

**Emmett Property Pty Ltd C/- Phillip Brunning & Associates**

Demolition of existing 2 storey non-heritage listed building and construction of a 14 storey high tourist accommodation building comprising 60 dual-key serviced apartments (120 bedrooms), together with associated reception, management offices and sales, porte cochère, and staff car parking at ground level and conference facilities, gymnasium, business lounge and BBQ deck at first floor and signage

**98-100 (A500) North Terrace, Adelaide**

DA 020/A040/18

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**OVERVIEW**

<b>Application No</b>	020/A040/18
<b>Unique ID/KNET ID</b>	2018/11576/01 (12977796)
<b>Applicant</b>	Emmett Property Pty Ltd C/- Phillip Brunning & Associates
<b>Proposal</b>	Demolition of existing 2 storey non-heritage listed building and construction of a 14 storey high tourist accommodation building comprising 60 dual-key serviced apartments (120 bedrooms), together with associated reception, management offices and sales, porte cochère, and staff car parking at ground level and conference facilities, gymnasium, business lounge and BBQ deck at first floor and signage
<b>Subject Land</b>	98-100 (A500) North Terrace, Adelaide
<b>Zone/Policy Area</b>	Capital City Zone, Central Business Policy Area 13
<b>Relevant Authority</b>	State Commission Assessment Panel
<b>Lodgement Date</b>	11 May 2018
<b>Council</b>	City of Adelaide
<b>Development Plan</b>	Adelaide (City) Council Consolidated 20 June 2017
<b>Type of Development</b>	Merit
<b>Public Notification</b>	Category 1
<b>Referral Agencies</b>	Government Architect
<b>Report Author</b>	Ben Green, Consultant Planner
<b>RECOMMENDATION</b>	Development Plan Consent subject to conditions

**EXECUTIVE SUMMARY**

The Applicant seeks Development Plan Consent for demolition of an existing 2 storey office building and construction of a 14 storey tourist accommodation building comprising 60 dual-key serviced apartments (120 bedrooms), conference facilities, together with ground floor porte cochère (drop-off zone), staff car parking and signage at 98-100 North Terrace, Adelaide.

The application is a consent on merit form of development, and Category 1 for the purposes of public notification. The application was formally referred to the Government Architect and informally referred to the City of Adelaide Council for comment.

Referral responses raised a number of concerns relating to matters such as function, architectural expression and materiality. The Applicant's Architect has addressed the concerns raised by way of amendments to the proposed plans and provision of an accompanying written statement.

The design refinements have generally improved the functionality of the building along with appropriate means of access relevant to the rights of way applicable to the subject land, and although the materiality has not been amended to a significant degree, the proposal has been amended sufficiently to satisfy the intent of the subject Zone and Development Plan generally.

Overall the development is considered an appropriate response to the Development Plan policy seeking to reinforce the Central Business Policy Area's role as the pre-eminent economic, and cultural hub for the State, characterised by stylish tall buildings that provide a hard edge to the street.

The application is considered to satisfactorily address key planning, design and technical issues to a degree that is consistent with the intent of the Development Plan, and accordingly it is recommended that Development Plan Consent be granted, subject to conditions.



## **ASSESSMENT REPORT**

### **1. BACKGROUND**

#### **1.1 Strategic Context**

On 30 May 2017 the Minister for Planning approved the Capital City Policy Review (Design Quality) Development Plan Amendment introducing new policy intended to reinforce the importance of design quality for new development and provide guidance regarding built form responses to context and streetscape character.

#### **1.2 Pre-Lodgement Process**

The proposal did not participate in the Department of Planning, Transport and Infrastructure (DPTI)'s pre-lodgement service, precluding opportunities to consider referral agency and Council feedback or participate in independent Design Review.

### **2. DESCRIPTION OF PROPOSAL**

The application is for the demolition of an existing 2 storey non-heritage listed office building and construction of a 14 storey tourist accommodation building comprising 60 dual-key serviced apartments (120 bed rooms), conference facilities with associated ground floor porte cochère (drop-off zone), staff car parking and signage.

The ground floor would include a reception and main lobby area accessible from North Terrace, a second lobby accessed via the porte cochere, 2 staff car parks, main linen room, fire pump, electrical switch room, rubbish store, transformer room, manager's office, sales room and amenity areas.

The porte cochère would provide vehicle access and visitor drop-off facility via Bernath Place that is directly accessible from North Terrace. 2 staff car parks would be provided in a secure garage area at the ground floor adjacent the porte cochère.

The first floor would include a conference room, open air BBQ deck, business lounge together with associated kitchen and storage areas, a gymnasium, staff room, mechanical services room, fire / water tank room and amenity areas.

The second floor through to fourteenth floor are each designed to contain 5 dual-key apartments, housekeeping rooms, lifts and stairwells. 3 of these floors (floors 6-8) include a combination of standard and disability accessible floor plans.

The development would be operated by the Quest Group. The Quest Apartment Hotels website indicates that their accommodation is a '*...home or office away from home, complete with separate living, dining and sleeping areas, separate work stations and fully equipped kitchens and laundry facilities*', and '*...attracts quests who stay for days, weeks or months at a time.*'

The South Australian Planning Policy Library outlines that both 'serviced apartments' and 'serviced holiday apartments' are not accepted terms within the library and suggests that a 'hotel', 'motel', 'residential flat building', or 'tourist accommodation' be used. Accordingly, as the proposal is principally catering for short and long-term (non-permanent) stays, the proposed development is considered to principally be for 'tourist accommodation'.

The conference room, business lounge, and other associated facilities are not highlighted as being inextricably linked to the tourist accommodation use, meaning these facilities could operate independently of the tourist accommodation. Despite this, these facilities are considered to be reasonably typical of this type of short to long-term accommodation generally for patrons of the building and as such are incidental and ancillary to a building of this nature.

The application does not detail the hours of operation or maximum number of patrons for these uses, however it is anticipated that the latter will be dictated by the building's requirement to satisfy the Building Code of Australia, should the proposed development be acceptable on planning grounds.

Application plans are contained in **Attachment 1** and summary of the proposal is provided in the following table:

<b>Land Use Description</b>	Tourist Accommodation
<b>Building Height</b>	14 storeys, 52.6 metres (above ground) (87.78 AHD)
<b>Description of levels</b>	<p><u>Ground</u>: Lobby, reception, administration offices, linen store, waste storage room, services areas, porte cochère and staff car parking (2 spaces)</p> <p><u>Levels 1</u>: conference room, business lounge, lobby, external deck, kitchen, gymnasium, staff rooms, guest laundry, amenities, storage rooms and service areas</p> <p><u>Levels 2-14</u>: 5 dual-key apartments, house keep rooms, lift lobby and stairwells</p> <p><u>Rooftop</u>: hot water storage tank, stair pressurization fans and air-conditioning condenser units</p>
<b>Serviced Apartment floor area (excluding balconies)</b>	<p><u>Unit 1</u>: 75m<sup>2</sup></p> <p><u>Unit 2</u>: 79m<sup>2</sup></p> <p><u>Unit 3</u>: 75m<sup>2</sup></p> <p><u>Unit 4</u>: 75m<sup>2</sup></p> <p><u>Unit 5</u>: 77.3m<sup>2</sup></p>
<b>Site Access</b>	Porte cochère would provide vehicle access and visitor drop-off facility via Bernath Place
<b>Car Parking</b>	2 car parking spaces for staff, 1 drop-off (standing) space in porte cochère
<b>Bicycle Parking</b>	10 bicycle parking racks (5 each for staff and visitors)
<b>Encroachments</b>	Pedestrian canopy over North Terrace

### 3. SITE AND LOCALITY

#### 3.1 Site Description

The subject land is comprised of a single allotment which retains an area of 675 square metres with frontage to North Terrace measuring 19.5 metres. The subject land is formally described as follows:

Lot No	Plan No	Street	Suburb	Hundred	Title
500	D37352	North Terrace	Adelaide	Adelaide	5174/32

The subject land is located on the southern side of North Terrace, east of the Morphet Street bridge, and is more commonly identified as 100 North Terrace. The site is currently occupied by the 2-storey former 'Metters Limited' building which has previously been used as an office.

The existing building is not listed as either a Local or State Heritage Place or a Contributory Item. The subject land is afforded the benefit of a series of easements

and rights of way over portions of surrounding land, including Bernath Place to the east which is proposed to be utilised for the principal means of vehicular access.



**Figure 1 – Former Metters Limited building**

The subject land is located at the northwest corner of the Primary Pedestrian Area as shown in Development Plan Map Adel/1 (Overlay 2A), which identifies an existing pedestrian link along the land's western boundary between North Terrace and Hindley Street, through the adjacent 'Cosmopolitan' car park and former Greater Union Cinema.

### **3.2 Locality**

The locality is characterised by a variety of established and contemporary multi-storey buildings including the Adelaide Convention Centre and Riverside Centre to the north, and recent tourism accommodation developments of comparable height and scale on the allotments east and west of the subject land (Oaks Embassy & Oaks Horizons).



**Figure 2 – Location Map**



#### 4. STATUTORY REFERRAL BODY COMMENTS

The following agency constitutes a mandatory referral in accordance with Schedule 8 of the *Development Regulations 2008*. The Panel must have regard to the advice received, a copy of which is provided in **Attachment 4**.

##### 4.1 Government Architect

The Government Architect (GA) reviewed the application details and identified the following elements that should be considered to ensure the most successful design outcome is achieved:

- Further holistic review of the proposed architectural expression and materiality, with consideration given to materials with finish and colour that are integral to the fabric, rather than an applied finish. This should be supported by a material samples board and schedule;
- Further consideration of the articulation of the west elevation, with a view to achieving greater visual interest when viewed from the adjoining development;
- Further consideration of ground floor internal planning and the car parking strategy, with a view to creating greater visual and physical connectivity between the two lobby spaces, and to maximise activation of Bernath Place;
- Further consideration of level one internal planning, including the location of the conference room kitchen, and light access to staff and corridor spaces; and
- Further review of the car parking strategy, cognisant of publicly accessible car parking in close proximity.

The applicant's architect provided a set of amended plans together with a written response to the matters raised, as discussed in further detail in Section 8.

#### 5. COUNCIL COMMENTS

The City of Adelaide provided comment on technical matters including traffic, roads and footpaths, stormwater management, lighting and electrical, waste, street trees and landscaping, which could generally be addressed as conditions or at the Council's discretion in terms of modification and/or reinstatement of Council infrastructure adjacent the subject land.

Council also made the following comments on planning issues:

- Residential amenity – notwithstanding that easements provide separation between the subject site and adjacent development to the west, the setbacks to windows/balconies from boundaries, (particularly to the west (less than 2 metres)), fails to provide an adequate setback (noting there is a 3-metre minimum prescribed within the Development Plan) to protect the amenity of the occupants of the adjoining sites. This is an undesirable outcome given internal amenity for occupants will be impacted in regards to outlook and access to light.
- Active street frontages – provision of active uses at ground and first floor to North Terrace is commended, as is the provision of a canopy to North Terrace.
- Encroachments – The development incorporates an encroachment of 2500mm deep by 19440mm wide for the canopy over the North Terrace Road Reserve – varying clearance to footpath between 3500 – 4000mm. This is a minor departure from Council's Encroachment Policy, which has been approved by the Associate Director, Planning and Development.

The City of Adelaide requested that 4 conditions be included in the decision should the State Commission Assessment Panel resolve to grant consent to the development. These conditions have been included in the below recommendation.



The applicant’s architect provided responses to some of the technical matters raised in Council’s referral and other issues as discussed in Section 8. A full copy of the Council’s referral response is included in **Attachment 5**.

## 6. PUBLIC NOTIFICATION

The application is a Category 1 form of development pursuant to PDC 40(a) of the Capital City Zone and as such public notification was not required.

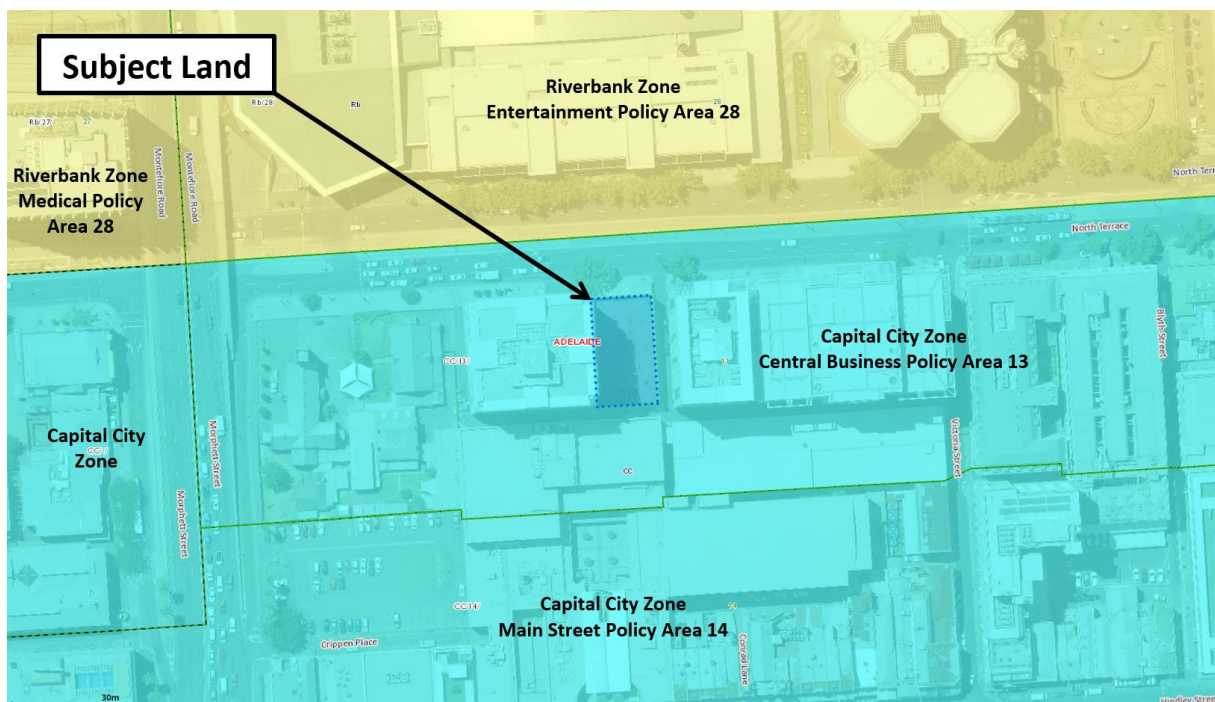
## 7. POLICY OVERVIEW

The subject site is located within the Capital City Zone and the Central Business Policy Area 13 as described within the Adelaide (City) Council Development Plan Consolidated 20 June 2017. Relevant planning policies are contained in **Attachment 7** and are summarised in the following sections:

### 7.1 Central Business Policy Area 13

Development in the subject Policy Area should include the highest concentration of employment, hospitality and entertainment activities with increased opportunities for residential, student and tourist accommodation to contribute to the area’s role and function as the State’s premier business district.

Stylish and evocative architecture exhibiting innovative, high-quality design is expected, featuring tall and imposing buildings presenting hard edges to the street. Localised character and legible differences between streetscapes are encouraged with integration of built form with the public realm.



**Figure 3 – Zoning Map**

### 7.2 Capital City Zone

The Capital City Zone is recognised as the principal focus for economic, social and political life of metropolitan Adelaide and the State. A wide range of employment, community, educational, entertainment, tourism and residential land uses are envisaged to reinforce the City as a place of diversity and vibrancy.

The City’s distinctive grid pattern will be reinforced through the creation of attractive boulevards characterised by buildings aligned to the street pattern, particularly at

street level. In Addition, development along North Terrace should reinforce the predominant scale and 'City wall' character of the Terrace frontage.

### 7.3 Council Wide

The Council Wide provisions provide direction on the desire for increased levels of activity and interest at ground level; a high standard of design; appropriate bulk and scale of buildings, safe and convenient servicing of sites, positive contribution to streetscapes and provision of employment opportunities.

### 7.4 Overlays

#### 7.4.1 Adelaide City Airport Building Heights

Referral to the Department of Transport, Regional Development and Cities through Adelaide Airport Limited is required where a development would exceed the Obstacle Limitation Surface (OLS) contours shown on Development Plan Map Adel/1 (Overlay 5).

In this instance, the OLS contour shown on Map Adel/1 (Overlay 5) is identified as 90 metres AHD. At a height of 52.6 metres above ground (or 83.92 metres AHD) the development would not penetrate the OLS contour and accordingly no referral is required.

## 8. PLANNING ASSESSMENT

The application has been assessed against the relevant provisions of the Adelaide (City Council) Development Plan, which are contained in **Attachment 7**.

### 8.1 Quantitative Provisions

	Development Plan Guideline	Proposed	Guideline Achieved	Comment
<b>Building Height</b>	No prescribed height limit	14 storeys, 52.6 metres (above ground)	YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> PARTIAL <input type="checkbox"/>	
<b>Car Parking</b>	No minimum or maximum recommended provision of car parking in the Capital City Zone	2 staff car parks	YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> PARTIAL <input type="checkbox"/>	
<b>Bicycle Parking</b>	1 per 20 employees 2 per 40 rooms + 1 for every additional 40 rooms =5 spaces	10 bicycle parking racks (5 each for staff and visitors)	YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> PARTIAL <input type="checkbox"/>	
<b>Front Setback</b>	Buildings should be positioned regularly on the site and built to the street frontage (except where a setback is required for outdoor dining or to provide a contextual response to a heritage place)	Zero setback	YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> PARTIAL <input type="checkbox"/>	
<b>Rear Setback</b>	As above	Zero setback	YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> PARTIAL <input type="checkbox"/>	
<b>Side Setback</b>	As above	Setback varies from zero to 6.8 metres	YES <input type="checkbox"/> NO <input type="checkbox"/> PARTIAL <input checked="" type="checkbox"/>	Refer to Section 8.4

## **8.2 Land Use**

The Capital City Zone is the economic and cultural focus of the State and includes a range of employment, community, educational, tourism and entertainment facilities. Likewise, the Central Business Policy Area 13 is identified as the pre-eminent hub for these activities.

The Capital City Zone outlines a range of envisaged forms of development, including but not limited to: 'hotel', 'motel', 'serviced apartment', 'residential flat building', 'student accommodation' and 'tourist accommodation'. Although not all of these terms are applicable to the proposed development, it is suffice to say that the Zone contemplates an array of accommodation types at various densities.

Furthermore, Council-wide principles encourage development to expand upon existing or create new tourism activities to maximise employment and the long-term economic, social and cultural benefits of developing the City as a competitive domestic and international tourist destination.

The proposal is purported to be in response to an identified demand for additional premium economic accommodation within the City, and it is agreed that the proposed location will afford guests with a high level of amenity and function in terms of access to City attractions.

The proposed tourist accommodation use together with ancillary conference facilities and the like are acceptable and supported uses in response to Development Plan policy and the context of the locality with other like uses adjoining the proposal.

## **8.3 Building Height**

The subject land is located within a portion of the Capital City Zone that has no prescribed height limit, however the Development Plan outlines that buildings should be of a height that ensures airport operational safety is not adversely affected.

Airport Building Heights MAP Adel/1 (Overlay 5) of the Development Plan stipulates that a referral is to be undertaken to Adelaide Airport Limited where a development would exceed Obstacle Limitation Surface (OLS) contours identified on the map. In this instance, the OLS value is between 90 and 100 metres AHD.

At the height of 52.6 metres above natural ground level equating to 87.78 metres AHD, the building would not penetrate the identified OLS contour. Any crane operations required during construction would likely encroach through the OLS which will require separate airspace approval.

The height of the proposed building is comparable and compatible with the 2 adjoining buildings on North Terrace, and overall the proposal is considered to comply with the Development Plan's requirements in this regard.

## **8.4 Design and Appearance**

High-scale development is envisaged in the Capital City Zone and Central Business Policy Area in the form of tall, imposing buildings with high street walls that provide a hard edge to the street and are of the highest design quality. A high standard of external appearance and visual interest are anticipated through built form articulation, use of quality materials and finishes and avoidance of large blank facades.

An interesting pedestrian environment is also to be created at ground floor through arrangement of fenestration, façade openings, verandahs, awnings and other design features that provide weather protection. The desired character of the Capital City Zone also seeks to ensure that new development will achieve high design quality by being: contextual, durable, inclusive, sustainable and amenable.

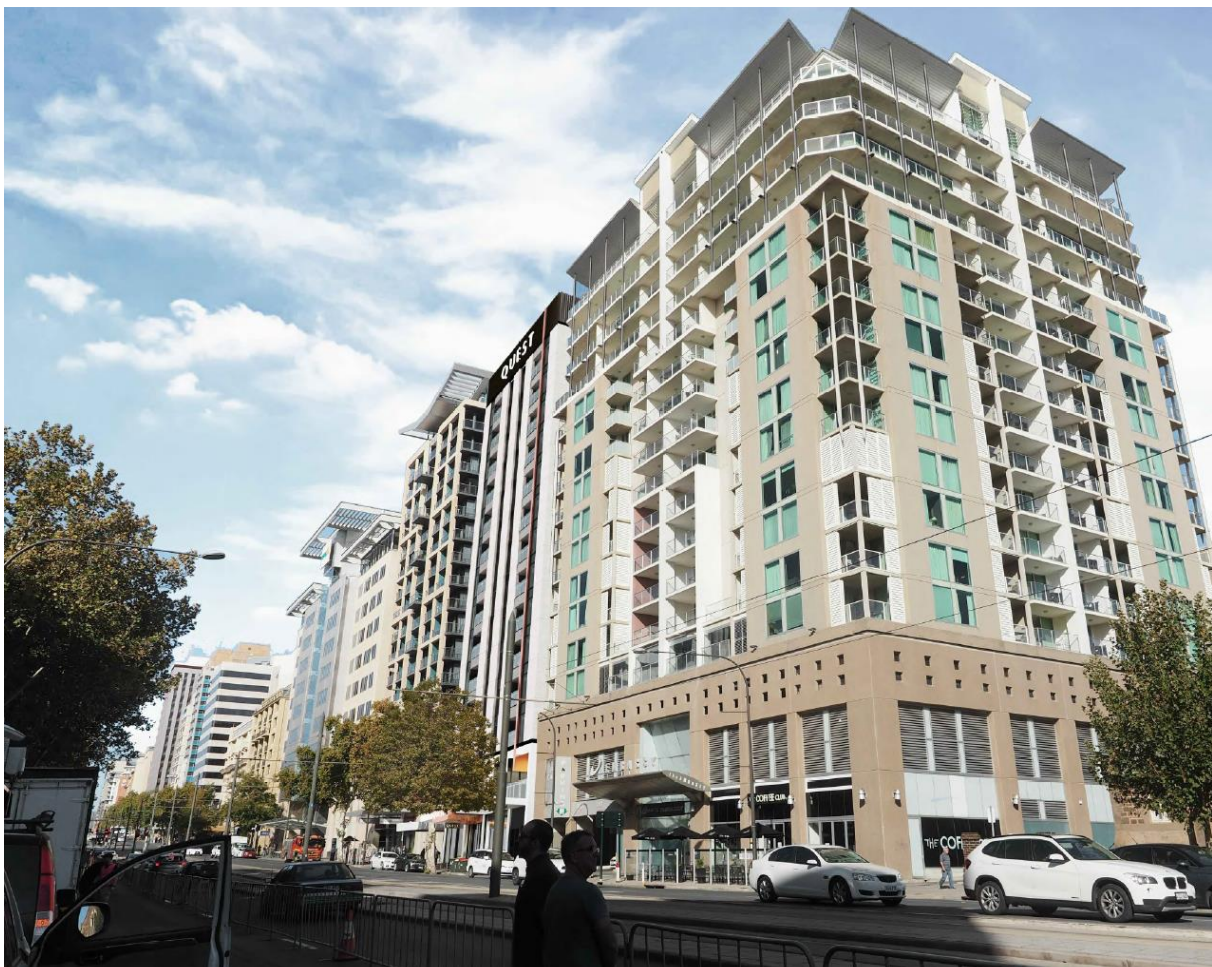


The proposed built form is characterised by precast concrete façade elements that reinforce a vertical expression and glass balustrades. The expression extends to the eastern elevation, which includes balconies and windows that overlook Bernath Place. External materials over upper levels consist of colour stained precast concrete panels, powder coated aluminium frame windows, louvres and cladding, together with metallic copper cladding accent strips.

The proposed development is positioned on the north and south site boundaries and does not include a physical podium setback to North Terrace, but rather defines the base of the building through the architectural expression. The ground and first floor levels have a higher-grade finish to improve pedestrian amenity and to assist in achieving a podium-like appearance. The principle construction material at this level is white coloured precast concrete columns and beams with a honed finish together with extensive aluminium framed glazing and doors.

The Oaks Embassy building to the west is setback approximately 1.5 metres from the common boundary, and the proposed development is to be built to this common boundary fronting North Terrace for a length of 12.75 metres (37% of the boundary length), and then setback 4.2 to 6.85 metres to the middle and rear of the site.

The Oaks Embassy contains windows and balconies on its north, south and eastern elevations, with the latter presenting some design challenges and interface issues for the subject land, however on balance the design outcome was supported by the Government Architect notwithstanding the adjacency issues and on the basis that the design approach infills a gap in the streetscape. The proposed outcome is considered to be acceptable in light of the desired character of the Zone which states upper level setbacks are not envisaged within the Central Business Policy Area.



**Figure 4 – Streetscape view from west**



As identified in Section 4, the Government Architect outlined a number of design elements that should be considered to ensure the most successful design outcome is achieved and in light of these comments, the application made a number of refinements to the plans and provided a written response.

The Government Architect encouraged holistic review of the proposed architectural expression and materiality, with consideration to be given to the use of materials with a finish and colour that are integral to the fabric rather than an applied finish. Revised plans illustrate a façade comprised of colour stained precast concrete panels, which when considering the applicant’s supporting written statement is understood to represent an applied finish post construction.

It is understood that this method is proposed to ensure uniformity and a consistent finish on the upper levels. As the proposed stain is almost white in colour, the outcome is not too dissimilar to light colour impregnated concrete with the ability to provide ongoing ease of maintenance to ensure longevity of the building façade.

In summary, the proposed building would introduce a suitable built form edge to North Terrace and make an appropriate contribution to the Zone and Policy Area, with a size and mass that is comparable to the adjoining buildings including the presentation of a podium at the lower level. Overall, the proposed development is considered to satisfy the requirements of the Development Plan in this regard.

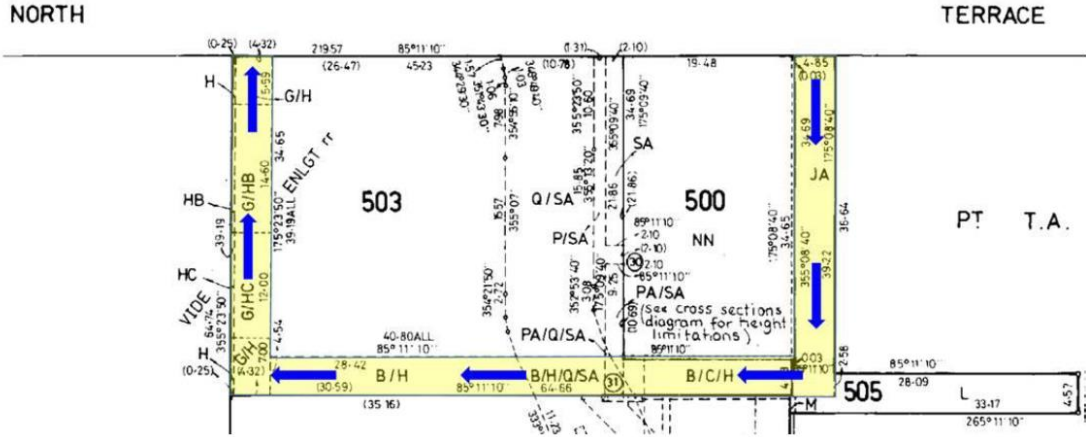
**8.5 Site Access and Vehicle Parking**

The Capital City Zone anticipates development of a comprehensive, safe and convenient traffic network focusing on new linkages to public and private land, and connections between important destinations and public transport including quality routes for bicycles and shared-use paths.

The application includes a supporting statement from GTA Consultants who were engaged to outline the anticipated transport implications of the proposed development including: the existing traffic and parking conditions surrounding the site; the suitability of the proposed car parking and bicycle parking in terms of supply and layout; traffic generation characteristics of the proposed development; proposed access arrangements for the site; and, the transport impact of the development proposal on the surrounding network, some of which are discussed in more detail in the following sections of the report.

**8.5.1 Rights of Way**

Vehicular access to and from the site would occur via a series of rights of way afforded to the subject land over the surrounding lane ways. The subject land is illustrated as Allotment 500 in the below diagram which illustrates the intended main flow of traffic along the rights of way.



**Figure 5 – Site Access**

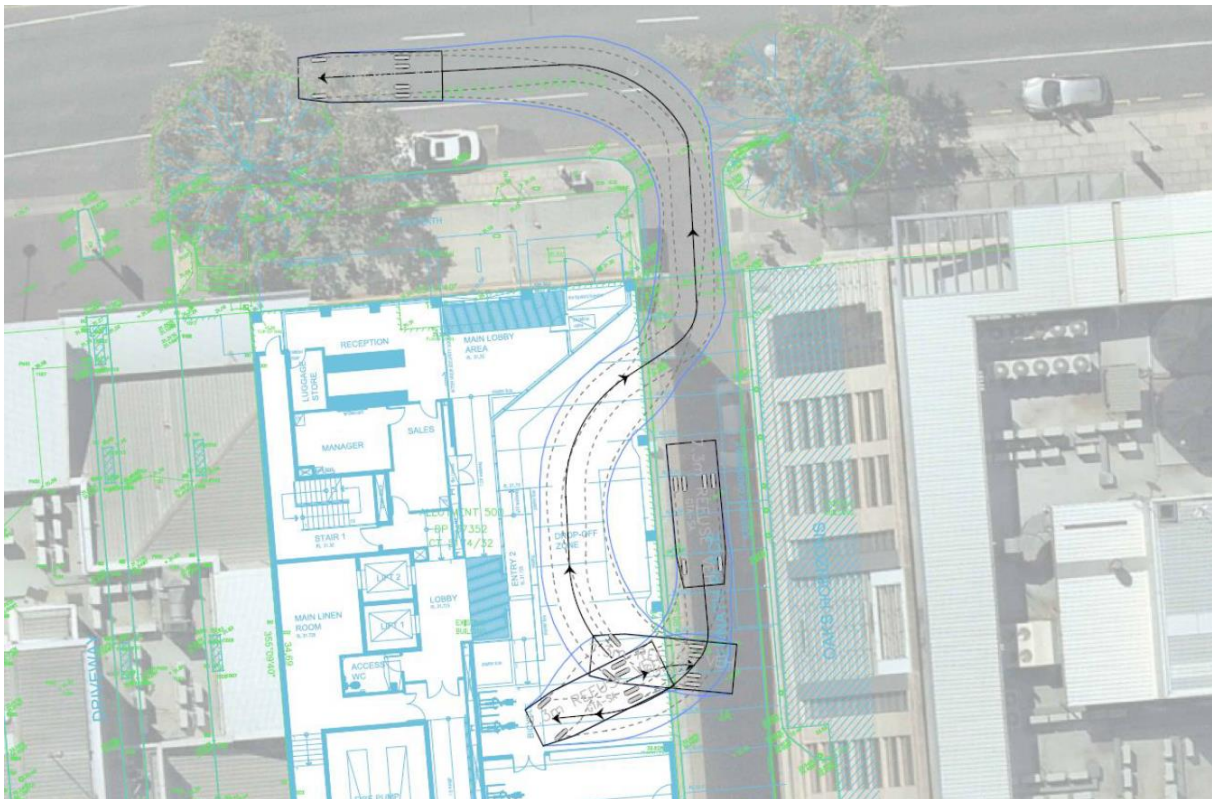
The rights of way are subject to a number of limitations, which are understood to relate to the height above ground level and which will have limited impacts with the proposed design of the building.

The subject land has rights of way over portions HB, HC, D, JA and H, with JA being Bernath Place to the east of the subject land. Documentation provided by the applicant illustrated egress via Q/SA which are the exit lanes of the adjoining multi-level car park, however upon further examination the subject land does not have rights over that land.

**8.5.2 Site Access**

Vehicular access and egress to/from the subject land would be via the abovementioned rights of way. Vehicles would enter Bernath Place in a forward direction from North Terrace and have the ability to enter the porte cochère for passenger drop-off and pick-up, and proceed in a forward direction through the adjoining laneway before returning to North Terrace on the western side of the Oaks Embassy. GTA has undertaken a swept path assessment of this route for a B99 light vehicle which illustrates the manoeuvre can be achieved in a continual forward direction.

Waste is proposed to be collected outside of peak periods every 3 days. GTA has demonstrated a possible access route for a 7.3 metre refuse truck to enter and exit the site in a forward direction via the porte cochère and Bernath Place. The refuse truck would enter Bernath Place via North Terrace, conduct a series of turns at the southern side of the porte cochère and proceed against the flow of the traffic in the porte cochère and back to North Terrace via Bernath Place as shown below.



**Figure 6 – Waste vehicle collection manoeuvres**

Whilst this manoeuvre is not ideal given the potential conflict with other vehicles entering the porte cochère, the manoeuvre is anticipated to occur outside of peak

traffic times and could be managed by temporarily restricting access into the porte cochère by appropriate traffic control measures during collection times.

### **8.5.3 Vehicle Parking**

The Capital City Zone identifies that car parking should be provided on site in accordance with Development Plan Table Adel/7, which does not specify a minimum or maximum number of car parking spaces for tourist accommodation within the Central Business Policy Area. That being said, the proposal includes 2 on-site car parking spaces accessible at the southern end of the porte cochère out of a duty of care to employees working late night and early mornings.

A total of 10 bicycle parking spaces are proposed, incorporating 5 visitor and 5 staff parking spaces. Visitor bicycle parking spaces are proposed to be located adjacent to the entry to the second lobby within the porte cochère, while the staff parks will be located inside the building next to the staff car parks.

Overall, whilst there are some reservations in relation to the refuse vehicle turn paths (causing conflict with drop off and pick up traffic movements from North Terrace), it has been demonstrated that it is possible to achieve appropriate vehicular movements to a sufficient degree that the proposal is considered to generally satisfy Development Plan requirements in relation to site access and vehicle parking.

## **8.6 Environmental Factors**

Development in the Council area should be designed to ensure that public safety and security are maintained and that essential services are provided without unreasonable disruption or disturbance to the community, micro-climatic impacts are minimised and that new built form is compatible with long-term environmental sustainability.

The development has been designed to adequately manage environmental impacts and consequence within the locality as discussed in the following sections.

### **8.6.1 Crime Prevention**

Safe and convenient pedestrian movement should be facilitated by a clearly designated network of east-west and north-south links connected to public transport and areas of public activity, through the promotion of natural surveillance and other suitable environmental design strategies.

Council recommended conditions of consent to ensure sufficient lighting to the awning at street level on North Terrace, and to the vehicle and pedestrian entries on Bernath Place during the hours of darkness. Together with the active street frontage and anticipated increase in visitation to the site, adjacent right of way and areas between the buildings, these conditional provisions are expected to provide an environment that would not be conducive to instances of crime or anti-social behaviour, and the proposal would generally comply with the requirements of the Development Plan in this regard.

### **8.6.2 Noise Emissions**

Development Plan policy encourages noise-sensitive development incorporating adequate noise attenuation measures to provide occupants with reasonable amenity when exposed to noise sources such as entertainment premises, commercial centres and activities contemplated nearby.

The application did not include specialist advice in relation to management of noise impacts for any aspect of the proposed development, be that for the proposed use or any associated plant equipment servicing the use.

Whilst the primary use of the land for tourist accommodation purposes is not expected to generate excessive noise, nor would it be particularly vulnerable to noise impacts arising in this setting given the adjoining buildings of similar use, a standard condition is proposed to be assigned to any consent granted requiring the provision of acoustic screening for mechanical plant and equipment to ensure acceptable noise levels and satisfy the intent of noise emissions policy.

### **8.6.3 Waste Management**

Council Wide Principle of Development Control 103 seeks to ensure that development greater than 2,000 square metres in floor area is designed to manage waste through the use of a dedicated area for collection and sorting of general waste, recyclables and organic waste.

The proposal includes a combined rubbish store at the rear of the building, accessible from Bernath Place via the combined staff car parking / loading area. The loading area is at the same location as the existing Oak Horizon waste storage room to the east.

The applicant has demonstrated calculations based on the average waste generated from the proposed development using the 'Design Guide for Residential Recycling Appendix 2: Waste Resource Generation Rates' produced by the City of Adelaide.

All waste is proposed to be collected and transported into the ground floor waste room by cleaning staff, and the ground floor waste storage room is purported to have adequate capacity for the building load requirements based on waste being collected every 3 days, outside of peak times.

A waste removal company would be contracted by the operator to manage waste collection every 3 days, with the waste bins to be wheeled out to Bernath Place via the staff parking / loading area on the ground floor. The applicant indicates that the waste removal will be at the same time as the Oaks Horizons building to the east. The ability for the refuse vehicle to access the site is discussed above.

The applicant has made suitable allowances for the development's waste management requirements which would align with the Development Plan's envisaged outcomes. To further reduce safety risks associated with service vehicle movements around the site, a condition is recommended that there be a requirement that waste collection is to be scheduled outside of peak periods.

### **8.6.4 Energy Efficiency**

Buildings in the Council area should minimise the need for energy use for heating, cooling and lighting through design measures specified in the Development Plan's Council Wide Environmental policy section. The applicant engaged Lucid Consulting Australia to outline the Ecologically Sustainable Design (ESD) initiatives and features incorporated into the concept design for the proposed development.

The outlined ESD initiatives consider renewable energy, passive design, energy efficiency, energy management and water efficiency. The information provided has substantiated the applicant's intent to provide development which would meet the Development Plan's energy efficiency provisions, and contribute to environmentally sustainable outcomes more generally.

### **8.6.5 Wind Analysis**

The Development Plan requires buildings that would exceed 21 metres in height and built at the street frontage should be designed to minimise wind tunnel



effects, particularly where there is potential for interference to ground level activity and uses that spill onto the footpath.

The applicant engaged Windtech to undertake a pedestrian wind environment report which presents an opinion on the likely impact of the proposed design on the local wind environment to the critical outdoor areas within and around the development.

The result of the assessment indicates the wind conditions within the various outdoor trafficable areas of the subject development are expected to be acceptable for their intended uses or better than equivalent existing wind conditions. The pedestrian footpath benefits from the shielding provided by the proposed and neighbouring buildings, and the proposed canopies along the North Terrace frontage would mitigate the potential down-wash wind effects off the building façade.

The first floor BBQ deck area and upper floor private balconies would benefit from shielding from the neighbouring buildings, which would be further increased given their recessed design into the building footprint. Overall the proposed development is considered to be generally consistent with Development Plan policy in this regard.

#### **8.6.6 Site Contamination**

Council Wide policy recommends that where there is evidence or reasonable suspicion that land may have been contaminated, development should only occur where it is demonstrated the land can be made suitable for its intended use prior to commencement of that use.

The application provided no evidence of any measures undertaken to ascertain whether potential exists for site contamination caused by previous use of the land. Having said this, the existing built form covers the land in its entirety, and the proposed development would also occupy the entire site. It is on this basis that any concerns relating to site contamination would be addressed.

### **8.7 Signage**

Advertisements in the Council area should use simple graphics and be restrained in their size, design and colour to present an overall consistency of signage along individual street frontages. Signage is restricted to 4 separate elements as follows:

- a 1500mm high x 150mm deep illuminated lettering signage reading as 'QUEST' set on the 'dark bronze' colour power coated aluminium parapet;
- a 500mm high x 2990mm wide x 50mm deep fabricated aluminium lettering with opal acrylic face panel and face illuminated lettering reading as 'QUEST' on the canopy protrusion above the main entry facing North Terrace;
- a 435mm high x 1200mm wide powder coated under awning signage with illuminated opal acrylic lettering reading as 'QUEST North Terrace' beneath the awning at the main entrance from North Terrace; and
- a 300mm high x 827.5 wide powder coated under awning sign with illuminated opal acrylic lettering reading as 'QUEST North Terrace' beneath the porte cochere.

The location and dimensions of the proposed signage is considered to be generally consistent with the requirements of the Development Plan as the lettering is simple, restrained, sympathetic to the overall design and logically placed on the building façade. The proposed development is considered to generally satisfy the Development Plan in this regard.

## **9. CONCLUSION**

Overall the development would contribute to the desired character of the Central Business Policy Area and Capital City Zone by introducing additional accommodation opportunities for tourist and travellers to the City over a short or long-term, and from a land use perspective the proposal is considered to be fundamentally appropriate on the subject land and befit the character of this part of the zone and policy and North Terrace generally.

The assessment noted reservations in relation to vehicle access arrangements, particularly for large vehicles including waste refuse vehicles. It is acknowledged however that the reservations could be mitigated via appropriate onsite management during waste collection times.

The materiality of the building is another matter that arose during the assessment as the applicants seek to achieve an applied finish to the concrete panels, as opposed to the Government Architect's preference for a colour that is integral to the material fabric.

Given that the façades would utilise an array of colours and materials and the fact that the lower floors incorporate a higher-end material finish that is readily visible from the public realm, the proposed materiality is not considered to be significantly at variance with the relevant Development Plan policy, and moreover the proposed materials are considered to generally comply with the intent of achieving a hard built form edge and 'City wall' character to North Terrace as envisaged by the Capital City Zone.

It is understood that this method is proposed to ensure uniformity and a consistent finish over upper building levels. As the proposed stain is almost white in colour, the outcome is not too dissimilar to light colour impregnated concrete with the ability to provide ongoing ease of maintenance to ensure longevity of the building façade.

Overall the development is considered to be an appropriate response to Development Plan policy and is likely to reinforce the Policy Area's role and function as the pre-eminent economic and cultural hub for the State. The proposed development is considered to generally comply with the provisions of the Development Plan, and therefore conditional Development Plan Consent is recommended.

## **10. RECOMMENDATION**

It is recommended that the State Commission Assessment Panel:

- 1) RESOLVE that the proposed development is NOT seriously at variance with the policies in the Development Plan.
- 2) RESOLVE that the State Commission Assessment Panel is satisfied that the proposal generally accords with the related Objectives and Principles of Development Control of the Adelaide (City) Development Plan.
- 3) RESOLVE to grant Development Plan Consent to the proposal by Emmett Property Pty Ltd C/- Phillip Brunning & Associates for the demolition of existing building and construction of a 14 storey tourist accommodation building comprising 60 dual-key serviced apartments (120 bedrooms), conference facilities, together with associated porte cochère, staff car parking and signage at 98-100 North Terrace, Adelaide subject to the following reserved matters and conditions of consent.

## **PLANNING CONDITIONS**

1. That except where minor amendments may be required by other relevant Acts, or by conditions imposed by this application, the development shall be established in strict accordance with the details and following plans submitted in Development Application No 020/A040/18:

<b>Title</b>	<b>Drawing No.</b>	<b>Date</b>
Location Plan	SK63	07/05/18
Ground Floor Plan	SK113A	08/08/18
First & Second Floor Plan	SK114A	08/08/18
Typical Apartment Plan	SK103	05/06/18
Roof Plan	SK104	05/06/18
North Elevation	SK115	17/07/18
East Elevation	SK116A	08/08/18
South Elevation	SK117	17/07/18
West Elevation	SK118	17/07/18
North Elevation - Streetscape	SK112A	07/08/18

2. The recommendations details in the Transport Impact Assessment by GTA Consultants (SA) Pty Ltd (Reference S138314) dated 29 May 2018 together with the updated response dated 9 August 2018 by GTA Consultants (SA) Pty Ltd, forming part of this consent shall be fully incorporated into the development to the reasonable satisfaction of the State Commission Assessment Panel.
3. All driveways, vehicle entry and manoeuvring areas shall be designed and constructed in accordance with Australian Standards (AS/NZ 2890.1:2004 and AS/NZS 2890.6:2009) and be constructed, drained and paved with bitumen, concrete or paving bricks in accordance with sound engineering practice and appropriately line marked in accordance with AS 2890.1 and AS1742 to the reasonable satisfaction of the State Commission Assessment Panel prior to the occupation or use of the development.
4. All bicycle parks shall be designed and constructed in accordance with Australian Standard 2890.3-2015.
5. Clear sight lines for users of the development shall be provided to ensure pedestrian safety along the North Terrace footpath and shall be provided at all times in accordance with AS/NZS 2890.1:2004 Off-street car parking.
6. The hours for waste collection and service vehicles (operated by private contractor/s) to enter and exit the subject land shall be scheduled to occur outside of peak usage periods.
7. Air conditioning or air extraction plant or ducting shall be acoustically screened such that no unreasonable nuisance or loss of amenity is caused to residents and users of properties in the locality to the reasonable satisfaction of the State Commission Assessment Panel.
8. All stormwater design and construction shall be in accordance with Australian Standard AS/NZS 3500.3:2015 (Part 3) to ensure that stormwater does not adversely affect any adjoining property or public road.
9. The finished floor level of the ground floor level at the entry points to the development shall match the existing footpath unless otherwise agreed to in writing by the City of Adelaide, to the satisfaction of the State Commission Assessment Panel
10. Landscaping shown on the approved plans shall be established prior to the operation of the development and shall be maintained and nurtured at all times with any diseased or dying plants being replaced.
11. All external lighting on the site shall be designed, constructed and installed to conform to Australian Standard - AS 4282-1997 (Control of the obtrusive effects of outdoor lighting).

