separation distances from watercourses and other water bodies
- f) Not contribute to an increase in salinity levels
- g) Avoid the water logging of soil or the release of toxic elements
- h) Maintain the natural hydrological systems and not adversely affect:
 - a. The quantity and quality of groundwater
 - b. The depth and directional flow of groundwater
 - c. The quality and function of natural springs.

PDC 8 – Water discharged from a development site should:

- a) Be of a physical, chemical and biological condition equivalent to or better than its pre-developed state
- b) Not exceed the rate of discharge from the site as it existed in pre-development conditions.

PDC 11 – Development should include stormwater management systems to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure the carrying capacities of downstream systems are not overloaded.

Orderly and Sustainable Development

OBJ 7 – Development of the town of Murray Bridge as the main regional service and community centre within the district and Murray Mallee generally.

PDC 1 – Development should not prejudice the development of a zone for its intended purpose.

PDC 3 – The economic base of the region should be expanded in a sustainable manner.

PDC 7 – Where development is expected to impact upon the existing infrastructure network (including the transport network), development should demonstrate how the undue effect will be addressed.

Siting and Visibility

PDC 1 – Development should be sited and designed to minimise its visual impact on:

- a) The natural, rural or heritage character of the area
- b) Areas of high visual or scenic value, particularly rural areas
- c) Views from the River Murray, public reserves, tourist routes and walking trails.

PDC 2 – Buildings should be sited in unobtrusive locations and, in particular, should:

- a) Be grouped together
- b) Where possible be located in such a way as to be screened by existing vegetation when viewed from public roads.

PDC 5 – Buildings and structures should be designed to minimise their visual impact in the landscape, in particular:

- a) The profile of buildings should be low and the roof lines should complement the natural form of the land
- b) The mass of buildings should be minimised by variations in wall and roof lines and by floor plans which complement the contours of the land
- c) Large eaves, verandahs and pergolas should be incorporated into designs so as to create shadowed areas that reduce the bulky appearance of buildings.

PDC 6 – The nature of external surface materials of buildings should not detract from the visual character and amenity of the landscape.

Tourism Development

PDC 6 – Major tourism developments should generally be located within designated areas and existing townships, towns or cities.

Transportation and Access

PDC 8 – Development should provide safe and convenient access for all anticipated modes of transport including cycling, walking, public and community transport, and motor vehicles.

PDC 9 – Development at intersections, pedestrian and cycle crossings, and crossovers to allotments should maintain or enhance sightlines for motorists, cyclists and pedestrians to ensure safety for all road users and pedestrians.

PDC 13 – Development should make sufficient provision on site for the loading, unloading and turning of all traffic likely to be generated.

PDC 19 – Developments should encourage and facilitate cycling as a mode of transport by incorporating end-of-journey facilities including:

- a) Showers, changing facilities, and secure lockers
- b) Signage indicating the location of bicycle facilities
- c) Secure bicycle parking facilities

PDC 22 – Development should have direct access from an all weather public road.

PDC 23 – Development should be provided with safe and convenient access which:

- a) Avoids unreasonable interference with the flow of traffic on adjoining roads
- b) Accommodates the type and volume of traffic likely to be generated by the development or land use and minimises induced traffic through over-provision
- c) Is sited and designed to minimise any adverse impacts on the occupants of and visitors to neighbouring properties.

PDC 30 – Development should provide off-street vehicle parking and specifically marked disable car parking places to meet anticipated demand in accordance with *MuBr/2 – Off Street Vehicle Parking Requirements*.

PDC 32 – Vehicle parking areas should be sited and designed in a manner that will:

- a) Facilitate safe and convenient pedestrian linkages to the development and areas of significant activity or interest in the vicinity of the development

- b) Include safe pedestrian and bicycles linkages that complement the overall pedestrian and cycling network
- c) Not inhibit safe and convenient traffic circulation
- d) Result in minimal conflict between customer and service vehicles
- e) Avoid the necessity to use public roads when moving from one part of a parking area to another
- f) Minimise the number of vehicle access points to public roads
- g) Where reasonably possible, provide the opportunity for shared use of car parking and integration of car parking areas with adjoining development to reduce the total extent of vehicle parking areas and the requirement for access points.
- h) Not dominate the character and appearance of a centre when viewed from public roads and spaces
- i) Provide landscaping that will shade and enhance the appearance of the vehicle parking areas.

