

## Barrio Developments

Demolition of existing building, and construction of a seven level residential flat building, comprising 20 apartments and 38 tourist accommodation rooms and ancillary car parking and landscaping.

### 2 Canning Street, Glenelg North

DA 110/M004/19

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## OVERVIEW

<b>Application No</b>	DA 110/M004/19
<b>Unique ID/KNET ID</b>	2019/03907/01
<b>Applicant</b>	Barrio Developments
<b>Proposal</b>	Demolition of existing building, and construction of a seven level residential flat building, comprising 20 apartments and 38 tourist accommodation rooms and ancillary car parking and landscaping
<b>Subject Land</b>	2 Canning Street, Glenelg North
<b>Zone/Policy Area</b>	Residential High Density Zone / Urban Glenelg Policy Area 15 / Precinct 4 Five Storey
<b>Relevant Authority</b>	State Commission Assessment Panel
<b>Lodgement Date</b>	12 June 2019
<b>Council</b>	City of Holdfast Bay
<b>Development Plan</b>	Holdfast Bay Council, consolidated 2 June 2016
<b>Type of Development</b>	Merit
<b>Public Notification</b>	Category 2
<b>Representations</b>	13 representors; 11 wish to be heard
<b>Referral Agencies</b>	Government Architect City of Holdfast Bay (technical comments)
<b>Report Author</b>	Will Gormly, Senior Planning Officer
<b>RECOMMENDATION</b>	Development Plan Consent, subject to conditions

## EXECUTIVE SUMMARY

This application was lodged on 12 June 2019, following the undertaking of the Case Management Service. The application proposes the demolition of the existing built form on the land, and the construction of a mixed-use seven level residential flat building which incorporates tourist accommodation and private residences, at 2 Canning Street, Glenelg North.

The subject land is located within the Residential High Density Zone and the Urban Glenelg Policy Area 15 and Precinct 4 Five Storey where development, including residential flat buildings serviced apartments and tourist accommodation is envisaged - of up to 5 storeys (or 18.5 metres) in height. The proposal is consistent with the desired land uses for the area, however at seven storeys exceeds the number of levels envisaged for the Precinct. The overall height of 22.9 metres (to the top of the roof) exceeds the desired height by 4.4 metres.

The application was subject to the pre-lodgement process, as described in Section 1.1 below. Through the process, there was evolution between the inception meeting, and the lodged (then amended) application. The Government Architect was consulted once during this period, at Design Review. The comments of the Government Architect are discussed in Section 5.1 of this report.

The application is subject to a mandatory referral to the Government Architect and a non-mandatory referral to the City of Holdfast Bay Council for their technical comments. The Government Architect recognises and acknowledges aspects of the proposal, however maintains concern with a number of elements – particularly around height, mass, scale, overshadowing, and privacy aspects. Council do not oppose the development, however have requested detail relating to the operation of the car parking arrangement.

The proposed development is considered, on fine balance, to satisfy the majority of Development Plan policies which relate to it. These policies are included as an attachment



to this report. Accordingly, the proposal has merit to warrant Development Plan Consent, subject to planning conditions as recommended at the end of this report.

## **ASSESSMENT REPORT**

### **1. BACKGROUND**

#### **1.1 Case-management/pre-lodgement service**

The applicant engaged with the case managed pre-lodgement service offered by the Department of Planning, Transport and Infrastructure. The process saw one Design Review, and three pre-lodgement panel meetings.

The concept and design iteration has evolved considerably since the project inception, to the documentation for assessment.

#### **1.2 Amended Documentation**

Following public notification, as a Category 2 form of development, the applicant elected to amend the design of the proposed building.

The main changes include a height reduction, comprising a reduced floor-to-ceiling height of level 1 and 2 by 150mm, reducing the overall height of the car stacker by 600mm, and setting down the car park floor level by 200mm. Further change include the introduction of an 800mm setback to Laycock Lane at ground level, and the reduction of wall height from 23.85 metres to 18.25 metres by way of setting the top level within a mansard roof form.

This report considers the final documentation provided. Comments by the Government Architect and Council include those which related to the originally submitted plans (with subsequent commentary included which relate to the amended documentation). It is noted that the amended application was not renotified, as it was considered that the changes made were not considered to significantly impact (or indeed worsen any previously proposed impacts) on the adjoining properties.