12. Lighting shall be installed to the awning at street level on North Terrace in accordance with Council's guidelines entitled 'Under Verandah/Awning Lighting Guidelines' prior to the occupation or use of the Development. Such lighting shall be operational during the hours of darkness at all times to the reasonable satisfaction of the State Commission Assessment Panel.
13. Lighting shall be provided to the vehicle and pedestrian entries on Bernath Place and shall be operational during the hours of darkness at all times to the reasonable satisfaction of the State Commission Assessment Panel.
14. All Council, utility or state-agency maintained infrastructure (i.e. roads, kerbs, drains, crossovers, footpaths etc.) that is demolished, altered, removed or damaged during the construction of the development shall be reinstated to Council, utility or state agency specifications. All costs associated with these works shall be met by the proponent.

### **ADVISORY NOTES**

- a. This Development Plan Consent will expire after 12 months from the date of this Notification, unless final Development Approval from Council has been received within that period or this Consent has been extended by the State Commission Assessment Panel.
- b. The applicant is also advised that any act or work authorised or required by this Notification must be substantially commenced within one (1) year of the final Development Approval issued by Council and substantially completed within three (3) years of the date of final Development Approval issued by Council, unless that Development Approval is extended by the Council.
- c. The applicant has a right of appeal against the conditions which have been imposed on this Development Plan Consent. Such an appeal must be lodged at the Environment, Resources and Development Court within two months from the day of receiving this notice or such longer time as the Court may allow. The applicant is asked to contact the Court if wishing to appeal. The Court is located in the Sir Samuel Way Building, Victoria Square, Adelaide (telephone number 8204 0289).
- d. Development Approval will not be granted until Building Rules Consent and an Encroachment Consent have been obtained. A separate application must be submitted for such consents. No building work or change of classification is permitted until the Development Approval has been obtained.
- e. As work is being undertaken on or near the boundary, the applicant should ensure that the boundaries are clearly defined, by a Licensed Surveyor, prior to the commencement of any building work.
- f. A Construction Environment Management Plan (CEMP) shall be prepared in collaboration with the City of Adelaide (Council) and implemented throughout construction in accordance with current industry standards – including the Local Nuisance and Litter Control Act 2016, the EPA publications "Handbook for Pollution Avoidance on Commercial and Residential Building Sites – Second Edition" and, where applicable, "Environmental Management of On-site Remediation" – to minimise environmental harm and disturbance during construction. The management plan should incorporate, without being limited to, the following matters:
  - timing, staging and methodology of the construction process and working hours;
  - traffic management strategies;
  - control and management of construction noise, vibration, dust and mud;



- management of infrastructure services during construction and re-establishment of local amenity and landscaping;
  - stormwater and groundwater management during construction;
  - site security, fencing and safety and management of impacts on local amenity for residents, traffic and pedestrians;
  - disposal of construction waste, any hazardous waste and refuse in an appropriate manner according to the nature of the waste;
  - protection and cleaning of roads and pathways; and
  - overall site clean-up.
- g. An Encroachment Permit will be separately issued for the proposed encroachment into the public realm when Development Approval is granted. In particular, your attention is drawn to the following:
- an annual fee may be charged in line with the Encroachment Policy;
  - permit renewals are issued on an annual basis for those encroachments that attract a fee; and
  - unauthorized encroachments will be required to be removed.
- h. Any activity in the public realm, whether it be on the road or footpath, requires a City Works Permit. 48 hours' notice is required before commencement of any activity. The City Works Guidelines detailing the requirements for various activities, a complete list of fees and charges and an application form can all be found on Council's website at [www.cityofadelaide.com.au](http://www.cityofadelaide.com.au).

When applying for a City Works Permit you will be required to supply the following information with the completed application form:

- A Traffic Management Plan (a map which details the location of the works, street, property line, hoarding/mesh, lighting, pedestrian signs, spotters, distances etc);
- Description of equipment to be used;
- A copy of your Public Liability Insurance Certificate (minimum cover of \$20 Million required);
- Copies of consultation with any affected stakeholders including businesses or residents.

Please note: Upfront payment is required for all city works applications. Applications can be received by Council via the following.

Email: [cityworks@cityofadelaide.com.au](mailto:cityworks@cityofadelaide.com.au)

Fax: 8203 7674

In Person: 25 Pirie Street, Adelaide.

- i. There is no objection to the proposed vehicle crossing place, however the work will be undertaken by Council and the cost of the work will be charged to the applicant. A separate application for the crossing place(s) is required and the applicant can obtain a form from Customer Service, 25 Pirie Street, Adelaide or telephone 8203 7236. A quotation for the work will be provided by Council prior to the work being undertaken.
- j. The applicant should ensure there is no objection from any of the public utilities in respect of underground or overhead services and any alterations that may be required are to be at the applicant's expense.

- k. The applicant is reminded of its general environmental duty, as required by Section 25 of the *Environment Protection Act 1993* to take all reasonable and practical measures to ensure that the activities on the whole site, including during construction, do not pollute the environment in a way which causes or may cause environmental harm.
- l. You are advised of the following requirements of the *Heritage Places Act 1993*:
  - (a) If an archaeological artefact believed to be of heritage significance is encountered during excavation works, disturbance in the vicinity shall cease and the SA Heritage Council shall be notified; and
  - (b) Where it is known in advance (or there is reasonable cause to suspect) that significant archaeological artefacts may be encountered, a permit is required prior to commencing excavation works.
- m. If Aboriginal sites, objects or remains are discovered during excavation works, the Aboriginal Heritage Branch of the Aboriginal Affairs and Reconciliation Division of the Department of the Premier and Cabinet (as delegate of the Minister) should be notified under Section 20 of the *Aboriginal Heritage Act 1988*.

**BEN GREEN  
CONSULTANT PLANNER  
DEVELOPMENT DIVISION  
DEPARTMENT OF PLANNING, TRANSPORT and INFRASTRUCTURE**





PROPOSED QUEST HOTEL,  
100 NORTH TERRACE, ADELAIDE

01 LOCATION PLAN  
PROPOSED PLAN  
SCALE: NTS  
0 1 2 3 4 5  
SCALE 1:100 METRES

QUEST\_100 NORTH TCE, ADELAIDE  
EMMETT  
18003 SK63 07/05/18 A1 DRAWN\_K.L.  
LOCATION PLAN  
T +61 (0)8 8267 4766  
202 Malvern Street,  
North Adelaide, SA 5006  
www.matthews.com.au



PRELIMINARY - NOT FOR CONSTRUCTION



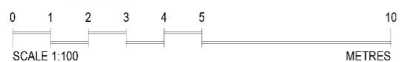


435 mm H x 1200 mm W powder coated under awning signage with illuminated Opal Acrylic lettering read as 'QUEST' North Terrace' and 200H x 1200 W grey powder coated Base.

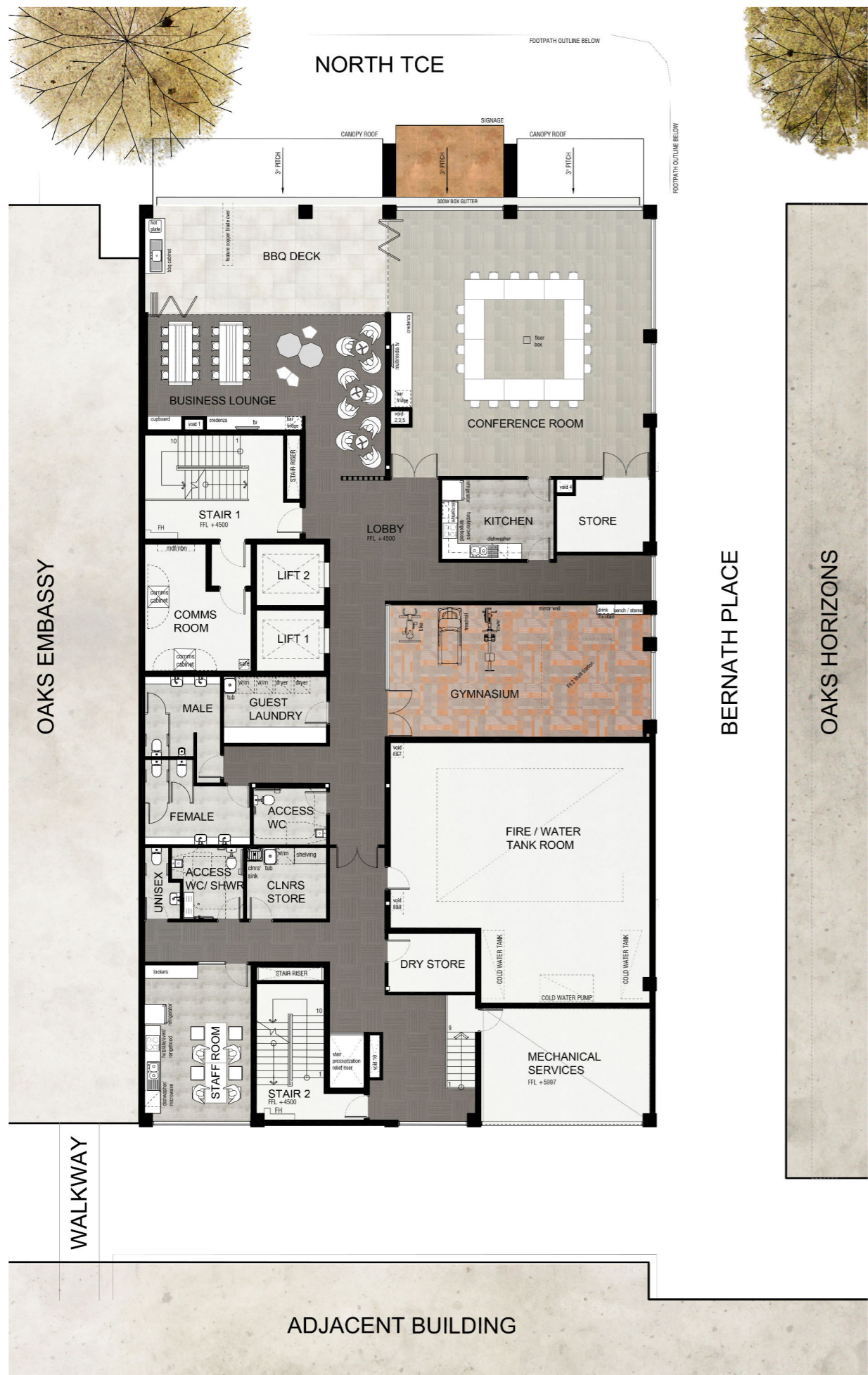
300 mm H x 827.5 mm W powder coated under awning signage with illuminated Opal Acrylic lettering read as 'QUEST' North Terrace' and 100mm H x 827.5 mm W grey powder coated Base.

GROUND FLOOR PLAN  
PROPOSED PLAN  
SCALE: 1:100

GROSS FLOOR AREA:  
BUILDING - 518m<sup>2</sup>







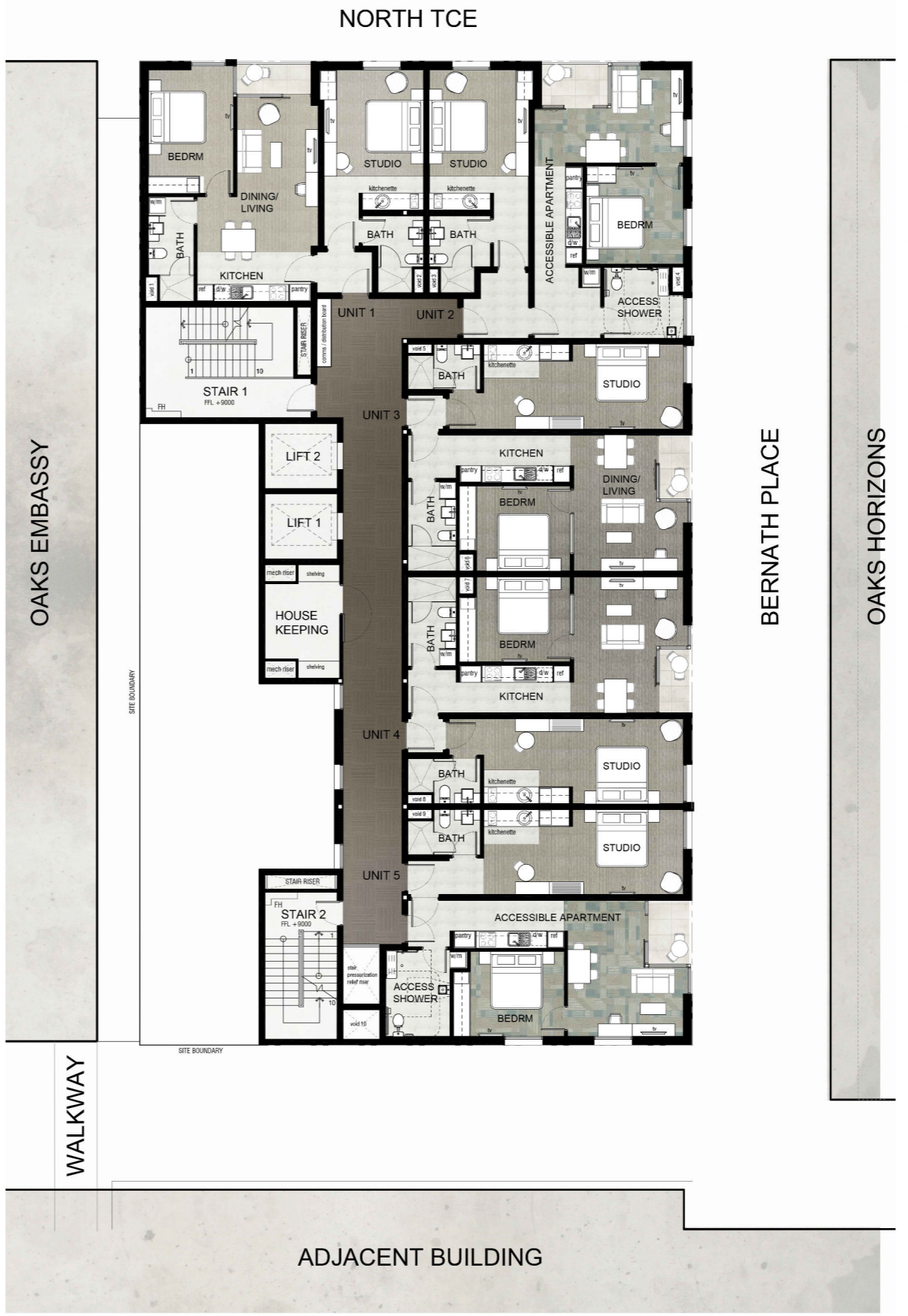
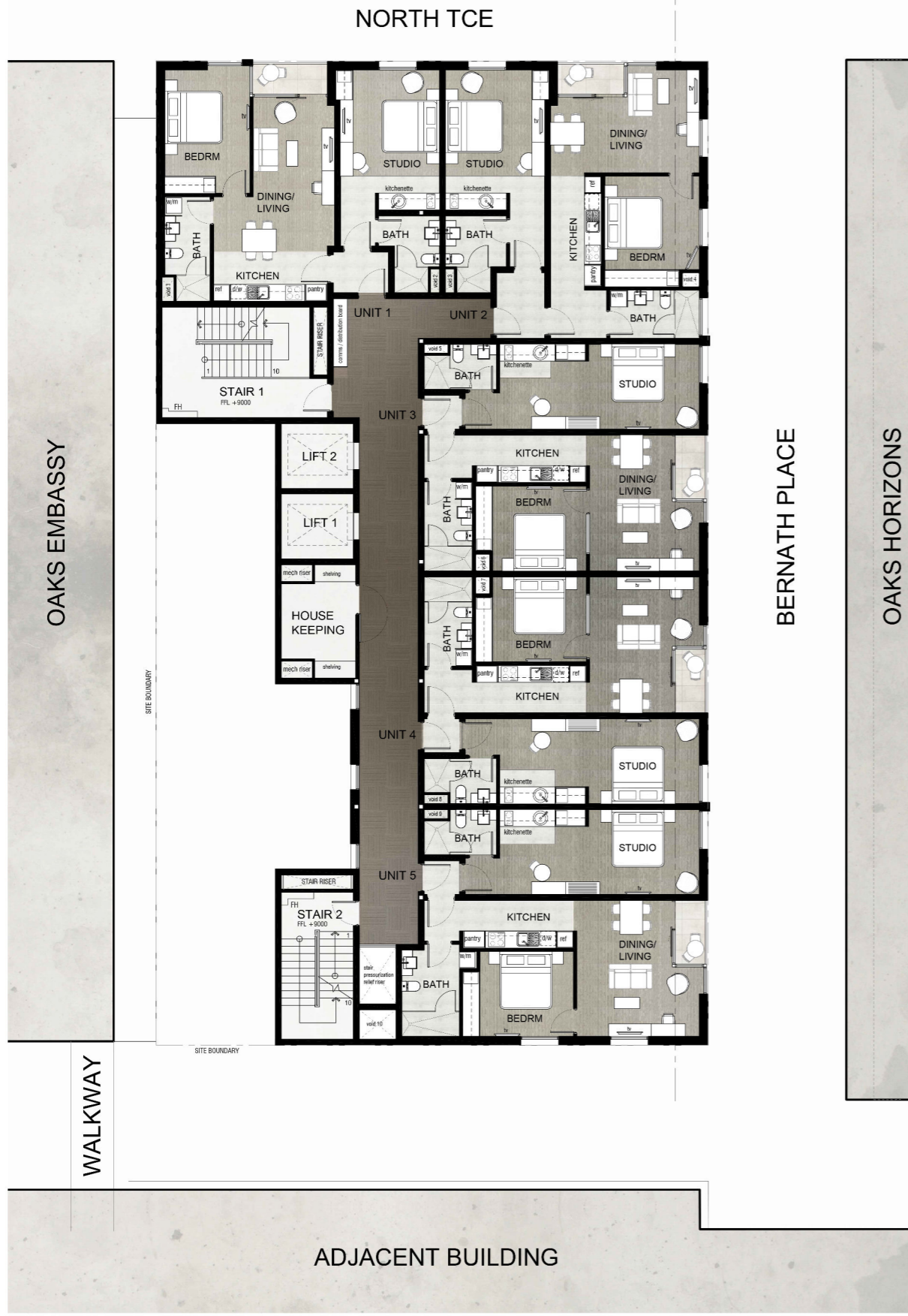
ROOM SUMMARY

ROOM	TYPE	INTERNAL FLOOR	BALCONY
UNIT 1	2BTK - STANDARD	75m <sup>2</sup>	2.7m <sup>2</sup>
UNIT 2	2BTK - STANDARD	79m <sup>2</sup>	2.5m <sup>2</sup>
UNIT 3	2BTK - STANDARD	75m <sup>2</sup>	2.3m <sup>2</sup>
UNIT 4	2BTK - STANDARD	75m <sup>2</sup>	2.3m <sup>2</sup>
UNIT 5	2BTK - STANDARD	77.3m <sup>2</sup>	2.3m <sup>2</sup>
GROSS FLOOR AREA:			
BUILDING - 550m <sup>2</sup>			
BALCONY - 12.1m <sup>2</sup>			

**FIRST FLOOR PLAN**  
 PROPOSED PLAN  
 SCALE: 1:100  
 GROSS FLOOR AREA:  
 BUILDING - 636m<sup>2</sup>  
 BALCONY/BBQ/DECKING - 36m<sup>2</sup>  
 SCALE 1:100 METRES

**SECOND FLOOR PLAN**  
 PROPOSED PLAN  
 SCALE: 1:100  
 GROSS FLOOR AREA:  
 BUILDING - 550m<sup>2</sup>  
 BALCONY - 12.1m<sup>2</sup>





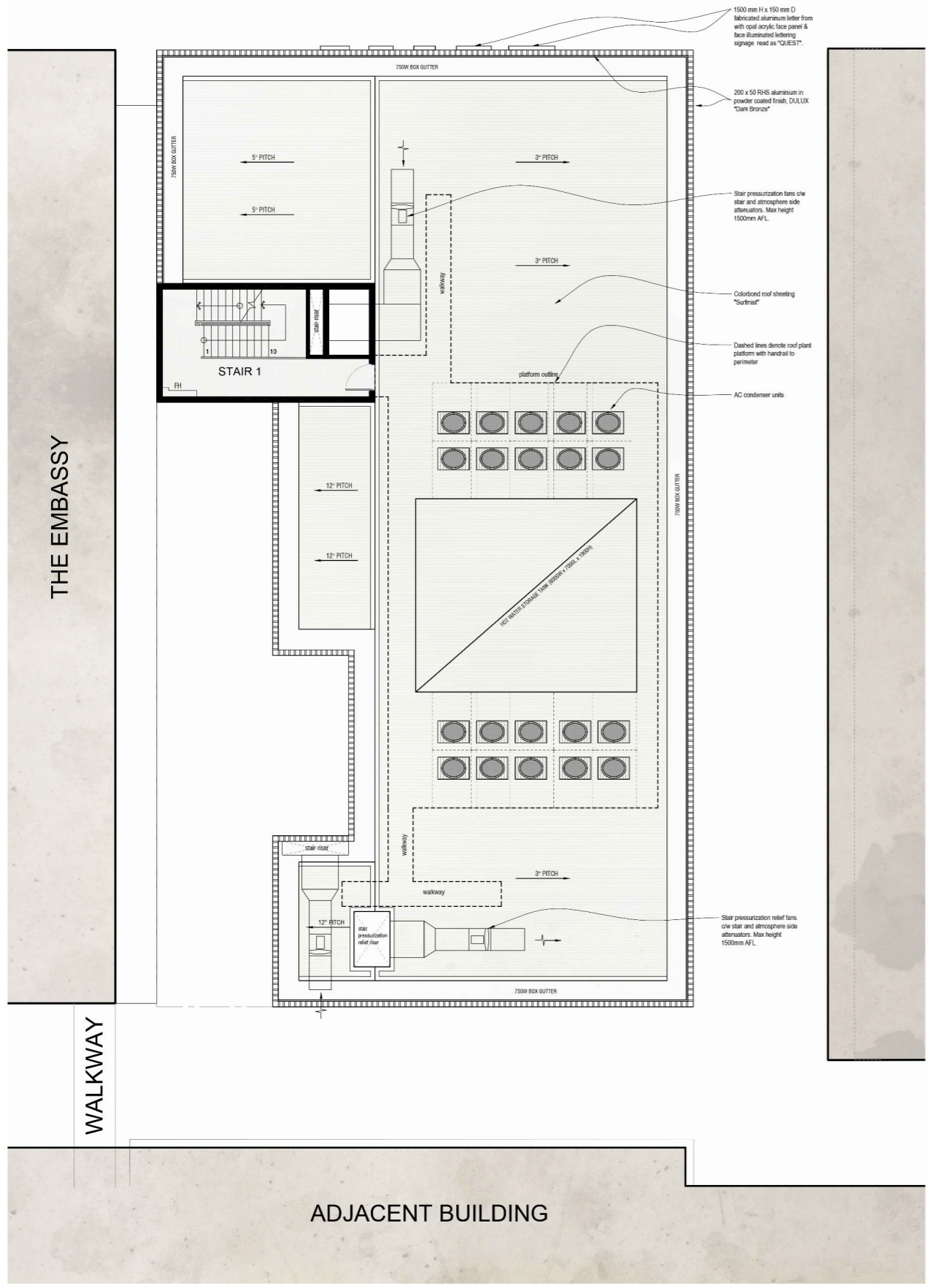
ROOM SUMMARY			
ROOM	TYPE	INTERNAL FLOOR	BALCONY
UNIT 1	2BTK - STANDARD	75m <sup>2</sup>	2.7m <sup>2</sup>
UNIT 2	2BTK - STANDARD	79m <sup>2</sup>	2.5m <sup>2</sup>
	2BTK - ACCESSIBLE	78m <sup>2</sup>	3.2m <sup>2</sup>
UNIT 3	2BTK - STANDARD	75m <sup>2</sup>	2.3m <sup>2</sup>
UNIT 4	2BTK - STANDARD	75m <sup>2</sup>	2.3m <sup>2</sup>
UNIT 5	2BTK - STANDARD	77.3m <sup>2</sup>	2.3m <sup>2</sup>
	2BTK - ACCESSIBLE	76.3m <sup>2</sup>	3.2m <sup>2</sup>
GROSS FLOOR AREA: BUILDING - 550m <sup>2</sup> BALCONY - 12.1m <sup>2</sup>			

STANDARD APARTMENT - TYPICAL PLAN (3RD TO 5TH, 9TH TO 14TH)  
 PROPOSED PLAN  
 SCALE: 1:100  
 0 1 2 3 4 5 10  
 SCALE 1:100 METRES

STANDARD & ACCESSIBLE APARTMENT - TYPICAL PLAN (6TH TO 8TH)  
 PROPOSED PLAN  
 SCALE: 1:100



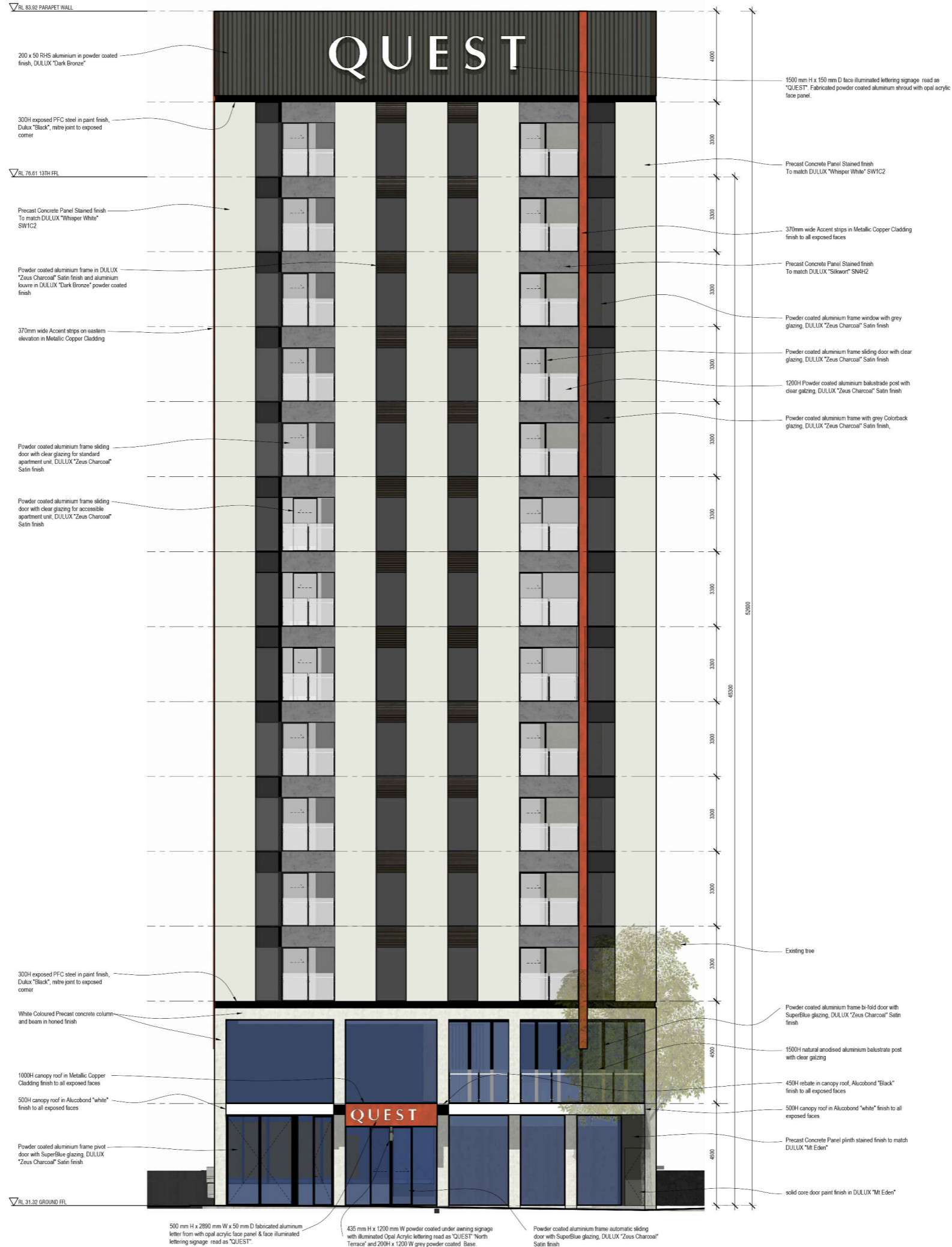
NORTH TCE



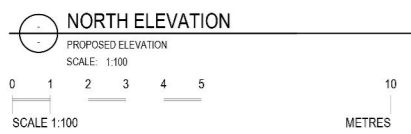
- 1500 mm H x 150 mm D fabricated aluminum letter from with opal acrylic face panel & face illuminated lettering signage read as "QUEST"
- 200 x 50 RHS aluminum in powder coated finish, DULUX "Dark Bronze"
- Stair pressurization fans c/w star and atmosphere side elevations. Max height 1500mm AFL.
- Colorbond roof sheeting "Sunfire"
- Dashed lines denote roof plant platform with handrail to perimeter
- AC condenser units
- Stair pressurization relief fans c/w star and atmosphere side elevations. Max height 1500mm AFL.

ROOF PLAN  
 PROPOSED PLAN  
 SCALE: 1:100

0 1 2 3 4 5 10  
 SCALE 1:100 METRES



- Precast Concrete Panel Stained finish To match DULUX "Silkwool" SMH2 Finish: AcraTex AcraShield Low Gloss
- Precast Concrete Panel Stained finish To match DULUX "M Eden" PN2A7 Finish: Matt
- External Door & Soffit Paint DULUX "M Eden" PN2A7 Finish: Low Sheen / Semi Gloss
- Precast Concrete Panel Stained finish To match DULUX "Whisper White" SW1C2 Finish: Matt
- 2-PAC Signage Paint DULUX "Vintage Green" SG5E8 Finish: Satin
- Aluminium Frame Powder Coat Paint DULUX "Zeus Charcoal Satin" 90687732 Finish: Satin
- Powder Coat Paint DULUX "Dark Bronze" 9068184K Finish: Satin
- Feature Paint DULUX "Black" S4E4H Finish: Semi Gloss
- Feature Cladding Metallic Copper
- Feature Cladding Alucobond "White" 16-101 Finish: Satin
- Feature Cladding Alucobond "Black" 326 Finish: Satin
- Window / Door Glazing Vitidian - Everage "SuperBlue"
- Window / Door Glazing Vitidian - Everage "Grey"
- Window / Door Glazing Vitidian - EnergyTech "Clear"
- Roof Sheeting Colorbond "Surfmat"







REL 83.92 PARAPET WALL

200 x 50 RHS aluminium in powder coated finish, DULUX "Dark Bronze"

300H exposed PFC steel in paint finish, Dulux "Black", mitre joint to exposed corner

REL 78.61 13TH FFL

370mm wide Accent strips in Metallic Copper Cladding finish to all exposed faces

Precast Concrete Panel Stained finish To match DULUX "Whisper White" SW1C2

Powder coated aluminium frame in DULUX "Zeus Charcoal" Satin finish, with aluminium louvre in powder coated finish, DULUX "Dark Bronze"

1200H Powder coated aluminium balustrade post with clear glazing, DULUX "Zeus Charcoal" Satin finish

Powder coated aluminium frame sliding door with clear glazing for accessible apartment unit, DULUX "Zeus Charcoal" Satin finish

Powder coated aluminium frame with grey Clearback glazing, DULUX "Zeus Charcoal" Satin finish

Powder coated aluminium frame sliding door with clear glazing for standard apartment unit, DULUX "Zeus Charcoal" Satin finish

300H exposed PFC steel in paint finish, Dulux "Black", mitre joint to exposed corner

White Coloured Precast concrete columns and beams in honed finish

Precast Concrete Panel wall to be 50mm set-back from feature columns/beams in stained finish to match DULUX "Mr Eden"

4000H perforated roller shutter in powder coated finish, DULUX "Zeus Charcoal" Satin finish

louvered door paint finish in DULUX "Mr Eden"

REL 32.87 UPPER GROUND FFL

1500 mm H x 150 mm D face illuminated lettering signage on North Terrace facade, read as "QUEST". Fabricated powder coated aluminium shroud with opal acrylic face panel

Precast Concrete Panel Stained finish To match DULUX "Mr Eden" PN2A7 Finish: Matt

External Door & Soffit Paint DULUX "Mr Eden" PN2A7 Finish: Low Sheen / Semi Gloss

Precast Concrete Panel Stained finish To match DULUX "Whisper White" SW1C2 Finish: Matt

370mm wide Accent strips in Metallic Copper Cladding finish to all exposed faces

2-PAC Signage Paint DULUX "Vintage Green" S65E8 Finish: Satin

Aluminium Frame Powder Coat Paint DULUX "Zeus Charcoal Satin" 90687732 Finish: Satin

Powder coated aluminium frame window with grey glazing, DULUX "Zeus Charcoal" Satin finish

Powder Coat Paint DULUX "Dark Bronze" 9068184K Finish: Satin

Feature Paint DULUX "Black" S4E4H Finish: Semi Gloss

Feature Cladding Metallic Copper

Feature Cladding Alucobond "White" 16-101 Finish: Satin

Feature Cladding Alucobond "Black" 326 Finish: Satin

Precast Concrete Panel Stained finish To match DULUX "Silkwool" SN4H2

Window / Door Glazing Vitidian - Everage "SuperBlue"

Window / Door Glazing Vitidian - Everage "Grey"

Window / Door Glazing Vitidian - EnergyTech "Clear"

Roof Sheeting Colorbond "Surfmat"

Powder coated aluminium frame window with SuperBlue glazing, DULUX "Zeus Charcoal" Satin finish

White Coloured Precast concrete column and beam in honed finish

300H bulkhead in DULUX "Mr Eden" paint finish

500H canopy roof in Alucobond "white" finish to all exposed faces

500 mm H x 2850 mm W x 50 mm D fabricated aluminium letter from with opal acrylic face panel & face illuminated lettering signage, read as "QUEST". Refer to North Elevation

435 mm H x 1200 mm W powder coated under awning signage with illuminated Opal Acrylic lettering read as "QUEST" North Terrace and 200H x 1200 W grey powder coated Base.

1000H canopy roof in Metallic Copper Cladding finish to all exposed faces

Powder coated aluminium frame window with SuperBlue glazing, DULUX "Zeus Charcoal" Satin finish

Precast Concrete Panel plinth stained finish to match DULUX "Mr Eden"


Powder coated aluminium frame automatic sliding door with SuperBlue glazing, DULUX "Zeus Charcoal" Satin finish

300 mm H x 827.5 mm W powder coated under awning signage with illuminated Opal Acrylic lettering read as "QUEST" North Terrace and 100mm H x 827.5 mm W grey powder coated Base. Refer to under awning signage on North Terrace

concrete planter box / seating

-  Precast Concrete Panel Stained finish To match DULUX "Silkwool" SN4H2 Finish: AcraTex AcraShield Low Gloss
-  Precast Concrete Panel Stained finish To match DULUX "Mr Eden" PN2A7 Finish: Matt
-  External Door & Soffit Paint DULUX "Mr Eden" PN2A7 Finish: Low Sheen / Semi Gloss
-  Precast Concrete Panel Stained finish To match DULUX "Whisper White" SW1C2 Finish: Matt
-  2-PAC Signage Paint DULUX "Vintage Green" S65E8 Finish: Satin
-  Aluminium Frame Powder Coat Paint DULUX "Zeus Charcoal Satin" 90687732 Finish: Satin
-  Powder Coat Paint DULUX "Dark Bronze" 9068184K Finish: Satin
-  Feature Paint DULUX "Black" S4E4H Finish: Semi Gloss
-  Feature Cladding Metallic Copper
-  Feature Cladding Alucobond "White" 16-101 Finish: Satin
-  Feature Cladding Alucobond "Black" 326 Finish: Satin
-  Window / Door Glazing Vitidian - Everage "SuperBlue"
-  Window / Door Glazing Vitidian - Everage "Grey"
-  Window / Door Glazing Vitidian - EnergyTech "Clear"
-  Roof Sheeting Colorbond "Surfmat"

**EAST ELEVATION**  
 PROPOSED ELEVATION  
 SCALE: 1:100

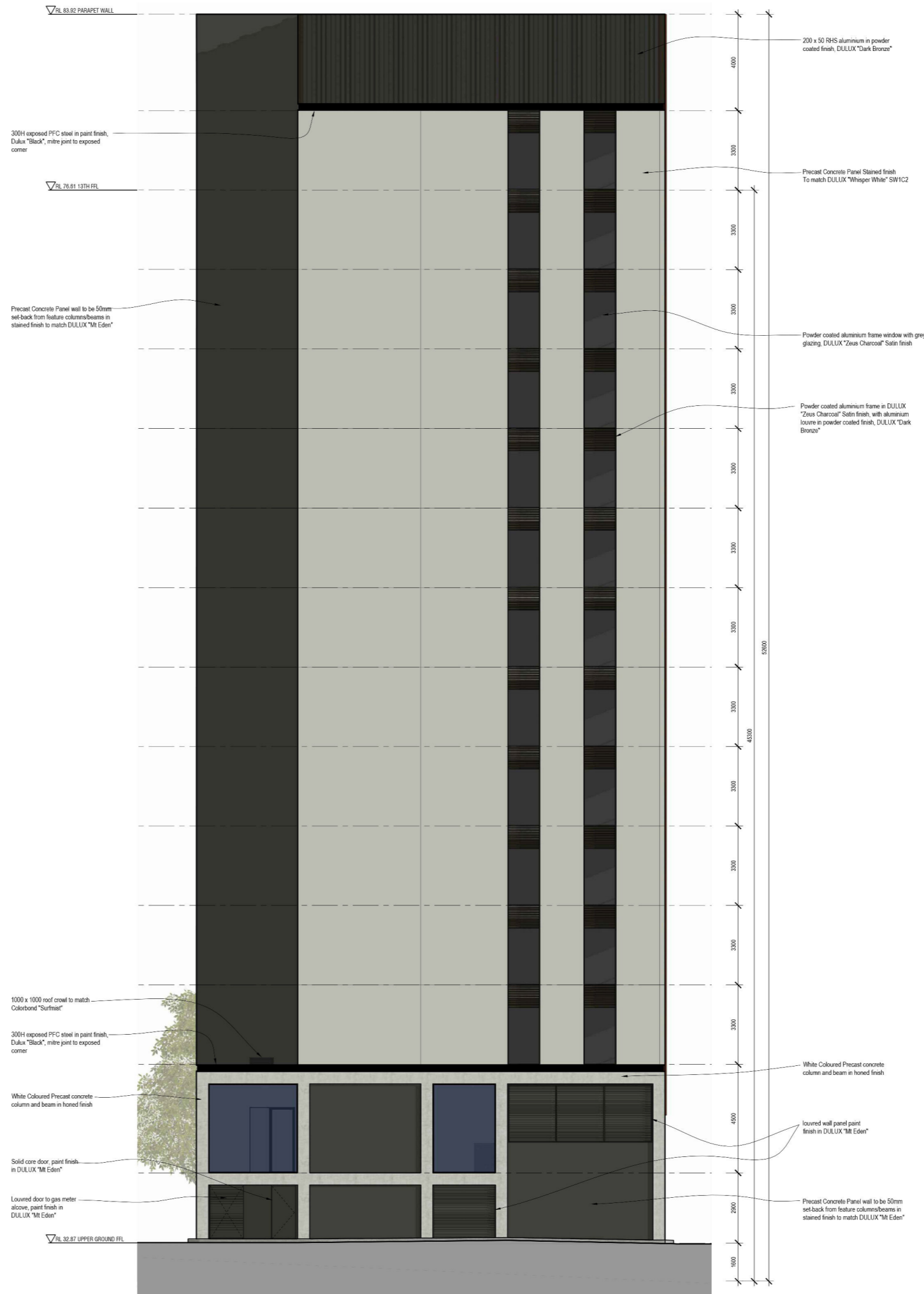


SCALE 1:100 METRES

QUEST\_100 NORTH TCE, ADELAIDE EAST ELEVATION  
 EMMETT 18003 SK116A 08/08/18 A1 DRAWN\_KL



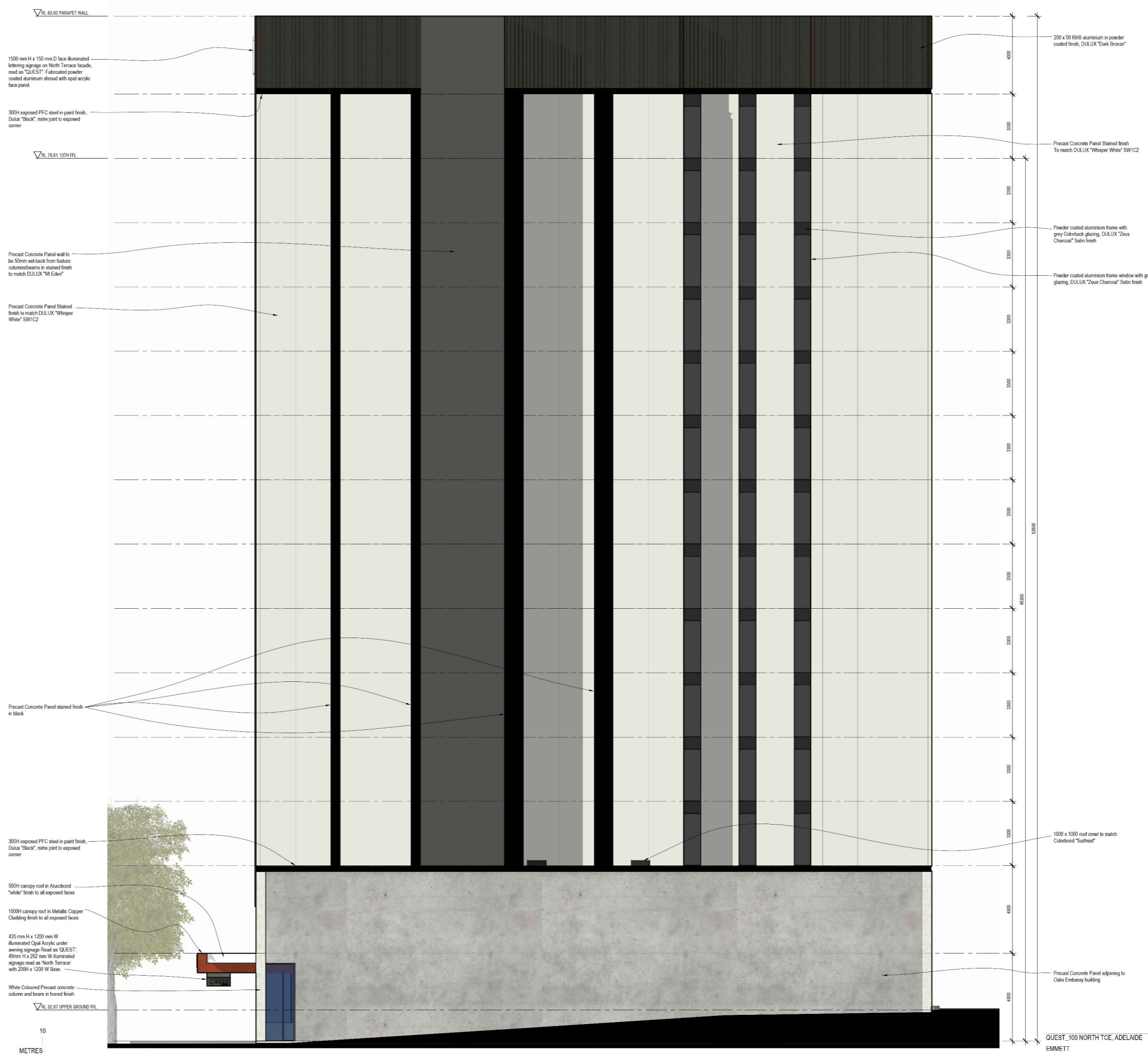
FOR PLANNING CONSENT - ISSUE 08TH AUGUST 2018