PDC 35 – Parking areas that are likely to be used during non daylight hours should provide floodlit entrance and exit points and site lighting directed and shaded in a manner that will not cause nuisance to adjacent properties or users of the car park.

Waste

PDC 6 – Development that involves the production and/or collection of waste and/or recyclable material should include designated collection and storage area(s) that are:

- a) Screened and separated from adjoining areas
- b) Located to avoid impacting on adjoining sensitive environments or land uses
- c) Designed to ensure that wastes do not contaminate stormwater or enter the stormwater collection system
- d) Located on an impervious sealed area graded to a collection point in order to minimise the movement of any solids or contamination of water
- e) Protected from wind and stormwater and sealed to prevent leakage and minimise the emission of odours
- f) Stored in such a manner that ensures that all waste is contained within the boundaries of the site until disposed of in an appropriate manner.

Current Development Plan Provisions

The subject land falls within the Regional Town Centre Zone, Retail Core Policy Area 11 and Bridge Street Precinct 1 of the Murray Bridge (RC) Development Plan consolidated on 23 January 2018. Changes made to the Regional Town Centre Zone since lodgement of the application are as a result of the Regional Town Centre Expansion DPA, completed 26 April 2017 and consolidated on 2 May 2017.

The Zone, Policy Area and Precinct provisions of the Development Plan as they currently stand can be found in the following ATTACHMENT.

The most notable changes in the policy as it currently applies to the subject land are as follows:

- a) Maximum building height has increased from 8m to 12m, however greater building heights are considered appropriate if a proposed development displays both significant design merit and adequately reflects the desired character.
- b) Specific provisions have been provided for the subject land (see Precinct 1 Bridge Street PDC 28). PDC 28 envisages buildings to be of a medium to high scale that provide an integration of land uses on the site.

Regional Town Centre Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A centre representing the primary focus for business and commercial services for the region providing a full range of shopping, administrative, cultural, community, entertainment, education, religious and recreational facilities, and public and private office development.
- 2 A centre providing a focus for public transport interchanges and networks.
- 3 Development of a visually and functionally cohesive and integrated regional centre.
- 4 A centre accommodating medium density residential development in conjunction with non- residential development in appropriate locations.
- 5 An accessible centre that encourages activities after hours, supported by high quality public spaces and streetscapes.
- 6 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

Function

The **Regional Town Centre Zone** supports an accessible and functioning Murray Bridge Central Business District. This zone encompasses a vibrant, economically viable and attractive centre. Future intensive retail, entertainment, business, administrative, civic, educational, cultural and community facilities will be located in this zone.

Residential development in association with non-residential development will provide an alternative housing option, and housing choices to meet the demands of residents and visitors. Uses will be distributed in such a manner that both accessibility and vibrancy are encouraged, during and after business hours.

It is desirable that the town centre develops into a walkable, compact, and orderly place, providing an attractive environment for all users. Accordingly, the centre will support the use and function of public transport, walking and cycling, with priority given to a safe and inclusive pedestrian network, complemented by slower speed/ shared environments. Therefore, in this zone, improved pedestrian, cycling and other linkages that acknowledge innovation, context and history and aim to create a place where both residents and visitors live/stay, work, socialise, shop and/or relax, in a comfortable, unique and safe environment are suited.

Design and Character

Contemporary development is appropriate within the centre, with careful consideration of the existing character and setting, of any Heritage items and Historic Conservation Areas that are a part of the zone. It is also expected that development will respond to important historic characteristics through appropriate height, design features (such as verandas and parapets), and the use of complementary materials and finishes. Upgrades of heritage buildings will follow the principles of preservation, restoration, reconstruction and adaptation.

New development will make a positive contribution to this regional centre through the provision of high quality urban design and the appropriate placement of buildings. This can be achieved by contributing

towards an interesting and attractive streetscape, through building facades that are meaningfully activated where they face a public road or public space, in order to make the town centre more pedestrian friendly.

An interesting pedestrian environment and human scale will remain the focus of this zone, with careful building articulation and fenestration, frequent openings in building facades, verandas, balconies, awnings and other features that provide weather protection. Long solid walling will be avoided; rather innovative designs better suit this zone.