### **2. DESCRIPTION OF PROPOSAL**

The application is for the demolition of the existing single-storey detached dwelling and associated structures on the land, and the construction of a seven level residential flat building, which is proposed to contain thirty-eight tourist accommodation rooms, and twenty private apartments. The private apartments provide a mix of one, two and three bedroom typologies. The proposal includes on-site parking for 40 vehicles.

<b>Land Use Description</b>	Demolition of existing building, and construction of a seven level residential flat building, comprising 20 apartments and 38 tourist accommodation rooms and ancillary car parking and landscaping	
<b>Building Height</b>	7 storeys (22.9 metres to roof)	
<b>Description of levels</b>	Basement	Pit for car stackers only
	Ground	Car parking, waste area, storage area, laundry room, tourist accommodation lobby, apartment lobby, bicycle parking, scooter parking
	First	Tourist accommodation – 18 rooms + 1 DDA room; six with accessible terraces plus one common terrace area

	Second	Tourist accommodation – 18 rooms + 1 DDA room; four with accessible terraces plus one common terrace area
	Third	Private apartments – 3 two-bedroom apartments and 2 three-bedroom apartments
	Fourth	Private apartments – 3 two-bedroom apartments and 2 three-bedroom apartments
	Fifth	Private apartments – 3 two-bedroom apartments and 2 three-bedroom apartments
	Sixth	Private apartments – 3 two-bedroom apartments and 2 one-bedroom apartments
	Roof	Solar panel arrays, air conditioning condenser units
<b>Apartment floor area (excluding balconies)</b>	Tourist studios	Ranges from 16 to 28 square metres (DDA studio 31 square metres)
	1 bedroom	56 and 58 square metres
	2 bedroom	62, 65, 75, 88, 106, and 108 square metres
	3 bedroom	125 square metres
<b>Site Access</b>	Pedestrian access from Canning Street Vehicle access from Laycock Lane	
<b>Car and Bicycle Parking</b>	On site bicycle parking for 14 bicycles On site vehicle parking for 40 vehicles On site scooter parking area provided	
<b>Encroachments</b>	Nil	
<b>Staging</b>	Staging not proposed	

### 3. SITE AND LOCALITY

#### 3.1 Site Description

The site comprises one allotment, and is described legally as:

Lot No	Section	Street	Suburb	Hundred	Title
1	D57894	Canning Street	Glenelg North	Noarlunga	CT 5857/966

The subject site is located at the western side of the intersection of Canning Street and Laycock lane. It has a frontage to Canning Street, its northern boundary, of approximately 28 metres, and approximately 27 metres to its eastern boundary to Laycock Lane. The irregular shaped allotment has a resultant site area of approximately 784 square metres.

The site currently contains a single storey detached dwelling set amongst a generous garden – typical of the low-scale development in the locality.

The site is generally flat, and features no notable vegetation.

#### 3.2 Locality

The locality is characterised by varying land uses, however is predominantly residential. These residences are generally in the form of detached dwellings, however semi-detached dwellings of a more recent construction exist in the locality. There are hotel land uses in close proximity to the site.

Directly adjacent the site is the twelve storey 'Aquarius' Tower, to the west. This tower has its address to Adelphi Tower, and is located on the corner of Canning Street.

All development in the immediate locality, with the exception of the Aquarius Tower, is of one and two storeys in height.

Canning Street is a two-lane, two-direction street. It does not have a dedicated bicycle path. It features footpaths on both sides of the street. Given its nature of serving low-scale and low-density residential development, it does not carry a high volume of traffic, in comparison with Adelphi Terrace which feeds from the Patawalonga Frontage through to Anzac Highway.



**Figure 1 – Location Map**

#### **4. COUNCIL COMMENTS or TECHNICAL ADVICE**

##### **4.1 City of Holdfast Bay**

The City of Holdfast Bay council were referred, as a non-mandatory referral, seeking technical comments.

The council provided a response, which considers local impact, public realm improvements, stormwater, waste removal, and traffic management.