- Precast Concrete Panel Stained finish  
To match DULUX "Silkwoor" SMH2  
Finish: AcraTex AcraShield Low Gloss
- Precast Concrete Panel Stained finish  
To match DULUX "Mt Eden" PNZAT  
Finish: Matt
- External Door & Soffit Paint  
DULUX "Mt Eden" PNZAT  
Finish: Low Sheen / Semi Gloss
- Precast Concrete Panel Stained finish  
To match DULUX "Whisper White" SW1C2  
Finish: Matt
- 2-PAC Signage Paint  
DULUX "Vintage Green" SG5E8  
Finish: Satin
- Aluminum Frame Powder Coat Paint  
DULUX "Zeus Charcoal Satin" 90087732  
Finish: Satin
- Powder Coat Paint  
DULUX "Dark Bronze" 9008184K  
Finish: Satin
- Feature Paint  
DULUX "Black" S4E4H  
Finish: Semi Gloss
- Feature Cladding  
Metallic Copper
- Feature Cladding  
Aucobond "White" 16-101  
Finish: Satin
- Feature Cladding  
Aucobond "Black" 326  
Finish: Satin
- Window / Door Glazing  
Vidlan - Everage "SuperBlue"
- Window / Door Glazing  
Vidlan - Everage "Grey"
- Window / Door Glazing  
Vidlan - EnergyTech "Clear"
- Roof Sheeting  
Colorbond "Surfmist"

**SOUTH ELEVATION**  
 PROPOSED ELEVATION  
 SCALE: 1:100  
 0 1 2 3 4 5 10  
 SCALE 1:100 METRES

12750 3460 21910



-  Precast Concrete Panel Stained finish To match DULUX 'Silkwoor' SMH2 Finish: AcraTex AcraShield Low Gloss
-  Precast Concrete Panel Stained finish To match DULUX 'Mt Eden' PN2A7 Finish: Matt
-  External Door & Soffit Paint DULUX 'Mt Eden' PN2A7 Finish: Low Sheen / Semi Gloss
-  Precast Concrete Panel Stained finish To match DULUX 'Whisper White' SW1C2 Finish: Matt
-  2-PAC Signage Paint DULUX 'Vintage Green' S65E8 Finish: Satin
-  Aluminium Frame Powder Coat Paint DULUX 'Zeus Charcoal Satin' 90687732 Finish: Satin
-  Powder Coat Paint DULUX 'Dark Bronze' 9068184K Finish: Satin
-  Feature Paint DULUX 'Black' S4E4H Finish: Semi Gloss
-  Feature Cladding Metallic Copper
-  Feature Cladding Alucobond 'White' 16-101 Finish: Satin
-  Feature Cladding Alucobond 'Black' 326 Finish: Satin
-  Window / Door Glazing Vitrolux - Evariante 'SuperBlue'
-  Window / Door Glazing Vitrolux - Evariante 'Grey'
-  Window / Door Glazing Vitrolux - EnergyTech 'Clear'
-  Roof Sheeting Colorbond 'Surfsteel'

**WEST ELEVATION**  
 PROPOSED ELEVATION  
 SCALE: 1:100  
 0 1 2 3 4 5 10  
 SCALE 1:100 METRES





**NORTH ELEVATION - STREETSCAPE**  
 PROPOSED ELEVATION  
 SCALE: 1:200







○ FACADE PERSPECTIVE 1  
SCALE: Not To Scale





○ FACADE PERSPECTIVE 2  
SCALE: Not To Scale





○ FACADE PERSPECTIVE 3  
SCALE: Not To Scale





Subject land – view from north



Subject land – view from northeast





Subject land – view from northwest





Subject land – view from west



Subject land – view from east





Subject land – view from west



Subject land adjacent Oaks Embassy Car Park – view from north





Subject land – view from east



Bernath Place – view from north





Service vehicle exiting Bernath Place



Bernath Place – view from north





Bernath Place – view to North Terrace from south



Continuation of Bernath Place – view to west





Oaks Embassy interface with subject land – view to west





Oaks Horizons interface with subject land – view to northeast





Continuation of Bernath Place across car park driveway – view to northwest



Car park entry off Bernath Place – view to south





Continuation of Bernath Place – view to west



Continuation of Bernath Place – view to North Terrace from south





Continuation of Bernath Place – view from North Terrace to south



Continuation of Bernath Place – view to east





View west from continuation of Bernath Place





View east from continuation of Bernath Place



# DEVELOPMENT APPLICATION FORM

PLEASE USE BLOCK LETTERS

**COUNCIL:** ADELAIDE CITY

**APPLICANT:** EMMETT PROPERTY PTY LTD

Postal Address: C/- PBA, 26 WAKEHAM STREET  
ADELAIDE SA 5000

**Owner:** ALDERGROVE PTY LTD

Postal Address: C/- 18 GREENHILL ROAD  
WAYVILLE SA 5034

**BUILDER:** EMMETT CONSTRUCTION

Postal Address: PO BOX 7165 HUTT STREET  
ADELAIDE SA 5000

Licence No: \_\_\_\_\_

**CONTACT PERSON FOR FURTHER INFORMATION**

Name: PHILLIP BRUNNING

Telephone: 82325686 [work] 0407019748 [Ah]

Fax: \_\_\_\_\_ [work] \_\_\_\_\_ [Ah]

**EXISTING USE:** OFFICE

**FOR OFFICE USE**

Development No: \_\_\_\_\_

Previous Development No: \_\_\_\_\_

Assessment No: \_\_\_\_\_

- Complying
- Non Complying
- Notification Cat 2
- Notification Cat 3
- Referrals/Concurrences
- DA Commission

Application forwarded to DA

Commission/Council on

/ /

Decision: \_\_\_\_\_

Type: \_\_\_\_\_

Date: / /

	Decision required	Fees	Receipt No	Date
Planning:	_____	_____	_____	_____
Building:	_____	_____	_____	_____
Land Division:	_____	_____	_____	_____
Additional:	_____	_____	_____	_____
<b>Development Approval</b>				

**DESCRIPTION OF PROPOSED DEVELOPMENT:** HOTEL IN THE FORM OF SERVICED APARTMENTS FOR SHORT

**LOCATION OF PROPOSED DEVELOPMENT:** AND LONG STAY

House No: 100 Lot No: 500 Street: NORTH TERRACE Town/Suburb: ADELAIDE

Section No [full/part] \_\_\_\_\_ Hundred: ADELAIDE Volume: 5174 Folio: 32

Section No [full/part] \_\_\_\_\_ Hundred: \_\_\_\_\_ Volume: \_\_\_\_\_ Folio: \_\_\_\_\_

**LAND DIVISION:**

Site Area [m<sup>2</sup>] \_\_\_\_\_ Reserve Area [m<sup>2</sup>] \_\_\_\_\_ No of existing allotments \_\_\_\_\_

Number of additional allotments [excluding road and reserve]: \_\_\_\_\_ Lease: YES  NO

**BUILDING RULES CLASSIFICATION SOUGHT:** \_\_\_\_\_ Present classification: \_\_\_\_\_

If Class 5,6,78 or 9 classification is sought, state the proposed number of employees: Male: \_\_\_\_\_ Female: \_\_\_\_\_

If Class 9a classification is sought, state the number o persons for whom accommodation is provided: \_\_\_\_\_

If Class 9b classification is sought, state the proposed number of occupants of the various spaces at the premises: \_\_\_\_\_

**DOES EITHER SCHEDULE 21 OR 22 OF THE DEVELOPMENT REGULATIONS 2008 APPLY?** YES  NO

**HAS THE CONSTRUCTION INDUSTRY TRAINING FUND ACT 2008 LEVY BEEN PAID?** YES  NO

**DEVELOPMENT COST** [do not include any fit-out costs]: \$ 18,000,000.00

I acknowledge that copies of this application and supporting documentation may be provided to interested persons in accordance with the Development Regulations 2008.

**SIGNATURE:** \_\_\_\_\_

Dated: 11 / 5 / 18



**DEVELOPMENT REGULATIONS 1993**  
**Form of Declaration**  
**(Schedule 5 clause 2A)**

To: STATE COMMISSION ASSESSMENT PANEL

From: EMMETT PROPERTY PTY LTD

Date of Application:     /     /

Location of Proposed Development: .....

House No: 100 Lot No: 500 Street: NORTH TERRACE Town/Suburb: ADELAIDE

Section No (full/part): .....Hundred: ADELAIDE

Volume: 5174 Folio: 32

Nature of Proposed Development:

I PHILLIP BRUNNING  
.....being ~~the applicant~~  
a person acting on behalf of the applicant (delete the inapplicable statement) for  
the development described above declare that the proposed development will  
involve the construction of a building which would, if constructed in accordance  
with the plans submitted, not be contrary to the regulations prescribed for the  
purposes of section 86 of the *Electricity Act 1996*. I make this declaration under  
clause 2A(1) of Schedule 5 of the *Development Regulations 1993*.

Date: 11 / 5 / 18

Signed:  .....

Note 1

This declaration is only relevant to those development applications seeking  
authorisation for a form of development that involves the construction of a building  
(there is a definition of 'building' contained in section 4(1) of the *Development Act*  
*1993*), other than where the development is limited to –

- a) an internal alteration of a building; or
- b) an alteration to the walls of a building but not so as to alter the shape of the building.



Note 2

The requirements of section 86 of the *Electricity Act 1996* do not apply in relation to:

- a) a fence that is less than 2.0 m in height; or
- b) a service line installed specifically to supply electricity to the building or structure by the operator of the transmission or distribution network from which the electricity is being supplied.

Note 3

Section 86 of the *Electricity Act 1996* refers to the erection of buildings in proximity to powerlines. The regulations under this Act prescribe minimum safe clearance distances that must be complied with.

Note 4

The majority of applications will not have any powerline issues, as normal residential setbacks often cause the building to comply with the prescribed powerline clearance distances. Buildings/renovations located far away from powerlines, for example towards the back of properties, will usually also comply.

Particular care needs to be taken where high voltage powerlines exist; where the development:

- is on a major road;
- commercial/industrial in nature; or
- built to the property boundary.

Note 5

Information brochures 'Powerline Clearance Declaration Guide' and 'Building Safely Near Powerlines' have been prepared by the Technical Regulator to assist applicants and other interested persons. Copies of these brochures are available from council and the Office of the Technical Regulator. The brochures and other relevant information can also be found at [www.technicalregulator.sa.gov.au](http://www.technicalregulator.sa.gov.au)

Note 6

In cases where applicants have obtained a written approval from the Technical Regulator to build the development specified above in its current form within the prescribed clearance distances, the applicant is able to sign the form.



REAL PROPERTY ACT, 1886



The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



## Certificate of Title - Volume 5174 Folio 32

**Parent Title(s)** CT 4401/108, CT 4401/109, CT 4401/110  
**Creating Dealing(s)** RTD 7515307, T 7515320  
**Title Issued** 09/03/1994      **Edition** 6      **Edition Issued** 16/11/2001

## Estate Type

FEE SIMPLE

## Registered Proprietor

ALDERGROVE PTY. LTD. (ACN: 008 172 898)  
OF C/- 18 GREENHILL ROAD WAYVILLE SA 5034

## Description of Land

ALLOTMENT 500 DEPOSITED PLAN 37352  
IN THE AREA NAMED ADELAIDE  
HUNDRED OF ADELAIDE

## Easements

TOGETHER WITH RIGHT(S) OF WAY AND EASEMENT(S) CREATED BY TG 7515305 WITH LIMITATIONS AS DESCRIBED IN T 7515306 OVER THE LAND MARKED HB AND HC APPURTENANT ONLY TO THE LAND MARKED NN

TOGETHER WITH RIGHT(S) OF WAY AND EASEMENT(S) WITH LIMITATIONS OVER THE LAND MARKED D APPURTENANT ONLY TO THE LAND MARKED NN (TG 7515305)

TOGETHER WITH FREE AND UNRESTRICTED RIGHT(S) OF WAY OVER THE LAND MARKED JA APPURTENANT ONLY TO THE LAND MARKED NN

TOGETHER WITH RIGHT(S) OF WAY AND EASEMENT(S) OVER THE LAND MARKED H APPURTENANT ONLY TO THE LAND MARKED NN (TG 7515305)

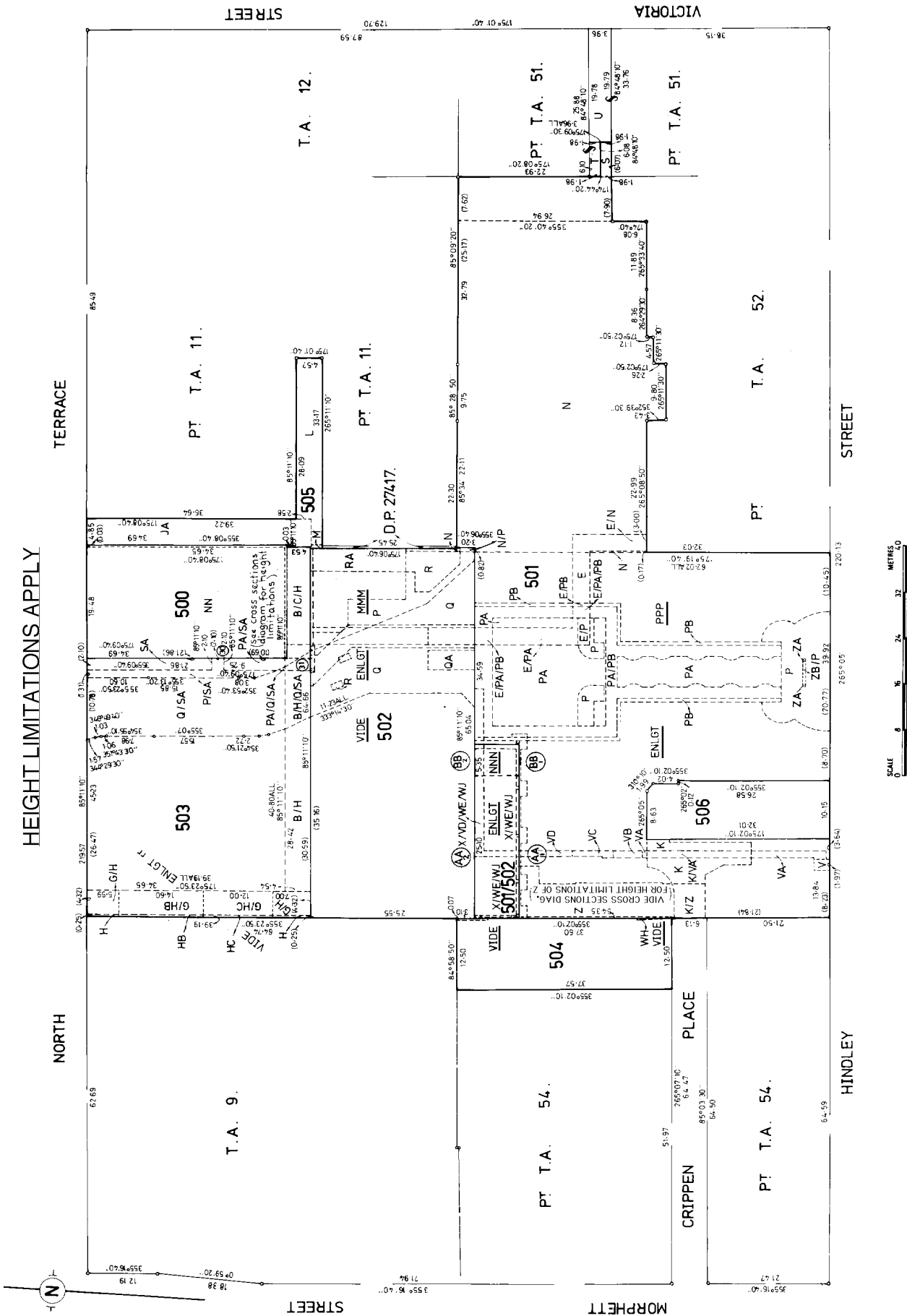
## Schedule of Dealings

Dealing Number	Description
9212430	MORTGAGE TO COMMONWEALTH BANK OF AUSTRALIA

## Notations

<b>Dealings Affecting Title</b>	NIL
<b>Priority Notices</b>	NIL
<b>Notations on Plan</b>	NIL
<b>Registrar-General's Notes</b>	NIL
<b>Administrative Interests</b>	NIL





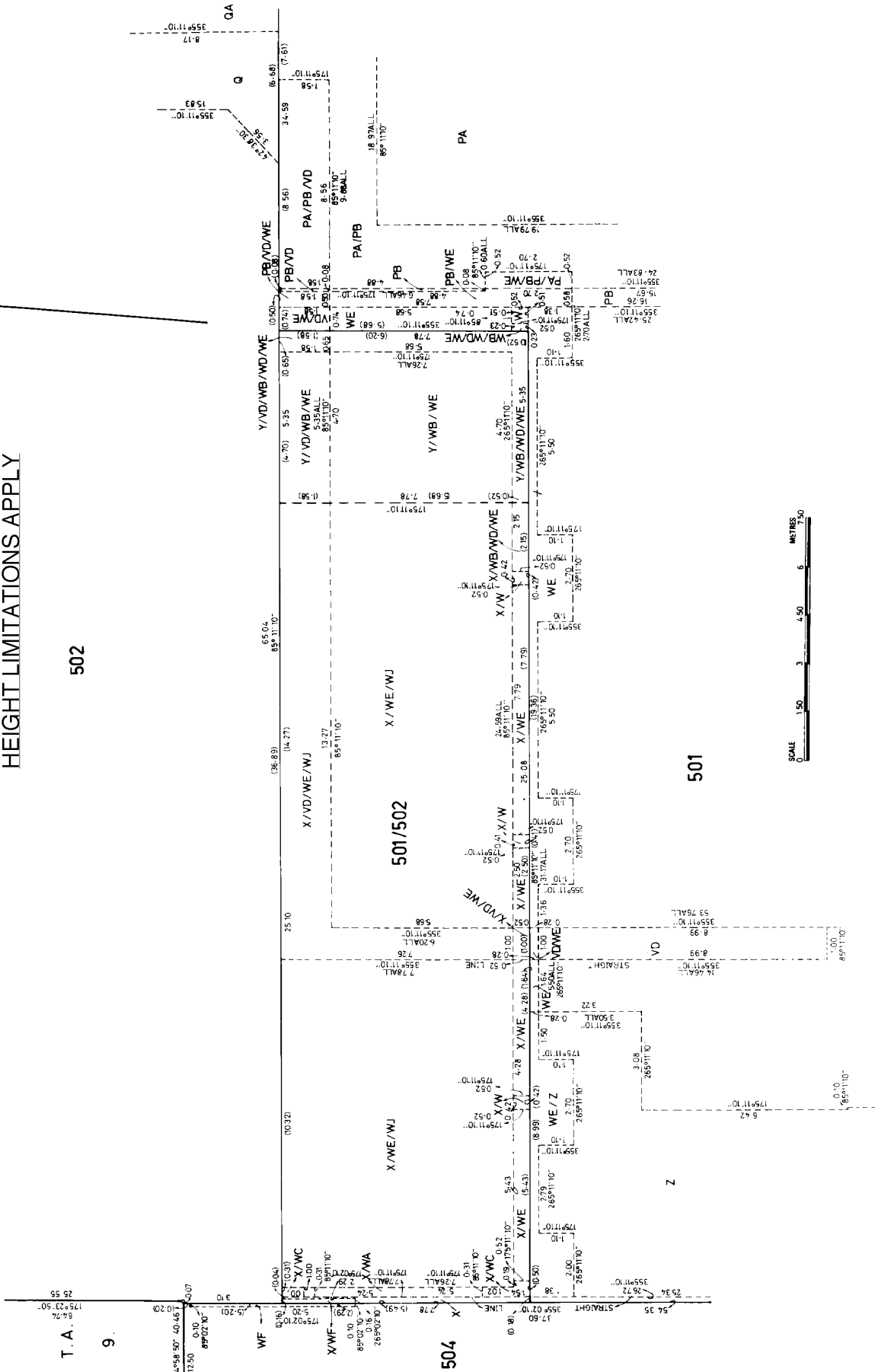


ENLARGEMENT NNN

HEIGHT LIMITATIONS APPLY

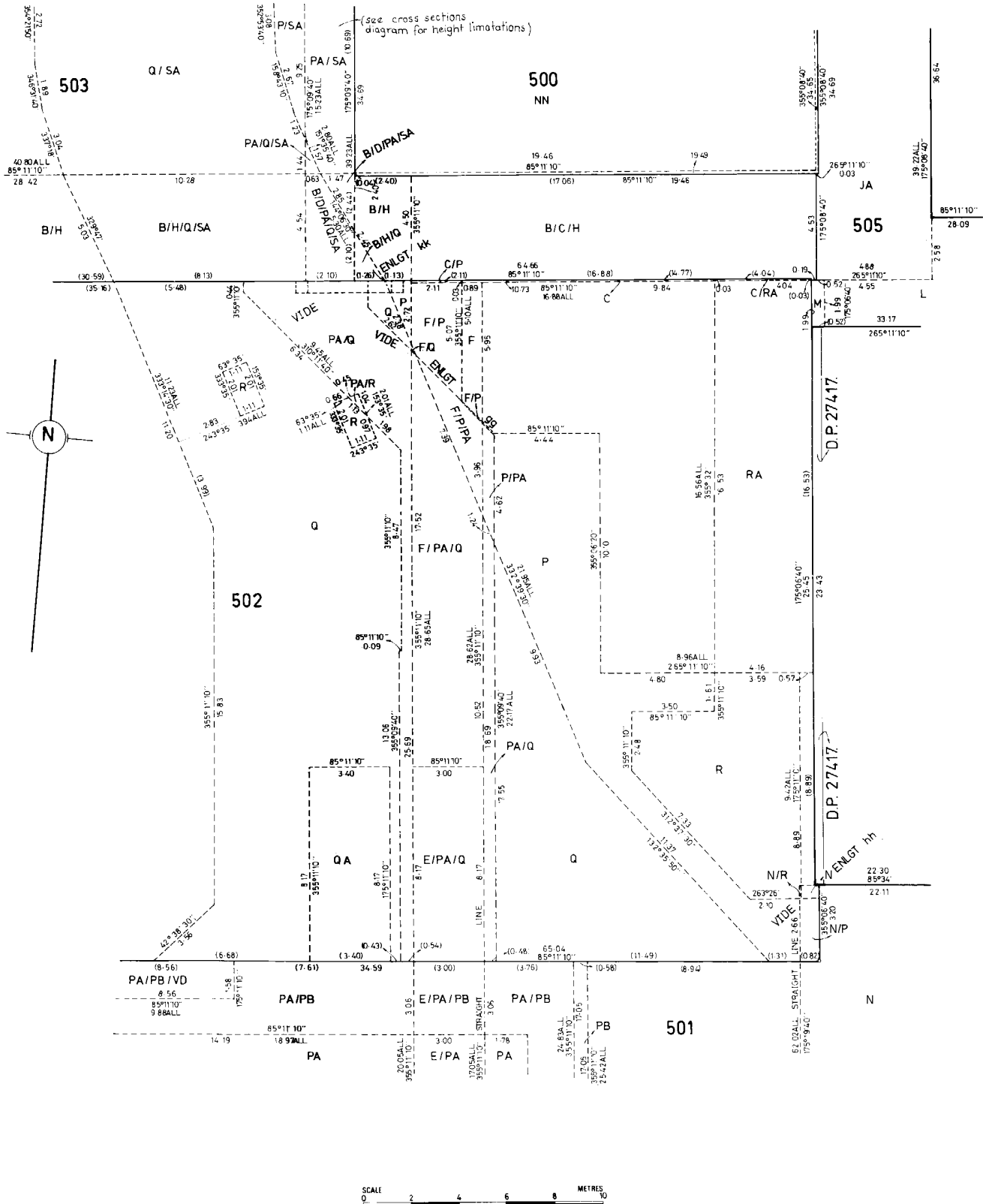
502

501

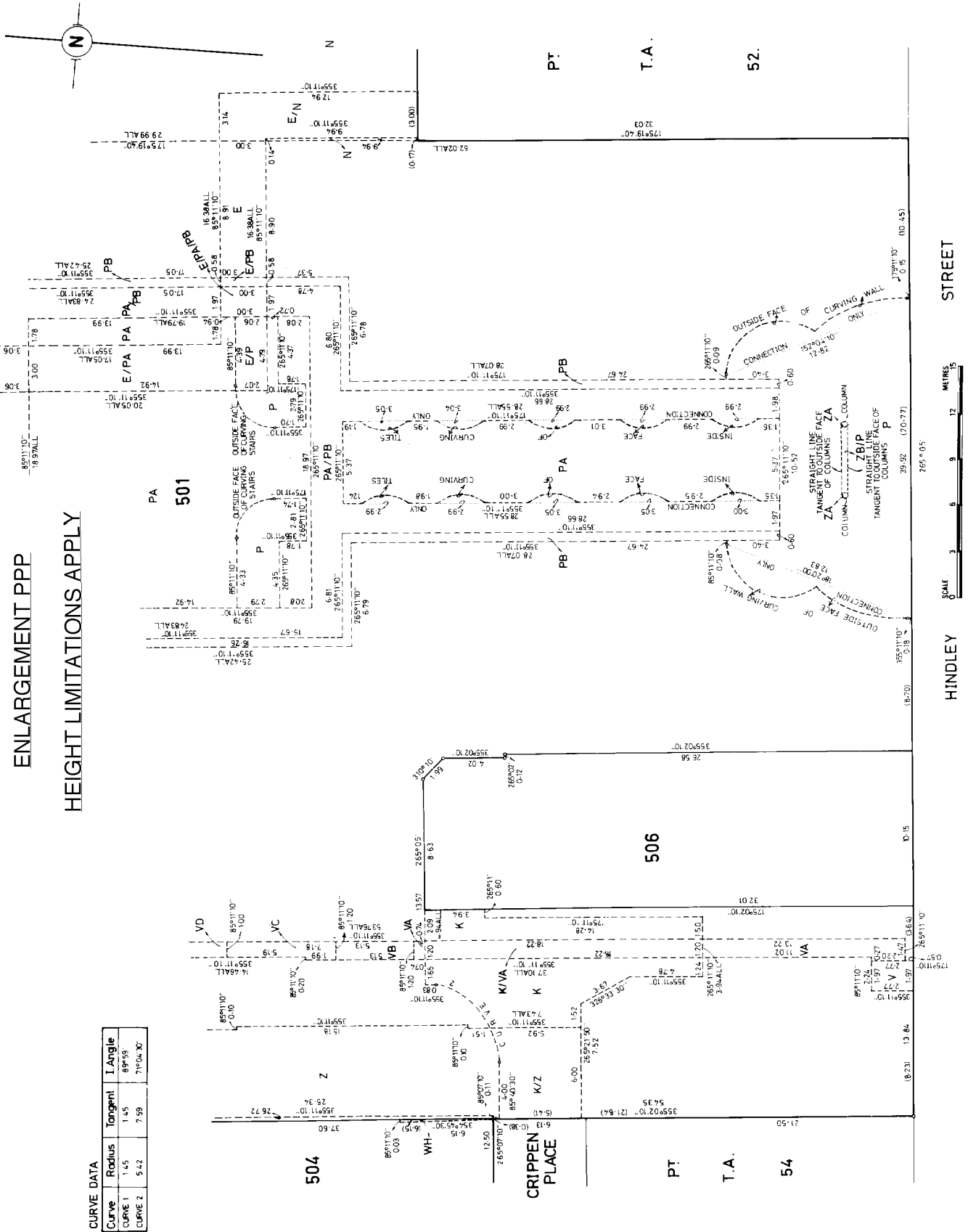




**ENLARGEMENT MMM**  
**HEIGHT LIMITATIONS APPLY**







**ENLARGEMENT PPP**  
**HEIGHT LIMITATIONS APPLY**

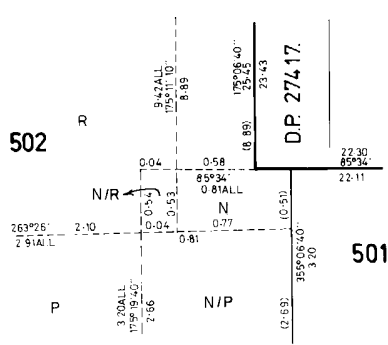
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CURVE 2	5.42	7.59	71°04'30"

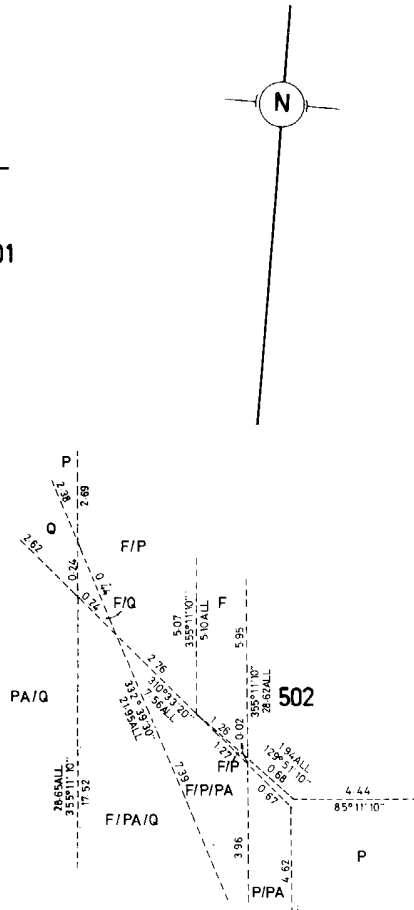




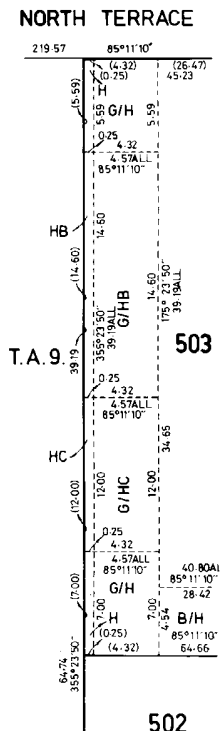




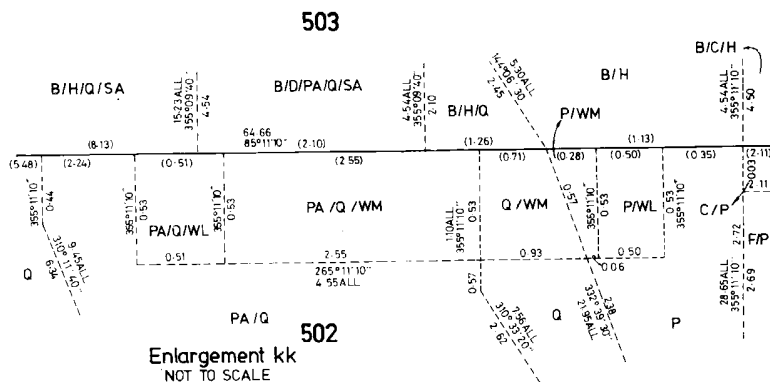
Enlargement hh  
NOT TO SCALE



Enlargement gg  
NOT TO SCALE



Enlargement rr  
NOT TO SCALE



Enlargement kk  
NOT TO SCALE





Town Planning  
Development Advice  
Strategic Management

11 May 2018

Mr Brett Miller  
Team Leader – Inner Metro Development Assessment  
Department of Planning Transport & Infrastructure  
GPO Box 1815  
ADELAIDE SA 5001

Dear Brett,

## **PROPOSED HOTEL DEVELOPMENT – 100 NORTH TERRACE, ADELAIDE**

I refer to the Development Application by Emmett Property that seeks Development Plan Consent to construct a 14 level hotel in the form of serviced apartments for short and long stay, comprising 120 rooms together with associated facilities on land located at 100 North Terrace, Adelaide.

I have been engaged by Emmett Property to review this proposal and provide my town planning opinion having regard to the existing condition of the land, its context together with the relevant provisions of the Adelaide (City) Development Plan.

### **1. PROPOSAL**

The proposed development is comprised of the following elements:

Ground Floor	Lobby, reception, administration offices linen store, waste management, staff parking (3) and a <i>porte cochere</i> accessed via a private lane over which there are rights of way.
First Floor	Conference room, business lounge, lobby waiting areas, BBQ deck, kitchen, gymnasium, bathrooms, staff rooms, house keeping, services and store rooms.
Second to Fourteenth Floors	5 two bedroom hotel rooms (dual key) Total 60 Units (120 bedrooms) Two lifts, housekeeping and stairs
Roof	Lift overrun, plant & equipment

The building will occupy the whole of the site and have a height of 83.92 metres AHD. A small set back is provided to the western property boundary for light and ventilation purposes, and a shallow canopy is proposed over the footpath to North Terrace.

The proposal is presented in a contemporary design style with facades modeled and detailed so as to provide a podium for the first two levels, with the floors above presenting windows and balconies to the east and north.

The serviced apartments will be operated by the Quest Group.

**Phillip Brunning & Associates**

ABN 40 118 903 021

26 Wakeham Street  
Adelaide SA 5000  
Telephone 08 8232 5686  
Mobile 0407 019 748  
[phil@phillipbrunning.com](mailto:phil@phillipbrunning.com)

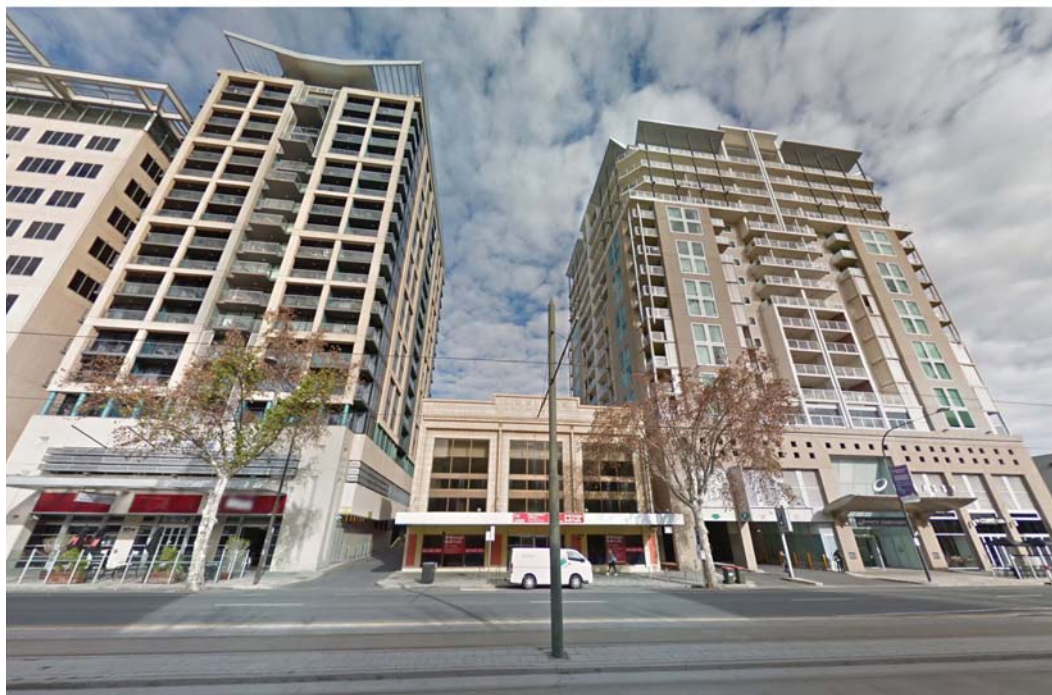


## 2. LAND & LOCALITY

The land has an area of some 675 square metres with a 19.48 metre frontage to North Terrace. The land is currently developed with a 3 level building which has no heritage significance (not a listed building).

The land is more particularly described as Allotment 500 in Deposited Plan 37352 within the Hundred of Adelaide, as recorded in Certificate of title Volume 5174 Folio 32. The land enjoys certain rights of way and access to easements.

The right of way marked 'JA' on the Deposited Plan is otherwise known as Bernath Place returns around this property and the adjoining to the west. While a private lane, public thoroughfare is not restricted.







### 3. DEVELOPMENT PLAN

The land is located within the Capital City Zone and more particularly within the Central Business Policy Area 13 of the Adelaide (City) Development Plan. The Relevant version of the Development Plan for the assessment of this application is that consolidated on 20 June 2017.

I note that two Section 39(2)(b)(ii) amendments have been made to the Development Plan (4 July 2017 and 19 December 2017) that have yet to be consolidated within the current document. Neither of these amendments has any relevant to the assessment of this proposal.

The following provisions of the Development Plan are relevant in the assessment of this proposal, acknowledging that these policies are expressed in an advisory, rather than mandatory sense. They are nonetheless the basis upon which a decision is to be made in respect to this application.

#### COUNCIL WIDE

##### Living Culture

Objectives: 1, 2, 3  
Principles of Development Control: 1,

##### Environmental

Crime Prevention Through Urban Design

Objectives: 24  
Principles of Development Control: 82

Operating Hours and Associated Activities of Licensed Premises

Objectives: 25  
Principles of Development Control: 87, 88



#### Waste Management

Objectives: 28  
Principles of Development Control: 101, 102, 103, 104,

#### Energy Efficiency

Objectives: 30  
Principles of Development Control: 106, 107, 108, 109, 111, 112,

#### Micro-climate and Sunlight

Objectives: 33, 34,  
Principles of Development Control: 119, 120, 122, 123, 124, 125

#### Stormwater Management

Objectives: 35  
Principles of Development Control: 128

#### **Built Form and Townscape**

Objectives: 46, 47, 48  
Principles of Development Control: 167

#### Height Bulk and Scale

Principles of Development Control: 168, 169, 170, 172

#### Composition and Proportion

Principles of Development Control: 180

#### Articulation and Modeling

Principles of Development Control: 182, 183, 184, 186

#### Materials, Colours and Finishes

Principles of Development Control: 187, 188, 189, 190

#### Sky and Roof Lines

Objectives: 49  
Principles of Development Control: 193, 194, 195

#### Active Street Frontage

Objectives: 50, 51  
Principles of Development Control: 196, 198

#### **Transport and Access**

Objectives: 60  
Principles of Development Control: 224

#### Pedestrian Access

Objectives: 61, 62, 63  
Principles of Development Control: 226, 227, 228, 230, 232

#### Bicycle Access

Objectives: 64, 65  
Principles of Development Control: 233, 234, 235, 236, 237, 238



Traffic and Vehicle Access

Objectives: 68, 70

Principles of Development Control: 241, 242, 243, 244, 245, 248, 249

### **Economic Growth and Land Use**

Objectives: 73, 74, 76

Principles of Development Control: 266, 268, 269, 271

### **CAPITAL CITY ZONE**

Objectives: 1, 2, 3, 4, 5, 6, 8

Principles of Development Control: 1, 5, 6, 7, 8, 9, 11, 12, 14, 15, 26, 27, 28, 33, 34,

Central Business Policy Area 13

Objectives: 1, 2, 3

Principles of Development Control: 1, 2, 3

## **4. ASSESSMENT**

### **4.1 Economic**

#### Investment

Objective 74 of the Development Plan seeks *a business environment which encourages investment from domestic and foreign sources, business development and employment.*

The proposal represents as significant capital that will give rise to positive economic impacts both initially as a result of construction activity but also in terms of employment and the construction of services.

#### Tourist Accommodation

Objectives 73 (c) more particularly seeks *the role of the city enhances as the gateway to the attractions of South Australia for international and interstate visitors by developing a wide range of visitor accommodation.*

This emphasis on the provision of tourist activities is reinforced by Principle of Development Control

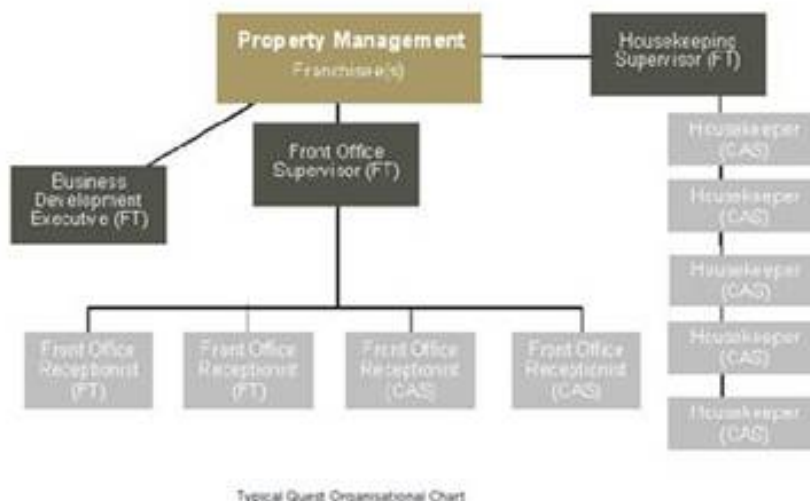
**268** Development is encouraged to develop and expand upon the existing or create new tourism activities to maximise employment and the long-term economic, social and cultural benefits of developing the City as a competitive domestic and international tourist destination.

This proposal seeks to respond to an identified demand for additional premium economy accommodation within the City. This location will afford guests with a high level of amenity and function in terms of access to City attractions.

#### Employment

Over and above initially construction employment, it is anticipated that there will be up to 20 persons employed (5 full time and 15 casual) in the administration, servicing and hospitality uses associated with this hotel.





## 4.2 Land Use

### Nature of Development

The proposed development in my opinion is for a *hotel*.

A *hotel* is defined by the Development Regulations, 2008 as:

**hotel** means premises licensed, or proposed to be licensed, as a hotel under the *Liquor Licensing Act 1985*, but does not include a motel;

Reference is made to the Liquor Licensing Act 1985 which has now been repealed and replaced by the Liquor Licensing Act 1997. Interestingly, a hotel is not defined more specifically in either piece of legislation.

The Macquarie Dictionary provides the following meaning for *hotel*:

*'a building in which accommodation and food, and sometimes other facilities, are available'*

I do however note the definition for motel provided within the Regulations:

**motel** means a building or group of buildings providing temporary accommodation for more than 5 travellers, and includes an associated restaurant facility, but does not include a hotel or residential flat building;

The Macquarie Dictionary provides the following meaning for *motel*:

*'a roadside hotel which provides accommodation for travelers in self contained, serviced units, with parking for their vehicle'*

This meaning differentiates between a motel and a hotel on the basis of the self contained nature of the accommodation and the ability for a guest to park a vehicle on site. There is no parking provided by this development

This differentiation between the two is explained by the origins of the term *motel* which is an abbreviated version of 'motor hotel' used to describe a mode of accommodation which originated in the United States of America in the 1920s.



### Envisaged Development

Regardless of whether the planning authority considers the proposal to be a *hotel* or a *motel*, both land uses are clearly identified as an '*envisaged*' form of development by Principle of Development Control 1 for the Capital City Zone.

Zone Principle of development Control 40 identifies that the proposal is to be assigned Category 1, i.e. no public notification. The site of the development is not adjacent to land in the City Living or Adelaide Historic (Conservation) Zone

The Development Plan clearly provides for this form of development.

## **4.3 Built Form & Townscape**

### Height & Set Back

Concept Plan Figure CC/1 Adelaide (City) Building Heights identifies that there is no prescribe height limit in Central Business Policy Area, save for considerations in respect to Airport Building Height Restrictions.