Water Sensitive Urban Design systems, including harvesting, treatment, storage and the reuse of stormwater, will be integrated throughout the zone at neighbourhood, street, site and building levels. Stormwater management will address potential water quality impacts upon the River Murray, while improving the aesthetic and functional value of open spaces, including public accessways and car parks.

Landscaping plans and detailed design schemes will support the desired character.

Vehicle access points will be consolidated, designed and located to reduce conflict with vehicle, pedestrian and cycle movements.

Public Realm

Future development will be sympathetic to and enhance the public realm. In particular, Bridge and Sixth Streets will maintain a high quality public realm, built form in this zone will encourage public interaction, incorporate passive surveillance and provide landscaping, that engages pedestrians and brings a sense of connection.

Pedestrian comfort will be provided through the use of green walls, green facades, canopies, shading and verandas. Facades will allow for the adaptive reuse of ground floor tenancies, including activities such as outdoor dining, which may spill out onto common areas and footpaths.

Street furniture, car parking areas, including lighting, signs, litter bins, seats and bollards, will be designed and located so as to complement the townscape's riverine character and heritage and improve public spaces. Development within the zone shall be in accordance with [Concept Plan Map MuBr/17 - Murray Bridge Town Centre](#).

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

1 The following forms of development are envisaged in the zone:

- alterations and additions to existing developed land uses
- bulky goods located in **Mixed Use Policy Area 9**
- child care centre
- community centre
- consulting room
- department store
- dwelling in conjunction with non-residential development
- educational establishment
- emergency services facility
- entertainment facility
- group dwelling located in **Mixed Use Policy Area 9**
- hospital
- hotel
- library
- motel
- motel repair station
- multiple dwelling
- office
- place of worship

- primary school located in the **Mixed Use Policy Area 9** or **Education Policy Area 14**
 - pre-school located in the **Mixed Use Policy Area 9** or **Education Policy Area 14**
 - recreation centre
 - public transport terminal
 - recreation area
 - residential flat building in conjunction with non-residential development
 - restaurant
 - row dwelling located in **Mixed Use Policy Area 9**
 - service industry
 - shop
 - swimming pool
 - tourist accommodation.
- 2 Development listed as non-complying is generally inappropriate.
- 3 Active uses such as shops, restaurants and cafes should be concentrated at ground level to ensure:
- (a) residential and other commercial uses are located on higher levels
 - (b) integration of land uses within buildings and adjacent public spaces
 - (c) promotion of street activation and surveillance.
- 4 Development comprising a variety of medium density residential and non-residential uses should only be developed if it does not prejudice the operation of existing or future non-residential activity within the zone.

Form and Character

- 5 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 6 Development should be designed and sited to promote linkages between the various developments within the centre and adjoining primary and secondary roads as identified in *Overlay Map MuBr/21 - Transport*.
- 7 Facilities within the centre should be sited and designed with a view to promoting after-hours use to reinforce the centre as the focus of social activity in the region.
- 8 Dwellings should be located only behind or above non-residential uses on the same allotment.
- 9 Where possible, new developments should provide car parking in an integrated manner. Such car parking should encourage pedestrian linkages between adjoining developments and policy areas.
- 10 Under-croft car parking should occur where practicable.
- 11 Access to car parking should be obtained from minor streets and lanes wherever possible.
- 12 Items defined in *Table MuBr/3 – Contributory Items*, *Table MuBr/4 - Local Heritage Places* and *Table MuBr/5 - State Heritage Places* as having either State, Local or Contributory heritage value should be conserved or adaptively reused or restored.
- 13 Advertisements (excluding its supports) should be of a scale appropriate for the development to which it relates.
- 14 Development should provide building frontages and facades that support street activity and engagement with pedestrians, options for achieving this include provision of large street facing windows, articulation of frontages and the use of verandas to improve walkability.
- 15 Unless specifically identified within either a policy area or precinct development should have a maximum building height of 8 metres.

- 16 Commercial uses/tenancies which incorporate a large footprint should provide primary and secondary pedestrian entries to the street so as to contribute to street activity.
- 17 Tourism accommodation should preferably involve the restoration and re-use of existing (heritage) buildings, with any new development being of a scale and form sympathetic to the character of the locality.