The council note the proposal is at variance with the Desired Character of the Precinct, which seeks buildings up to 5 storeys or 18.5 metres in height. The proposed building has a wall height of 21.25 metres, and an overall roof height of 25.65 metres AHD. They note that the Development Plan does not offer any over-height dispensation, unlike other parts of the Zone which allow transition to adjacent taller buildings. The council acknowledge that the site shares a boundary with a Precinct which allows 12 storey (and 43 metre) buildings, with an existing neighbouring building standing at 12 storeys. Whilst they acknowledge the context may be appropriate to allow taller

buildings, they see that the eastern part of the proposal building should be lowered to 5 storeys to assist with this transition.

The council note that the development will have significant impact on the adjoining southern property through visual, shading, and overlooking impacts.

The council acknowledge on-site parking compliance with Development Plan requirements, however request a condition that refuse collection is undertaken by a private contractor. The council are not satisfied of the refuse collection or how the tourist parking arrangements are to occur.

The council advise that the stormwater disposal system should cater for a 10 year rainfall event, and that post development stormwater flows should not exceed pre-development flows (or 10 litres per second with any excess to be detained on site).

Council request advisory notes relating to the compliance of Health legislation be applied if a cooling tower is proposed, and that the obsolete Canning Street crossover be closed.

I concur with Council's request for condition, and advisory notes.

The referral response from the City of Holdfast Bay is contained in the attachments.

## 5. STATUTORY REFERRAL BODY COMMENTS

The Government Architect is the only mandatory referral body for this application, pursuant to Schedule 8 of the Development Regulations 2008. The SCAP must have regard to the advice of the Government Architect.

The referral response from the Government Architect is contained as an attachment to this report.

### 5.1 Government Architect

The Government Architect was referred the original submitted plans at the time of lodgement, and then asked to provide additional comments based on the amended plans following public notification.

In the referral response, the Government Architect (GA) acknowledges and supports the raised lobbies which are in response to Council stormwater requirements, the 800mm setback of the Laycock Lane frontage at ground level, the additional privacy measures to tourist accommodation levels, and the increase in depth of balcony to the west at level one and two.

The Government Architect remains concerned by:

- Full height walls to the west and south elevations in Brightonlite concrete.
- The mansard style roof, which she considers emphasises the upper level element of the over-height building, which is not recessive and does not minimise visual impact.

The Government Architect recommends further consideration of:

- Screening strategy to the balconies of the southern tourist accommodation level
- Height, mass, and setbacks do not respect the existing residential context, and the management of interfaces with neighbouring buildings in particular the private open space of the existing residence to the immediate south
- Screening of roof-top plant areas be shown on elevations and sections

- Demonstration of effective solar shading to the north elevations for levels one and two, central corridor spaces to levels one, and full height glazing sections to the west elevation, to ensure delivery of the ESD intent for each elevation and all levels of the development
- Apartment floor configuration, with a view to reduce the building footprint, mass and bulk, and address interface issues
- Exploration and incorporation of ESD principles
- High quality of materials supported by a materials sample board.

A copy of the referral response, and the additional comments, are included as an attachment to this report.

## **6. PUBLIC NOTIFICATION**

The application was notified as a Category 2 development pursuant to the Procedural Matters of the Holdfast Bay Council Development Plan, where development with an overall height exceeding 11.5 metres (excluding gables) measured from the natural ground level is prescribed Category 2. Public notification was undertaken by writing to adjoining owners and occupiers of the land and 12 valid representations were received.

These concerns raised by each of the representors are summarised in the table below.