Map Adel/1 (Overlay 5) Adelaide (City) Airport Building Heights identifies a Obstacle Limitation Surface between 90 and 100 metres AHD. The height of the building at 83.92 metres would not penetrate this surface.

The proposal therefore need not be referred to the Adelaide Airports Limited.

The Desired Character statement for the Capital City Zone includes the following passage which speaks to high scale development, with high street walls that frame streets.

That said, there is an emphasis on achieving interesting an pedestrian environment at street level, having a human scale to be achieved through the use of building articulation, fenestration, verandahs, balconies and/or awnings.

High-scale development is envisaged in the Zone with high street walls that frame the streets. However an interesting pedestrian environment and human scale will be created at ground floor levels through careful building articulation and fenestration, frequent openings in building façades, verandahs, balconies, awnings and other features that provide weather protection.

In important pedestrian areas, buildings will be set back at higher levels above the street wall to provide views to the sky and create a comfortable pedestrian environment. In narrow streets and laneways the street setback above the street wall may be relatively shallow or non-existent to create intimate spaces through a greater sense of enclosure. In the Central Business Policy Areas, upper level setbacks are not envisaged.

More specifically in relation to the pattern of streets and squares, the Development Plan articulates the following as such relates to North Terrace as a major cultural boulevard that provides an important northern edge of the City square mile.

The City's boulevards, terraces and Squares will be developed as follows:

- (a) North Terrace will be reinforced as an important pedestrian promenade and cultural boulevard that provides an important northern edge to the City square mile.

Development fronting North Terrace, King William Street, Wakefield Street, Grote Street, the Squares, and in the Main Street Policy Area, will reflect their importance though highly contextual design that reflects and responds to their setting and role.



The proposed development would further the achievement of this desired character and form of development in so far as the proposed building would fill the gap which currently exists between the two adjacent buildings of significantly greater scale.

While there is some discussion as to the set back of buildings at higher levels to achieve views to sky, this may be relatively shallow or non-existent in minor streets and not at all within the Central Business Policy Area.

Whereas Principle 12 for the Capital City Zone seeks a podium/street wall height and upper level set back in the order of 3 to 6 metres part (h) makes it very clear that this does not apply in the Central Business Policy Area.

In response to Council wide policies set out under the heading Height, Bulk and Scale, the proposed design is an appropriate response to the intense urban setting and built form context of the Capital City Zone.

More particularly in respect to Principle 170, I note that the proposal:

- reinforces the desired character and built form sought in this locality being tall and imposing buildings that provide a hard edge to the street;
- displays a façade treatment at the lower levels which is compatible with the parapet lines and mass of existing buildings along North Terrace Street;
- avoids massive unbroken facades that are visible from the public realm;
- achieves a comfortable human scale at street levels including the provision of a verandah balcony element that affords pedestrian shelter to North Terrace.

It is clear that the Development Plan provides for an intensive form of development at a scale commensurate with the role and function of this location as the economic and cultural focus of the State.

The design response in this instance is considered entirely appropriate in terms of its scale and form, utilising this relative small site in an efficient and effective manner that provides for the required level of accommodation.

### Design Quality

The Development Plan deals with the concept of design quality in a number of locations including within the desired character statements for the Capital City Zone and the Central Business Policy Area, i.e.

New development will achieve high design quality by being:

- (a) **Contextual** – so that it responds to its surroundings, recognises and carefully considers the adjacent built form, and positively contributes to the character of the immediate area.
- (b) **Durable** – by being fit for purpose, adaptable and long lasting, and carefully considers the existing development around it.
- (c) **Inclusive** – by integrating landscape design to optimize pedestrian and cyclist usability, privacy, and equitable access, and also promote the provision of quality spaces integrated with the public realm that can be used for access and recreation and help optimize security and safety both internally and into the public realm, for occupants and visitors alike.
- (d) **Sustainable** – by integrating sustainable systems into new buildings and the surrounding landscape design to improve environmental performance and minimise energy consumption.
- (e) **Amenable** – by providing natural light and ventilation to habitable spaces.



Contemporary juxtapositions will provide new settings for heritage places. Innovative design is expected in areas of identified street character with an emphasis on contemporary architecture that responds to site context and broader streetscape, while supporting optimal site development. The addition of height, bulk and massing of new form should be given due consideration in the wider context of the proposed development.

Buildings will exhibit innovative design approaches and produce stylish and evocative architecture, including tall and imposing buildings that provide a hard edge to the street and are of the highest design quality. A wide variety of design outcomes of enduring appeal are expected. Complementary and harmonious buildings in individual streets will create localised character and legible differences between streets, founded on the existing activity focus, building and settlement patterns, and street widths.

This approach is reinforced by Capital City Zone Objective 5 and Principles of Development Control 6 and 7 in addition to that expressed more generally within the Council wide Section of the Development Plan.

**Objective 5:** Innovative design approaches and contemporary architecture that respond to a building's context.

- 6 Development should be of a high standard of architectural design and finish which is appropriate to the City's role and image as the capital of the State.
- 7 Buildings should achieve a high standard of external appearance by:
  - (a) the use of high quality materials and finishes. This may be achieved through the use of materials such as masonry, natural stone, prefinished materials that minimise staining, discolouring or deterioration, and avoiding painted surfaces particularly above ground level;
  - (b) providing a high degree of visual interest through articulation, avoiding any large blank facades, and incorporating design features within blank walls on side boundaries which have the potential to be built out;
  - (c) ensuring lower levels are well integrated with, and contribute to a vibrant public realm; and
  - (d) ensuring any ground and first floor level car parking elements are sleeved by residential or non-residential land uses (such as shops, offices and consulting rooms) to ensure an activated street frontage.

Acknowledging that judgments in respect to design quality are invariably subjective, I am of the view that the proposal displays a high standard of design, in respect to its composition and proportions, articulation and modeling.

The proposal is a considered response to its site and context amongst other buildings of greater scale, pays appropriate respect to nearby heritage buildings and would make a positive contribution to the streetscape of North Terrace.

An appropriate palette of materials and finishes have been selected that would achieve the required building performance in terms of energy efficiency and complement that typically evident within the locality.

In this regard the proposal may reasonably be described as contextual, durable, inclusive, sustainable and amenable within the meaning of the desired character statement provided above.

### Activation & Vibrancy

As articulated within the Vision for the City provided as a preface to the Development Plan and expressed below within the desired character for the Capital City activation at street level and vibrancy is sought.

This Zone is the economic and cultural focus of the State and includes a range of employment, community, educational, tourism and entertainment facilities. It is anticipated that an increased population within the Zone will complement the range of opportunities and experiences provided in the City and increase its vibrancy.



The Zone will be active during the day, evening and late night. Licensed entertainment premises, nightclubs and bars are encouraged throughout the Zone, particularly where they are located above or below ground floor level to maintain street level activation during the day and evening.

Non-residential land uses at ground floor level that generate high levels of pedestrian activity such as shops, cafés and restaurants will occur throughout the Zone. Within the Central Business Policy Area, residential land uses at ground level are discouraged. At ground level, development will continue to provide visual interest after hours by being well lit and having no external shutters. Non-residential and / or residential land uses will face the street at the first floor level to contribute to street vibrancy.

The ground and first levels of the proposed building will engage positively with the street providing activation and vibrancy via the hotel reception, lobby area, courtyard, business lounge and conference room.

The proposed building provides for a human or pedestrian scale at ground level with the canopy providing a degree of weather protection acknowledging the need to achieve suitable underside clearance.

The facade at street level has been composed to provide for ease of pedestrian movement in and out of the building while providing suitable amenity and function for the spaces within.

The arrangement of windows and balconies associated with hotel rooms at level 2 and above in the north and east facing facades provides for passive or casual surveillances over the street below.

#### 4.4 Environmental

##### Micro-climate

In respect to glare arising from building materials, in particular glazing the Development Plan expresses the following.

- 122** Glazing on building facades should not result in glare which produces discomfort or danger to pedestrians, occupants of adjacent buildings and users of vehicles.

**Design Techniques** (these are ONE WAY of meeting the above Principle)

**122.1** Design solutions may include:

- (a) reducing the quantity of glass used by having a higher proportion of masonry or other non-reflective materials in the building exterior;
- (b) recessing glass into the building;
- (c) shading or angling the glass;
- (d) selecting glass that has a low level of reflection; and/or
- (e) avoiding the use of large expanses of highly reflective materials.

While the glazing of windows has not yet been specified, I would not expect that glare will be an issue given not only the proportion of facades which are glazed but also that the majority of windows are recessed given the arrangement of balconies.

- 125** Development that is over 21 metres in building height and is to be built at or on the street frontage should minimise wind tunnel effect.

**Design Techniques** (these are ONE WAY of meeting the above Principle)

**125.1** Methods to reduce the potential for a wind tunnel effect may include:

- (a) a podium built at the base of a tall tower and aligned with the street to deflect wind away from the street;
- (b) substantial verandahs around a building to deflect downward travelling wind flows; and/or
- (c) placing one building windward of another building.



The proposal is unlikely to contribute to a wind tunnel effect at street level in this location given the inclusion of the canopy element to north Terrace which will assist in dispersing and moderating the negative effects of down drafts.

Specific analysis in this regard has been undertaken by Windtech.

### Energy Efficiency

As an objective the Development Plan seeks the following in respect to energy efficiency, acknowledging that the performance of the building in this regard will be assessed more specifically at the Building Rules Consent stage

**Objective 30:** Development which is compatible with the long term sustainability of the environment, minimises consumption of non-renewable resources and utilises alternative energy generation systems.

The Applicant has commissioned Lucid to advise in respect to Ecologically Sustainable Design , with their report addressing building management, indoor environment quality, energy, water, emissions and building services.

### Waste Management

The Development Plan seeks the following in respect to waste management.

- 103** Development greater than 2 000 square metres of total floor area should manage waste by:
- (a) containing a dedicated area for the collection and sorting of construction waste and recyclable building materials;
  - (b) on-site storage and management of waste;
  - (c) disposal of non-recyclable waste; and
  - (d) incorporating waste water and stormwater re-use including the treatment and re-use of grey water.

To the extent not apparent on the plans, the Applicant provides a waste management plan that assesses the nature and volume arising from the proposed development and an appropriate management strategy

### Licensed Premises

The Development Plan seeks the following in respect to licensed premises.

**Objective 25:** Operating hours of licensed premises or licensed entertainment premises, together with associated activities of such premises, established and operated so as to reinforce the desired character of the locality and appropriate behavioural activities.

While matters pertaining to liquor licensing will be dealt with by a separate authority, the nature of the licensed premises proposed, i.e. a bar and restaurant associated with a hotel are considered entirely appropriate in this location.

## **4.5 Access & Movement**

### Car Parking

On site car parking is not sought by the Development Plan for this location within the Capital City Zone and the Core Pedestrian Area. Nonetheless, three spaces are provided for use by staff.



### Bike Parking

The Development Plan makes a strong policy expression in favour of the use of bicycles and that appropriate facilities should be provided within new development to encourage such

- Objective 64:** Greater use of bicycles for travel to and within the City and the improvement of conditions, safety and facilities for cyclists.
- Objective 65:** Adequate supply of secure, short stay and long stay bicycle parking to support desired growth in City activities.
- 234** An adequate supply of on-site secure bicycle parking should be provided to meet the demand generated by the development within the site area of the development. Bicycle parking should be provided in accordance with the requirements set out in [Table Adel/6](#).

While specific rate is not identified for hotel, I do note that for motel a rate of 1 space ought to be provided per 20 employees and that 2 spaces should be provided for the first 40 guest rooms plus 1 for every additional 40 rooms

The proposal provides for 10 spaces in this regard.

### Vehicle Access

The Development Plan seeks safe and convenient access.

- Objective 60:** Access to and movement within the City that is easy, safe, comfortable and convenient with priority given to pedestrian and cyclist safety and access.
- 224** Development should provide safe, convenient and comfortable access and movement.

As discussed above, the proposal will utilise right of way over the land immediately to the east of the site in order to provide for staff parking, service and delivery vehicles, including for waste management.

Provision is made for the largest size vehicle anticipated to be used in conjunction with this development, to be able to manoeuvre in a manner that would enable entry and exit from the site in a forward direction.

Given the low frequency of traffic movements through this driveway and that it is not serving on site car parking for guests and staff, I suggest that the potential for conflict with pedestrians to be minimal.

GTA Consultants will provide a traffic report shortly.

### Pedestrian Movement

Subject to the matters discussed above being satisfactorily addressed, which I have every confidence of being able to do so, the proposal is considered to achieve the following objectives in respect to pedestrian movement.

- Objective 61:** Development that promotes the comfort, enjoyment and security of pedestrians by providing shelter and reducing conflict with motor vehicles.
- Objective 62:** Development that contributes to the quality of the public realm as a safe, secure and attractive environment for pedestrian movement and social interaction.



In particular the active frontage to this building and the manner in which it will relate and function to North Terrace, including the provision of the canopy over head is entirely consistent with that envisaged.

## 5. CONCLUSION

For the reasons that I have discussed above, I am of the view that the proposal is an acceptable form of development that generally accords with relevant Development Plan policies such that warrants consent.

To the extent that the proposal may depart from certain provisions of the Development Plan, no serious impacts are anticipated and certainly not to an extent that would render this proposal as being unacceptable.

The use of the proposed building as hotel is entirely appropriate in this location.

The height, form and arrangement of the proposed building sits within the parameters set out in the Development Plan and would further the desired objectives in terms of investment, activation, vibrancy and employment.

Accordingly, consent should be granted

Yours faithfully

**PHILLIP BRUNNING & ASSOCIATES PTY LTD**



**PHILLIP BRUNNING** RPIA  
Registered Planner





# Quest North Terrace 100 North Terrace, Adelaide Transport Impact Assessment


**Client //** Emmett Property  
**Office //** SA  
**Reference //** S138314  
**Date //** 29/05/2018

Quest North Terrace  
100 North Terrace, Adelaide  
Transport Impact Assessment

Issue: A 29/05/2018

Client: Emmett Property  
Reference: S138314  
GTA Consultants Office: SA

**Quality Record**

Issue	Date	Description	Prepared By	Checked By	Approved By	Signed
A	29/05/2018	Final	Joy Yu	Tim Jones	David Kwong	



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# 1. Introduction

---

## 1.1 Background

A development application is currently being undertaken for proposed serviced apartments located at 100 North Terrace, Adelaide. The proposed development incorporates 60 two-bedroom units, associated ancillary uses, drop-off zone, and staff car parking spaces.

GTA Consultants was commissioned by the applicant to undertake a transport impact assessment of the proposed development.

## 1.2 Purpose of this Report

This report sets out an assessment of the anticipated transport implications of the proposed development, including consideration of the following:

- i existing traffic and parking conditions surrounding the site
- ii suitability of the proposed car parking and bicycle parking in terms of supply (quantum) and layout
- iii traffic generation characteristics of the proposed development
- iv proposed access arrangements for the site
- v transport impact of the development proposal on the surrounding road network.

## 1.3 References

In preparing this report, reference has been made to the following:

- o Adelaide (City) Development Plan (Consolidated 20 June 2017)
- o Australian Standard/ New Zealand Standard, Parking Facilities, Part 1: Off-Street Car Parking AS/NZS 2890.1:2004
- o Australian Standard, Parking Facilities, Part 2: Off-Street Commercial Vehicle Facilities AS 2890.2:2002
- o Australian Standard / New Zealand Standard, Parking Facilities, Part 6: Off-Street Parking for People with Disabilities AS/NZS 2890.6:2009
- o plans for the proposed development prepared by Matthews Architects
- o various technical data as referenced in this report
- o an inspection of the site and its surrounds
- o other documents as nominated.

## 2. Existing Conditions

### 2.1 Subject Site

The subject site is located at 100 North Terrace in Adelaide. The site of approximately 650 m<sup>2</sup> has frontages of 19.5 metre to North Terrace and 34.5 metre to Bernath Place. The site is located within a Capital City Zone in the Central Business Policy Area.

The surrounding properties include a mix of residential, commercial, retail and educational land uses. The location of the subject site and the surrounding environs is shown in Figure 2.1.

Figure 2.1: Subject Site and its Environs



(PhotoMap courtesy of NearMap Pty Ltd)

### 2.2 Road Network

#### 2.2.1 Adjoining Roads

##### North Terrace

North Terrace functions as a two-way sub-arterial road aligned in an east/west direction under the care and control of the Adelaide (City) Council. It is set within a 40-metre-wide road reserve and configured with two carriageways divided by trams line and median. Each carriageway in vicinity of the proposed site is approximately 9.5-metre-wide and has two traffic lanes.

Kerbside parking is not permitted on southern side of North Terrace adjacent the subject site. A timed loading zone is present adjacent the subject site with no-stopping restrictions applied between 7am -10am and 4pm - 7pm Monday to Friday.



North Terrace is subject to a posted speed limit of 50km/h and carries approximately 28,100 vehicles per day<sup>1</sup> in vicinity of the site.

### Bernath Place

Bernath Place is a road located directly adjacent the subject site. to the east of the subject site it is currently 5.8-metres-wide between the existing buildings to the east, and 4.4 metres wide between buildings to the south. A loading zone of approximately 15 metres in length currently exists on the east side of the road. The maximum clearance height is 3.2 metres for the first 1 metre of the road width on the east side of the road.

## 2.2.2 Surrounding Intersections

The following intersections currently exist in the vicinity of the site:

- North Terrace/ Bernath Place (unsignalised)

## 2.2.3 Accident Statistics

A review of the reported accident casualty history for the roads and intersections adjoining the subject site has been sourced from the Department of Planning, Transport and Infrastructure (DPTI) crash database. A summary of the crashed recorded in the last available five-year period (2012- end of 2016) is presented in Figure 2.2.

**Figure 2.2: Recorded Crash Data (2012-2016)**



<sup>1</sup> Source: 'Traffic Volume Estimates' dataset accessed via LocationSA, volume base year 2008

## 2.3 Sustainable Transport Infrastructure

### 2.3.1 Public Transport

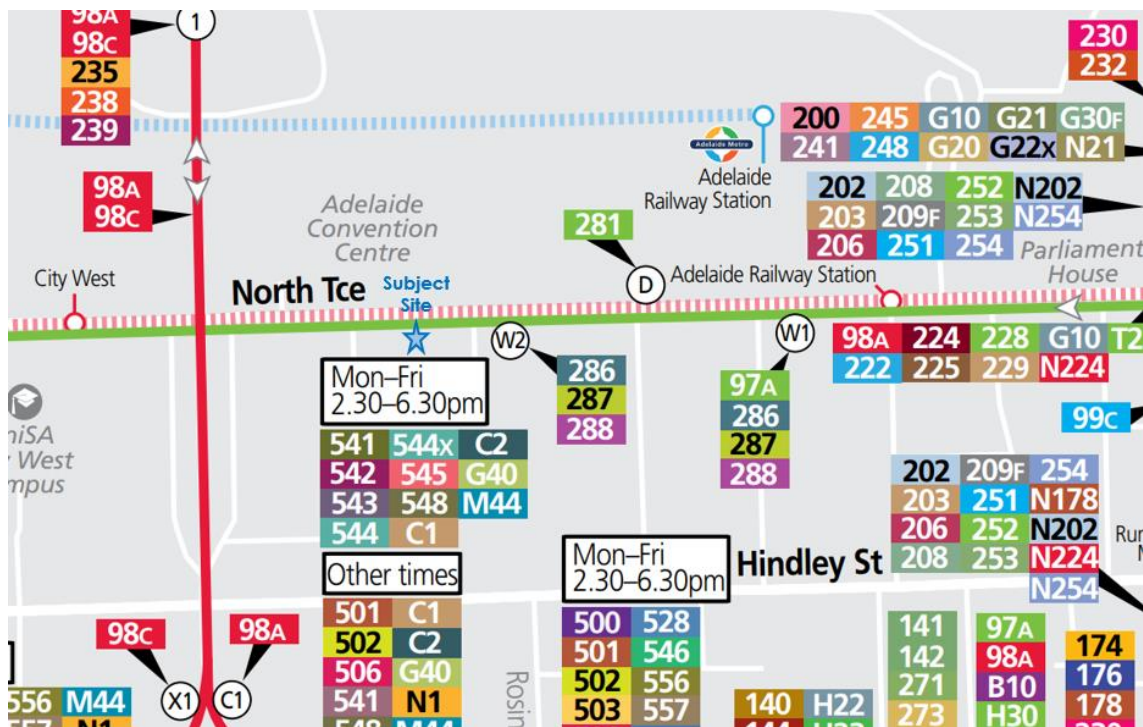
Figure 2.3 shows the subject site in relation to existing public transport routes within its vicinity. North Terrace is identified as one of the High Concentration Public Transport Route in the Adelaide (City) Development Plan.

The subject site is located within 300 metres walking distance to the Adelaide Railway Station, which provides reliable train services to and from Belair, Gawler, Outer Harbor, Seaford and Tonsley at regular intervals.

Tram stop 'Adelaide Railway Station' and 'City West' are both located approximately 200 metres to the subject site on North Terrace. The tram provides free services between the Entertainment Centre stop and South Terrace stop.

The high frequency bus services in vicinity of the site provide a variety of links to most urban centres as well as destinations within the CBD.

**Figure 2.3: Public Transport Map**



### 2.3.2 Pedestrian Infrastructure

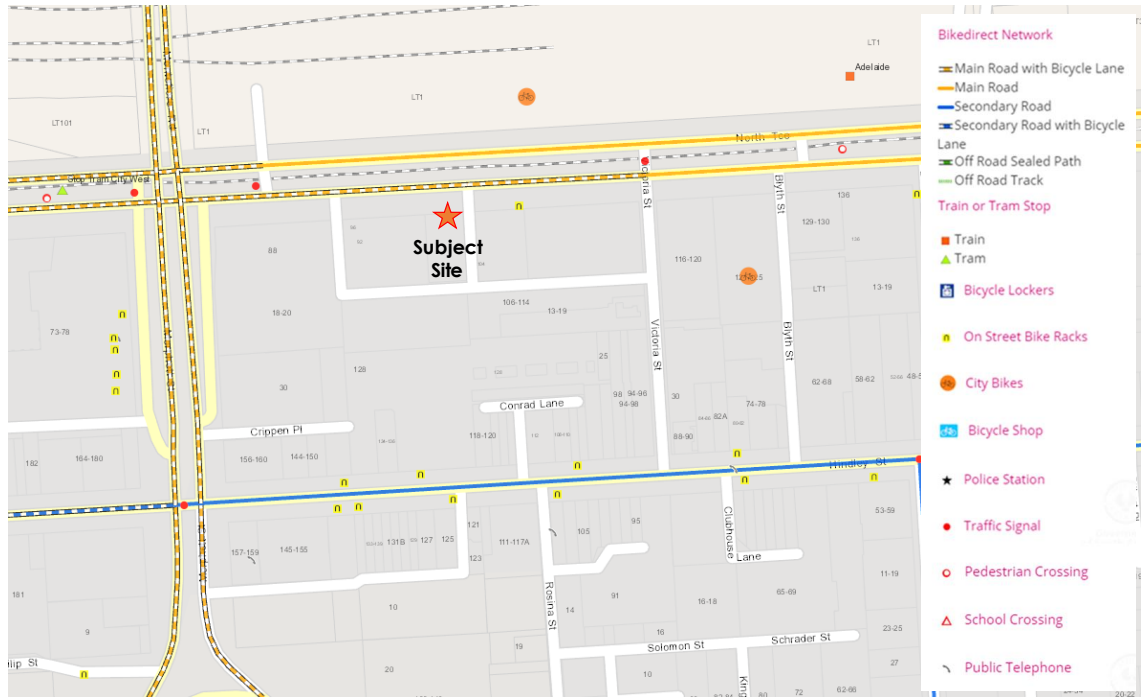
The subject site is located within the Primary Pedestrian Area in the Adelaide (City) Council Development Plan. Pedestrian paths are located on either side of North Terrace.

### 2.3.3 Cycle Infrastructure

Cycle infrastructure in vicinity of the site is shown in Figure 2.4. Bicycle lanes are located on southern side of North Terrace adjacent the proposed site. Five bicycle racks are located in front of the subject site and an additional four racks are located on the footpaths on North Terrace approximately 60 metres to the east of the site.



**Figure 2.4: Cycle Infrastructure**

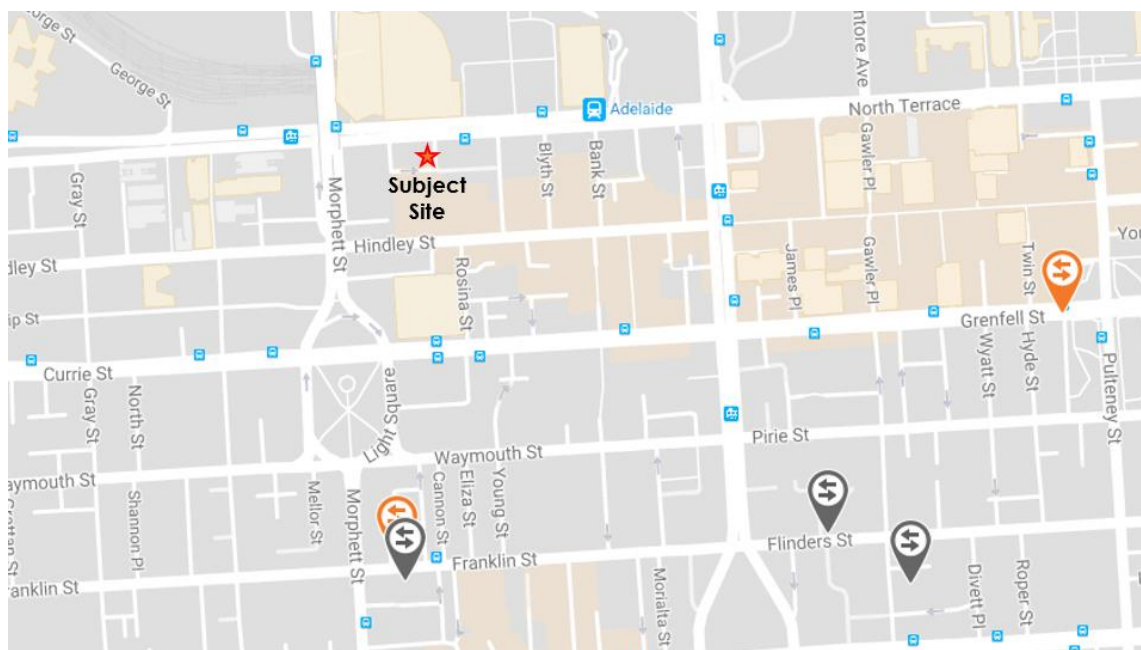


(Reproduced from CycleInstead Website)

### 2.3.4 Local Car Sharing Services

Car sharing is a commercial alternative to car ownership for individuals and businesses allowing members to access shared vehicles for periods of time. This is achieved through hourly rates and subscriptions to the service. Car sharing is best suited to locations with good access to other transport modes such as public transport, walking and cycling. Figure 2.5 shows the locations of car sharing pods operated by GoGet. As the subject site is close to the free tram service and readily available public transport, these car sharing pods can be easily reached.

**Figure 2.5: Car Sharing Locations**



## 3. Development Proposal

### 3.1 Land Uses

The proposal includes the construction of an apartment hotel with 60 two-bedroom units. The land uses for each level is summarised in Table 3.1.

**Table 3.1: Development Schedule**

Level	Use
Ground Floor	Reception, staff office, service rooms, main lobby, drop-off zone and staff parking
First Floor	Business lounge, conference rooms, staff rooms, gym, and amenities
12 Storeys (Second Floor and above)	5 two-bedroom apartments on each level

### 3.2 Car Parking

Two (2) staff parking are proposed, accessed via the drop off zone.

### 3.3 Drop Off / Pick Up Zone

A drop-off zone is proposed at the eastern end of the proposed building accessible via Bernath Place.

### 3.4 Vehicle Access

Vehicle access to the drop-off zone and service vehicle access to the loading bay will be via Bernath Place off North Terrace. Light vehicles will access the site via Bernath Place and use the existing Right of Way (ROW) (servicing The Terrace Public Car Park) to reenter North Terrace.

### 3.5 Bicycle Facilities

A total of ten (10) bicycle parking spaces are proposed, incorporating five (5) visitor and five (5) staff parking spaces. Visitor bicycle parking spaces are proposed to be located within the drop off zone, in close proximity to the drop off zone entry, and the staff parking spaces are located within the staff parking area.

Five existing bicycle racks are located directly in front of the proposed site on footpath on North Terrace. Additional four bicycle racks are located within 60 metres walking distance to the east of the proposed site on North Terrace.

### 3.6 Pedestrian Facilities

Pedestrian access will be provided directly onto North Terrace and within the pickup/set down area. Pedestrian paths will be provided within the pickup/set down area to provide access to the entrances.



### 3.7 Deliveries and Refuse Collection

It is proposed that waste from the site would be collected by refuse vehicles up to the size of 7.3m in length and 2.4m in height. The specifications of the anticipated refuse vehicle is enclosed in Appendix A. It is understood that refuse collection will occur outside of peak operating periods.

It is anticipated that deliveries to the proposed development would be made primarily by light vehicles, with the occasional delivery by vehicles up to a 6.4m SRV. Light vehicle deliveries would either utilise the existing loading zone spaces on Bernath Place or the staff parking area to undertake loading within the site. Occasional deliveries by vehicles up to a 6.4m SRV would occur in the same manner as the refuse collection vehicle and occur outside of peak operating periods.

## 4. Car Parking

---

### 4.1 Development Plan Car Parking Requirements

The parking requirement applicable for this development is listed in Table Adel/7 in the Adelaide (City) Development Plan (Dated 20 June 2017). There is no minimum parking requirement applicable for this type of development use in the Capital City Zone and within the Primary Pedestrian Area.

### 4.2 Adequacy of Parking Supply

The development proposes two garaged staff parking spaces accessible via Bernath Lane. The provision of the two parking spaces is considered adequate given the operational nature of the development, and as the Development Plan requires no minimum car parking for the proposed development.

A drop-off zone is proposed for customer pick-up or drop-off through Bernath Lane.

### 4.3 Car Parking Layout

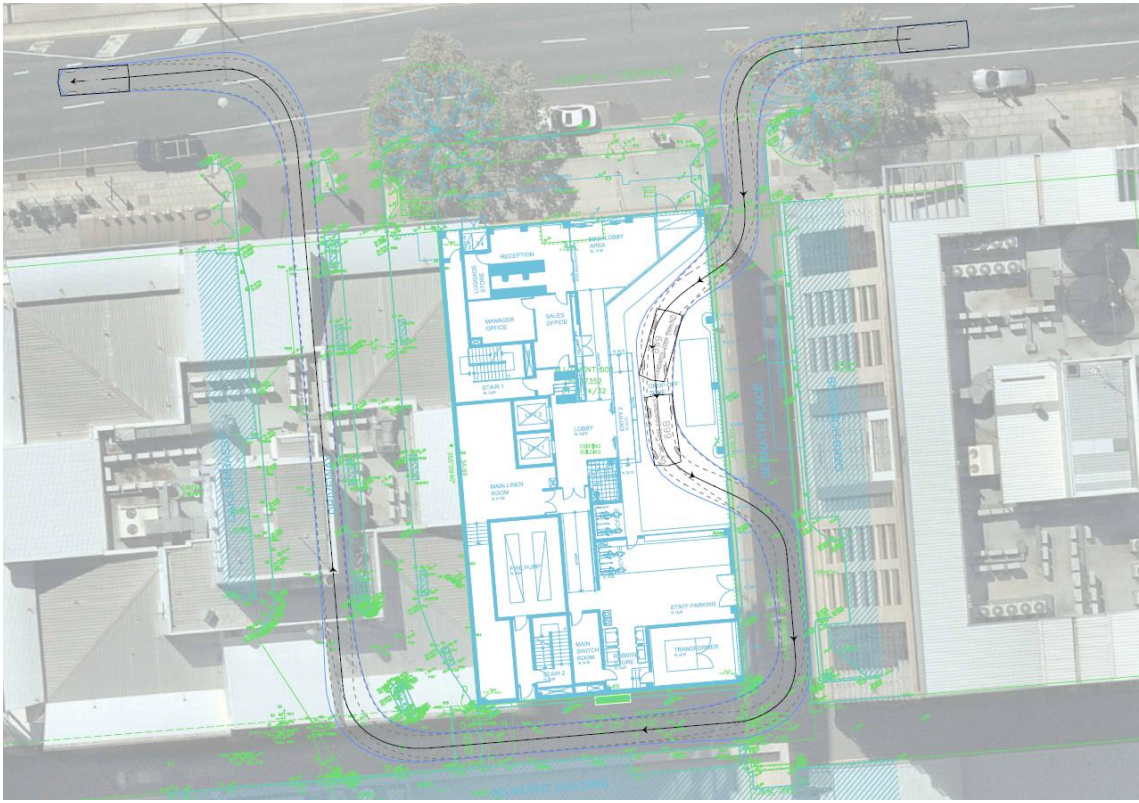
The parking layout has been designed in accordance with Australian Standard/New Zealand Standard for Off Street Car parking (AS/NZS2890.1:2004 and AS/NZS2890.6:2009).

Some of the design features are as follows:

- Double garage door width measured to be 5.8 metres wide and internal garage length measured to be 6.0 metres, exceeding the minimum length requirements for garage parking spaces.
- The proposed configuration of pickup/set down area will be able to cater for movements of a B99 vehicle as shown in Figure 4.1.



Figure 4.1: B99 Vehicle Movements



## 5. Sustainable Transport Infrastructure

### 5.1 Bicycle End of Trip Facilities

The recommended bicycle parking rates for new developments in Adelaide City Council is listed in Table Adel/6 of the Adelaide (City) Development Plan (consolidated 20 June 2017). The bicycle parking rates that are applicable to the proposed development are shown as follows:

<i>Land Use</i>	<i>Employees</i>	<i>Visitors</i>
Serviced Apartment	1 per 20 employees	2 for the first 40 rooms, plus 1 for every additional 40 rooms

The number of employees on-site is assumed to be less than 20. The development plan requirement for the provision of bicycle facilities for the subject use is set out in Table 5.1.

**Table 5.1: Standard Requirement for Bicycle Facilities**

Use	Size	Development Plan Requirement		Total
		Employee	Visitor	
Hotel Apartments	60 two-bedroom units (120 rooms)	1	4	5

### 5.2 Adequacy of Bicycle Parking Supply

The proposed supply of ten (10) bicycle spaces, incorporating five (5) visitor and five (5) staff, is considered to be appropriate as it meets the development plan bicycle parking space requirements.

In addition, five (5) bicycle racks are located on footpaths directly in front of the proposed site, with capacity to accommodate ten bicycles. Four (4) bicycle racks are also available approximately 60 metres to the east of the proposed site.

### 5.3 Walking and Cycling Network

Existing pedestrian facilities (footpaths and kerb ramps) and bicycle lanes will be maintained adjacent the site on North Terrace.

### 5.4 Public Transport

The site is accessible by public transport as discussed in Section 2.3.1.



## 6. Loading Facilities

### 6.1 Development Plan Requirements

Principle of Development Control (PDC) 241 in the Transport and Access section of the Adelaide (City) Development Plan (dated 20 June 2017) provides guidance for loading/unloading facilities. PDC 241 is as follows;

*“Facilities for the loading and unloading of courier, delivery and service vehicles and access for emergency vehicles should be provided on-site as appropriate to the size and nature of the development. Such facilities should be screened from public view and designed, where possible, so that vehicles may enter and leave in a forward direction.”*

### 6.2 Refuse Collection

It is proposed that waste from the site would be collected by refuse vehicles up to the size of 7.3m in length and will occur outside of peak operating periods to minimise conflicts with light vehicles accessing Bernath Place. The vehicle specifications of the 7.3m refuse vehicle are enclosed in Appendix A.

As required by the Development Plan, the loading and waste collection area will be accessed via forward entry and exit from North Terrace. The refuse collection vehicle perform collection at the southern end of Bernath Place, perform a three-point turn, and exit back onto North Terrace in a forward direction.

The ability for a 7.3m Refuse Vehicle to undertake a three-point turn within Bernath Place is shown in Figure 6.1.

**Figure 6.1: 7.3m Refuse Vehicle Three Point Turn**



### 6.3 Proposed Loading Arrangements

It is anticipated that deliveries to the proposed development would be made primarily by light vehicles, with the occasional delivery by vehicles up to a 6.4m SRV and restricted to vehicles with a maximum vehicle height of 2.9m (to enable 300mm clearance to the obstruction 3.2m high above the roadway). Any deliveries by light vehicles would be able to utilise the staff garage parking to perform on site loading. In addition to the onsite loading, the available on street loading facilities may also be utilised. An SRV would access Bernath Place in the same manner as the 7.3m Refuse Vehicle.



## 7. Traffic Impact Assessment

### 7.1 Traffic Generation

#### 7.1.1 Drop-off Zone

GTA has considered empirical traffic generation data for similar developments within Adelaide to estimate traffic generation for the proposed development.

In this regard, survey observations have been made at the drop-off/pick-up areas during a typical weekday AM period at three hotels in Adelaide. The results of the surveys are summarised in Table 7.1.

**Table 7.1: GTA Survey Data of Drop-off/Pick-up Areas of Adelaide Hotels**

Site	Rooms Available	Trips Recorded	Trip Rate per Room
Rockford Hotel	78	18	0.23
Hilton Hotel	374	58	0.16
Holiday Inn	245	20	0.08
<b>Average</b>			<b>0.16</b>

GTA's surveys for serviced apartments in Melbourne obtained similar trip rates. The AM peak traffic generation for serviced apartments in Melbourne is averaged to be 0.28 vehicle movements per occupied unit. If the number of bedrooms for each unit is assumed to be two, the trip rate is 0.14 vehicle trips per room, which is consistent to GTA's survey data for hotels in Adelaide.

Based on the average Adelaide Hotel survey rate of 0.16 per room, the proposed 60 two-bedroom units (120 rooms) will likely to generate in the order of 20 vehicle trips in the AM peak hour. Assuming a peak-to-daily ratio of 10% the proposed development is likely to generate up to 200 vehicle trips daily.

### 7.2 Distribution and Assignment

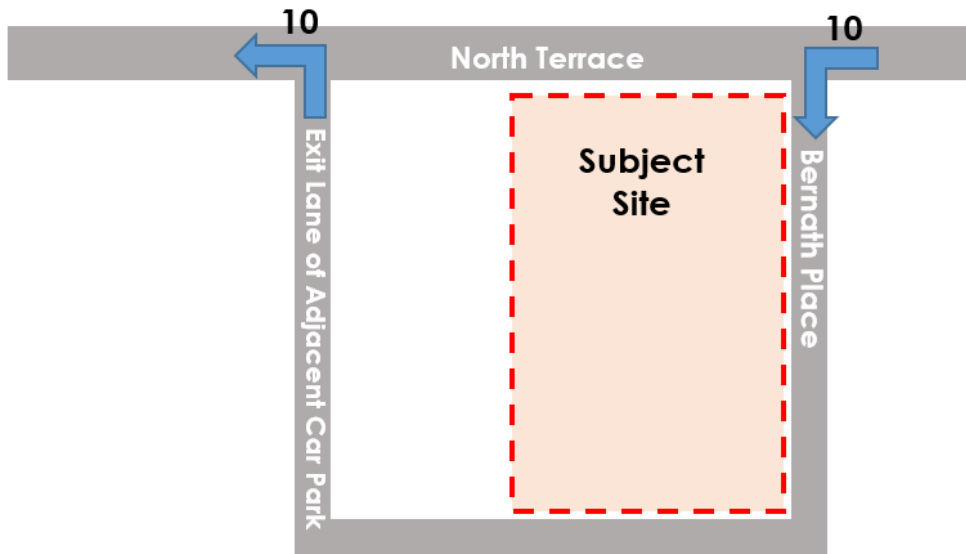
The directional distribution and assignment of traffic generated by the proposed development will be influenced by a number of factors, including:

- i configuration of the road network in the immediate vicinity of the site;
- ii existing operation of intersections providing access around the local road network;
- iii distribution of households in the vicinity of the site;
- iv configuration of access points to the site;

Vehicle access from and to North Terrace would be left-in and left out only, due to the raised centre median and tram lines on North Terrace. All vehicle movements will be entering from North Terrace onto Bernath Place and exiting onto North Terrace through the existing exit lane of the adjacent car park development.

Based on the traffic generation estimated during peak period, the estimated turning counts for the AM peak hour are shown in Figure 7.1.

Figure 7.1: AM Peak Hour Traffic Volume



### 7.3 Traffic Impact

Against the existing traffic volumes in the vicinity of the site (28,100 vehicles per day), the addition of 200 vehicle movements per day generated by the proposed development is not expected to compromise the safety or function of the surrounding road network.



## 8. Conclusion

Based on the analysis and discussions presented within this report, the following conclusions are made:

- i The proposed development will have no minimum car parking requirement according to the development plan.
- ii The proposed supply of 2 car parking spaces is considered to be appropriate as the development plan sets no minimum or maximum parking requirement for the proposed development type in the Capital City Zone in the Central Business Policy Area.
- iii The proposed parking layout is generally consistent with the dimensional requirements as set out in the Australian/New Zealand Standards for Off Street Car Parking (AS/NZS2890.1:2004 and AS/NZS2890.6:2009).
- iv The provision of ten (10) bicycle parking spaces is considered adequate as it meets the Development Plan requirement.
- v Refuse collection will occur outside of peak periods, using vehicles up to 7.3m in length, entering and entering North Terrace in a forward direction.
- vi Loading is anticipated to occur via light vehicles, with the occasional delivery by vehicles up to a 6.4m SRV in length. Light vehicles will perform loading on-site or within the existing loading bays on Bernath Place. Deliveries by vehicles up to a 6.4m SRV will occur in a similar manner to refuse collection.
- vii The site is expected to generate up to 20 and 200 vehicle movements in any peak hour and daily respectively.
- viii There will be adequate capacity in the surrounding road network to cater for the traffic generated by the proposed development.