Residential Development

- 18 Residential development above non-residential land uses should be designed and located to:
 - (a) be appropriately and adequately shielded (through the use of specific designs and materials) from the extension of commercial activity uses outside of traditional business hours
 - (b) to provide passive surveillance over surrounding streets and public spaces
 - (c) minimise the impact of noise, light and odour resulting from envisaged uses within the zone.
- 19 Innovative design techniques incorporating articulation, passive surveillance, landscaping, privacy considerations and noise reduction should be used to ensure commercial land uses are sympathetic to existing residential land uses.

Car Parking

- 20 Development should provide off-street vehicle parking and specifically marked accessible car parking places to meet anticipated demand in accordance with [Table MuBr/2 - Off Street Vehicle Parking Requirements](#) unless all the following conditions are met:
 - (a) an agreement is reached between the Council and the applicant for a reduced number of parking spaces
 - (b) a financial contribution is paid into the Council Car Parking Fund specified by the Council, in accordance with the gazetted rate per car park. [Concept Plan Map Mu/Br18 - Car Parking Fund](#) identifies the contribution areas where the car parking fund applies.
- 21 Vehicle parking spaces and multi-level vehicle parking structures within buildings should:
 - (a) enhance active street frontages by providing land uses such as commercial, retail or other non-car park uses along ground floor street frontages
 - (b) complement the surrounding built form in terms of height, massing and scale
 - (c) incorporate facade treatments along major street frontages that are sufficiently enclosed and detailed to complement neighbouring buildings consistent with the desired character of the locality
 - (d) use Water Sensitive Urban Design policies and incorporate landscaping and greening initiatives.
- 22 In mixed use buildings, the provision of vehicle parking may be reduced in number and shared where the operating hours of commercial activities complement the residential use of the site.

Land Division

- 23 Land division in the zone is appropriate provided new allotments are of a size and configuration to ensure the objectives of the zone can be achieved.

Retail Core Policy Area 11

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 The reinforcement of the role of Bridge Street as the traditional 'main street' of Murray Bridge.
- 2 The establishment of an attractive local centre which improves the existing environment and the surrounding townscape of South Terrace and any adjacent residential development.
- 3 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

Bridge Street is the primary focus and historical retail centre of Murray Bridge. Its geographical prominence and the activities conducted in the street give it an important role in establishing an image for not only the town centre but also the whole of Murray Bridge. Development and activity in Bridge Street must therefore not only cater for the needs of consumers, but it also needs to exhibit a high quality and image that reflects a prosperous and pleasant township.

The policy area will maintain the prominence of Bridge Street but extend its character and sense of place throughout the policy area to the immediately surrounding streets. It is to be developed as the principal retail area for specialised goods in the Council area. The role of Bridge Street and the wider policy area as the traditional 'main street' area of Murray Bridge will be reinforced by:

- (a) ensuring the economic viability and vitality of specialised retailing, entertainment, cultural and tourist services
- (b) encouraging new development to create a cohesive lineal streetscape appearance and where the scale of new development is compatible with the existing built-form
- (c) encouraging development to make greater use of the street for pedestrian and social activities, creating an atmosphere where people want to spend time
- (d) establishing a high visual amenity and strong local character to create a sense of place and focal point within the town.

Continuous retail frontages including eating places such as cafes, restaurants and entertainment facilities will be characteristic of the policy area. These frontages will comprise small retail tenancies opening onto the street.

Development in the policy area will respect the character and integrity of the locality in respect of its design, siting, scale, building and roof shape, bulk, height, building materials and colours, advertising signs and external illumination.

Car parking will not disrupt the continual flow of retail frontages throughout the policy area and will be located at the rear of buildings or off adjoining side streets, wherever possible. Pedestrian access to the car parking areas will be designed with safety and convenience for the pedestrian in mind.

Precinct 1 - Bridge Street

The success of the Regional Town Centre is reliant on the success of both Bridge and Sixth Streets and their ability to encourage a rich mixture of retail, commercial and tourism activities. Bridge Street is of particular importance given its role as the original centre and mainstreet of Murray Bridge. Development will encourage land uses such as small-scale retail, cafes and restaurants which will promote activity throughout both day and in the evening, allowing for extended hours of operation within the precinct.