<b>Map ID</b>	<b>Concerns</b>
R1	<ul style="list-style-type: none"> <li>• Over height</li> <li>• Car park undersupply</li> <li>• Overlooking</li> </ul>
R2	<ul style="list-style-type: none"> <li>• Overshadowing</li> <li>• Building height</li> </ul>
R3	<ul style="list-style-type: none"> <li>• Traffic congestion</li> </ul>
R4	<ul style="list-style-type: none"> <li>• Parking, over-development</li> </ul>
R5	<ul style="list-style-type: none"> <li>• Vehicle access and frequency of vehicle movements</li> <li>• Visitor parking arrangement</li> <li>• Deliveries and loading</li> <li>• Overshadowing</li> <li>• Overlooking</li> <li>• Over-height</li> </ul>
R6	<ul style="list-style-type: none"> <li>• Access to property</li> <li>• Noise</li> <li>• Parking</li> <li>• Loss of natural light</li> <li>• Taxi/Uber access</li> <li>• Waste disposal</li> </ul>
R7	<ul style="list-style-type: none"> <li>• Noise</li> <li>• Traffic</li> <li>• Waste disposal</li> <li>• Size of development</li> </ul>
R8	<ul style="list-style-type: none"> <li>• Over-height</li> <li>• Density</li> <li>• Car parking numbers</li> <li>• Car parking access/movements</li> <li>• Disabled car parking provision</li> </ul>
R9	<ul style="list-style-type: none"> <li>• Building height</li> <li>• Pedestrian and vehicle movements</li> <li>• Overshadowing</li> <li>• Loss of privacy and amenity</li> <li>• Waste management</li> </ul>
R10	<ul style="list-style-type: none"> <li>• Traffic increase</li> <li>• Overshadowing</li> </ul>



Map ID	Concerns
R11	<ul style="list-style-type: none"> <li>• Loss of privacy</li> <li>• Over-height</li> <li>• Infrastructure impacts</li> <li>• Parking</li> <li>• Traffic</li> <li>• Noise</li> <li>• Overlooking</li> </ul>
R12	<ul style="list-style-type: none"> <li>• Height and scale</li> <li>• Insufficient car parking</li> <li>• Significant overshadowing</li> <li>• Overlooking</li> <li>• Setbacks</li> <li>• Noise</li> <li>• Solar panel glare</li> </ul>