# Appendix A

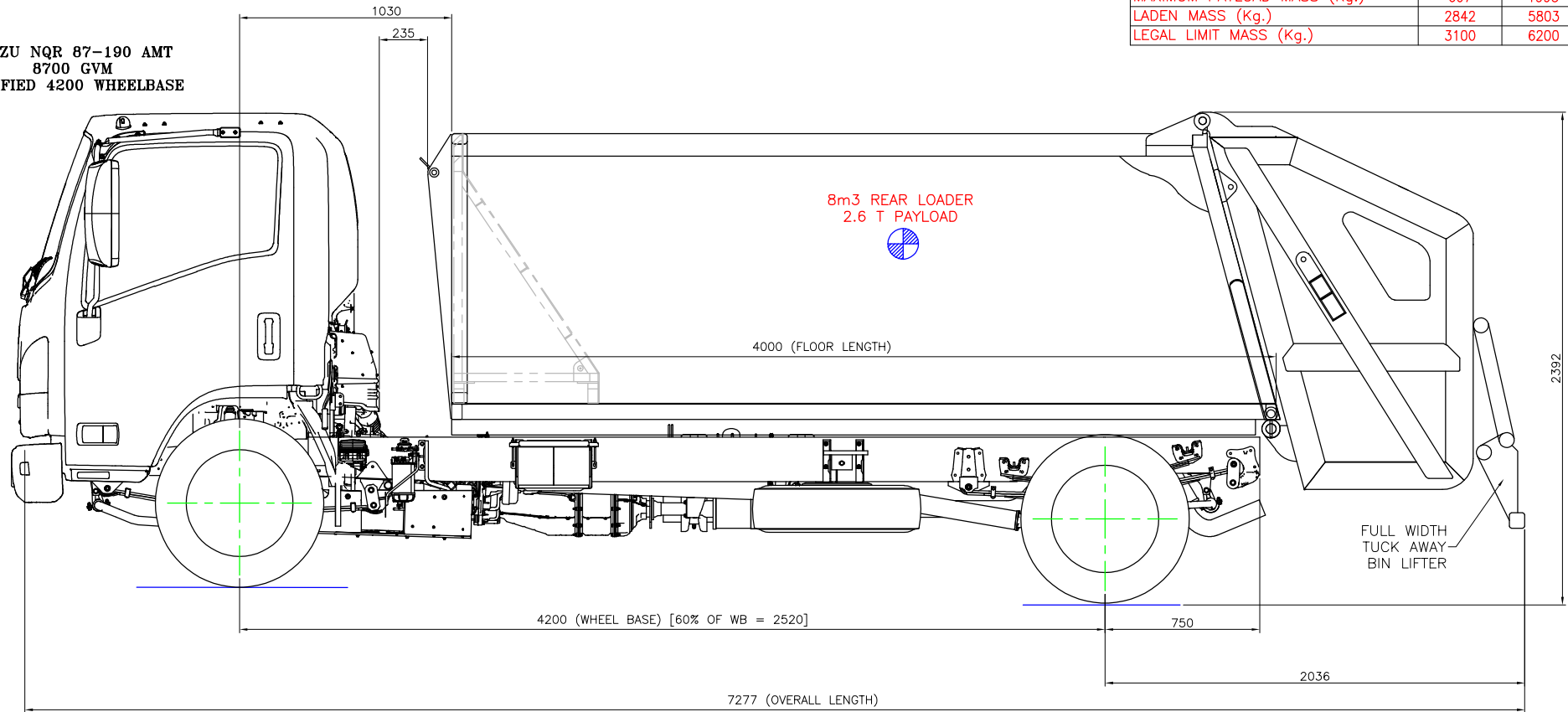
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## 7.3m Refuse Vehicle Specifications



ISUZU NQR 87-190 AMT  
8700 GVM  
MODIFIED 4200 WHEELBASE

	FRONT AXLE	REAR AXLE	TOTAL
CAB CHASSIS MASS (Kg.)	1840	1025	2865
BODY MASS (Kg.)	310	2790	3100
DRIVER (Kg.)	86	-6	80
FINISHED VEHICLE MASS (Kg.)	2236	3809	6045
MAXIMUM PAYLOAD MASS (Kg.)	607	1993	2600
LADEN MASS (Kg.)	2842	5803	8645
LEGAL LIMIT MASS (Kg.)	3100	6200	8700



WELDING NOTE:- ALL WELDING TO AS1554

PAINT NOTE:- CUSTOMER TO ADVISE

GENERAL NOTE:-

1. MASSES & DIMENSIONS SPECIFIED ARE THEORETICAL AND ARE SUBJECT TO FINAL SPECIFICATION & MANUFACTURING TOLERANCE
2. THE INDICATED PAYLOAD CENTER OF GRAVITY POSITION IS AN ESTIMATE ONLY. THE COG OF EACH LOAD WILL DETERMINE THE MAXIMUM PAYLOAD.
3. VEHICLE OWNER TO CONFIRM AXLE WEIGHT WITH RELATIVE AUTHORITIES AND VEHICLE SPECIFICATION WITH BODY MANUFACTURER.
4. OVERLOAD OVER AND ABOVE SPECIFICATION CAN IMPACT ON THE PAYLOAD.
5. MASSES SPECIFIED INCLUDE FUEL AS PER TRUCK MANUFACTURER SPECIFICATION & FULL HYDRAULIC OIL.

**CLIENT'S APPROVAL**

COMPANY :-  
NAME :-  
DATE :-  
SIGN :-

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REV	DESCRIPTION	DATE	REVD BY	DCR/ECN
A	ISSUED FOR APPROVAL	07/03/2017	V.Patel	-

SCALE : NOT TO SCALE			
DATE	REVD BY	DCR/ECN	
07/03/2017	V.Patel	-	

TOLERANCES
UNLESS OTHERWISE SPECIFIED
LINERAR
0-100 = +-0.5
101-300 = +-1.0
301-1500 = +-2.0
>1500 = +-4.0
ANGULAR
+- 0.5'

DESIGNED BY :-	DATE :-
V.Patel	07/03/2017
CHECKED BY :-	DATE :-
R.Patel	07/03/2017
APPROVED BY :-	DATE :-
C.Papas	07/03/2017

**PAPAS** WASTE EQUIPMENT INTERNATIONAL Pty Ltd

TITLE  
8m3 REAR LOADER ON ISUZU NQR 87-190 AMT

SHEET SIZE  
-

DWG NO  
RL-SALES-0018

REV  
A

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Melbourne

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P +618 6169 1000  
E perth@gta.com.au





## Waste Management Statement

Location: 100 North Terrace, Adelaide  
 Developer: Emmett  
 Proposed Development: 14 level hotel in the form of serviced apartments for short and long stay, comprising 120 rooms together with associated facilities  
 Land Use: Hotel

The proposed development is to construct a 14 level hotel in the form of serviced apartments for short and long stay, comprising 120 rooms together with associated facilities on land located at 100 North Terrace, Adelaide. The serviced apartments will be operated by the Quest Group. The building includes a loading dock which is accessed via Bernath Place on the Eastern side of the property, at the same location as Oak Horizon waste storage room.

Using the Design Guide for Residential Recycling appendix 2: Waste Resource Generation Rates produced by the Adelaide City Council, the following waste management solutions are provided.

### Calculation of average waste generation for 100 North Terrace

LAND USE	WASTE excluding Food	CO-MINGLED RECYCLING	GREEN ORGANICS Including food
<b>Hotel</b> (120 hotel guest rooms)	5 litres / bedroom / day	1 litres / bedroom / day	1 litres / bedroom / day
<b>Total/ Day:</b>	<b>600 litres / day</b>	<b>120 litres / day</b>	<b>120 litres / day</b>
<b>Conference Room</b> (100m <sup>2</sup> )	25 litres / 10m <sup>2</sup> dining area/ day	5 litres / 10m <sup>2</sup> dining area/ day	0.25 litres / 10m <sup>2</sup> dining area/ day
<b>Total/ Day:</b>	<b>250 litres / day</b>	<b>50 litres / day</b>	<b>2.5 litres / day</b>
<b>Business Lounge</b> (40m <sup>2</sup> )	25 litres / 10m <sup>2</sup> dining area/ day	5 litres / 10m <sup>2</sup> dining area/ day	0.25 litres / 10m <sup>2</sup> dining area/ day
<b>Total/ Day:</b>	<b>100 litres / day</b>	<b>20 litres / day</b>	<b>1 litres / day</b>
<b>Total/ Day:</b>	<b>950 litres / day</b>	<b>190 litres / day</b>	<b>123.5 litres / day</b>
<b>Total/ 3 Days:</b>	<b>2850 litres / 3 days</b>	<b>570 litres / 3 days</b>	<b>370.5 litres / 3 days</b>

All waste shall be collected and transported down into the ground floor waste room by the hotel cleaning staff. The ground floor waste storage room has adequate capacity for the buildings load requirements base on waste being collected every 3 days.



The following table is a summary of number of waste bins requirements in the building, based on waste being collected every 3 days:

WASTE REMOVAL	GENERAL WASTE BIN (RED)	RECYCLABLE BIN (YELLOW)	GREEN WASTE BIN (GREEN)
Total Bin/ 3 Days:	1100L General Waste Bin (red): 2  660L General Waste Bin (red): 1	660L Recyclable Materials (yellow): 1	240L Organic Waste (green): 2

A waste removal company will be contracted by the hotel operator to manage waste collection at every 3 days, the waste bins will be wheeled out to Bernath Place via Staff Parking on ground floor. The location of this loading area is on Bernath Place adjacent to Oaks Horizons' waste store room. Waste removal will be the same as the Oaks Horizons building, Cleanaway service the Oaks Horizons will service the proposed development at the same time.





PEDESTRIAN WIND ENVIRONMENT STATEMENT  
QUEST, 100 NORTH TERRACE, ADELAIDE

WE272-01F02(REV0)- WS REPORT

MAY 31, 2018

Prepared for:

Emmett Property

205 Hutt Street

Adelaide SA 5000

**WINDTECH Consultants Pty Ltd** ABN 72 050 574 037

Head Office: 607 Forest Road, Bexley, NSW 2207, Australia

**P** +61 2 9503 0300 **E** [reception@windtechglobal.com](mailto:reception@windtechglobal.com) **W** [www.windtechconsult.com](http://www.windtechconsult.com)

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## DOCUMENT CONTROL

Date	Revision History	Issued Revision	Prepared By (initials)	Instructed By (initials)	Reviewed & Authorised by (initials)
May 31, 2018	Initial.	0	TH	AB	TH

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*This document is issued subject to review and authorisation by the Team Leader noted by the initials printed in the last column above. If no initials appear, this document shall be considered as preliminary or draft only and no reliance shall be placed upon it other than for information to be verified later.*

*This document is prepared for our Client's particular requirements which are based on a specific brief with limitations as agreed to with the Client. It is not intended for and should not be relied upon by a third party and no responsibility is undertaken to any third party without prior consent provided by Windtech Consultants. The information herein should not be reproduced, presented or reviewed except in full. Prior to passing on to a third party, the Client is to fully inform the third party of the specific brief and limitations associated with the commission.*



## EXECUTIVE SUMMARY

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This report is in relation to the proposed development located at 100 North Terrace, Adelaide, and presents an opinion on the likely impact of the proposed design on the local wind environment to the critical outdoor areas within and around the subject development. The effect of wind activity is examined for the four predominant wind directions for the Adelaide region; namely the north-easterly, south-westerly, westerly and north-westerly winds. The analysis of the wind effects relating to the proposed development was carried out in the context of the local wind climate, building morphology and land topography.

The conclusions of this report are drawn from our extensive experience in this field and are based on an examination of the architectural drawings which have been prepared by the project architect Matthews Architects, received May, 2018. No wind tunnel testing has been undertaken for the subject development, and hence this report addresses only the general wind effects and any localised effects that are identifiable by visual inspection. Any recommendations in this report are made only in-principle and are based on our extensive experience in the study of wind environment effects.

The results of the assessment indicate the wind conditions within the various outdoor trafficable areas of the subject development are expected to be acceptable for its intended uses or better than or equivalent existing wind conditions. The pedestrian footpaths benefits from the shielding provided by the subject and neighbouring buildings, and the proposed canopies along the North Terrace frontage mitigating the potential down-wash wind effects off the building façade. Hence the proposed canopies are recommended to be retained in the final design of the development. Similarly the BBQ deck area and private balconies benefits from the shielding from the subject and neighbouring buildings, as well as their recessed design into the building footprint. The inclusion of the proposed balustrade along the perimeter edge is expected to be effective in further enhancing the wind conditions, hence it is recommended to be retained in the final design of the development.

## **1 DESCRIPTION OF THE DEVELOPMENT AND SURROUNDINGS**

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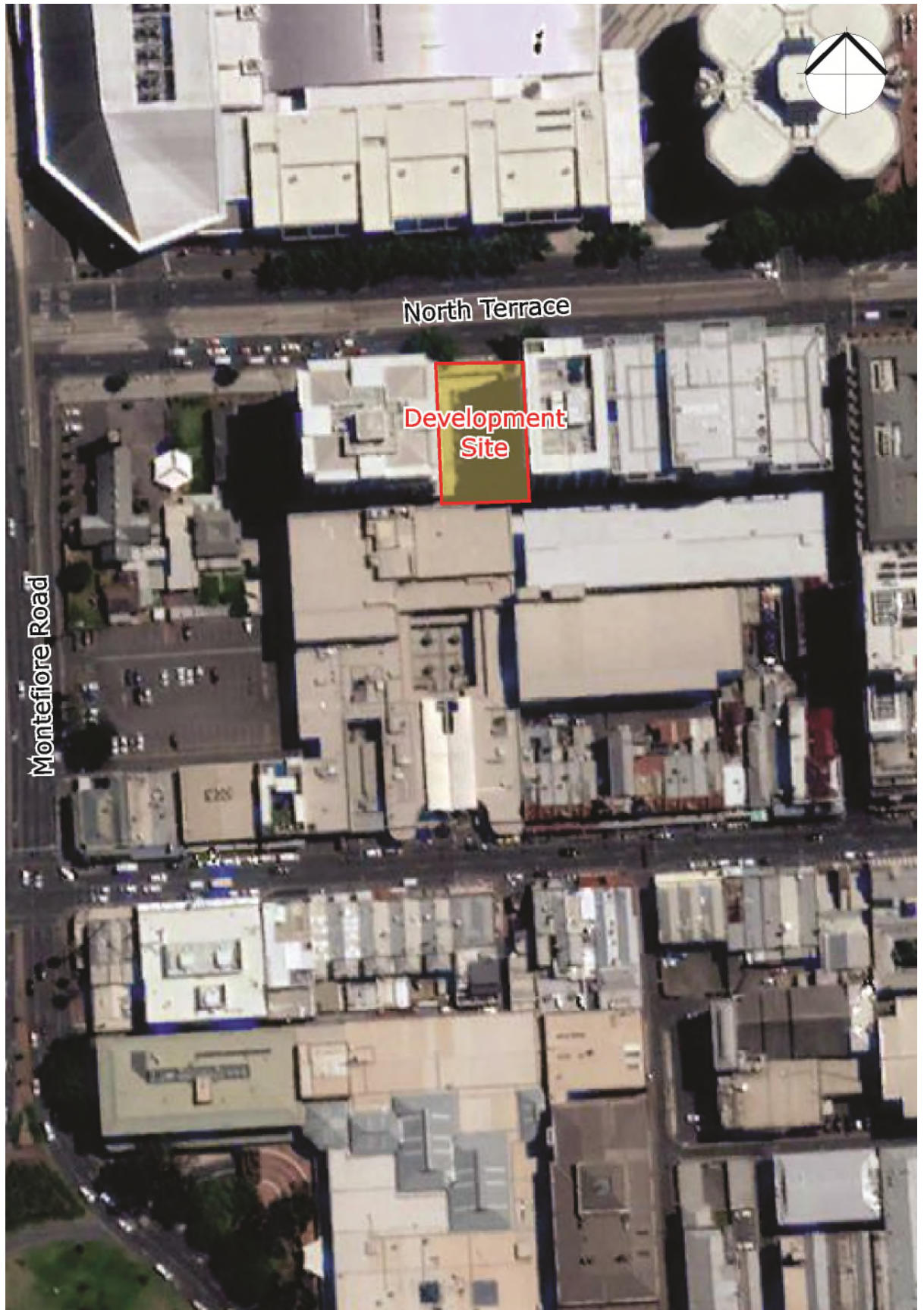
The development site is located at 100 North Terrace, Adelaide, and is bound by North Terrace to the north, Bernath Place to the east and south, and the Oaks Embassy building to the west. The site is located along the northern boundary of the Adelaide CBD that is comprised predominantly low-rise to mid-rise commercial/retail buildings. Further away to the north of the site is the Adelaide Convention Centre and the River Torrens. A survey of the local land topography indicates a general decline towards the north of the site to the River Torrens. An aerial image of the site and the surroundings is shown in Figure 1.

The proposed development consists of a 15 storey high mixed-use building with lobbies, business related tenancies and mechanical services proposed on the ground and first floor, with the remaining floors comprised of private residential tenancies.

The critical trafficable outdoor areas associated with the proposed development, which are the focus of this assessment with regards to wind effects, are detailed as follows:

- The ground floor pedestrian footpath along the North Terrace frontage of the site.
- The ground floor pedestrian footpath along the western boundary of the site.
- The ground floor pedestrian footpath along the Bernath Place frontage of the site.
- The BBQ deck area on the first floor.
- Various private balconies throughout the development.





**Figure 1: Aerial Image of the Site Location**

## 2 WIND CLIMATE OF THE ADELAIDE REGION

---

The Adelaide region is governed by four principle wind directions, and these can potentially affect the subject development. These winds prevail from the north-east, south-west, west and north-west. A summary of the principal time of occurrence of these winds throughout the year is presented in Table 1. This summary is based on an analysis of data recorded from 1955 to 2002 obtained by the Bureau of Meteorology from the meteorological observation station located at Adelaide Airport.

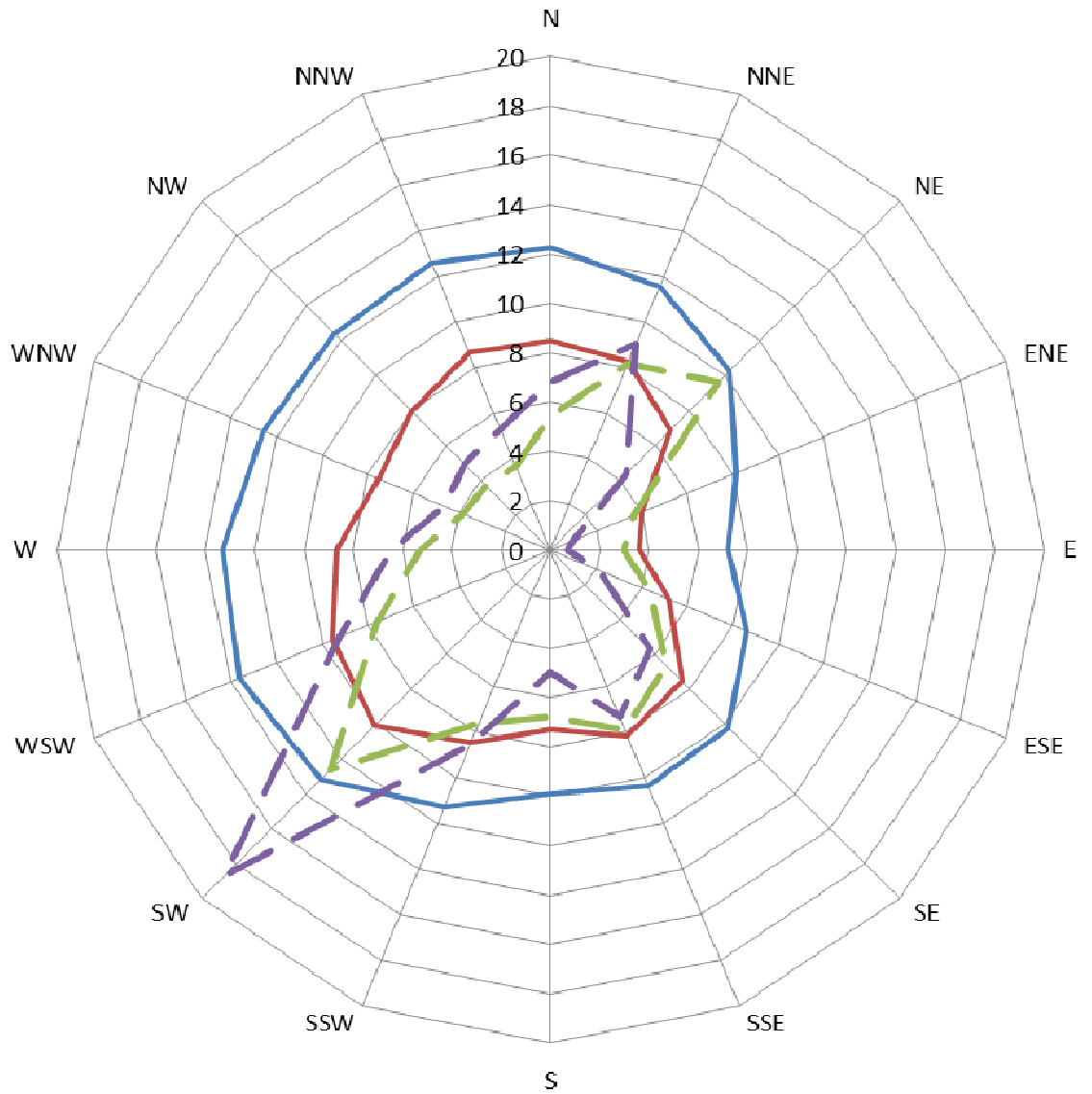
**Table 1: Principal Time of Occurrence of Winds for the Adelaide Region**

Month	Wind Direction			
	South-Westerly	Westerly	North-Westerly	North-Easterly
Summer	X			
Autumn	X	X		X
Winter	X	X	X	X
Spring	X	X		X

A directional plot of the annual and weekly recurrence winds for the Adelaide region is shown in Figure 2. The frequency of occurrence of these winds is also shown in Figure 2. This plot has been produced based on an analysis of 48 years of recorded directional mean wind speed data (from 1955 to 2016) obtained from the meteorological observation station located at Adelaide Airport.

The strongest winds in Adelaide tend to occur during the spring season and are predominantly from the south-westerly through to the westerly and the north-easterly directions. Westerly through to north-westerly winds do not occur frequently, however when they do occur they tend to be fairly strong, which is usually during the afternoons of the winter months. Winds from the south-west are by far the most frequent and occur throughout the entire year.





- Maximum 1 year recurrence (annual) mean winds (m/s)
- Maximum weekly recurrence mean winds (m/s)
- - - Directional Frequency (%)
- - - Directional Frequency of winds greater than 20 kph(%)

**Figure 2: Annual and Weekly Recurrence Mean Wind Speeds, and Frequencies of Occurrence, for the Adelaide Region**

### 3 WIND EFFECTS ON PEOPLE

The acceptability of wind in any area is dependent upon its use. For example, people walking or window-shopping will tolerate higher wind speeds than those seated at an outdoor restaurant. Various other researchers, such as Davenport, Lawson, Melbourne, Penwarden, etc, have published criteria for pedestrian comfort for pedestrians in outdoor spaces for various types of activities. Some Councils and Local Government Authorities have adopted elements of some of these into their planning control requirements in Australia.

The following table is an example, which was developed by Penwarden in 1975, and describes the effects of various wind intensities on people. Note that the applicability column relates to the indicated wind conditions occurring frequently (exceeded approximately once per week on average). Higher ranges of wind speeds can be tolerated for rarer events.

**Table 2: Summary of Wind Effects on People (Penwarden, 1975)**

Type of Winds	Mean Wind Speed (m/s)	Effects	Applicability
Calm, light air	0 - 1.5	Calm, no noticeable wind.	Generally acceptable for Stationary, long exposure activities such as in outdoor restaurants, landscaped gardens and open air theatres.
Light breeze	1.6 - 3.3	Wind felt on face.	
Gentle breeze	3.4 - 5.4	Hair is disturbed, Clothing flaps.	
Moderate breeze	5.5 - 7.9	Raises dust, dry soil and loose paper. Hair disarranged.	Generally acceptable for walking & stationary, short exposure activities such as window shopping, standing or sitting in plazas.
Fresh breeze	8.0 - 10.7	Force of wind felt on body.	Acceptable as a main pedestrian thoroughfare
Strong breeze	10.8 - 13.8	Umbrellas used with difficulty, Hair blown straight, Difficult to walk steadily, Wind noise on ears unpleasant.	Acceptable for areas where there is little pedestrian activity or for fast walking.
Near gale	13.9 - 17.1	Inconvenience felt when walking.	
Gale	17.2 - 20.7	Generally impedes progress, Great difficulty with balance.	Unacceptable as a public accessway.
Strong gale	20.8 - 24.4	People blown over by gusts.	Completely unacceptable.

It should be noted that wind speeds can only be accurately quantified with a wind tunnel study. This assessment addresses only the general wind effects and any localised effects that are identifiable by visual inspection and the acceptability of the conditions for outdoor areas are determined based on their intended use (rather than referencing specific wind speeds). Any recommendations in this report are made only in-principle and are based on our extensive experience in the study of wind environment effects.



## 4 RESULTS AND DISCUSSION

---

The expected wind conditions are discussed in the following sub-sections of this report for the various outdoor areas within and around the subject development for each of the four predominant wind directions for the Adelaide region. The interaction between the wind and the building morphology in the area is considered and important features taken into account including the distances between the surrounding buildings and the proposed building form, their overall heights and bulk, as well as the surrounding landform. Note that only the potentially critical wind effects are discussed in this report.

### 4.1 Ground Level Areas

The pedestrian footpath along the North Terrace frontage of the site benefits from the shielding provided by the subject development and neighbouring buildings to the direct south-westerly winds, north-westerly and north-easterly winds. However due to the east-west alignment of North Terrace, it is exposed to the direct westerly winds. It should be noted that this is an existing wind condition for the site, and the inclusion of the subject development is not expected to adversely affect the wind conditions along the pedestrian footpath when the prevailing wind is from the west. The pedestrian footpath may potentially be exposed to prevailing north-westerly and north-westerly winds captured off the tower façade and down-washed onto the footpath below. The proposed impermeable canopies are expected to be effective in mitigating these potential down-wash wind effects, hence they are recommended to be retained in the final design of the development.

The wind conditions on the pedestrian footpath under the neighbouring Oaks Embassy building on the western boundary of the subject development site are expected to be similar to the existing wind conditions with the inclusion of the subject development. The pedestrian footpath benefits from the shielding provided by the subject and neighbouring buildings to the direct wind effects from the prevailing wind directions. The pedestrian footpath may be potentially exposed to existing down-wash wind effects off the northern façade of the Oaks Embassy building. The inclusion of the subject development is not expected to exacerbate these down-wash wind effects due to the 2m gap between the two components of the buildings likely to funnel the prevailing winds into the gap than redirecting it onto the footpath below.

The wind conditions on the pedestrian footpath along the Bernath Place frontage of the site are expected to be acceptable for its intended uses due to its location deep under the building overhang and the shielding provided by the subject development and neighbouring buildings.

### 4.2 BBQ Deck Area on the First Floor

The wind conditions within the BBQ deck area is expected to be acceptable for its intended use due to the recessed design of the BBQ deck area into the building footprint and the recessed design of the balconies into the building footprint and the shielding provided by the subject and surrounding buildings. The inclusion of the proposed balustrades along the northern perimeter

of the balconies is expected to be effective in further enhancing the wind conditions within the bbq deck area, hence it is recommended to be retained in the final design of the development.

### **4.3 Private Balconies**

The wind conditions for the various private balconies are expected to be acceptable for their intended use the recessed design of the balconies into the building footprint and the shielding provided by the subject and surrounding buildings. The inclusion of the proposed balustrades along the perimeter of the balconies is expected to be effective in further enhancing the local wind conditions. Hence the balustrades are recommended to be retained in the final design of the development.



7 May 2018

Emmett Property Pty Ltd  
205 Hutt Street  
ADELAIDE SA 5000

ATTENTION: Mr N Emmett

AJG Ref: LCE14885 - 004

Dear Nick

## **QUEST HOTEL DEVELOPMENT - 100 NORTH TERRACE, ADELAIDE ECOLOGICALLY SUSTAINABLE DEVELOPMENT (ESD) FEATURES**

As requested, we provide a summary of the ESD initiatives and features that have been incorporated into the concept design for the above development.

### **RENEWABLE ENERGY**

- Solar thermal panels, roof mounted evacuated tubes, providing domestic hot water to the serviced apartments.

### **PASSIVE DESIGN**

- Natural ventilation provided to serviced apartments.
- Solar performance glazing with shading coefficients that reduces thermal load on the air conditioning system.

### **ENERGY EFFICIENCY**

- High efficiency LED lighting generally throughout the serviced apartments, corridors, public spaces and back of house areas.
- Intelligent energy saving devices provided to the serviced apartments whereby lighting and air conditioning systems can only be operated when the hotel card is inserted within the room unit.
- High efficiency, 5 Star equivalent, variable refrigerant flow/volume (VRF/V) air conditioning systems serving the building.
- Minimum 4 Star energy rating appliances.

### **ENERGY MANAGEMENT**

- Energy sub-metering devices throughout the building to enable building managers to monitor and track energy across various end-uses.

### **WATER EFFICIENCY**

- Minimum 4.5 WELS Star rating appliances.
- Shower heads with a WELS rating of not less than 3 stars (7.5 L/min)
- Water closets with a WELS rating of not less than 4 stars (3.5 L/flush, dual flush)
- Basins and taps with a WELS rating of not less than 5 stars

We trust the above is satisfactory. Please contact the undersigned should you require further information.

Yours faithfully

**LUCID CONSULTING AUSTRALIA**

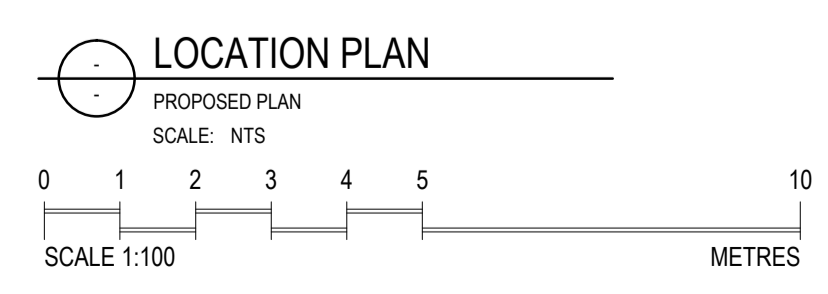


**ADAM GREAVES**  
DIRECTOR





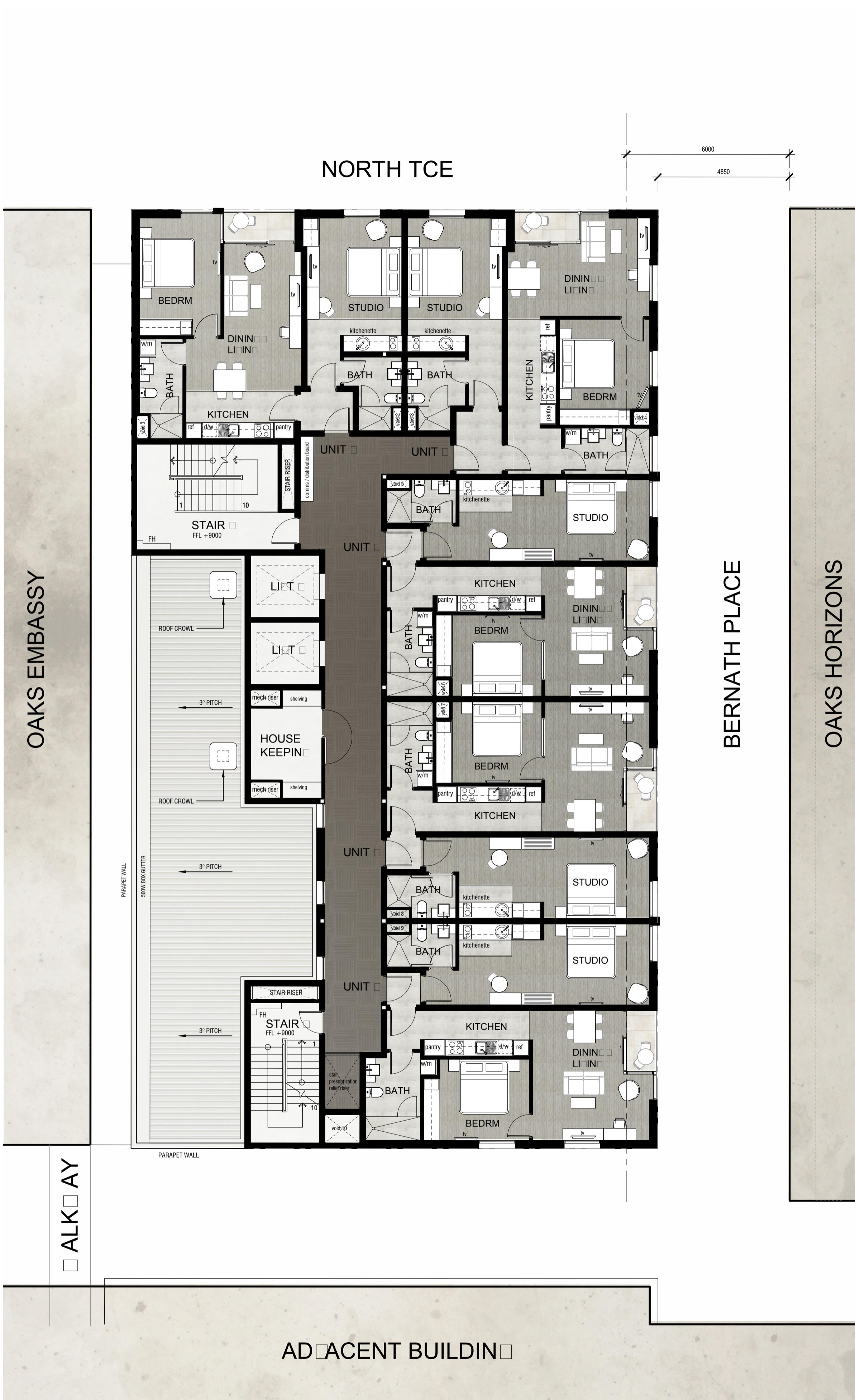
PROPOSED QUEST HOTEL,  
100 NORTH TERRACE, ADELAIDE











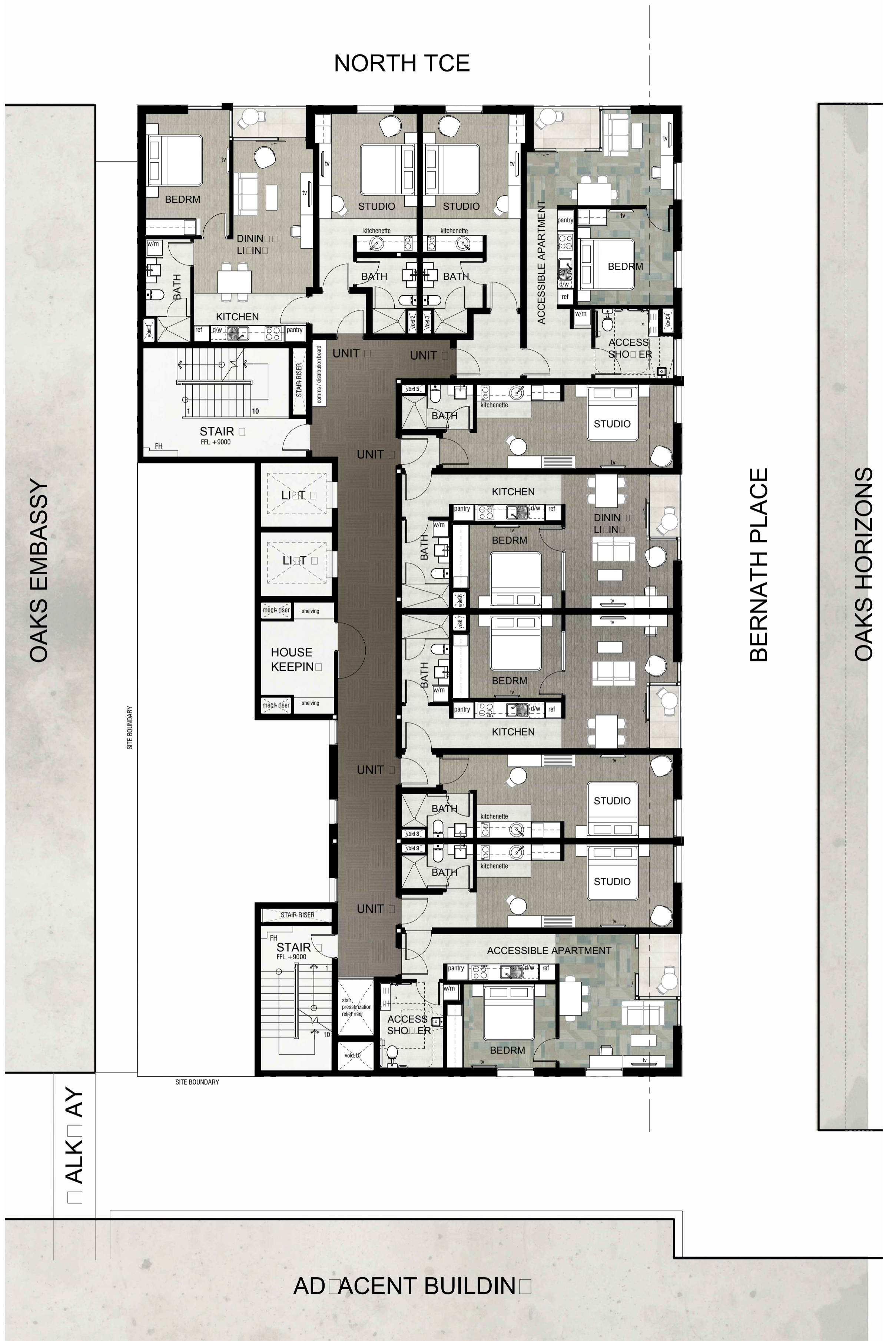
**ROOM SUMMARY**

ROOM	TYPE	INTERNAL FLOOR	BALCONY
UNIT 1	2BTK - STANDARD	75m <sup>2</sup>	2.7m <sup>2</sup>
UNIT 2	2BTK - STANDARD	79m <sup>2</sup>	2.5m <sup>2</sup>
UNIT 3	2BTK - STANDARD	75m <sup>2</sup>	2.3m <sup>2</sup>
UNIT 4	2BTK - STANDARD	75m <sup>2</sup>	2.3m <sup>2</sup>
UNIT 5	2BTK - STANDARD	77.3m <sup>2</sup>	2.3m <sup>2</sup>
GROSS FLOOR AREA:		BUILDING - 550m <sup>2</sup>	BALCONY - 12.1m <sup>2</sup>

**FIRST FLOOR PLAN**  
 PROPOSED PLAN  
 SCALE: 1:100  
 GROSS FLOOR AREA:  
 BUILDING - 636m<sup>2</sup>  
 BALCONY/BBQ DECKING - 36m<sup>2</sup>

**SECOND FLOOR PLAN**  
 PROPOSED PLAN  
 SCALE: 1:100  
 GROSS FLOOR AREA:  
 BUILDING - 550m<sup>2</sup>  
 BALCONY - 12.1m<sup>2</sup>





ROOM SUMMARY

ROOM	TYPE	INTERNAL FLOOR	BALCONY
UNIT 1	2BTK - STANDARD	75m <sup>2</sup>	2.7m <sup>2</sup>
UNIT 2	2BTK - STANDARD	79m <sup>2</sup>	2.5m <sup>2</sup>
	2BTK - ACCESSIBLE	78m <sup>2</sup>	3.2m <sup>2</sup>
UNIT 3	2BTK - STANDARD	75m <sup>2</sup>	2.3m <sup>2</sup>
UNIT 4	2BTK - STANDARD	75m <sup>2</sup>	2.3m <sup>2</sup>
UNIT 5	2BTK - STANDARD	77.3m <sup>2</sup>	2.3m <sup>2</sup>
	2BTK - ACCESSIBLE	76.3m <sup>2</sup>	3.2m <sup>2</sup>

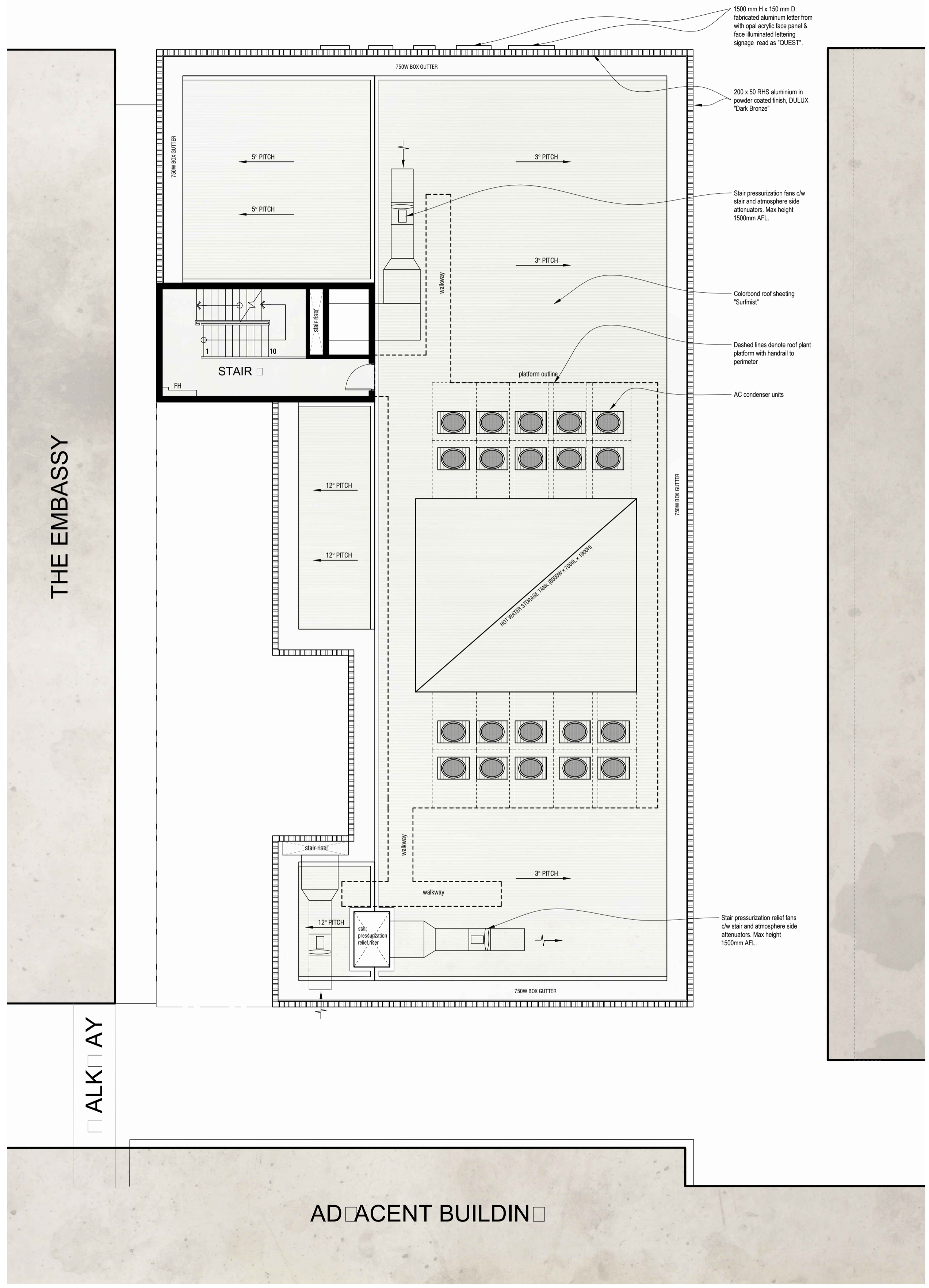
GROSS FLOOR AREA:  
 BUILDING - 550m<sup>2</sup>  
 BALCONY - 12.1m<sup>2</sup>

STANDARD APARTMENT - TYPICAL PLAN (3RD TO 5TH, 9TH TO 14TH)  
 PROPOSED PLAN  
 SCALE: 1:100

STANDARD & ACCESSIBLE APARTMENT - TYPICAL PLAN (6TH TO 8TH)  
 PROPOSED PLAN  
 SCALE: 1:100



NORTH TCE

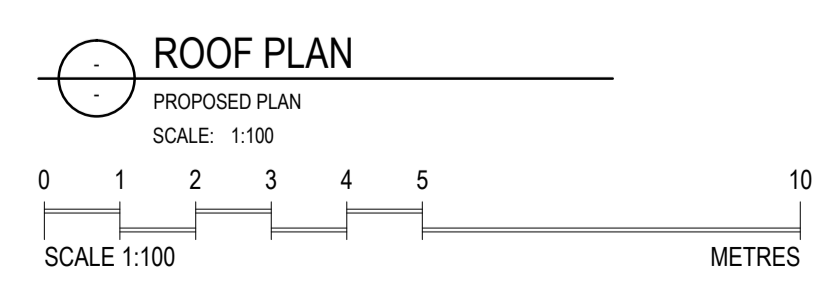


- 1500 mm H x 150 mm D fabricated aluminium letter from with opal acrylic face panel & face illuminated lettering signage read as "QUEST".
- 200 x 50 RHS aluminium in powder coated finish, CULULUX "Dark Bronze"
- Stair pressurization fans c/w stair and atmosphere side attenuators. Max height 1500mm AFL.
- Colorbond roof sheeting "Sunfire"
- Dashed lines denote roof plant platform with handrail to perimeter
- AC condenser units
- Stair pressurization relief fans c/w stair and atmosphere side attenuators. Max height 1500mm AFL.