It is important that development reinforces the continuity of building facades and the predominant townscape character of the frontages to Bridge Street. It is expected that buildings adjacent or in proximity to Bridge Street will address the street or outdoor public spaces and incorporate verandas and which extend across footpaths in order to provide shelter for pedestrians where appropriate.

The use of financial contributions associated with the Car Parking Fund applies across the Regional Town Centre Zone; however the provision of off street parking for new developments may be somewhat limited within the Bridge Street Precinct. Car parking provisions within the precinct will seek to utilise existing on street parking rather than provide a significant increase in car parking.

This precinct includes the Railway Terrace, East Terrace and Bridge Street intersection located at the western end of the State Heritage listed Murray Bridge Road Bridge, considered to be the entrance to the township when travelling in a westerly direction. Development within this locality will be of a very high standard of design and utilise the highest quality materials in order to reflect the significance of the location and provide an entrance statement that truly reflects the desired character as a vibrant, energetic and progressive centre that embraces its heritage.

Precinct 2 - Sixth Street

Sixth Street is at the centre of Murray Bridge's cultural district. It contains key civic buildings and the art gallery. It is envisaged that the streetscape of this area will be enhanced through designs that will complement existing heritage buildings, and that this precinct will be improved through a public art corridor leading from Sturt Reserve and other riverfront reserves, to the art gallery and back.

Buildings will provide visual interest to the pedestrian; contain frequent pedestrian entries and clear views to the street. Buildings will establish a width and pattern of façades that support a variety of tenancies. Buildings will be built with zero set back from Sixth street site boundary, with the occasional section of building set back to create outdoor dining areas, visually interesting building entrances and active spaces. The footpath will be sheltered with verandas, shelters and the like to enhance the pedestrian experience.

This precinct provides opportunity for alfresco dining, planting and seating, and can be used for events such as day-time and night-time markets. The qualities of such flexible community uses on this street will be protected and enhanced.

Development in the zone will achieve high quality urban design in both the public and private realm. Buildings will contribute to the provision of a coherent public realm by shaping the street space and, in particular, the physical and functional character of this precinct.

Any future development, including car parking, micro climate and alternative hours of use will be considered after reference to [Concept Plan Map MuBr/17 - Murray Bridge Town Centre](#).

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

1 The following forms of development are envisaged in the policy area:

- café
- civic buildings
- entertainment facility e.g. movie cinema
- hotel
- information centre
- office
- public art and street furniture
- restaurant
- small-scale specialty shops
- tourist accommodation
- tourist facilities.

- 2 Development in Bridge, Sixth and Seventh Streets should provide the principal focus for cultural and tourist activity within the policy area and more broadly the whole zone.
- 3 Higher order retail uses such as department stores and discount department stores should not be developed in this policy area.

Form and Character

- 4 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 5 Development located within the side streets off Bridge Sixth and Seventh Streets should provide interpretative frontages to the street that promote pedestrian movement throughout and across policy areas.
- 6 Direct vehicular access to car parking or service areas of the development should not be provided from Bridge, Sixth or Seventh Streets.
- 7 Developments should not encourage large vehicles and trucks to travel through Bridge Street.
- 8 Buildings should incorporate design elements such as verandas and balconies and decoration and ornamentation that are consistent with local features.
- 9 The frontages to Bridge and Sixth Streets at street level should be composed predominantly of display windows, avoiding bland surfaces and solid infills.
- 10 The use of large expanses of very bright colours should be avoided.
- 11 Development should create a cohesive townscape appearance throughout the policy area through the blending of new development compatible with the traditional architecture.
- 12 Advertisements and/or advertising hoardings should use simple graphics and be restrained in size, design and colour to help create a unified character throughout the policy area, particularly for pedestrian interpretive signs.
- 13 Advertisements or advertising hoardings should be positioned at the junction of the veranda and main face of the buildings, applied directly to, or hung from, gables or facades of verandas or main buildings, displayed on parapets or facades, or hung underneath verandas.
- 14 Development should establish a cohesive policy area character by:
 - (a) defining Bridge Street and Sixth Street as important public spaces by:
 - (i) ensuring that buildings maintain or establish a relatively continuous edge of built-form consistent with adjoining development
 - (ii) emphasising the importance of development at the intersections of major streets with corner buildings of a scale and form appropriate to their location consistent with adjoining development
 - (b) ensuring that the design, external materials, colours and finishes of buildings have regard to their townscape context.
- 15 Development involving or adjacent to buildings of historic character or significance should
 - (a) be similar in roof form, fenestration, height, scale and proportion, and should be constructed of materials that complement and reinforce the historic character and design elements of existing buildings, and