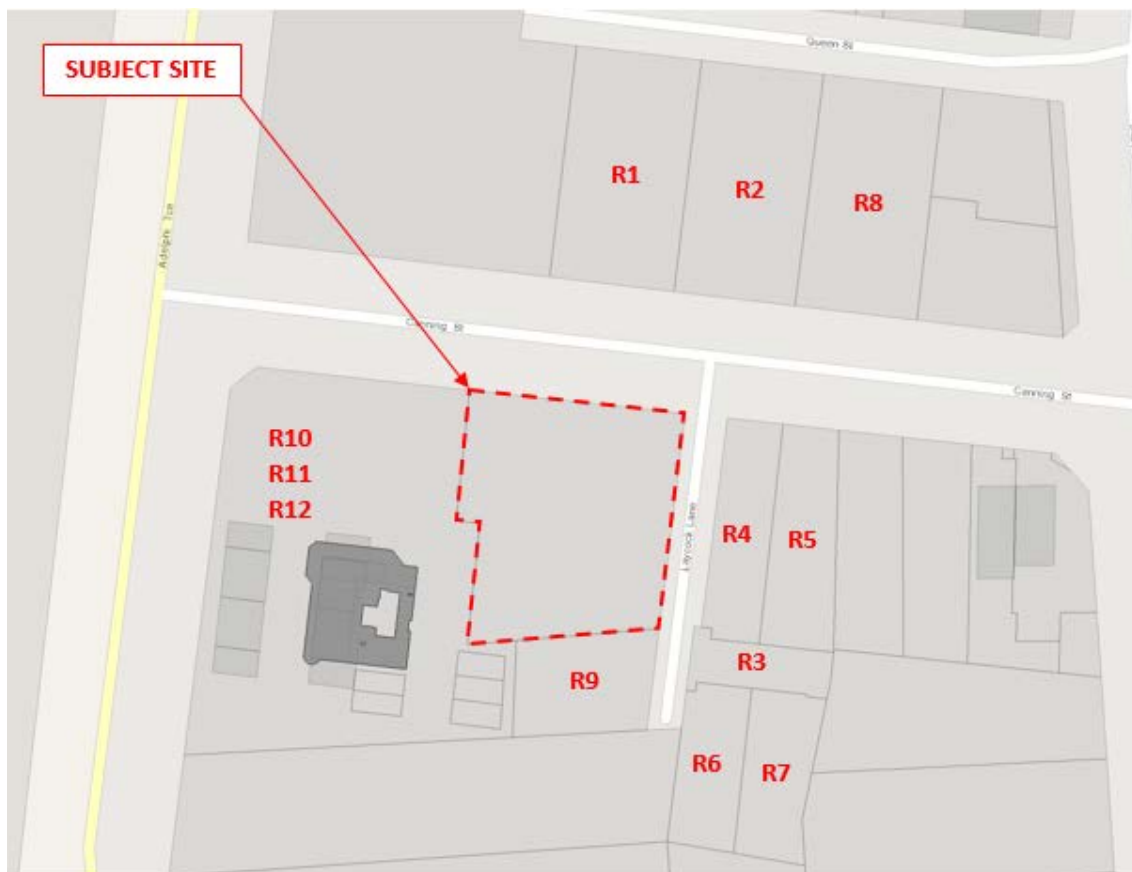


Figure 2 – Location Map

A response to representations was received by the applicant on 11 September 2019. The response responds only to building height, density, overlooking, overshadowing, and access and car parking. It does not respond to all of the aspects raised by representors.

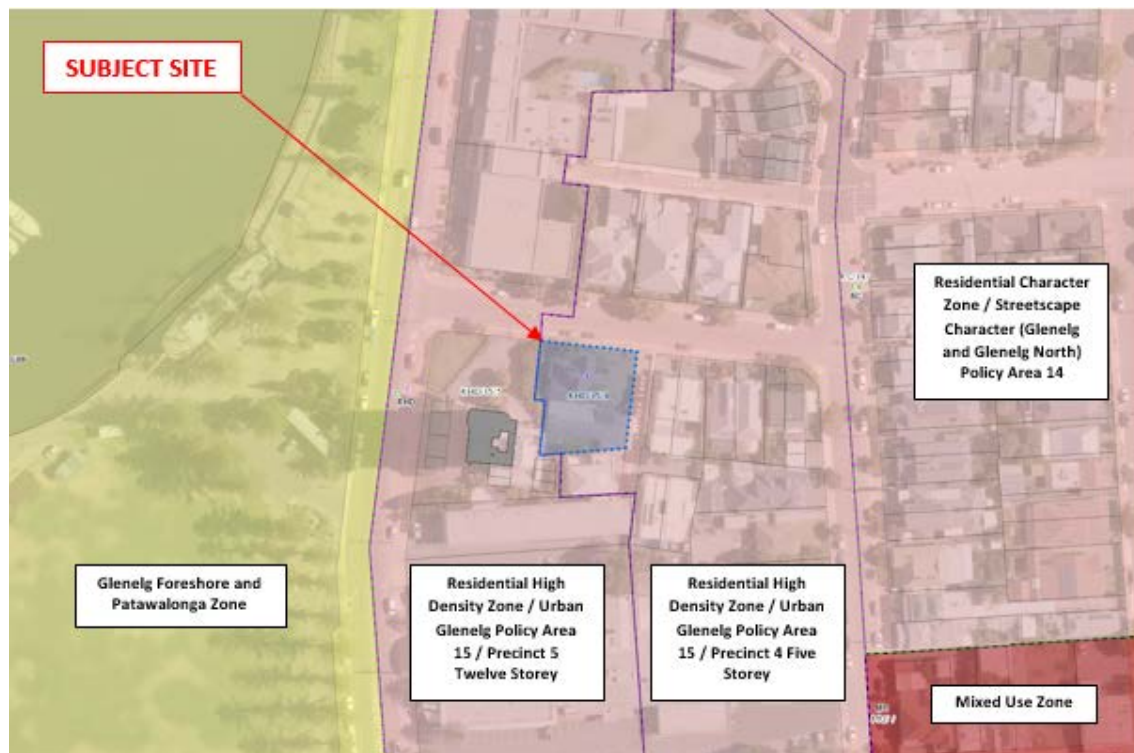
Of the twelve representors, eleven wish to be heard by the State Commission Assessment Panel. As previously mentioned the application was not renotified in response to the amended plans.

A copy of each representation and the applicant's response to representations is contained in the attachments of this report.

## **7. POLICY OVERVIEW**

The subject site is within the Residential High Density Zone and the Urban Glenelg Policy Area 15 and Precinct 4 Five Storey as described within the Holdfast Bay Council Development Plan Consolidated 2 June 2016.