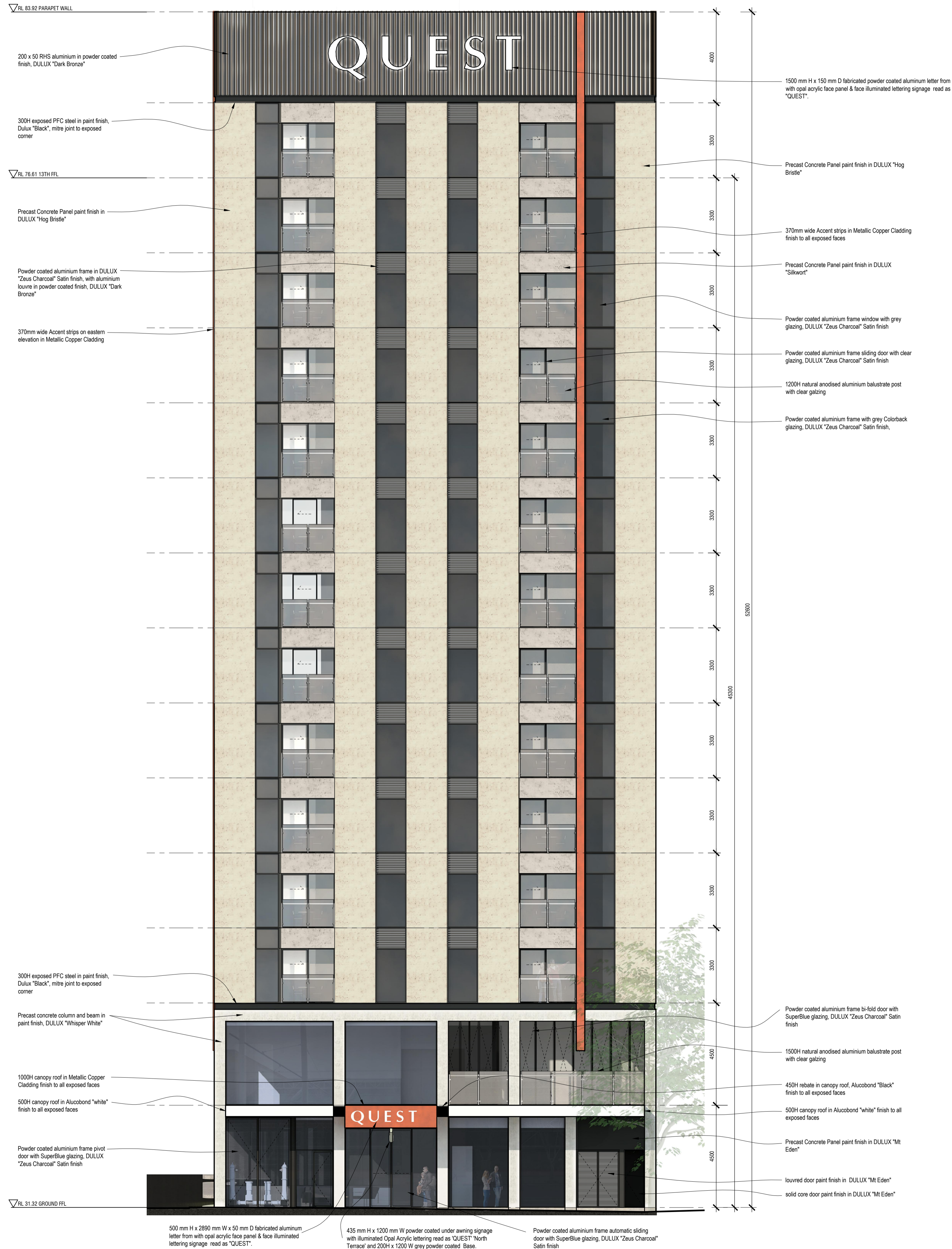
THE EMBASSY

ALK AY

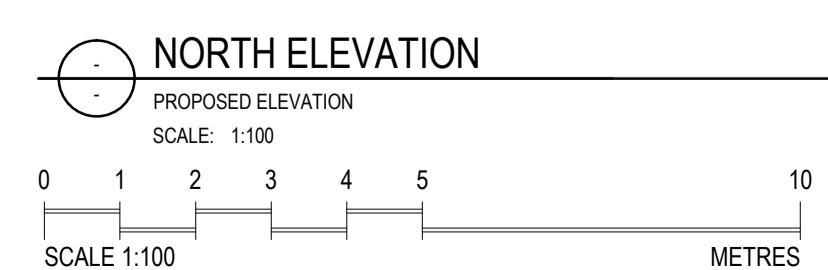
ADJACENT BUILDING







- Wall Paint  
DULUX "Teahouse" SN4G6  
Finish: AcraTex AcraShield Low Gloss
- Wall Paint  
DULUX "Hog Bristle" S14D1  
Finish: AcraTex AcraShield Low Gloss
- Wall Paint  
DULUX "Silkwool" SN4H2  
Finish: AcraTex AcraShield Low Gloss
- Wall and Door Paint  
DULUX "Mt Eden" PN2A7  
Finish: Low Sheen / Semi Gloss
- Wall Paint  
DULUX "Whisper White" SW1C2  
Finish: AcraTex AcraShield Low Gloss
- 2-PAC Signage Paint  
DULUX "Vintage Green" SG5E8  
Finish: Satin
- Powder Coat Paint  
DULUX "Zeus Charcoal Satin" 90087732  
Finish: Satin
- Powder Coat Paint  
DULUX "Dark Bronze" 9068184K  
Finish: Satin
- Feature Paint  
DULUX "Black" SH6H9  
Finish: Semi Gloss
- Feature Cladding  
Metallic Copper
- Feature Cladding  
Alucobond "White" 16-101  
Finish: Satin
- Feature Cladding  
Alucobond "Black" 326  
Finish: Satin
- Window / Door Glazing  
Vidlan - Everage "SuperBlue"
- Window / Door Glazing  
Vidlan - Everage "Grey"
- Window / Door Glazing  
Vidlan - EnergyTech "Clear"
- Roof Sheetting  
Colorbond "Surfimet"



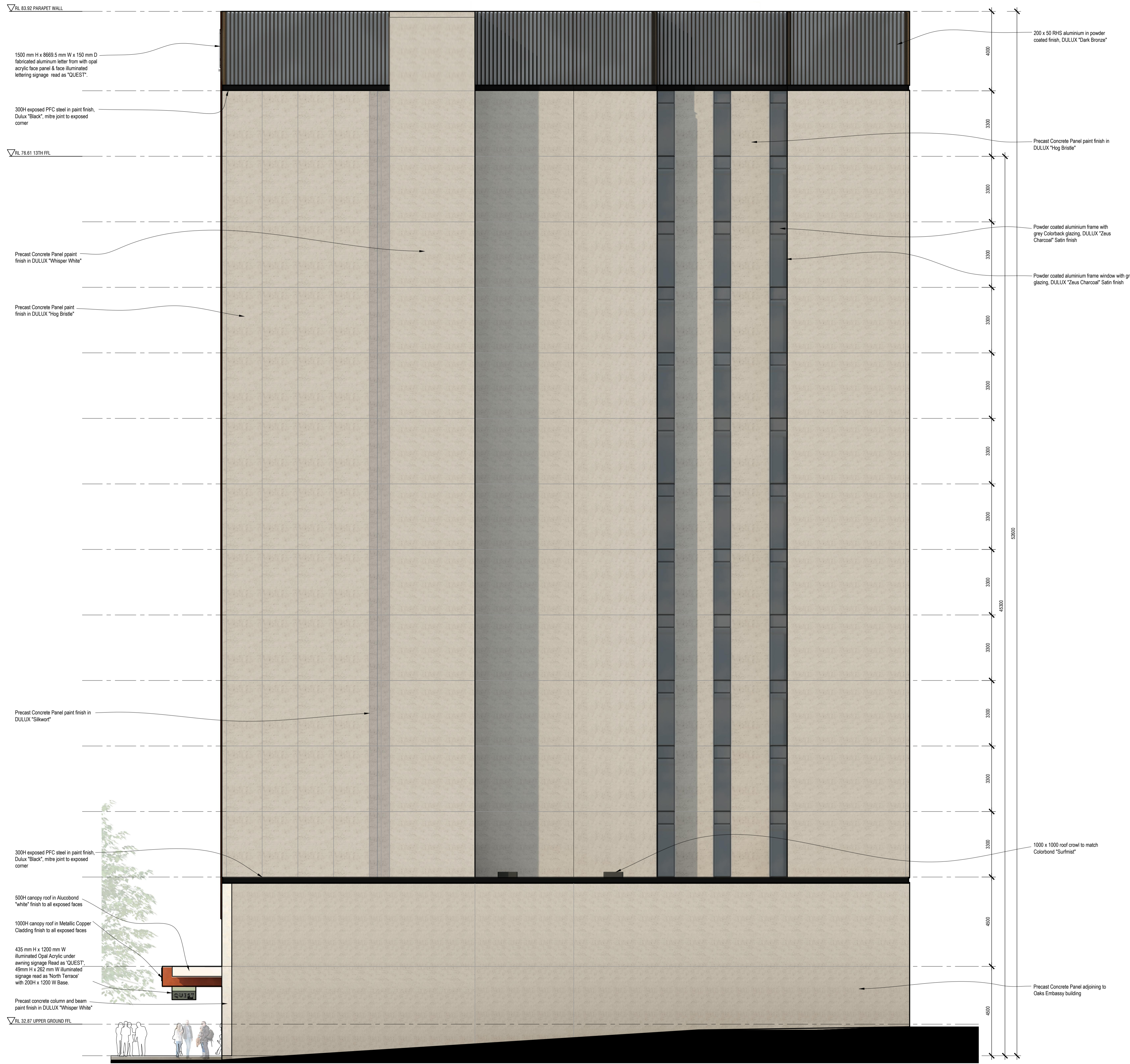




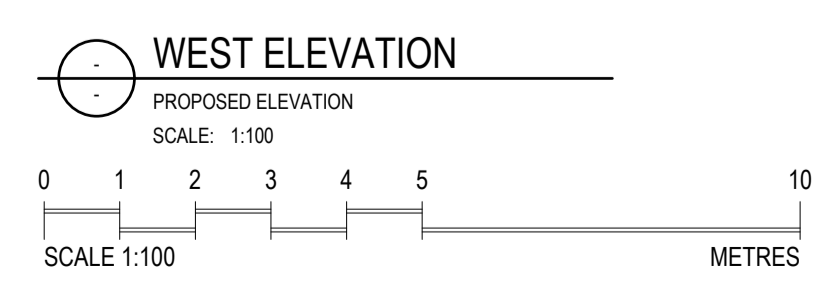








- Wall Paint  
DULUX "Tealhouse" SN4G6  
Finish: AcraTex AcraShield Low Gloss
- Wall Paint  
DULUX "Hog Bristle" S14D1  
Finish: AcraTex AcraShield Low Gloss
- Wall Paint  
DULUX "Silkwoor" SN4H2  
Finish: AcraTex AcraShield Low Gloss
- Wall and Door Paint  
DULUX "Mt Eden" PN2A7  
Finish: Low Sheen / Semi Gloss
- Wall Paint  
DULUX "Whisper White" SW1C2  
Finish: AcraTex AcraShield Low Gloss
- 2-PAC Signage Paint  
DULUX "Vintage Green" SG5E8  
Finish: Satin
- Powder Coat Paint  
DULUX "Zeus Charcoal Satin" 90087732  
Finish: Satin
- Powder Coat Paint  
DULUX "Dark Bronze" 9068184K  
Finish: Satin
- Feature Paint  
DULUX "Black" SH6H9  
Finish: Semi Gloss
- Feature Cladding  
Metallic Copper
- Feature Cladding  
Alucobond "White" 16-101  
Finish: Satin
- Feature Cladding  
Alucobond "Black" 326  
Finish: Satin
- Window / Door Glazing  
Vidlan - Everage "SuperBlue"
- Window / Door Glazing  
Vidlan - Everage "Grey"
- Window / Door Glazing  
Vidlan - EnergyTech "Clear"
- Roof Sheetling  
Colorbond "Surfmist"







FACADE PERSPECTIVE 1  
SCALE: Not To Scale





○ FACADE PERSPECTIVE 2  
SCALE: Not To Scale

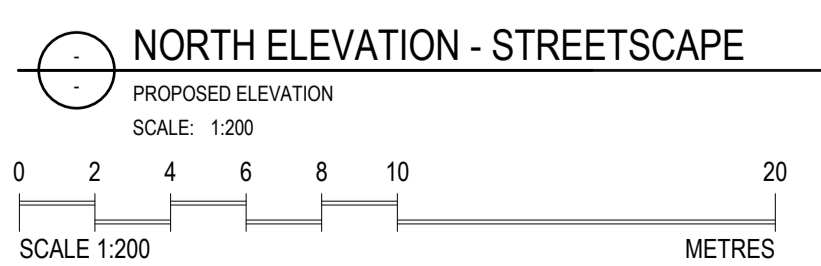






FACADE PERSPECTIVE 3  
SCALE: Not To Scale







File No:  
2014/11234/01

28 June 2018

Ref No:  
12854614

Benjamin Scholes  
Planning Officer  
Department of Planning, Transport and Infrastructure  
Level 5, 50 Flinders Street  
Adelaide SA 5000

Benjamin.Scholes@sa.gov.au

For the attention of the State Commission Assessment Panel (SCAP)

## **100 North Terrace, Adelaide**

Further to the referral 020/A040/18 received 11 May 2018 pertaining to the development application at the above address and in my capacity as a statutory referral in the State Commission Assessment Panel, I would like to offer the following comments for your consideration.

The project was not presented to the Design Review panel.

The proposed scheme is for a 14 storey hotel building in the form of long and short stay serviced apartments, with ground floor lobby, reception, services, staff parking and porte cochere. I support the proposed hotel use, and configuration that aims to activate both North Terrace and part of Bernath Place. However, I am not convinced that the proposal achieves a high quality design outcome. I am of the view that development along North Terrace has a responsibility to deliver a high benchmark for design, given its prominence and importance as a pedestrian promenade and cultural boulevard. Fulfilling this responsibility will be contingent on achieving a high quality design outcome, particularly in terms of architectural expression, materiality and contribution to the public realm.

The 675 square metre site is located at 100 North Terrace. This section of North Terrace is characterised by commercial development, including the Adelaide Convention Centre and Riverside Centre to the north, and recent hotel developments directly east and west. The State heritage listed Holy Trinity Church is located approximately 80 metres away on the corner of North Terrace and Morphett Street. Bernath Place, a single lane service road, wraps around the site's east and south boundaries. Vehicle egress from Bernath Place to North Terrace is via an existing Right of Way in the form of a driveway that runs through the adjacent development and accesses the public carpark to the south. The approximately 56.5 metre tall Oaks Horizon development to the east of the subject site includes a precast concrete base with aluminium framed shopfronts and canopy to North Terrace. The built form above is characterised by precast concrete facade elements that reinforce a vertical expression, and glass balustrades. This expression extends to the western facade, which includes balconies and windows that overlook the subject site. The top of the Oaks Horizon is capped by a butterfly

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style roof. The approximately 53 metre tall Oaks Embassy development is located to the west of the subject site. This development also features a precast concrete base with a three lane wide carpark entrance at its eastern end, aluminium shopfronts, louvres and square penetrations. The Oaks Embassy is generally setback approximately 1.5 metres from the subject site's western boundary, however level one of the podium adjoins the existing building. The built form above features similar characteristics to the Oaks Horizon, including precast panelling, expressed floorplates, glass balustrades, and articulated roof elements. The eastern facade includes corner balconies with sliding louvre panels to the lower levels, large scale decorative poles offset from the mid-level balconies, and splayed upper level balconies. This facade also includes full height glazing located flush to the boundary and balconies with glass balustrades that overlook the subject site. The subject site currently includes the three storey 'Meters Limited' building, which contrasts with development that has established along this section of North Terrace.

The built form of the proposed development is positioned on the north and south site boundaries. The proposal does not include a physical podium setback to North Terrace, but rather defines the base of the building through the architectural expression, an approach that is generally consistent with the existing context. The boundary of the adjoining Oaks Embassy site to the west is offset approximately 1.5 metres. The scheme proposes to build to this western boundary where fronting North Terrace, with setbacks of four to 6.5 metres behind. I acknowledge the challenges presented by the design of the Oaks Embassy development in regards to setbacks and the resulting interface issues. However, on balance I support the approach that infills the gap in the streetscape, as in my view achieving a continuous frontage to North Terrace is important, notwithstanding the resulting adjacencies issues.

The approach to the architectural expression is for a base, middle and top, with exposed painted steel parallel flange channels trimming the transition between each element. The painted concrete frame of the building base is articulated into a series of irregularly proportioned elements. The North Terrace facade includes glazing or glass balustrades set within the expressed frame, while both of the Bernath Place frontages include glazing, solid infills painted a contrasting colour or powdercoated louvres that screen services. A canopy featuring metal cladding fascias, and 'Quest' signage is proposed along the length of the North Terrace frontage. While I support the inclusion of a canopy, in my view further work is required to improve the quality of the base of the building from a materiality and articulation perspective, with the view to enhancing the public realm and pedestrian experience. The expression of the built form above the base is characterised by continuous vertically proportioned painted precast concrete bands, with corresponding continuous vertically proportioned glazing elements, and glass balustrades. The spandrel panels are either glazed or powdercoated louvres. 370mm wide metallic copper cladding strips are positioned on the north and east facades. The building is capped by a four metre tall screening element that conceals the rooftop plant, comprising powdercoated aluminium battens with an illuminated 1.5 metre tall 'Quest' sign fixed to the North Terrace frontage. I am not convinced that the proposed architectural expression is achieving a high quality design outcome, particularly in terms of the proposed painted precast concrete panelling. I do not support the use of painted finishes for longevity reasons. In my opinion, buildings of this scale have a responsibility to maintain their integrity in the long

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term. Furthermore, in my view the North Terrace context demands a high quality design outcome, given its prominence and importance as a pedestrian promenade and cultural boulevard. I recommend holistic review of the proposed architectural expression and materiality, with consideration given to the use of materials with finish and colour that is integral to the fabric rather than an applied finish. The adjoining Oaks Embassy development includes balconies and windows that directly interface with and overlook the west elevation of the proposed development, which predominantly comprises painted precast that offers limited articulation and visual interest. While I acknowledge the existing conditions, I recommend further review of the articulation of the west facade with a view to achieving visual interest and to assist in mitigating the amenity impacts resulting from the development adjacencies.

The ground floor is configured to include a main lobby and reception area accessed via sliding doors off North Terrace, with a second entrance from the Bernath Place porte cochere that directly accesses the lift lobby. I support the inclusion of full height glazing that extends activation into Bernath Place. In my view, however the opportunity exists to reconfigure the layout of ground floor public spaces and access WC to create greater visual and physical connectivity between the two lobby spaces, and to maximise activation of Bernath Place. I support the location of the transformer in the south east corner and service functions at the rear of the site. However, I typically advocate locating services off of the primary frontage to maximise activation. On balance, I support the approach that expresses the fire hydrant booster through glazed facade treatments. However, I recommend further review of the location or alternatively the expression of the gas meter/backflow valve on the North Terrace frontage, with the view to achieving an integrated outcome. The first floor is configured to include a business lounge with BBQ deck, conference room, and gymnasium that overlook North Terrace and Bernath Street, which I support. In my view, the opportunity exists to further improve user amenity and passive surveillance by reconfiguring the conference room kitchen to achieve light access and outlook. I also urge consideration of light access to the level one staff room and corridor spaces. The serviced apartment floors above are configured as dual key studios or one bedroom units. While I anticipate the rooms have been configured to meet the operator's requirements, I typically advocate direct natural light and ventilation access for all bedrooms to maximise potential adaptive reuse. With the exception of the balcony sliding doors, natural ventilation is achieved via high level aluminium louvres that form part of the glazing system. While I support the consideration given to fresh air access, my support for this approach is contingent on the louvres providing environmental control for occupants. I support the two metre corridor width, and light access afforded by the western windows.

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The proposal includes three staff car parking spaces accessed from the porte cochere. Given the proximity of this development to a public car park, I recommend further review of onsite car parking in order to maximise benefits to the public realm. Guest bicycle parking is proposed within the porte cochere, which I support, however in my view the opportunity exists to review the position of the access WC in order to locate guest bike parking in a more visible and accessible location.





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To ensure the most successful design outcome is achieved the State Commission Assessment Panel may like to consider particular aspects of the project, which would benefit from protection as part of the planning permission:

- Further holistic review of the proposed architectural expression and materiality, with consideration given to materials with finish and colour that is integral to the fabric, rather than an applied finish. This should be supported by a material samples board and schedule.
- Further consideration of the articulation of the west elevation, with a view to achieving greater visual interest when viewed from the adjoining development.
- Further consideration of ground floor internal planning and the car parking strategy, with a view to creating greater visual and physical connectivity between the two lobby spaces, and to maximise activation of Bernath Place.
- Further consideration of level one internal planning, including the location of the conference room kitchen, and light access to staff and corridor spaces.
- Further review of the car parking strategy, cognisant of publically accessible car parking in close proximity.

Yours sincerely



Kirsteen Mackay  
South Australian Government Architect

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ABN 20 903 762 572

**Enquiries:** Matthew Field 8203 7023

**Reference:** S10/27/2018

13 July 2018

State Commission Assessment Panel  
GPO Box 1815  
Adelaide SA 5001

**Attention: State Commission Assessment Panel**

Dear Sir/Madam

**Application:** S10/27/2018

**Applicant:** EMMETT CONSTRUCT P/L

**Address:** GF-1F 98-100 North Terrace, ADELAIDE SA 5000

**Description:** Demolish existing building and construct a 14-level serviced apartment building (120 rooms)

Council has the following comment(s) to make on the above application:

**TECHNICAL COMMENTS**

**ROADS / FOOTPATHS  
ENGINEERING**

Any damage caused to CoA's road, footpath and kerb infrastructure during development will be the responsibility of the developer to rectify to a standard that equals or improves the pre-development condition.

CoA will inspect the works after completion for standards and specification compliance.

Footpath, road and kerb reinstatements associated with works will need to match surrounding materials and pavement composition (construction details can be provided by CoA Design and Engineering department)

Existing boundary (back of path) levels must not be modified. Finished floor levels should be based around retaining the existing back of path levels subject to the following:

- If the level difference between top of kerb and back of path is less than 50 mm





- If the existing cross fall(s) exceed 4% (1:25)

If any of the above conditions exist for any footpath infrastructure that services the perimeter of the site boundary then please contact the Lead Asset Consultant Streets prior to setting finished floor levels.

## **TORRENS & STORM WATER**

Stormwater runoff from the proposed development must be retained within the property boundaries, collected and discharged to the North Terrace road reserves.

Councils stormwater management systems (minor and major rainfall events) have been designed to manage gravitational flows only. Any proposed siphonic roof drainage systems must be designed to attenuate discharge flows to Councils stormwater management systems to equivalent gravitational flows.

Any proposed grated inlet pits or stormwater openings within the building must be designed with an adequate freeboard to the 1% AEP flood level assumed to be top of kerb level adjacent to the stormwater discharge point in North terrace.

## **LIGHTING / ELECTRICAL / CCTV**

All works to be undertaken to be fit for purpose in the public realm.

All modifications requiring temporary removal/relocation/provision of temporary lighting/reinstatement of existing Council and/or SA Power Network's public lighting (including associated infrastructure such as cabling etc.) shall meet Councils' requirements. The works shall be carried out to meet Councils' requirements and all costs borne directly by the developer.

If temporary hoarding or site works require modification of existing Council and/or SA Power Network's public lighting (including associated infrastructure such as cabling etc.) shall meet Councils' requirements. The works shall be carried out to meet Councils' requirements and all costs borne directly by the developer.

Obtrusive Lighting – Lighting design and installation to be fully compliant with Australian Standard - AS 4282 – 1997 Control of the obtrusive effects of outdoor lighting. Sign off by consultant required to confirm compliance. In addition, provide relevant lighting calculation grid detailing property boundary lines for Councils review and records.



If new canopies are to be constructed as part of these works, then lighting to meet CoA's under veranda requirements shall be installed.

Existing underground services shall be identified and marked in the locality prior to undertaking any excavation works.

All damage to CoA's infrastructure, including damage to public lighting and u/g ducting etc caused by projects works or loading of site crane onto pathways will be repaired to meet Councils requirements and the cost of the developer.

If building mounted lit signage is to be installed onto the building, further review and approvals will be required by City of Adelaide.

Any CCTV camera owned and maintained by City of Adelaide, for any modifications to this asset contact City of Adelaide for further advice on process. SAPOL monitor this camera and will also be required to be consulted with for their approval.

All assets to be handed over to CoA to own and maintain shall be constructed to Councils' requirements and applicable legislative standards and requirements. All equipment gifted shall be Councils standards and applicable requirements.

## **TRAFFIC / TRANSPORT**

Concern is raised on the proposed increase in traffic on the private lanes in this precinct. This is of concern given:

- The lack of available sight distance at the various intersections through the private laneways area.
- The car park connection being used for exits and the constraints at the Bernath Place (private lane) interface with North Terrace.

In addition, it appears that drivers using the drop off area and wanting to use the adjacent car park, would need to head back to North Terrace the way they came in, and then turn left into the car park. This would need to occur given there appears to be inadequate manoeuvring space to turn left from the lane into the car park entrance.

No analysis has been provided on the adequacy of the proposed pick up and drop off area to cater for movements. Excessive demand for this area would result in safety concerns at the North Terrace interface, and result in



vehicles blocking passage for the lane and potentially the footpath area.

The swept path for a waste vehicle undertaking a 3-point turn appears to collide with walls.

The staff parking area appears to require part of the door opening space out to Bernath Place.

## **WASTE**

From the supplied documentation, there are no major concerns from a waste management perspective.

It is also important to note that rear load collection trucks (MRV) will need to be used for the servicing of the proposed site as the site next door uses a front lift service.

## **STREET TREE / LANDSCAPE**

Any requirement to prune CoA trees is to be done ONLY by CoA staff once permission is granted.

All works around street trees is done in accordance with AS 4970-2009 Protection of Trees on Development Sites.

Tree removal will only be considered if it can be clearly demonstrated and substantiated no alternative exists.

Tree removals will activate the application of the CoA Amenity Tree Valuation Formula. The resultant valuation will be added to all other tree removal/replacement costs to be borne by the developer.

## **PLANNING RELATED COMMENTS**

Council Administration has not undertaken a thorough planning assessment of the proposal but makes the following comments in relation to the proposed development:

### **RESIDENTIAL AMENITY**

Notwithstanding that easements provide separation between the subject site and adjacent development to the west, the setbacks to windows/balconies from boundaries, (particularly to the west (less than 2 metres)), fails to provide an adequate setback (noting there is a 3-metre minimum prescribed within the Development Plan) to protect the amenity of the occupants of the adjoining sites. This is an undesirable outcome given internal amenity for occupants will be impacted in regards to outlook and access to light.

### **ACTIVE STREET FRONTAGES**

The provision of active uses at ground and first floor to North Terrace is commended.

The provision of a canopy to North Terrace is also commended.



## **ENCROACHMENTS**

The development incorporates the following encroachments:

- 2500 mm deep by 19440 mm wide canopy to North Terrace – varying clearance to footpath between 3500 - 4000 mm;

Based on the above, portions of the proposed canopy do not achieve the requirements of Council's Encroachment Policy which states that canopies should have a maximum height not more than 3.7 metres above the level of the footpath measured to the underside of the awning. However, given the minor departure, The Associate Director, Planning & Development has approved the minor variation to the Policy under delegation.

## **GROUND ANCHORS**

The City of Adelaide's current policy position is that the installation of temporary ground anchors is not supported. Should a request demonstrate exceptional circumstances for the use of temporary ground anchors, a report will be submitted to Council for a determination.

Should a proposal be supported by Council, there will be conditions applied to the permit that will include fees calculated at a rate of \$40/m<sup>2</sup> (the area beyond the boundary that the anchors will temporarily occupy) and a bond of \$5000/anchor.

As such, it is recommended that alternate methods of retention are investigated to avoid any delays.

## **CLADDING**

The plans indicate the use of a metal cladding product (ACP). The applicant should be aware that a variation application may be required to vary the material should a change in material be required.

## **SUGGESTED CONDITIONS**



It is requested that the following conditions be included should SCAP resolve to grant consent to the development:

- 1. The finished floor level of the ground floor level at the entry points to the development shall match the existing footpath unless otherwise agreed to by the Council in writing.**

*Reason: The Corporation of the City of Adelaide WILL NOT adjust footpath levels to suit finished building levels. The existing footpath levels are to be retained and entrance levels of the development must meet the existing back of footpath.*

- 2. Lighting shall be installed to the awning at street level on North Terrace in accordance with Council's guideline entitled "Under Verandah/Awning Lighting Guidelines" at all times to the reasonable satisfaction of the Council and prior to the occupation or use of the Development. Such lighting shall be operational during the hours of darkness at all times to the reasonable satisfaction of Council.**

*Reason: To ensure the development does not create public areas with insufficient lighting.*

- 3. Lighting shall be provided to the vehicle and pedestrian entries on Bernath Place and shall be operational during the hours of darkness at all times to the reasonable satisfaction of Council.**

*Reason: To ensure the development does not create areas with insufficient lighting.*

- 4. Clear sight lines for users of the development shall be provided to ensure pedestrian safety along the North Terrace footpath and shall be provided at all times in accordance with in accordance with AS/NZS 2890.1:2004 Off-street Car Parking.**

*Reason: To ensure that the Development meets the requirements of the relevant Australian Standards.*

## **SUGGESTED ADVICES**

It is requested that the following advices be included should SCAP resolve to grant consent to the development:

1. Development Approval will not be granted until Building Rules Consent and an Encroachment Consent have been obtained. A separate application must be submitted for such consents. No building work or change of classification is permitted until the Development Approval has been obtained.
2. An Encroachment Permit will be separately issued for the proposed encroachment into the public realm when Development Approval is granted. In particular, your attention is drawn to the following:
  - An annual fee may be charged in line with the Encroachment Policy.
  - Permit renewals are issued on an annual basis for those encroachments that attract a fee.



- Unauthorised encroachments will be required to be removed.
3. Any activity in the public realm, whether it be on the road or footpath, requires a City Works Permit. 48 hours' notice is required before commencement of any activity.

The City Works Guidelines detailing the requirements for various activities, a complete list of fees and charges and an application form can all be found on Council's website at [www.cityofadelaide.com.au](http://www.cityofadelaide.com.au)

When applying for a City Works Permit you will be required to supply the following information with the completed application form:

- A Traffic Management Plan (a map which details the location of the works, street, property line, hoarding/mesh, lighting, pedestrian signs, spotters, distances etc.);
- Description of equipment to be used;
- A copy of your Public Liability Insurance Certificate (minimum cover of \$20 Million required);
- Copies of consultation with any affected stakeholders including businesses or residents.

Please note: Upfront payment is required for all city works applications.

Applications can be received by Council via the following:

**Email:** cityworks@cityofadelaide.com.au

**Fax:** 8203 7674

**In Person:** 25 Pirie Street, Adelaide

4. There is no objection to the proposed vehicle crossing place, however the work will be undertaken by Council and the cost of the work will be charged to the applicant. A separate application for the crossing place(s) is required and the applicant can obtain a form from Customer Service, 25 Pirie Street, Adelaide, or telephone 8203 7236. A quotation for the work will be provided by Council prior to the work being undertaken.
5. Section 779 of the Local Government Act provides that where damage to Council footpath / kerbing / road pavement / verge occurs as a result of the development, the owner / applicant shall be responsible for the cost of Council repairing the damage.

Yours faithfully

Per

Rebecca Rutschack

**MANAGER - PLANNING ASSESSMENT**





Town Planning  
Development Advice  
Strategic Management

23 July 2018

Mr Ben Scholes  
Project Officer  
Inner Metropolitan Development Assessment  
Department of Planning Transport & Infrastructure  
GPO Box 1815  
ADELAIDE SA 5001

Dear Ben,

**DEVELOPMENT APPLICATION NO. 020/A040/18**

I refer to the Development Application by Emmett Property that seeks Development Plan Consent to demolish an existing building and construct a 14 level serviced apartment building (120 rooms) on land at 100 North Terrace, Adelaide.

I understand that following statutory referral procedures letters of advice have been received from the South Australian Government Architect and the City of Adelaide (the Council). I provide the following for your consideration.

**South Australian Government Architect**

Matthews Architects have provided a detailed response to the various matters raised by the Government Architect in her letter of 28 June 2018. Can I ask that you have primary regard to this document supplemented by the following comments:

- an applied finish to concrete panels is a legitimate response in the circumstance and provides for a more consistent finish over its extent;
- nonetheless, the podium will be finished with a polished (honed) concrete finish;
- suitable variation and visual interest is to be provided to the west elevation;
- the extent of on boundary construction is limited to 12.75 metres (37%), the balance of this elevation is set back between 4.2 and 6.85 metres
- the ground floor has been adjusted to provide greater visual connection and connectivity with the public realm;
- the first floor plan has been adjusted to provide for more natural light and improved circulation, and therefore enhanced amenity;
- minimal parking is provided on site and is provided out of a duty of care to employees working late night early morning
- this limited parking area has been designed so that it may be adapted for an alternate use into the future should the opportunity arise;

**Phillip Brunning & Associates**

ABN 40 118 903 021

26 Wakeham Street  
Adelaide SA 5000  
Telephone 08 8232 5686  
Mobile 0407 019 748  
[phil@phillipbrunning.com](mailto:phil@phillipbrunning.com)



## City of Adelaide

Noting that Council is afforded the opportunity to provide advice in respect to technical matters related to City assets and management, I provide the following for your consideration when undertaking your assessment:

- advice regarding condition of and any damage to road, footpath and engineering assets are acknowledge and accepted;
- given the current extent of impervious surfaces on the land, little if any increase in stormwater would arise from this development;
- to the extent that may be necessary at this early stage, the Council would accept a condition of approval requiring design detail for approval prior to connection;
- advice in respect to lighting, electrical and security measures are noted and will be addressed at detailed design stage in the usual manner;
- the proposal has been assessed by a qualified and experienced traffic engineer and is considered to be in accord with the relevant standard;
- the constraints posed by the existing lane system are noted and will not be exacerbated by the proposed development;
- manoeuvring spaces for vehicles have checked by GTA Consultants and considered to be acceptable;
- the function of the drop of zone has been assessed by GTA Consultants in their report at page 13;
- there will be no need for vehicles to block footpaths and in any event such is contrary to the road rules;
- it is noted that the waste management details provided have been accepted;
- advice regarding permission to prune trees and/or removal are acknowledged and may be reflected as notes on the approval in the usual manner;

In respect to the planning comments offered, I respond by saying:

- the reference to a requirement for a 3 metre set back from boundaries (CW PDC 67 I presume) is not applicable in this instance;
- PDC 67 only applies where a habitable room window, balcony roof garden, terrace or deck is proposed adjacent to site boundaries;
- all windows associated with the proposed development are located more than 3 metres away from a shared property boundary
- the failure of the adjoining property to the to provide the required set back to this boundary ought not unreasonably constraint the Applicant's site;
- in any event the extent of on boundary construction is limited to length of 12.75 metres, being 37 of this western boundary being;



- advice regarding active street frontage and canopy over the footpath to North terrace are noted including the need for a licence to encroach;
- advice regarding the use of ground anchors is noted;
- the Applicant acknowledges that departure away from the specified schedule of materials and finishes will require prior approval from the planning authority; and
- the applicant takes no issue with the suggested conditions and notes to be reflected on the Decision Notification Form granting Development Plan Consent.

I trust that this additional information enables you to now finalise your assessment report and present the application to the State Commission Assessment Panel for a decision with a recommendation for consent.

In closing, I reaffirm my previously expressed opinion that the proposal is an appropriate form of development that substantively accords with the provisions of the Development Plan and therefore warrants Consent.

The proposal does not seek any major dispensations or departures away from Development Plan policy, and will result in a desired use within a quality building befitting this location.

Yours faithfully

**PHILLIP BRUNNING & ASSOCIATES PTY LTD**



**PHILLIP BRUNNING** RPIA  
Registered Planner

Enc. Architectural Response





# QUEST

## NORTH TERRACE

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ARCHITECTURAL RESPONSES  
TO PLANNING QUERIES AND  
COMMENTS PROVIDED BY  
ODASA.

100 North Terrace,  
Adelaide SA 5000



### Planning Query 1:

Further holistic review of the proposed architectural expression and materiality, with consideration given to the materials with finish and colour that is integral to the fabric, rather than applied finish. This should be supported by a material samples board and schedule.

In light of the comments provided we have reviewed the proposed external materials and made changes which we believe will further improve the visual appearance of the building. Please refer to our amended external elevations.

The bulk of the facade is comprised of pre-finished off-form concrete panels and aluminium framed glazing.

The concrete panels are structural elements of the facade. In order to ensure that uniformity of colour is maintained between all panels we propose to use an applied finish. This system provides greater control and consistency of colour and can be repaired and retouched if required. We have adjusted the proposed colour of these panels to a lighter (near white) tone.

We have considered in detail various pre-finished off-form options and all of these options come with risks in terms of finish and wear over time; some gather dust and dirt and discolour with rain. Furthermore, given that panels are made with different batches of concrete, getting the desired uniformity over the building is not possible without a precisely controlled applied finish. Furthermore, pre-finished panels can be damaged in erection and repairs cannot be concealed. We want a uniform and consistent finish throughout on the upper levels. Therefore on balance, we have formed the view that we should paint our panels, with the exception of the podium levels fronting North Terrace and Bernath Place.

At the podium levels (ground and first floor) the concrete "framing" that creates the language of the design at street level will be constructed of polished (honed) concrete columns and beams. A light (near white) coloured concrete and light coloured aggregates will be exposed during the honing process. This will create a subtle yet high-quality material finish, which we believe is particularly important at the street level.

The aluminium framing to the building will be in a powdercoat finish as previously stated. The glazing to the facades has been selected to create a clear language of dark vertical strips that form a rhythm around each elevation. A dark glass has been chosen both for external visual effect and for internal comfort. Where glazing is set back from the primary facade, within balcony spaces, a clear glass has been selected to deliberately counterpoint the dark glass elements.

The metallic copper vertical accent strips that punctuate the facades are an integral part of the rhythm and have been carefully positioned to form a pleasing and elegant asymmetrical composition.

### Planning Query 2:

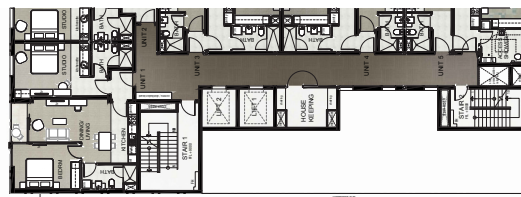
Further consideration of the articulation of the west elevation, with a view to achieving greater visual interest when viewed from the adjoining development.

Following receipt of the comments provided we have reviewed the design of the western facade and we have made design adjustments which we believe strike the right balance. Please refer to our amended external elevations.

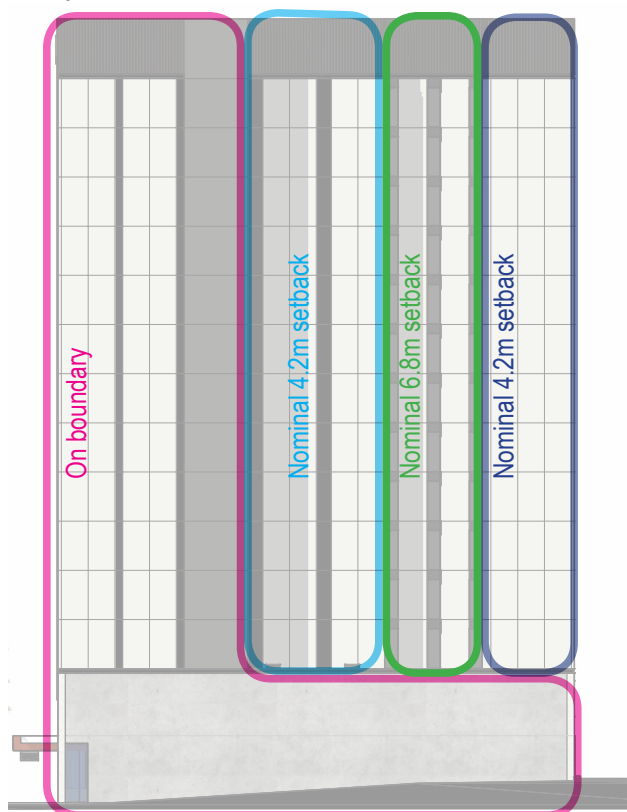
It is our intention with this design to create a calm and articulated facade without flamboyance or unnecessary distraction. This facade is composed of walls that exist of four different planes. This is not readily expressed by orthogonal 2D elevations and must be read in conjunction with the floor plans.

This elevation is a composition of pre-coloured concrete panels, vertical glazing strips and dark vertical accent strips. The composition emphasises the verticality of the form. This, together with the well articulated form of this facade breaks up the horizontal experience of this elevation for neighboring occupants.

### Western Facade Setback Study



Plan



Elevation

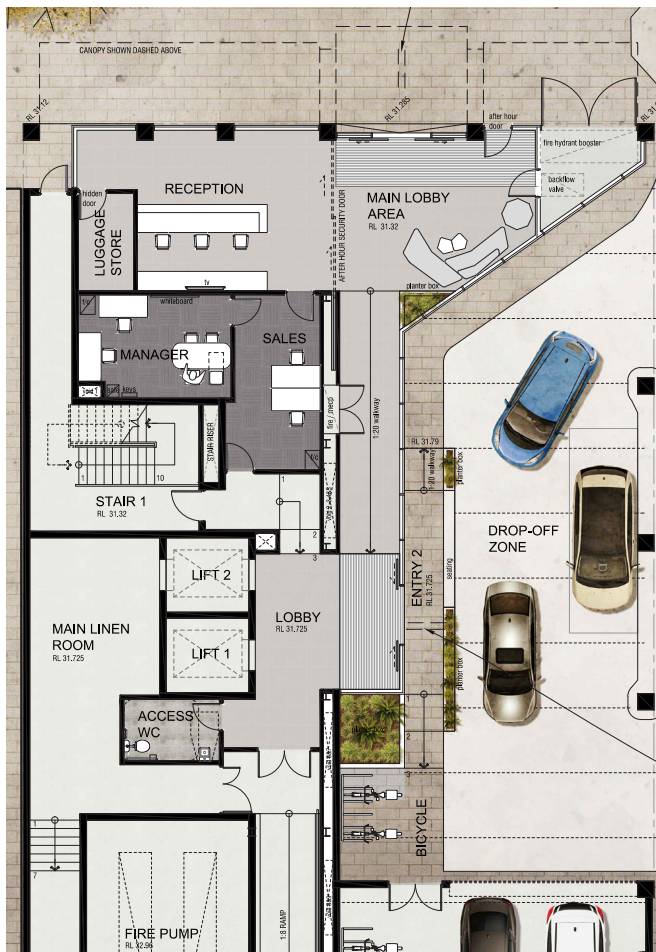


*Planning Query 3:*

*Further consideration of the ground floor internal planning and the car parking strategy, with a view to creating greater visual and physical connectivity between the two lobby spaces, and to maximise activation of Bernath Place.*

We have reviewed the ground level arrangement and have made adjustments in response to this comment. We have adjusted the internal arrangement of the building in order to improve the visual transparency and the spatial experience of the public spaces within ground level. This has created additional opportunity for landscaping within the covered drop-off area.

Please refer to our adjustment ground floor plan.



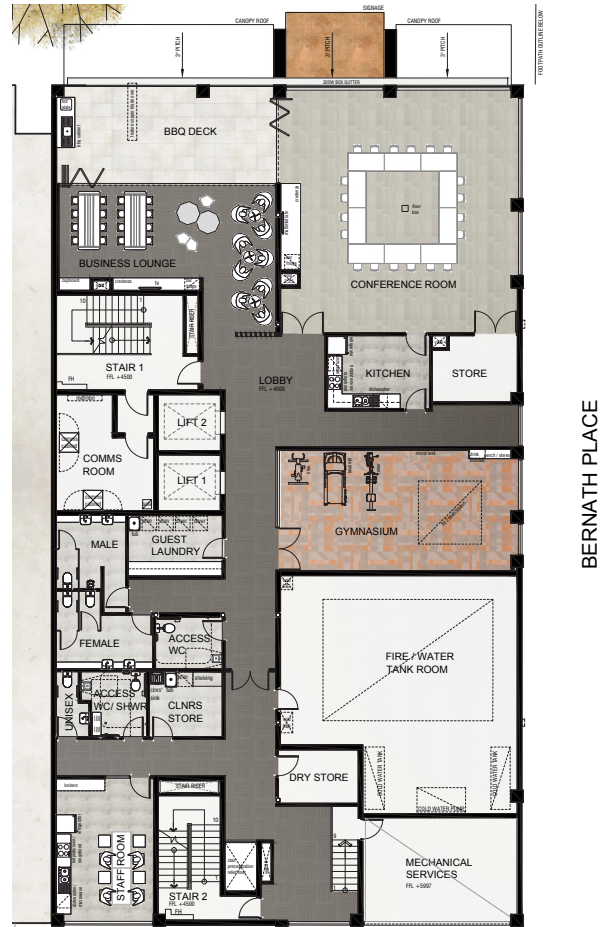
Ground Level

*Planning Query 4:*

*Further consideration of level one internal planning, including the location of the conference room kitchen, and light access to staff and corridor spaces.*

We have adjusted the first floor plan arrangement to provide opportunities for windows into the circulation spaces and staff areas. We believe this will greatly improve the occupant experience.