- (b) not use highly reflective materials, glass curtain walls or bright external colours as they are not appropriate.
- 16 Pedestrian shelter and shade should be provided over footpaths through the use of structures such as awnings, canopies and verandas.
- 17 Development should incorporate appropriate landscaping plans and suitable tree plantings. Green walls, creepers or living plants in landscaping plans should be maintained in good condition.

PRECINCT SPECIFIC PROVISIONS

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this precinct.

Precinct 1 Bridge Street

- 18 Development should not be undertaken unless it is consistent with the desired character of the precinct.
- 19 Development involving or adjacent to sites containing buildings of historic interest should be designed so as to maintain the historic integrity and character of those existing buildings.
- 20 Additions and new constructions in the vicinity of a place of heritage significance should be in harmony with the existing structure using high quality design and materials.
- 21 Development should be primarily for shops, restaurants, cafes, tourism and tourist based retail, community and cultural facilities.
- 22 Developments that generate large volumes of traffic or that require expansive areas of vehicle parking should ensure proper management of both pedestrian and traffic flows, to the mainstreet, Bridge and River.
- 23 Advertisements should be designed to respect the integrity, heritage value and overall appearance of Bridge Street and in particular any heritage item.
- 24 Services and both mechanical plant and equipment, should not be visible from public areas including reserves.
- 25 The spatial character of Bridge Street, established by the width of the street relative to its generally one-to-two-storey building height, should predominate and be maintained.
- 26 Buildings located along Bridge Street should have a maximum height of 12 metres although greater building heights are considered appropriate if a proposed development displays both significant design merit and adequately reflects the desired character.
- 27 Appropriate car parking areas should be located in the rear part of new developments.
- 28 Buildings that are located at the intersection of Bridge Street and East terrace should:
 - (a) be medium to high scale developments of high quality
 - (b) be integrated with other land uses on the same site, with shops, cafes or restaurants located on lower levels, to increase street-level activation and on upper levels to provide surveillance to both Bridge Street and the River
 - (c) maintain the prominence and visibility of the Bridge, River Murray and mainstreet
 - (d) ensure the flow of traffic to and from Bridge Street be carefully managed, to minimise the impact on pedestrian movement along Bridge Street and South Terrace, and that that both pedestrian and traffic flow to the Bridge and River are not restricted. Well-designed pedestrian walkways to Sturt Reserve and the surrounding locality should also be provided

- (e) demonstrate a high quality of design that takes into consideration adjoining land uses and mitigates potential interface issues
- (f) be articulated and terraced to follow the slope of the land to maintain overall river and mainstreet views
- (g) contain appropriate car parking areas at the rear, with access from a secondary street.

Precinct 2 Sixth Street

- 29 Development should not be undertaken unless it is consistent with the desired character of the precinct.
- 30 New development and alterations to existing developments should enhance the existing form and character of this precinct.
- 31 Where possible new development should provide opportunities for public art, through the use of appropriate sculptures, murals and unique street furnishings.
- 32 Materials used in this precinct should complement and transition to those selected by the Marketplace.
- 33 New development and alterations to existing developments should ensure retractable bollards in the vicinity of private development to enable closure to vehicles between Bridge and Fifth Street for events and markets.
- 34 New development and alterations to existing developments should encourage activation of laneways and rears of buildings through the use of glass, appropriate design and access from laneways.
- 35 Buildings located along Sixth Street should have a maximum height of 12 metres although greater building heights are considered appropriate if a proposed development displays both significant design merit and adequately reflects the desired character.
- 36 Advertisements should be designed to respect the integrity, heritage value and overall appearance of Sixth Street and in particular any heritage item.