Please refer to our adjusted first floor plan.



First Floor Level

*Planning Query 5:*

*Further review of the car parking strategy, cognisant of publicly accessible car parking in close proximity.*

Minimal car parking has been provided on site. We are aware of the planning desire for the site to include no car parking, however the hotel operators (Quest) have a duty of care to employees, particularly those working late or early shifts, to ensure they can arrive and depart safely. For this reason, a very small number of car parks have been provided on the site. The design has been formulated to allow this space to be adapted with minimal disruption in the future, if and when on-site car parking is deemed by the operators to be unnecessary. These spaces will then be used as short term parking for valet services.



Reference: #S138314

9 August 2018

Philip Brunning & Associates  
26 Wakeham Street  
ADELAIDE SA 5000

**Attention: Mr. Philip Brunning**

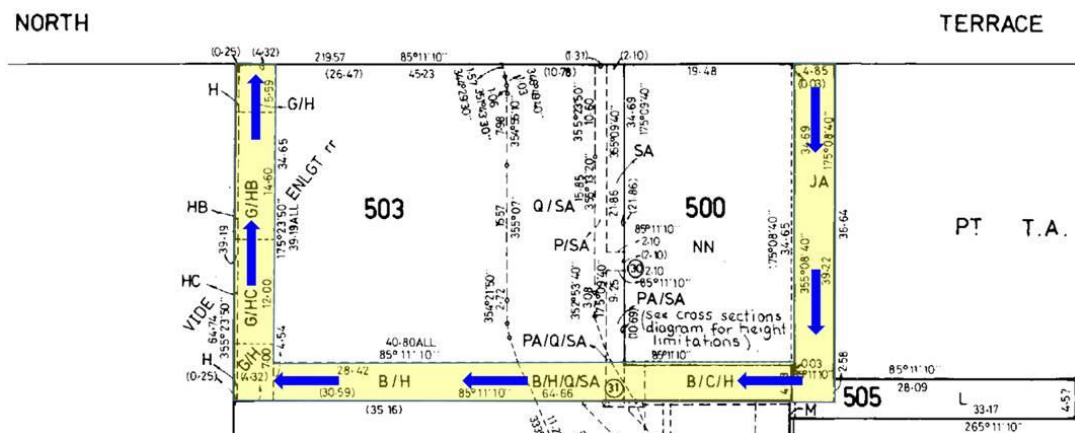
Dear Philip

**RE: DA 020/A040/18 - 98-100 NORTH TERRACE, ADELAIDE**  
**RESPONSE TO DPTI COMMENTS**

I refer to your correspondence with the Department of Planning, Transport and Infrastructure (DPTI) dated 7 and 8 August 2018 relating to a traffic and access related matters. The following are requests for further information raised by DPTI followed by our response:

*GTA consultants has indicated that "Light vehicles will access the site via Bernath Place and use the existing Right of Way (servicing The Terrace Public Car Park) to reenter North Terrace".*

*The Certificate of Title provided shows rights of way marked HB, HC, D, JA, and H appurtenant to lot 500 (the subject land), suggesting a right of movement exists along the path highlighted below, but not via the adjoining Terrace Car Park driveway:*



*Can you arrange for GTA consultants to review the applicable rights of way and confirm whether vehicle movement through that driveway would be permissible, or otherwise?*

Based on the highlighted route as illustrated by DPTI above, GTA has undertaken a swept path assessment of this route for a B99 light vehicle. Figure 1 illustrates that the proposed right of way route can accommodate access for a B99 light vehicle in a forward direction between entering from North Terrace (eastern end) and exiting back out to North Terrace (western end).

VIC | NSW | QLD  
ACT | SA | WA

Level 5,  
75 Hindmarsh Square  
ADELAIDE SA 5000

PO Box 119  
RUNDLE MALL SA 5000  
t// +618 8334 3600



Figure 1: B99 Light Vehicle Right of Way Access



Can you also confirm the applicant's right of way over allotment 505 to enable waste collection vehicles to perform the reversing manoeuvre required to exit the land, as depicted in GTA's diagram below?

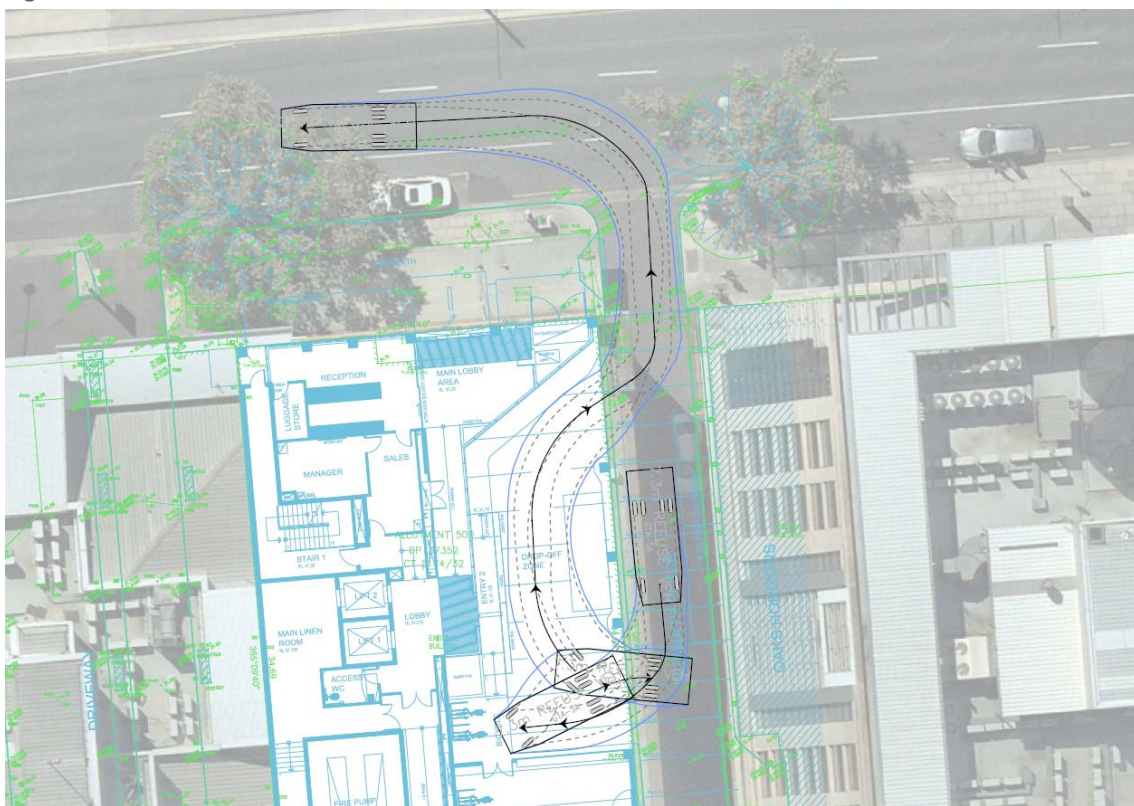
Figure 6.1: 7.3m Refuse Vehicle Three Point Turn



*This detail will be necessary to confirm the development is capable of operating as intended.*

In light of information relating to constraints on rights of way over lot 505, amended plans prepared by Matthews Architects (dated 08/08/18, enclosed) will enable a 7.3m refuse truck to enter and exit in a forward direction with the use of the right of way section marked as "JA" (east of Lot 500 - subject site). Figure 2 illustrates the refuse truck entering and exiting the refuse collection area via North Terrace and right of way section marked "JA" in a forward direction. It is understood refuse collection will occur outside of peak set-down/pick-up periods.

**Figure 2: 7.3m Refuse Vehicle Access**



Please contact me if you have any questions

Yours sincerely

**GTA CONSULTANTS**



**David Kwong**  
**Associate Director**

Attachments

Mathews Architects Drawing 18003 SK113A





435 mm H x 1200 mm W powder coated under eavey signage with Burnt Red Qat Acrylic lettering read as 'QUEST' North Terrace and 200 x 1.000 W grey powder coated base

300 mm H x 827.5 mm W powder coated under eavey signage with Burnt Red Qat Acrylic lettering read as 'QUEST' North Terrace and 1500mm H x 827.5 mm W grey powder coated base

**GROUND FLOOR PLAN**



GROUND FLOOR AREA  
BUILDING: 519m²

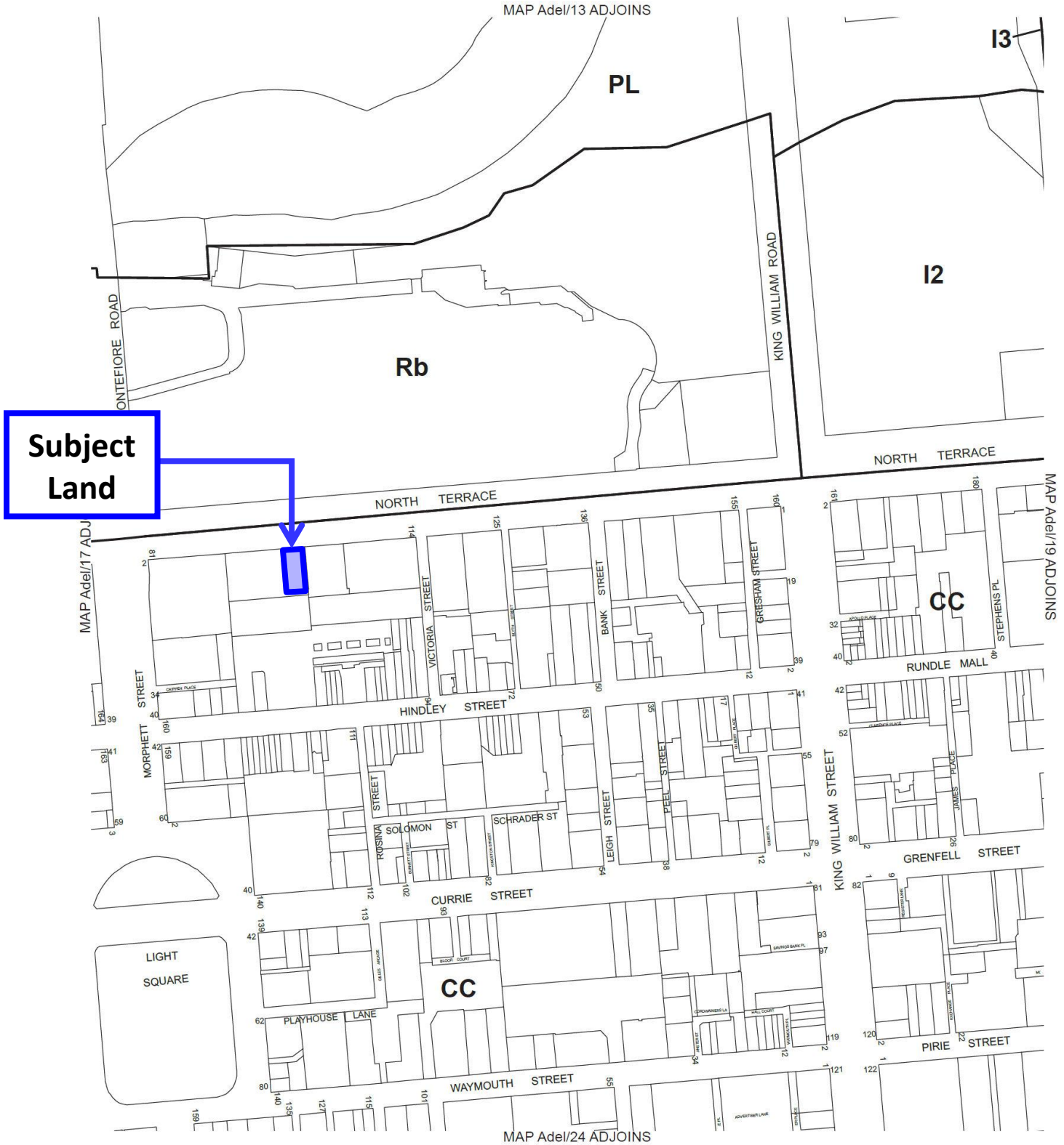


QUEST, 100 NORTH TCE, ADELAIDE  
EMMETT

GROUND FLOOR PLAN  
18003 SK113A 08/08/18 A1 DRAWN\_K.L



140, rue de la République  
69002 LYON FRANCE  
www.matthews.com.au



**NOTE : For Policy Areas See MAP Adel/49**

- CC** Capital City Zone
- I2** Institutional (Government House) Zone
- I3** Institutional (University/Hospital) Zone
- PL** Park Lands Zone
- Rb** Riverbank Zone

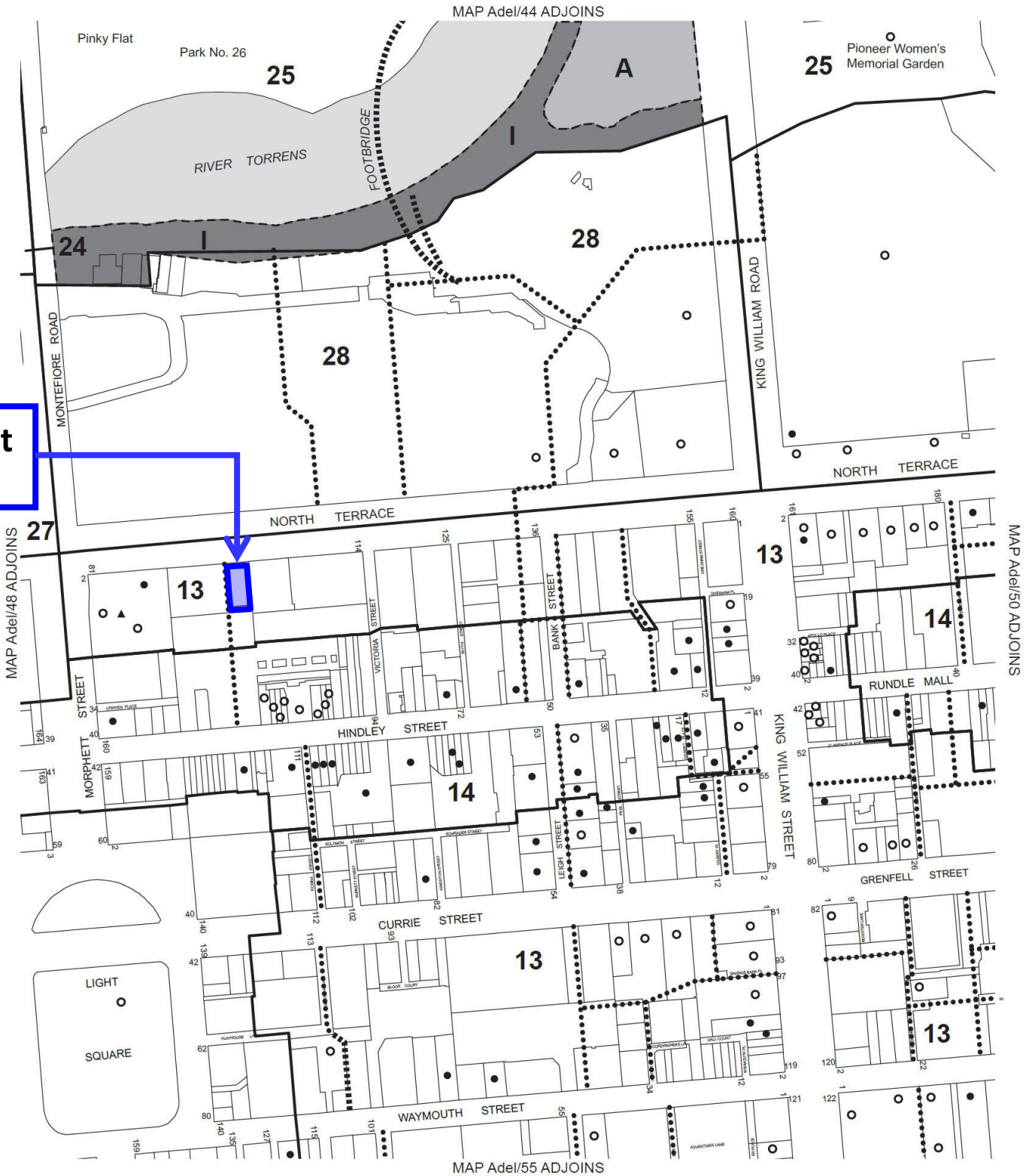


- Zone Boundary
- Development Plan Boundary

**ADELAIDE (CITY)**  
**ZONES**  
**MAP Adel/18**



**Subject Land**



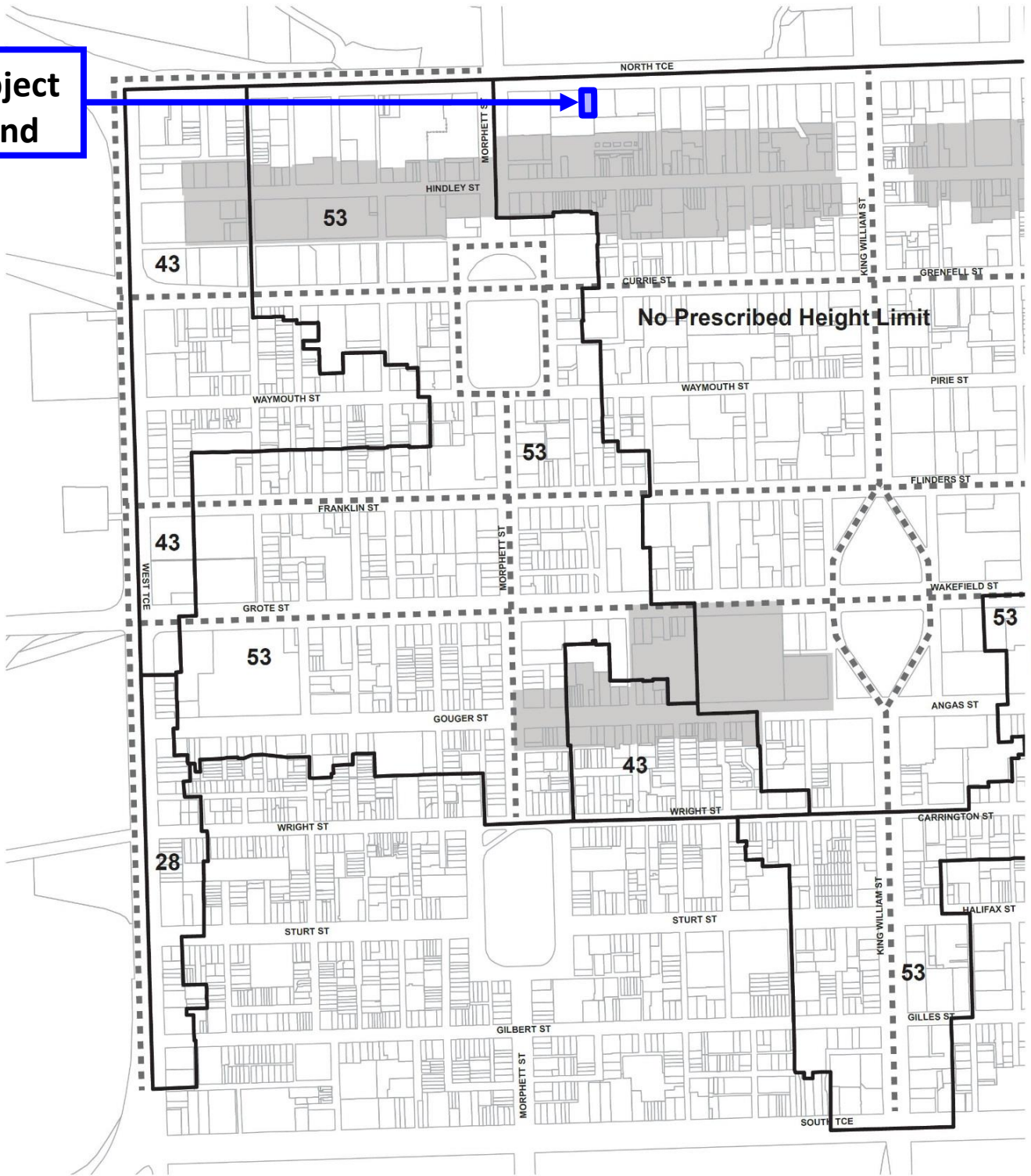
- 13 Central Business Policy Area
- 14 Main Street Policy Area
- 24 River Torrens West Policy Area
- 25 Adelaide Oval Policy Area
- 27 Health Policy Area
- 28 Entertainment Policy Area

- Special Events
- Area I
- State Heritage Place
- Local Heritage Place
- Significant Tree
- Pedestrian Link
- Pedestrian and Cycling Link
- Policy Area Boundary

Scale 1:4150  
0metres 100 200 300

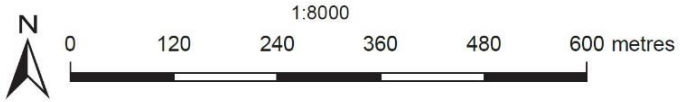
**ADELAIDE (CITY)**  
**POLICY AREAS**  
**MAP Adel/49**

**Subject Land**



MAP CC/2 ADJOINS

-  Maximum Building Height (metres) within Capital City zoned land  
Note: Airport Building Height Restrictions Apply. Refer Map Adel/1 (Overlay 5).
-  City Boulevards and Terraces
-  Policy Areas of a 'Main Street' type



**ADELAIDE (CITY)**  
**BUILDING HEIGHTS**  
**Concept Plan Figure CC/1**



## **Central Business Policy Area 13**

### **Introduction**

The Objectives and Principles of Development Control that follow apply to the Policy Area as shown on [Maps Adel/49, 50, 55 and 56](#). They are additional to those expressed for the Zone and, in cases of apparent conflict, take precedence over the Zone provisions. In the assessment of development, the greatest weight is to be applied to satisfying the Desired Character for the Policy Area.

### **DESIRED CHARACTER**

The Central Business Policy Area is the pre-eminent economic, governance and cultural hub for the State. This role will be supported by educational, hospitality and entertainment activities and increased opportunities for residential, student and tourist accommodation.

Buildings will exhibit innovative design approaches and produce stylish and evocative architecture, including tall and imposing buildings that provide a hard edge to the street and are of the highest design quality. A wide variety of design outcomes of enduring appeal are expected. Complementary and harmonious buildings in individual streets will create localised character and legible differences between streets, founded on the existing activity focus, building and settlement patterns, and street widths.

### **OBJECTIVES**

- Objective 1:** A concentration of employment, governance, entertainment and residential land uses that form the heart of the City and central place for the State.
- Objective 2:** Development of a high standard of design and external appearance that integrates with the public realm.
- Objective 3:** Development that contributes to the Desired Character of the Policy Area.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

#### **Land Use**

- 1** Development should contribute to the area's role and function as the State's premier business district, having the highest concentration of office, retail, mixed business, cultural, public administration, hospitality, educational and tourist activities.
- 2** Buildings should be of a height that ensures airport operational safety is not adversely affected.
- 3** To enable an activated street level, residential development or similar should be located above ground floor level.

## **CAPITAL CITY ZONE**

### **Introduction**

The Desired Character, Objectives and Principles of Development Control that follow apply in the whole of the Capital City Zone shown on [Maps Adel/17 to 20, 23 to 26 and 29 to 31](#). They are additional to those expressed for the whole of the Council area and in cases of apparent conflict, take precedence over the more general provisions. In the assessment of development, the greatest weight is to be applied to satisfying the Desired Character for the Zone.

### **DESIRED CHARACTER**

This Zone is the economic and cultural focus of the State and includes a range of employment, community, educational, tourism and entertainment facilities. It is anticipated that an increased

population within the Zone will complement the range of opportunities and experiences provided in the City and increase its vibrancy.

The Zone will be active during the day, evening and late night. Licensed entertainment premises, nightclubs and bars are encouraged throughout the Zone, particularly where they are located above or below ground floor level to maintain street level activation during the day and evening.

High-scale development is envisaged in the Zone with high street walls that frame the streets. However an interesting pedestrian environment and human scale will be created at ground floor levels through careful building articulation and fenestration, frequent openings in building façades, verandahs, balconies, awnings and other features that provide weather protection.

In important pedestrian areas, buildings will be set back at higher levels above the street wall to provide views to the sky and create a comfortable pedestrian environment. In narrow streets and laneways the street setback above the street wall may be relatively shallow or non-existent to create intimate spaces through a greater sense of enclosure. In the Central Business Policy Areas, upper level setbacks are not envisaged.

Non-residential land uses at ground floor level that generate high levels of pedestrian activity such as shops, cafés and restaurants will occur throughout the Zone. Within the Central Business Policy Area, residential land uses at ground level are discouraged. At ground level, development will continue to provide visual interest after hours by being well lit and having no external shutters. Non-residential and / or residential land uses will face the street at the first floor level to contribute to street vibrancy.

New development will achieve high design quality by being:

- (a) **Contextual** – so that it responds to its surroundings, recognises and carefully considers the adjacent built form, and positively contributes to the character of the immediate area.
- (b) **Durable** – by being fit for purpose, adaptable and long lasting, and carefully considers the existing development around it.
- (c) **Inclusive** – by integrating landscape design to optimize pedestrian and cyclist usability, privacy, and equitable access, and also promote the provision of quality spaces integrated with the public realm that can be used for access and recreation and help optimize security and safety both internally and into the public realm, for occupants and visitors alike.
- (d) **Sustainable** – by integrating sustainable systems into new buildings and the surrounding landscape design to improve environmental performance and minimise energy consumption.
- (e) **Amenable** – by providing natural light and ventilation to habitable spaces.

Contemporary juxtapositions will provide new settings for heritage places. Innovative design is expected in areas of identified street character with an emphasis on contemporary architecture that responds to site context and broader streetscape, while supporting optimal site development. The addition of height, bulk and massing of new form should be given due consideration in the wider context of the proposed development.

There will also be a rich display of art that is accessible to the public and contextually relevant.

### **Adelaide's pattern of streets and squares**

The distinctive grid pattern of Adelaide will be reinforced through the creation of a series of attractive boulevards as shown on Concept Plan [Figures CC/1 and 2](#). These boulevards will provide a clear sense of arrival into the City and be characterised by buildings that are aligned to the street pattern, particularly at ground level.

Views to important civic landmarks, the Park Lands and the Adelaide Hills will be retained as an important part of the City's charm and character.

The City's boulevards, terraces and Squares will be developed as follows:



- (a) North Terrace will be reinforced as an important pedestrian promenade and cultural boulevard that provides an important northern edge to the City square mile.
- (b) King William Street will be enhanced as the City's principal north-south boulevard and will be reinforced as the City's commercial spine.
- (c) Grote Street-Wakefield Street will be enhanced as the City's principal east-west boulevard and will be developed to provide a strong frame that presents a sense of enclosure to the street.
- (d) East Terrace will be characterised by buildings that maximise views through to the Park Lands and provide a distinct City edge.
- (e) West Terrace will be reinforced as the western 'gateway' to the City centre and will form an imposing frontage to the western City edge. Buildings will be constructed to the front and side boundaries, and designed to maximise views through to the Park Lands. Corner sites at the junctions of West Terrace and the major east-west streets will be developed as strongly defined visual gateways to the City. This will provide an imposing frontage to the western edge of the City, which comprises a mixture of commercial, showroom and residential development.
- (f) Pulteney and Morphett streets are key north-south boulevards. A sense of activation and enclosure of these streets will be enhanced through mixed use development with a strong built form edge. Pulteney Street will include residential, office and institutional uses, and retail activities. These boulevards will become important tree-lined commercial corridors.
- (g) Currie, Grenfell, Franklin and Flinders streets, as wider east-west boulevards provide important entry points to the City. Currie and Grenfell streets will become a key focus for pedestrians, cycling and public transport. These streets also provide long views to the hills as their closing vistas and these view corridors should remain uncluttered.
- (h) Victoria, Hindmarsh and Light Squares will have a continuous edge of medium to high-scale development that frames the Squares and increases ground level activity.

The Zone also includes a number of Main Street areas, encompassing Rundle Mall, Rundle Street, Hindley Street and Gouger Street, which are envisaged to have a wide range of retail, commercial and community uses that generate high levels of activity. These areas will have an intimately scaled built form with narrow and frequent building frontages. These areas are shown on Concept Plan [Figures CC/1 and 2](#).

Development fronting North Terrace, King William Street, Wakefield Street, Grote Street, the Squares, and in the Main Street Policy Area, will reflect their importance through highly contextual design that reflects and responds to their setting and role.

Minor streets and laneways will have a sense of enclosure (a tall street wall compared to street width) and an intimate, welcoming and comfortable pedestrian environment with buildings sited and composed in a way that responds to the buildings' context. There will be a strong emphasis on ground level activation through frequent window openings, land uses that spill out onto the footpath, and control of wind impacts.

Development in minor streets and laneways with a high value character will respond to important character elements and provide a comfortable pedestrian environment, particularly in the following streets: Gray, Leigh, Union, Chesser, Coromandel, Tucker, Cardwell, Kenton, Market, Ruthven, Cannon, Tatham, Bentham streets, Murrays Lane and Wright Court.

A comprehensive, safe and convenient movement network throughout the City will develop, focusing on the provision of linkages on both public and private land between important destinations and public transport. A high quality system of bicycle or shared pedestrian and bicycle routes will be established within the Zone.

## OBJECTIVES

### General

- Objective 1:** The principal focus for the economic, social and political life of metropolitan Adelaide and the State.
- Objective 2:** A vibrant mix of commercial, retail, professional services, hospitality, entertainment, educational facilities, and medium and high density living.
- Objective 3:** Design and management of City living to ensure the compatibility of residential amenity with the essential commercial and leisure functions of the Zone.
- Objective 4:** City streets that provide a comfortable pedestrian environment.
- Objective 5:** Innovative design approaches and contemporary architecture that respond to a building's context.
- Objective 6:** Buildings that reinforce the gridded layout of Adelaide's streets and respond to the underlying built-form framework of the City.
- Objective 7:** Large sites developed to their full potential while ensuring a cohesive scale of development and responding to a building's context.
- Objective 8:** Development that contributes to the Desired Character of the Zone.

## PRINCIPLES OF DEVELOPMENT CONTROL

### Land Use

- 1 The following types of development, or combinations thereof, are envisaged:
  - Affordable housing
  - Aged persons accommodation
  - Community centre
  - Consulting room
  - Convention centre
  - Dwelling
  - Educational establishment
  - Emergency services facility
  - Hospital
  - Hotel
  - Indoor recreation centre
  - Licensed entertainment premises
  - Library
  - Motel
  - Office
  - Pre-school
  - Personal service establishment
  - Place of worship
  - Serviced apartment
  - Restaurant
  - Residential flat building
  - Student accommodation
  - Shop or group of shops
  - Tourist accommodation
- 2 Land uses that are typically closed during the day should be designed to maximise daytime and evening activation at street level and be compatible with surrounding land uses, in particular residential development.



- 3 Low impact industries should be located outside the Central Business Policy Area and have minimal off-site impacts with respect to noise, air, water and waste emissions, traffic generation and movement.
- 4 Development listed as non-complying is generally inappropriate.

#### **Form and Character**

- 5 Development should be consistent with the Desired Character for the Zone.

#### **Design and Appearance**

- 6 Development should be of a high standard of architectural design and finish which is appropriate to the City's role and image as the capital of the State.
- 7 Buildings should achieve a high standard of external appearance by:
  - (a) the use of high quality materials and finishes. This may be achieved through the use of materials such as masonry, natural stone, prefinished materials that minimise staining, discolouring or deterioration, and avoiding painted surfaces particularly above ground level;
  - (b) providing a high degree of visual interest through articulation, avoiding any large blank facades, and incorporating design features within blank walls on side boundaries which have the potential to be built out;
  - (c) ensuring lower levels are well integrated with, and contribute to a vibrant public realm; and
  - (d) ensuring any ground and first floor level car parking elements are sleeved by residential or non-residential land uses (such as shops, offices and consulting rooms) to ensure an activated street frontage.
- 8 Buildings should present an attractive pedestrian-oriented frontage that adds interest and vitality to City streets and laneways.
- 9 The finished ground floor level of buildings should be at grade and/or level with the footpath to provide direct pedestrian access and street level activation.
- 10 Providing footpath widths and street tree growth permit, development should contribute to the comfort of pedestrians through the incorporation of verandahs, balconies, awnings and/or canopies that provide pedestrian shelter.
- 11 Buildings should be positioned regularly on the site and built to the street frontage, except where a setback is required to accommodate outdoor dining or provide a contextual response to a heritage place.
- 12 Buildings should be designed to include a podium/street wall height and upper level setback (in the order of 3-6 metres) that:
  - (a) relates to the scale and context of adjoining built form;
  - (b) provides a human scale at street level;
  - (c) creates a well-defined and continuity of frontage;
  - (d) gives emphasis and definition to street corners to clearly define the street grid;
  - (e) contributes to the interest, vitality and security of the pedestrian environment;
  - (f) maintains a sense of openness to the sky for pedestrians and brings daylight to the street;  
and

- (g) achieves pedestrian comfort by minimising micro climatic impacts (particularly shade/shelter, wind tunnelling and downward drafts);

other than (h) or (i):

- (h) in the Central Business Policy Area;

- (i) where a lesser (or zero) upper level setback and/or podium height is warranted to correspond with and complement the form of adjacent development, in which case alternative design solutions should be included to achieve a cohesive streetscape, provided parts (b) to (g) are still achieved.

- 13 Buildings north of Rundle Mall, Rundle Street, Hindley Street and Gouger Street should have a built form that incorporates slender tower elements, spaces between buildings or other design techniques that enable sunlight access to the southern footpath.
- 14 Buildings, advertisements, site landscaping, street planting and paving should have an integrated, coordinated appearance and should enhance the urban environment.
- 15 Building façades should be strongly modelled, incorporate a vertical composition which reflects the proportions of existing frontages, and ensure that architectural detailing is consistent around corners and along minor streets and laneways.
- 16 Development that exceeds the maximum building height shown in Concept Plan [Figures CC/1 and 2](#), and meets the relevant quantitative provisions should demonstrate a significantly higher standard of design outcome in relation to qualitative policy provisions including site configuration that acknowledges and responds to the desired future character of an area but that also responds to adjacent conditions (including any special qualities of a locality), pedestrian and cyclist amenity, activation, sustainability, and public realm and streetscape contribution.

#### *The Squares (Victoria, Hindmarsh and Light)*

- 17 Outdoor eating and drinking facilities associated with cafés and restaurants are appropriate ground floor uses and should contribute to the vitality of the Squares and create a focus for leisure.
- 18 Buildings fronting the Squares should:
  - (a) provide a comfortable pedestrian and recreation environment by enabling direct sunlight to a minimum of 75 percent of the landscaped part of each Square at the September equinox; and
  - (b) reinforce the enclosure of the Squares with a continuous built-form with no upper level setbacks.

#### *The Terraces (North, East and West)*

- 19 Development along the terraces should contribute to a continuous built form to frame the City edge and activate the Park Lands.
- 20 Development along North Terrace should reinforce the predominant scale and 'City wall' character of the Terrace frontage.

#### **Building Height**

- 21 Development should not exceed the maximum building height shown in Concept Plan [Figures CC/1 and 2](#) unless;
  - (a) it is demonstrated that the development reinforces the anticipated city form in Concept Plan [Figures CC/1 and 2](#), and



(b) only if:

- (i) at least two of the following features are provided:
  - (1) the development provides an orderly transition up to an existing taller building or prescribed maximum building height in an adjoining Zone or Policy Area;
  - (2) the development incorporates the retention, conservation and reuse of a building which is a listed heritage place;
  - (3) high quality universally accessible open space that is directly connected to, and well integrated with, public realm areas of the street;
  - (4) universally accessible, safe and secure pedestrian linkages that connect through the development site as part of the cities pedestrian network on [Map Adel/1 \(Overlay 2A\)](#);
  - (5) on site car parking does not exceed a rate of 0.5 spaces per dwelling, car parking areas are adaptable to future uses or all car parking is provided underground;
  - (6) residential, office or any other actively occupied use is located on all of the street facing side of the building, with any above ground car parking located behind;
  - (7) a range of dwelling types that includes at least 10% of 3+ bedroom apartments;
  - (8) more than 15 per cent of dwellings as affordable housing.
- (ii) plus all of the following sustainable design measures are provided:
  - (1) a rooftop garden covering a majority of the available roof area supported by services that ensure ongoing maintenance;
  - (2) a greenroof, or greenwalls / façades supported by services that ensure ongoing maintenance;
  - (3) innovative external shading devices on all of the western side of a street facing façade; and
  - (4) higher amenity through provision of private open space in excess of minimum requirements, access to natural light and ventilation to all habitable spaces and common circulation areas.

**22** Development should have optimal height and floor space yields to take advantage of the premium City location and should have a building height no less than half the maximum shown on Concept Plan [Figures CC/1 and 2](#), or 28 metres in the Central Business Policy Area, except where one or more of the following applies:

- (a) a lower building height is necessary to achieve compliance with the Commonwealth Airports (Protection of Airspace) Regulations;
- (b) the site is adjacent to the City Living Zone or the Adelaide Historic (Conservation) Zone and a lesser building height is required to manage the interface with low-rise residential development;
- (c) the site is adjacent to a heritage place, or includes a heritage place;
- (d) the development includes the construction of a building in the same, or substantially the same, position as a building which was demolished, as a result of significant damage caused by an event, within the previous 3 years where the new building has the same, or substantially the same, layout and external appearance as the previous building.

## Interface

- 23** Development should manage the interface with the City Living Zone or the Adelaide Historic (Conservation) Zone in relation to building height, overshadowing, massing, building proportions and traffic impacts and should avoid land uses, or intensity of land uses, that adversely affect residential amenity.
- 24** Development on all sites on the southern side of Gouger Street - Angas Street and adjacent to a northern boundary of the City Living Zone or the Adelaide Historic (Conservation) Zone should not exceed 22 metres in building height unless the Council Wide overshadowing Principles of Development Control are met.
- 25** Parts of a development that exceed the prescribed maximum building height shown on Concept Plan [Figures CC/1 and 2](#) that are directly adjacent to the City Living, Main Street (Adelaide) and Adelaide Historic (Conservation) Zone boundaries should be designed to minimise visual impacts on sensitive uses in the adjoining zones and to maintain the established or desired future character of the area. This may be achieved through a number of techniques such as additional setback, avoiding tall sheer walls, centrally locating taller elements, providing variation of light and shadow through articulation to provide a sense of depth and create visual interest, and the like.

## Movement

- 26** Pedestrian movement should be based on a network of pedestrian malls, arcades and lanes, linking the surrounding Zones and giving a variety of north-south and east-west links.
- 27** Development should provide pedestrian linkages for safe and convenient movement with arcades and lanes clearly designated and well-lit to encourage pedestrian access to public transport and areas of activity. Blank surfaces, shutters and solid infills lining such routes should be avoided.
- 28** Development should ensure existing through-site and on-street pedestrian links are maintained and new pedestrian links are developed in accordance with [Map Adel/1 \(Overlay 2A\)](#).
- 29** Car parking should be provided in accordance with [Table Adel/7](#).
- 30** Multi-level car parks should locate vehicle access points away from the primary street frontage wherever possible and should not be located:
- (a) within any of the following areas:
    - (i) the Core Pedestrian Area identified in [Map Adel/1 \(Overlays 2, 2A and 3\)](#)
    - (ii) on frontages to North Terrace, East Terrace, Rundle Street, Hindley Street, Currie Street, Wymouth Street (east of Light Square), Victoria Square or King William Street;
  - (b) where they conflict with existing or projected pedestrian movement and/or activity;
  - (c) where they would cause undue disruption to traffic flow; and
  - (d) where it involves creating new crossovers in North Terrace, Rundle Street, Hindley Street, Currie Street and Wymouth Street (east of Light Square), Grenfell Street and Pirie Street (west of Pulteney Street), Victoria Square, Light Square, Hindmarsh Square, Gawler Place and King William Street or access across primary City access and secondary City access roads identified in [Map Adel/1 \(Overlay 1\)](#).
- 31** Multi-level, non-ancillary car parks are inappropriate within the Core Pedestrian Area as shown on [Map Adel/1 \(Overlays 2, 2A and 3\)](#).
- 32** Vehicle parking spaces and multi-level vehicle parking structures within buildings should:
- (a) enhance active street frontages by providing land uses such as commercial, retail or other non-car park uses along ground floor street frontages;



- (b) complement the surrounding built form in terms of height, massing and scale; and
- (c) incorporate façade treatments along major street frontages that are sufficiently enclosed and detailed to complement neighbouring buildings consistent with the Desired Character of the locality.

### **Advertising**

- 33** Other than signs along Hindley Street, advertisements should use simple graphics and be restrained in their size, design and colour.
- 34** In minor streets and laneways, a greater diversity of type, shape, numbers and design of advertisements are appropriate provided they are of a small-scale and located to present a consistent message band to pedestrians.
- 35** There should be an overall consistency achieved by advertisements along individual street frontages.
- 36** In Chesser Street, French Street and Coromandel Place advertisements should be small and preferably square and should not be located more than 3.7 metres above natural ground level or an abutting footpath or street. However, advertisements in these streets may be considered above 3.7 metres at locations near the intersections with major streets.
- 37** Advertisements on the Currie Street frontages between Topham Mall and Gilbert Place and its north-south prolongation should be of a size, shape and location complementary to the desired townscape character, with particular regard to the following:
  - (a) On the southern side of Currie Street, advertisements should be fixed with their underside at a common height, except where the architectural detailing of building façades precludes it. At this 'canopy' level advertisements should be of a uniform size and fixed without the support of guy wires. Where architectural detailing permits, advertisements may mark the major entrances to buildings along the southern side of Currie Street with vertical projecting advertisements 1.5 metres high by 1.2 metres wide at, or marginally above, the existing canopy level. Painted wall or window signs should be restrained.
  - (b) On the northern side of Currie Street, advertisements should be of a uniform fixing height and consistent dimensions to match those prevailing in the area.

### **PROCEDURAL MATTERS**

#### **Complying Development**

- 38** Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

In addition, the following forms of development are assigned as **complying**:

- (a) Other than in relation to a State heritage place, Local heritage place (City Significance), or Local heritage place, work undertaken within a building which does not involve a change of use or affect the external appearance of the building;
- (b) Temporary depot for Council for a period of no more than 3 months where it can be demonstrated that appropriate provision has been made for:
  - (i) dust control;
  - (ii) screening, including landscaping;
  - (iii) containment of litter and water; and
  - (iv) securing of the site.

- (c) Change in the use of land from a non-residential use to an office, shop or consulting room (excluding any retail showroom, adult entertainment premises, adult products and services premises or licensed premises).

### **Non-complying Development**

**39** The following kinds of development are **non-complying**:

A change in use of land to any of the following:

Amusement machine centre

Advertisements involving any of the following:

- (a) third party advertising except on Hindley Street, Rundle Mall or on allotments at the intersection of Rundle Street and Pulteney Street, or temporary advertisements on construction sites;
- (b) advertisements located at roof level where the sky or another building forms the background when viewed from ground level;
- (c) advertisements in the area bounded by West Terrace, Grote Street, Franklin Street and Gray Street;
- (d) animation of advertisements along and adjacent to the North Terrace, King William Street and Victoria Square frontages.

Total demolition of a State Heritage Place (as identified in [Table Adel/1](#)).

Vehicle parking except:

- (a) where it is ancillary to an approved or existing use;
- (b) it is a multi-level car park located outside the Core Pedestrian Area as indicated on [Map Adel/1 \(Overlay 2, 2A and 3\)](#); or
- (c) it is within an existing building located outside the Core Pedestrian Area as indicated on [Map Adel/1 \(Overlay 2, 2A and 3\)](#).

### **Public Notification**

**40** Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

In addition, the following forms of development, or any combination of (except where the development is non-complying), are assigned:

- (a) **Category 1**, public notification not required:

All forms of development other than where it is assigned Category 2.

- (b) **Category 2**, public notification required. Third parties do not have any appeal rights.

Any development where the site of the development is adjacent land to land in the City Living Zone or Adelaide Historic (Conservation) Zone and it exceeds 22 metres in building height.

*Note: For Category 3 development, public notification is required. Third parties may make written representations, appear before the relevant authority on the matter, and may appeal against a development consent. This includes any development not classified as either Category 1 or Category 2.*

### **COUNCIL WIDE**



## **Environmental**

### **Crime Prevention Through Urban Design**

#### **OBJECTIVES**

**Objective 24:** A safe and secure, crime resistant environment that:

- (a) ensures that land uses are integrated and designed to facilitate natural surveillance;
- (b) promotes building and site security; and
- (c) promotes visibility through the incorporation of clear lines of sight and appropriate lighting.

#### **PRINCIPLES OF DEVELOPMENT CONTROL**

- 1 Development should promote the safety and security of the community in the public realm and within development. Development should:
  - (a) promote natural surveillance of the public realm, including open space, car parks, pedestrian routes, service lanes, public transport stops and residential areas, through the design and location of physical features, electrical and mechanical devices, activities and people to maximise visibility by:
    - (i) orientating windows, doors and building entrances towards the street, open spaces, car parks, pedestrian routes and public transport stops;
    - (ii) avoiding high walls, blank facades, carports and landscaping that obscures direct views to public areas;
    - (iii) arranging living areas, windows, pedestrian paths and balconies to overlook recreation areas, entrances and car parks;
    - (iv) positioning recreational and public space areas so they are bound by roads on at least two road frontages or overlooked by development;
    - (v) creating a complementary mix of day and night-time activities, such as residential, commercial, recreational and community uses, that extend the duration and level of intensity of public activity;
    - (vi) locating public toilets, telephones and other public facilities with direct access and good visibility from well-trafficked public spaces;
    - (vii) ensuring that rear service areas and access lanes are either secured or exposed to surveillance; and
    - (viii) ensuring the surveillance of isolated locations through the use of audio monitors, emergency telephones or alarms, video cameras or staff eg by surveillance of lift and toilet areas within car parks.
  - (b) provide access control by facilitating communication, escape and path finding within development through legible design by:
    - (i) incorporating clear directional devices;
    - (ii) avoiding opportunities for concealment near well travelled routes;
    - (iii) closing off or locking areas during off-peak hours, such as stairwells, to concentrate access/exit points to a particular route;

- (iv) use of devices such as stainless steel mirrors where a passage has a bend;
  - (v) locating main entrances and exits at the front of a site and in view of a street;
  - (vi) providing open space and pedestrian routes which are clearly defined and have clear and direct sightlines for the users; and
  - (vii) locating elevators and stairwells where they can be viewed by a maximum number of people, near the edge of buildings where there is a glass wall at the entrance.
- (c) promote territoriality or sense of ownership through physical features that express ownership and control over the environment and provide a clear delineation of public and private space by:
- (i) clear delineation of boundaries marking public, private and semi-private space, such as by paving, lighting, walls and planting;
  - (ii) dividing large development sites into territorial zones to create a sense of ownership of common space by smaller groups of dwellings; and
  - (iii) locating main entrances and exits at the front of a site and in view of a street.
- (d) provide awareness through design of what is around and what is ahead so that legitimate users and observers can make an accurate assessment of the safety of a locality and site and plan their behaviour accordingly by:
- (i) avoiding blind sharp corners, pillars, tall solid fences and a sudden change in grade of pathways, stairs or corridors so that movement can be predicted;
  - (ii) using devices such as convex security mirrors or reflective surfaces where lines of sight are impeded;
  - (iii) ensuring barriers along pathways such as landscaping, fencing and walls are permeable;
  - (iv) planting shrubs that have a mature height less than one metre and trees with a canopy that begins at two metres;
  - (v) adequate and consistent lighting of open spaces, building entrances, parking and pedestrian areas to avoid the creation of shadowed areas; and
  - (vi) use of robust and durable design features to discourage vandalism.
- 2** Residential development should be designed to overlook streets, public and communal open space to allow casual surveillance.
- 3** To maximise security and safety, buildings should be designed to minimise access between roofs, balconies and windows of adjacent buildings.
- 4** Security features should be incorporated within the design of shop fronts to complement the design of the frontage and allow window shopping out of hours. If security grilles are provided, these should:
- (a) be transparent and illuminated to complement the appearance of the frontage;
  - (b) provide for window shopping; and
  - (c) allow for the spill of light from the shop front onto the street.

Solid shutters with less than 75 percent permeability are not acceptable.



- 5** Public toilets should be designed and located to:
- (a) promote the visibility of people entering and exiting the facility by avoiding recessed entrances and dense shrubbery which obstructs passive surveillance;
  - (b) limit opportunities for vandalism through the use of vandal proof lighting on the public toilet buildings and nearby;
  - (c) avoid features which facilitate loitering, such as seating or telephones immediately adjacent the structure; and
  - (d) maximise surveillance through location near public transport links, pedestrian and cyclist networks.

## **Operating Hours and Associated Activities of Licensed Premises**

### **OBJECTIVE**

**Objective 25:** Operating hours of licensed premises or licensed entertainment premises, together with associated activities of such premises, established and operated so as to reinforce the desired character of the locality and appropriate behavioural activities.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

- 6** Licensed premises and licensed entertainment premises or similar should:
- (a) be located, designed and operated in order to reinforce the desired character of a locality, as expressed in the relevant Zone or Policy Area;
  - (b) be located, designed and operated so as to not negatively impact on peoples orderly use and enjoyment of a locality, such as through disorderly behavioural activities and/or disorderly behavioural movement to and from such land uses; and
  - (c) incorporate best practice measures to effectively manage the behaviour of users moving to and from such land uses.
- 7** Licensed premises and licensed entertainment premises or similar should operate with operating hours to reinforce the desired character of the locality.

## **Noise Emissions**

### **OBJECTIVES**

**Objective 26:** Development that does not unreasonably interfere with the desired character of the locality by generating unduly annoying or disturbing noise.

**Objective 27:** Noise sensitive development designed to protect its occupants from existing noise sources and from noise sources contemplated within the relevant Zone or Policy Area and that does not unreasonably interfere with the operation of non-residential uses contemplated within the relevant Zone or Policy Area.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

#### **Noise Sources**

- 8** Development with potential to emit significant noise (including licensed entertainment premises and licensed premises) should incorporate appropriate noise attenuation measures in to their design to prevent noise from causing unreasonable interference with the amenity and desired character of the locality, as contemplated in the relevant Zone and Policy Area.

- 9** Development of licensed premises or licensed entertainment premises or similar in or adjacent to a City Living Zone, the Adelaide Historic (Conservation) Zone or the North Adelaide Historic (Conservation) Zone should include noise attenuation measures to achieve the following when assessed at the nearest existing or envisaged future noise sensitive development:
- (a) the music noise ( $L_{10, 15 \text{ min}}$ ) is:
    - (i) less than 8 dB above the level of background noise<sup>2</sup> ( $L_{90,15 \text{ min}}$ ) in any octave band of the sound spectrum; and
    - (ii) less than 5 dB(A) above the level of background noise ( $L_{A 90,15 \text{ min}}$ ) for the overall (sum of all octave bands) A-weighted level.
- 10** Development of licensed premises or licensed entertainment premises or similar in the Capital City, Main Street, Mixed Use and City Frame Zones should include noise attenuation measures to achieve the following when assessed at:
- (a) the nearest existing noise sensitive location in or adjacent to that Zone:
    - (i) music noise ( $L_{10, 15 \text{ min}}$ ) less than 8 dB above the level of background noise ( $L_{90,15 \text{ min}}$ ) in any octave band of the sound spectrum; and
    - (ii) music noise ( $L_{A10, 15 \text{ min}}$ ) less than 5 dB(A) above the level of background noise ( $L_{A90,15 \text{ min}}$ ) for the overall (sum of all octave bands) A-weighted levels; or
  - (b) the nearest envisaged future noise sensitive location in or adjacent to that Zone:
    - (i) music noise ( $L_{10, 15 \text{ min}}$ ) less than 8dB above the level of background noise ( $L_{90,15 \text{ min}}$ ) in any octave band of the sound spectrum and music noise ( $L_{10, 15 \text{ min}}$ ) less than 5dB(A) above the level of background noise ( $L_{A90,15 \text{ min}}$ ) for the overall (sum of all octave bands) A-weighted levels; or
    - (ii) music noise ( $L_{10, 15 \text{ min}}$ ) less than 60dB(Lin) in any octave band of the sound spectrum and the overall ( $L_{A10,15 \text{ min}}$ ) noise level is less than 55 dB(A).
- 11** Mechanical plant or equipment should be designed, sited and screened to minimise noise impact on adjacent premises or properties. The noise level associated with the combined operation of plant and equipment such as air conditioning, ventilation and refrigeration systems when assessed at the nearest existing or envisaged noise sensitive location in or adjacent to the site should not exceed
- (a) 55 dB(A) during daytime (7.00am to 10.00pm) and 45 dB(A) during night time (10.00pm to 7.00am) when measured and adjusted in accordance with the relevant environmental noise legislation except where it can be demonstrated that a high background noise exists.
  - (b) 50 dB(A) during daytime (7.00am to 10.00pm) and 40 dB(A) during night time (10.00pm to 7.00am) in or adjacent to a City Living Zone, the Adelaide Historic (Conservation) Zone, the North Adelaide Historic (Conservation) Zone or the Park Lands Zone when measured and adjusted in accordance with the relevant environmental noise legislation except where it can be demonstrated that a high background noise exists.
- 12** To ensure minimal disturbance to residents:
- (a) ancillary activities such as deliveries, collection, movement of private waste bins, goods, empty bottles and the like should not occur:
    - (i) after 10.00pm; and
    - (ii) before 7.00am Monday to Saturday or before 9.00am on a Sunday or Public Holiday.



- (b) typical activity within any car park area including vehicles being started, doors closing and vehicles moving away from the premises should not result in sleep disturbance when proposed for use after 10.00pm as defined by the limits recommended by the World Health Organisation.

### **Noise Receivers**

- 13** Noise sensitive development should incorporate adequate noise attenuation measures into their design and construction to provide occupants with reasonable amenity when exposed to noise sources such as major transport corridors (road, rail, tram and aircraft), commercial centres, entertainment premises and the like, and from activities and land uses contemplated in the relevant Zone and Policy Area provisions.
- 14** Noise sensitive development in mixed use areas should not unreasonably interfere with the operation of surrounding non-residential uses that generate noise levels that are commensurate with the envisaged amenity of the locality.
- 15** Noise sensitive development adjacent to noise sources should include noise attenuation measures to achieve the following:
- (a) satisfaction of the sleep disturbance criteria in the bedrooms or sleeping areas of the development as defined by the limits recommended by the World Health Organisation;
  - (b) the maximum satisfactory levels in any habitable room for development near major roads, as provided in the Australian/New Zealand Standard AS/NZS 2107:2000 - 'Acoustics - Recommended Design Sound Levels and Reverberation Times for Building Interiors'; and
  - (c) noise level in any bedroom, when exposed to music noise ( $L_{10}$ ) from existing entertainment premises, being:
    - (i) less than 8 dB above the level of background noise ( $L_{90,15 \text{ min}}$ ) in any octave band of the sound spectrum; and
    - (ii) less than 5 dB(A) above the level of background noise ( $L_{A90,15 \text{ min}}$ ) for the overall (sum of all octave bands) A-weighted levels

Background noise within the habitable room can be taken to be that expected in a typical residential/apartment development of the type proposed, that is inclusive of internal noise sources such as air conditioning systems, refrigerators and the like as deemed appropriate.

### **Waste Management**

#### **OBJECTIVE**

**Objective 28:** Development which supports high local environmental quality, promotes waste minimisation, re-use and recycling, encourages waste water, grey water and stormwater re-use and does not generate unacceptable levels of air, liquid or solid pollution.

#### **PRINCIPLES OF DEVELOPMENT CONTROL**

- 16** A dedicated area for on-site collection and sorting of recyclable materials and refuse should be provided within all new development.
- 17** A dedicated area for the collection and sorting of construction waste and the recycling of building materials during construction as appropriate to the size and nature of the development should be provided and screened from public view.
- 18** Development greater than 2 000 square metres of total floor area should manage waste by:

- (a) containing a dedicated area for the collection and sorting of construction waste and recyclable building materials;
  - (b) on-site storage and management of waste;
  - (c) disposal of non-recyclable waste; and
  - (d) incorporating waste water and stormwater re-use including the treatment and re-use of grey water.
- 19** Development should not result in emission of atmospheric, liquid or other pollutants, or cause unacceptable levels of smell and odour which would detrimentally affect the amenity of adjacent properties or its locality. Land uses such as restaurants, shops, cafés or other uses that generate smell and odour should:
- (a) ensure extraction flues, ventilation and plant equipment are located in appropriate locations that will not detrimentally affect the amenity of adjacent occupiers in terms of noise, odours and the appearance of the equipment;
  - (b) ensure ventilation and extraction equipment and ducting have the capacity to clean and filter the air before being released into the atmosphere; and
  - (c) ensure the size of the ventilation and extraction equipment is suitable and has the capacity to adequately cater for the demand generated by the potential number of patrons.

## **Contaminated Sites**

### **OBJECTIVE**

**Objective 29:** A safe and healthy living and working environment.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

- 20** Where there is evidence of, or reasonable suspicion that land, buildings and/or water, including underground water, may have been contaminated, or there is evidence of past potentially contaminating activity/ies, development should only occur where it is demonstrated that the land, buildings and/or water can be made suitable for its intended use prior to commencement of that use.

## **Energy Efficiency**

### **OBJECTIVE**

**Objective 30:** Development which is compatible with the long term sustainability of the environment, minimises consumption of non-renewable resources and utilises alternative energy generation systems.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

#### **All Development**

- 21** Buildings should provide adequate thermal comfort for occupants and minimise the need for energy use for heating, cooling and lighting by:
- (a) providing an internal day living area with a north-facing window, other than for minor additions\*, by:

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\* Minor additions have a floor area less than 50 percent of the existing dwelling and do not include a day living area.



- (i) arranging and concentrating main activity areas of a building to the north for solar penetration; and
  - (ii) placing buildings on east-west allotments against or close to the southern boundary to maximise northern solar access and separation to other buildings to the north.
- (b) efficient layout, such as zoning house layout to enable main living areas to be separately heated and cooled, other than for minor additions;
- (c) locating, sizing and shading windows to reduce summer heat loads and permit entry of winter sun;
- (d) allowing for natural cross ventilation to enable cooling breezes to reduce internal temperatures in summer;
- (e) including thermal insulation of roof, walls, floors and ceilings and by draught proofing doors, windows and openings;
- (f) ensuring light colours are applied to external surfaces that receive a high degree of sun exposure, but not to an extent that will cause glare which produces discomfort or danger to pedestrians, occupants of adjacent buildings and users of vehicles;
- (g) providing an external clothes line for residential development; and
- (h) use of landscaping.
- 22** All development should be designed to promote naturally ventilated and day lit buildings to minimise the need for mechanical ventilation and lighting systems.
- 23** Energy reductions should, where possible, be achieved by the following:
- (a) appropriate orientation of the building by:
    - (i) maximising north/south facing facades;
    - (ii) designing and locating the building so the north facade receives good direct solar radiation;
    - (iii) minimising east/west facades to protect the building from summer sun and winter winds;
    - (iv) narrow floor plates to maximise the amount of floor area receiving good daylight; and/or
    - (v) minimising the ratio of wall surface to floor area.
  - (b) window orientation and shading;
  - (c) adequate thermal mass including night time purging to cool thermal mass;
  - (d) appropriate insulation by:
    - (i) insulating windows, walls, floors and roofs; and
    - (ii) sealing of external openings to minimise infiltration.
  - (e) maximising natural ventilation including the provision of openable windows;
  - (f) appropriate selection of materials, colours and finishes; and
  - (g) introduction of efficient energy use technologies such as geo-exchange and embedded, distributed energy generation systems such as cogeneration\*, wind power, fuel cells and

solar photovoltaic panels that supplement the energy needs of the building and in some cases, export surplus energy to the electricity grid.

- 24** Orientation and pitch of the roof should facilitate the efficient use of solar collectors and photovoltaic cells.
- 25** Buildings, where practical, should be refurbished, adapted and reused to ensure an efficient use of resources.
- 26** New buildings should be readily adaptable to future alternative uses.
- 27** Selection of internal materials for all buildings should be made with regard to internal air quality and ensure low toxic emissions, particularly with respect to paint and joinery products.

### **Residential Development**

- 28** New residential development and residential extensions should be designed to minimise energy consumption and limit greenhouse gas emissions.
- 29** Development is encouraged to avoid heat loss by incorporating treatments, such as double glazing of windows along the southern elevation, or by minimizing the extent of windows facing south.

### **Micro-climate and Sunlight**

#### **OBJECTIVES**

**Objective 33:** Buildings which are designed and sited to be energy efficient and to minimise micro-climatic and solar access impacts on land or other buildings.

**Objective 34:** Protection from rain, wind and sun without causing detriment to heritage places, street trees or the integrity of the streetscape.

#### **PRINCIPLES OF DEVELOPMENT CONTROL**

- 30** Development should be designed and sited to minimise micro-climatic and solar access impact on adjacent land or buildings, including effects of patterns of wind, temperature, daylight, sunlight, glare and shadow.
- 31** Development should be designed and sited to ensure an adequate level of daylight, minimise overshadowing of buildings, and public and private outdoor spaces, particularly during the lunch time hours.
- 32** Development should not significantly reduce daylight to private open space, communal open space, where such communal open space provides the primary private open space, and habitable rooms in adjacent City Living Zone, Adelaide Historic (Conservation) Zone and North Adelaide Historic (Conservation) Zone.
- 33** Glazing on building facades should not result in glare which produces discomfort or danger to pedestrians, occupants of adjacent buildings and users of vehicles.
- 34** Buildings within the Core and Primary Pedestrian Areas identified in [Map Adel/1 \(Overlays 2, 2A and 3\)](#), unless specified otherwise within the relevant Zone or Policy Area, should be designed to provide weather protection for pedestrians against rain, wind and sun. The design of canopies, verandahs and awnings should be compatible with the style and character of the building and adjoining buildings, as well as the desired character, both in scale and detail.
- 35** Weather protection should not be introduced where it would interfere with the integrity or heritage value of heritage places or unduly affect street trees.



- 36** Development that is over 21 metres in building height and is to be built at or on the street frontage should minimise wind tunnel effect.

## **Infrastructure**

### **OBJECTIVES**

- Objective 40:** Minimisation of the visual impact of infrastructure facilities.
- Objective 41:** Provision of services and infrastructure that are appropriate for the intended development and the desired character of the Zone or Policy Area.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

- 37** Provision should be made for utility services to the site of a development, including provision for the supply of water, gas and electricity and for the satisfactory disposal and potential re-use of sewage and waste water, drainage and storm water from the site of the development.
- 38** Service structures, plant and equipment within a site should be designed to be an integral part of the development and should be suitably screened from public spaces or streets.

## **Built Form and Townscape**

### **OBJECTIVES**

- Objective 46:** Reinforcement of the city's grid pattern of streets through:
- (a) high rise development framing city boulevards, the Squares and Park Lands
  - (b) vibrant main streets of a more intimate scale that help bring the city to life
  - (c) unique and interesting laneways that provide a sense of enclosure and intimacy.
- Objective 47:** Buildings should be designed to:
- (a) reinforce the desired character of the area as contemplated by the minimum and maximum building heights in the Zone and Policy Area provisions;
  - (b) maintain a sense of openness to the sky and daylight to public spaces, open space areas and existing buildings;
  - (c) contribute to pedestrian safety and comfort; and
  - (d) provide for a transition of building heights between Zone and Policy Areas where building height guidelines differ.
- Objective 48:** Development which incorporates a high level of design excellence in terms of scale, bulk, massing, materials, finishes, colours and architectural treatment.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

- 39** Where development significantly exceeds quantitative policy provisions, it should demonstrate a significantly higher standard of design outcome in relation to qualitative policy provisions including pedestrian and cyclist amenity, activation, sustainability and public realm and streetscape contribution.

## **Height, Bulk and Scale**

### **PRINCIPLES OF DEVELOPMENT CONTROL**

- 40** Development should be of a high standard of design and should reinforce the grid layout and distinctive urban character of the City by maintaining a clear distinction between the following:

- (a) the intense urban development and built-form of the town acres in the Capital City, Main Street, Mixed Use, City Frame and City Living Zones;
  - (b) the less intense and more informal groupings of buildings set within the landscaped environment of the Institutional Zones;
  - (c) the historic character of the Adelaide and North Adelaide Historic (Conservation) Zones and groups of historic housing within the City Living Zone; and
  - (d) the open landscape of the Park Lands Zone.
- 41** The height and scale of development and the type of land use should reflect and respond to the role of the street it fronts as illustrated on [Map Adel/1 \(Overlay 1\)](#).
- 42** The height, scale and massing of buildings should reinforce:
- (a) the desired character, built form, public environment and scale of the streetscape as contemplated within the Zone and Policy Area, and have regard to:
    - (i) maintaining consistent parapet lines, floor levels, height and massing with existing buildings consistent with the areas desired character;
    - (ii) reflecting the prevailing pattern of visual sub-division of neighbouring building frontages where frontages display a character pattern of vertical and horizontal sub-divisions; and
    - (iii) avoiding massive unbroken facades.
  - (b) a comfortable proportion of human scale at street level by:
    - (i) building ground level to the street frontage where zero set-backs prevail;
    - (ii) breaking up the building facade into distinct elements;
    - (iii) incorporating art work and wall and window detailing; and
    - (iv) including attractive planting, seating and pedestrian shelter.
- 43** Where possible, large sites should incorporate pedestrian links and combine them with publicly accessible open space.
- 44** Buildings and structures should not adversely affect by way of their height and location the long-term operational, safety and commercial requirements of Adelaide International Airport. Buildings and structures which exceed the heights shown in [Map Adel/1 \(Overlay 5\)](#) and which penetrate the Obstacle Limitation Surfaces (OLS) should be designed, marked or lit to ensure the safe operation of aircraft within the airspace around the Adelaide International Airport.
- 45** Buildings within the Capital City Zone should be built to the street edge to reinforce the grid pattern, create a continuity of frontage and provide definition and enclosure to the public realm whilst contributing to the interest, vitality and security of the pedestrian environment.

### **Composition and Proportion**

- 46** Development should respect the composition and proportion of architectural elements of building facades that form an important pattern which contributes to the streetscape's distinctive character in a manner consistent with the desired character of a locality by:
- (a) establishing visual links with neighbouring buildings by reflecting and reinforcing the prevailing pattern of visual sub-division in building facades where a pattern of vertical and/or horizontal sub-divisions is evident and desirable, for example, there may be strong horizontal lines of verandahs, masonry courses, podia or openings, or there may be vertical proportions in the divisions of facades or windows; and



(b) clearly defining ground, middle and roof top levels.

**47** Where there is little or no established building pattern, new buildings should create new features which contribute to an areas desired character and the way the urban environment is understood by:

- (a) frontages creating clearly defined edges;
- (b) generating new compositions and points of interest;
- (c) introducing elements for future neighbouring buildings; and
- (d) emphasising the importance of the building according to the street hierarchy.

### **Articulation and Modelling**

**48** Building facades fronting street frontages, access ways, driveways or public spaces should be composed with an appropriate scale, rhythm and proportion which responds to the use of the building, the desired character of the locality and the modelling and proportions of adjacent buildings.

**49** Balconies should:

- (a) respond to the street context and building orientation; and
- (b) incorporate balustrade detailing to reflect the balcony type and location and the materials and detail of the building facade.

**50** No part of any fully enclosed building should extend over property boundaries, including streets and public spaces, whether above a balcony at a lower level or not.

### **Materials, Colours and Finishes**

**51** The design, external materials, colours and finishes of buildings should have regard to their surrounding townscape context, built form and public environment, consistent with the desired character of the relevant Zone and Policy Area.

**52** Development should be finished with materials that are sympathetic to the design and setting of the new building and which incorporate recycled or low embodied energy materials. The form, colour, texture and quality of materials should be of high quality, durable and contribute to the desired character of the locality. Materials, colours and finishes should not necessarily imitate materials and colours of an existing streetscape

**53** Materials and finishes that are easily maintained and do not readily stain, discolour or deteriorate should be utilised.

**54** Development should avoid the use of large expanses of highly reflective materials and large areas of monotonous, sheer materials (such as polished granite and curtained wall glazing).

### **Sky and Roof Lines**

#### **OBJECTIVE**

**Objective 49:** Innovative and interesting skylines which contribute to the overall design and performance of the building.

#### **PRINCIPLES OF DEVELOPMENT CONTROL**

**55** Where a prevailing pattern of roof form assists in establishing the desired character of the locality, new roof forms should be complementary to the shape, pitch, angle and materials of adjacent building roofs.

- 56** Buildings should be designed to incorporate well designed roof tops that:
- (a) reinforce the desired character of the locality, as expressed in the relevant Zone or Policy Area;
  - (b) enhance the skyline and local views;
  - (c) contribute to the architectural quality of the building;
  - (d) provide a compositional relationship between the upper-most levels and the lower portions of the building;
  - (e) provide an expression of identity;
  - (f) articulate the roof, breaking down its massing on large buildings to minimise apparent bulk;
  - (g) respond to the orientation of the site; and
  - (h) create minimal glare.

### **Active Street Frontages**

#### **OBJECTIVES**

**Objective 50:** Development that enhances the public environment and, where appropriate provides activity and interest at street level, reinforcing a locality's desired character.

**Objective 51:** Development designed to promote pedestrian activity and provide a high quality experience for City residents, workers and visitors by:

- (a) enlivening building edges;
- (b) creating welcoming, safe and vibrant spaces;
- (c) improving perceptions of public safety through passive surveillance; and
- (d) creating interesting and lively pedestrian environments.

#### **PRINCIPLES OF DEVELOPMENT CONTROL**

- 57** Development should be designed to create active street frontages that provide activity and interest to passing pedestrians and contribute to the liveliness, vitality and security of the public realm.
- 58** Retail frontages should be designed to provide interest to passing pedestrians at street level and relief to building mass.
- 59** Commercial buildings should be designed to ensure that ground floor facades are rich in detail so they are exciting to walk by, interesting to look at and to stand beside.

### **Outdoor Dining**

#### **OBJECTIVE**

**Objective 52:** Development that contributes to the vibrancy, activity and desired character of a locality.

#### **PRINCIPLES OF DEVELOPMENT CONTROL**

- 60** Outdoor dining should:



- (a) be located outside the associated premises;
- (b) provide sufficient set-backs, such as from kerbs and property boundaries, and clearances, such as from buildings;
- (c) be located in an area safe for patrons where the security of the building is not compromised;
- (d) ensure the dining area is set back from the building line at street intersections;
- (e) ensure unimpeded pedestrian flow through free and uninterrupted pedestrian paths; and
- (f) ensure wheelchair access to pedestrian ramps is not compromised.

**61** Structures should:

- (a) be of high quality design and form an integral part of the streetscape;
- (b) not restrict public access;
- (c) not detract or restrict views of significant sightlines, buildings and landmarks;

**62** Signage that identifies the business name or logo, or advertises goods sold on the premises is only appropriate on glass and canvas screens and umbrellas and should meet the following:

- (a) signage and advertisements should be designed to improve and complement the amenity of the premises, be of an appropriate design and consistent with the desired character of the locality;
- (b) advertisements on outdoor dining items such as umbrellas and canvas screens should not exceed a portion that covers 10 percent of the total available space on each outdoor dining item, up to half of which may be commercial advertisements in the form of product logos used or sold by the premises;
- (c) advertisements should not be illuminated or animated; and
- (d) third party advertising on outdoor dining items is inappropriate.

## **Landscaping**

### **OBJECTIVE**

**Objective 55:** Water conserving landscaping that enhances the local landscape character and creates a pleasant, safe and attractive living environment.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

**63** Landscaping should:

- (a) be selected and designed for water conservation;
- (b) form an integral part of the design of development; and
- (c) be used to foster human scale, define spaces, reinforce paths and edges, screen utility areas and enhance the visual amenity of the area.

**64** Landscaping should incorporate local indigenous species suited to the site and development, provided such landscaping is consistent with the desired character of the locality and any heritage place.

**65** Landscaping should be provided to all areas of communal space, driveways and shared car parking areas.

- 66** Landscaping between the road and dwellings should be provided to screen and protect the dwellings from dust and visual impacts of the road.

## **Advertising**

### **OBJECTIVE**

**Objective 56:** Outdoor advertisements that are designed and located to:

- (a) reinforce the desired character and amenity of the locality within which it is located and rectify existing unsatisfactory situations;
- (b) be concise and efficient in communicating with the public, avoiding a proliferation of confusing and cluttered displays or a large number of advertisements; and
- (c) not create a hazard.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

- 67** Advertisements should be designed to respect and enhance the desired character and amenity of the locality by the means listed below:

- (a) the scale, type, design, location, materials, colour, style and illumination of any advertisements should be compatible with the design and character of the buildings and land to which it is related, and should be in accordance with provisions for the Zone and Policy Area in which it is situated and any relevant adjacent Zones or Policy Areas;
- (b) advertisements should be integrated with the architectural form, style and colour of buildings and wherever possible, requirements for advertisements should be considered in the design of new buildings;
- (c) advertisements should be artistically interesting in terms of graphics and construction with intricacy and individuality in design encouraged while maintaining consistency in design and style where co-ordinated advertisements are appropriate;
- (d) structural supports should be concealed from public view or of minimal visual impact;
- (e) advertisements on individual premises should be co-ordinated in terms of type and design and should be limited in number to minimize visual clutter;
- (f) advertisements should be displayed on fascia signs or located below canopy level;
- (g) advertisements on buildings or sites occupied by a number of tenants should be co-ordinated, complementary and the number kept to a minimum; and
- (h) advertisements on or adjacent to a heritage place should be designed and located to respect the heritage value of the heritage place.

- 68** Product advertisements illustrating products sold on the premises in conjunction with the business name should not exceed 25 percent of the area of any advertisement.

- 69** Advertisements should not endanger public safety or detrimentally affect the amenity of adjacent premises by reason of their location, position, construction or design and should:

- (a) not emit excessive glare or reflection from internal or external illumination;
- (b) not obscure road users' and pedestrians' views of vehicles, pedestrians or potentially hazardous road features;
- (c) not cause confusion with, or reduce the effectiveness of traffic control devices;



- (d) have a clearance between the footpath and base or underside of projecting signage of at least 2.5 metres for permanent advertisements and 2.3 metres for temporary advertisements, and between the kerb face and outside edge of the sign of at least 600 millimetres; and
  - (e) permit safe and convenient pedestrian movement.
- 70** Temporary advertisement hoardings or shrouds required for the screening of construction sites or for creating visual interest should occur only where they are:
- (a) of a high standard of design;
  - (b) displayed only during the period of construction;
  - (c) comprised of high quality opaque, solid and non-reflective material that is durable, low maintenance and appropriate to the City context;
  - (d) required to conceal wiring and conduits; and
  - (e) do not create undue risk to public or private safety.

## Transport and Access

### Access and Movement

#### OBJECTIVE

**Objective 60:** Access to and movement within the City that is easy, safe, comfortable and convenient with priority given to pedestrian and cyclist safety and access.

#### PRINCIPLES OF DEVELOPMENT CONTROL

- 71** Development should provide safe, convenient and comfortable access and movement.
- 72** Vehicle access points along primary and secondary city access roads and local connector roads, as shown on [Map Adel/1 \(Overlay 1\)](#) should be restricted.

### Pedestrian Access

#### OBJECTIVES

- Objective 61:** Development that promotes the comfort, enjoyment and security of pedestrians by providing shelter and reducing conflict with motor vehicles.
- Objective 62:** Development that contributes to the quality of the public realm as a safe, secure and attractive environment for pedestrian movement and social interaction.
- Objective 63:** Safe and convenient design of and access to buildings and public spaces, particularly for people with disabilities.

#### PRINCIPLES OF DEVELOPMENT CONTROL

- 73** Development should reflect the significance of the paths and increase the permeability of the pedestrian network identified within [Map Adel/1 \(Overlay 2\)](#) by ensuring:
  - (a) pedestrians are not disrupted or inconvenienced by badly designed or located vehicle access ramps in footpaths or streets; and
  - (b) vehicle and service entry points are kept to a minimum to avoid adverse impact on pedestrian amenity.

- 74** Within the Core, Primary and Secondary Pedestrian Areas identified within [Map Adel/1 \(Overlays 2, 2A and 3\)](#), development should be designed to support the establishment and maintenance of continuous footpaths so that pedestrian flow is free and uninterrupted. Pedestrian access should be provided at ground level mid-block between all streets.
- 75** Development should provide and maintain pedestrian shelter, access and through-site links in accordance with the walking routes identified within [Map Adel/1 \(Overlays 2, 2A and 3\)](#) and the provisions of the Zone or Policy Area in which it is located. Such facilities should be appropriately designed and detailed to enhance the pedestrian environment, have regard to the mobility needs of people with disabilities, and be safe, suitable and accessible.
- 76** Corner buildings in the Central Business Policy Area of the Capital City Zone, buildings adjacent to street intersections and buildings along a high concentration public transport route or along public transport pedestrian routes identified within [Map Adel/1 \(Overlay 4\)](#) should provide weather protection for pedestrians in the form of verandahs, awnings or canopies. Where verandahs or awnings are provided which block street lighting, they should include additional lighting beneath the canopy.
- 77** Permanent structures over a footpath should have a minimum clearance of 3.0 metres above the existing footpath level, except for advertisements which should have a minimum clearance of 2.5 metres and temporary structures and retractable canopies which should have a minimum clearance of 2.3 metres above the existing footpath level.
- 78** Where posts are required to support permanent structures, they should be located at least 600 millimetres from the kerb line.
- 79** Access for people with disabilities should be provided to and within all buildings to which members of the public have access in accordance with the relevant Australian Standards. Such access should be provided through the principal entrance, subject to heritage considerations and for exemptions under the relevant legislation.

## **Bicycle Access**

### **OBJECTIVES**

- Objective 64:** Greater use of bicycles for travel to and within the City and the improvement of conditions, safety and facilities for cyclists.
- Objective 65:** Adequate supply of secure, short stay and long stay bicycle parking to support desired growth in City activities.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

- 80** Development should have regard to the bicycle routes identified within [Map Adel/1 \(Overlay 3\)](#) by:
- (a) limiting vehicular access points; and
  - (b) ensuring that vehicles can enter and leave the site in a forward direction, thereby avoiding reverse manoeuvres.
- 81** An adequate supply of on-site secure bicycle parking should be provided to meet the demand generated by the development within the site area of the development. Bicycle parking should be provided in accordance with the requirements set out in [Table Adel/6](#).
- 82** Onsite secure bicycle parking facilities for residents and employees (long stay) should be:
- (a) located in a prominent place;
  - (b) located at ground floor level;
  - (c) located undercover;



- (d) located where passive surveillance is possible, or covered by CCTV;
- (e) well lit and well signed;
- (f) close to well used entrances;
- (g) accessible by cycling along a safe, well lit route;
- (h) take the form of a secure cage with locking rails inside or individual bicycle lockers; and
- (i) in the case of a cage have an access key/pass common to the building access key/pass.

**83** Onsite secure bicycle parking facilities for short stay users (i.e. bicycle rails) should be:

- (a) directly associated with the main entrance;
- (b) located at ground floor level;
- (c) located undercover;
- (d) well lit and well signed;
- (e) located where passive surveillance is possible, or covered by CCTV; and
- (f) accessible by cycling along a safe, well lit route.

**84** Access to bicycle parking should be designed to:

- (a) minimise conflict with motor vehicles and pedestrians;
- (b) ensure the route is well signed and well lit including the use of road markings such as a bicycle logo if appropriate to help guide cyclists; and
- (c) ensure the route is unhindered by low roof heights.

## **Public Transport**

### **OBJECTIVES**

**Objective 66:** Development that promotes the use of sustainable transport consistent with State Government objectives and initiatives.

**Objective 67:** Accessible public transport for all metropolitan residents and visitors and safe and attractive facilities for public transport users.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

**85** Development along a high concentration public transport route should be designed to ensure that activity and interest for public transport passengers is maximised through the incorporation of active street frontages.

**86** Development along high concentration public transport routes identified in [Map Adel/1 \(Overlay 4\)](#) should:

- (a) ensure there are pedestrian links through the site if needed to provide access to public transport;
- (b) provide shelter (e.g. verandahs) for pedestrians against wind, sun and rain;
- (c) provide interest and activity at street level; and

- (d) where possible, avoid vehicle access across high concentration public transport routes identified in [Map Adel/1 \(Overlay 4\)](#). Where unavoidable, vehicle access should be integrated into the design of the development whilst retaining active street frontages.

## **Traffic and Vehicle Access**

### **OBJECTIVES**

- Objective 68:** Development that supports a shift toward active and sustainable transport modes (i.e. public transport, cycling and walking).
- Objective 69:** An enhanced City environment and the maintenance of an appropriate hierarchy of roads to distribute traffic into the City to serve development in preference to through traffic.
- Objective 70:** Adequate off-street facilities for loading and unloading of courier, delivery and service vehicles and access for emergency vehicles.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

- 87** Development should be designed so that vehicle access points for parking, servicing or deliveries, and pedestrian access to a site, are located to minimise traffic hazards and vehicle queuing on public roads. Access should be safe, convenient and suitable for the development on the site, and should be obtained from minor streets and lanes unless otherwise stated in the provisions for the relevant Zone or Policy Area and provided residential amenity is not unreasonably affected.
- 88** Facilities for the loading and unloading of courier, delivery and service vehicles and access for emergency vehicles should be provided on-site as appropriate to the size and nature of the development. Such facilities should be screened from public view and designed, where possible, so that vehicles may enter and leave in a forward direction.
- 89** Where practicable, development sites should contain sufficient space for the location of construction equipment during the course of building construction, so that development does not rely on the use of Council road reserves to locate such equipment.
- 90** Vehicular access to development located within the Core and Primary Pedestrian Areas identified in [Map Adel/1 \(Overlay 2A\)](#) should be limited and designed to minimise interruption to street frontages.
- 91** Where vehicular access to a development is gained by an existing crossing in the Core Pedestrian Area identified in [Map Adel/1 \(Overlay 2A\)](#), there should be no increase in the number of parking spaces served by the crossing, nor any increase in the number of existing crossings serving that development.

## **Car Parking**

### **OBJECTIVES**

- Objective 71:** To meet community expectation for parking supply while supporting a shift toward active and sustainable transport modes.
- Objective 72:** An adequate supply of short-stay and long-stay parking to support desired growth in City activities without detrimental affect on traffic and pedestrian flows.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

- 92** Car parking areas should be located and designed to:
- (a) ensure safe and convenient pedestrian movement and traffic circulation through and within the car parking area;



- (b) include adequate provision for manoeuvring and individually accessible car standing areas;
  - (c) enable, where practical, vehicles to enter and leave the site in a forward direction;
  - (d) minimise interruption to the pattern of built form along street frontages;
  - (e) provide for access off minor streets and for the screening from public view of such car parking areas by buildings on the site wherever possible;
  - (f) minimise adverse impacts on adjoining residential properties in relation to noise and access and egress;
  - (g) minimise loss of existing on-street parking spaces arising through crossovers and access;
  - (h) incorporate secure bicycle parking spaces and facilitate convenient, safe and comfortable access to these spaces by cyclists; and
  - (i) provide landscaping, such as semi-mature trees, to shade parked vehicles and reduce the visual impact of the car parking area while maintaining direct sight lines and informal visual surveillance.
- 93** All development should provide car parking spaces for people with disabilities in accordance with the requirements in the Building Code of Australia (BCA). For classes of buildings not covered by the requirements of the BCA, the number of spaces should be provided in accordance with [Table Adel/7](#) and such car parking spaces should comply with Australian Standard 2890.1: 'Parking Facilities - Off-street Car Parking'.
- 94** Off-street parking should:
- (a) be controlled in accordance with the provisions for the relevant Policy Area;
  - (b) be located away from street frontages or designed as an integral part of buildings on the site. Provision of parking at basement level is encouraged; and
  - (c) not include separate garages or carports in front of buildings within front set-backs.
- 95** Off-street parking in the Core Pedestrian Area identified in [Map Adel/1 \(Overlay 2A\)](#) will only be appropriate where:
- (a) parking is ancillary to another activity carried out on the land;
  - (b) it can be provided without loss of pedestrian amenity; and
  - (c) it is not separately created on a strata title or community title basis (unless in association with another title held on the site).
- 96** Multi-level car parks and short stay public use of ancillary car parking spaces are discouraged at ground floor street frontages in the Primary Pedestrian Area identified in [Map Adel/1 \(Overlays 2, 2A and 3\)](#). Multi-level car parks, short stay public use of ancillary car parking spaces or non-ancillary car parking use of an existing building may be appropriate where it:
- (a) is located away from ground floor street frontages to major streets;
  - (b) ensures vehicle access is from the road with less pedestrian activity in instances where a site has access to more than one road frontage;
  - (c) has no more than one entry lane and one exit lane;
  - (d) has a controlled exit at the property boundary to stop vehicles before travelling across the footpath;

- (e) has no more than one left in and one left out access point;
  - (f) avoids access points along high concentration public transport routes identified in [Map Adel/1 \(Overlay 4\)](#); and
  - (g) with respect to ancillary parking, is provided at basement level, or undercroft if located behind other uses which provide activity on the street frontage.
- 97** In areas outside the Core and Primary Pedestrian Areas identified in [Map Adel/1 \(Overlays 2, 2A and 3\)](#), car parking may be provided to serve a development within the site of the development or elsewhere. Where car parking is provided, it should be:
- (a) provided with vehicle access points that do not cross major walking routes identified in [Map Adel/1 \(Overlay 2\)](#); and
  - (b) located away from frontages to major streets wherever possible.

## **Economic Growth and Land Use**

### **OBJECTIVES**

**Objective 73:** The role of the City enhanced as:

- (a) the community, civic and cultural heart of South Australia and as a driving force in the prosperity of the State;
- (b) the State centre for business, administration, services, employment, education, political and cultural activities, government and public administration;
- (c) a welcoming, secure, attractive and accessible meeting place for the people of metropolitan Adelaide and beyond for leisure, entertainment, civic and cultural activity, specialty shopping, personal and community services;
- (f) the gateway to the attractions of South Australia for international and interstate visitors by developing a wide range of visitor accommodation, facilities and attractions, particularly attractions which showcase the particular strengths of South Australia; and
- (g) a great place to live, with a growing diversity of accommodation for different incomes and lifestyles.

**Objective 74:** A business environment which encourages investment from domestic and foreign sources, business development and employment.

**Objective 75:** Development which reinforces clusters and nodes of activity and distinctive local character.

**Objective 76:** A diverse mix of commercial, community, civic and residential activities to meet the future needs of the Capital City of South Australia.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

**98** Development, particularly within the Capital City and Institutional Zones, is encouraged to:

- (a) provide a range of shopping facilities in locations that are readily accessible;
- (b) provide for the growth in economic activities that sustain and enhance the variety and mix of land uses and the character and function of the City;
- (c) maximise opportunities for co-location, multiple use and sharing of facilities;



- (d) be accessible to all modes of transport (particularly public transport) and safe pedestrian and cycling routes; and
  - (e) have minimal impact on the amenity of residential areas.
- 99** Development is encouraged to develop and expand upon the existing or create new tourism activities to maximise employment and the long-term economic, social and cultural benefits of developing the City as a competitive domestic and international tourist destination.
- 100** Tourist facilities should be compatible with the prevailing character of the area, within close proximity to public transport facilities and well designed and sited.
- 101** Development located either abutting, straddling or within 20 metres of a Zone or Policy Area boundary should provide for a transition and reasonable gradation from the character desired from one to the other.
- 102** Development should not unreasonably restrict the development potential of adjacent sites, and should have regard to possible future impacts such as loss of daylight/sunlight access, privacy and outlook.