Relevant planning policies are contained in the appendices attached to this report, and summarised in the headings of this section below.



**Figure 3 – Zoning Map**

### **7.1 Residential High Density Zone**

The Residential High Density Zone is a residential zone comprising a range of high density dwellings, including a minimum of 15 per cent affordable housing, primarily in the form of row dwellings and residential flat buildings, designed to integrate with areas of open space, neighbouring centres or public transport nodes.

Development in the zone will support the viability of community services and infrastructure.

Ranges of envisaged land uses in the zone include affordable housing; residential flat building; row dwelling; small scale non-residential use that serve the local community for example child care facility, open space, recreation area, shop, office or consulting room; and supported accommodation.

## 7.2 Urban Glenelg Policy Area 15

The policy area provides the Council's premier coastal medium and high density living opportunities. It includes areas of Glenelg North around the foreshore and the Patawalonga, and within Glenelg and Glenelg South along the foreshore and extending into small parts of the suburban landscape, and along Colley Terrace.

The Glenelg District Centre and Glenelg's major foreshore reserves provide important features adjacent to the policy area (including Wigley and Colley Reserves, the beach and Glenelg foreshore and the Anzac Highway streetscape) and entertainment, retail, residential and visitor apartment accommodation provided nearby in the Holdfast Shores site to the western side of Colley and Wigley Reserve.

The policy area is a premier location with excellent accessibility to views, beach, public spaces, centre services, facilities and public transport. The policy area adds to the choice of accommodation within Holdfast Bay and the wider metropolitan area by providing for a variety of medium and higher density dwelling types, including apartments for residential purposes and visitor accommodation.

Development will be of the highest architectural standard, contemporary in style and contribute positively to the quality of the public realm. Its built form will contrast with the open character of the adjacent foreshore and reserve public spaces. It will capitalise on the highly desirable location through significant scale, with built form between three and twelve stories in height. This development will demonstrate excellence in urban design. It will create design relationships between buildings at ground level and the street frontage that acknowledge and respect the existing context, ensuring that scale and the built form edge protects and enhances significant visual and movement corridors (including key vistas to the sea and views through to public spaces). Views into and out of development sites will also reinforce visual connectivity and way-finding within the policy area.

Building form and setbacks will vary to provide large-scale articulation within the streetscape. Building form will also use light and shade through articulation, eaves, verandas, canopies and balconies, to provide architectural detail, summer shade and promote greater energy efficiency. Likewise, buildings will use a balanced approach to the use of solid materials and glazing so to provide an attractive backdrop to key public spaces and streets.

The policy area is well provisioned with quality public open spaces and accessible by public transport (in the form of buses and tram). Accordingly, there is a recognised reduced need for provision of private car parking and private open space (when compared to suburban localities in other zones and policy areas). Similarly, a higher degree of overshadowing and loss of privacy is expected in the policy area given the medium-to-high density nature of development (and heights).

Roofs will be designed to be integrated into the overall façade and composition of buildings and provide enclosed places for the screening of plant and service equipment (if not provided in basements) in locations away from living areas that do not visually detract from the amenity of adjoining spaces.

Landscaping will contribute to the high quality of the adjacent public areas, open space and streetscapes. Car parking areas that are not visible from public spaces will be shared and consolidated.

### **7.3 Precinct 4 Five Storey**

Development within Precinct 4 Five Storey will be predominantly in the form of residential flat buildings, serviced apartments and tourist accommodation of up to 5 storeys (or 18.5 metres) in height. Development may also include small scale non-residential uses such shops, restaurants and cafes, offices and consulting rooms at ground and first levels where site conditions permit.

Development will be of the highest architectural standard and contribute positively to the public realm through establishing clearly defined space between buildings on adjoining sites and building design that incorporates articulated facades and built form elements including balconies to create light and shadow. Building design will complement the scale, proportions, siting and materials of the existing heritage places in the locality.

Development on land fronting the South Esplanade immediately adjacent Precinct 5 Twelve Storey may accommodate additional building height over 5 storeys to achieve a transition in scale from the taller building anticipated in Precinct 5, down to the 5 storey scale anticipated in Precinct 4, provided buildings are designed to minimise any impacts on adjoining land within Precinct 4 or adjoining residential zones.

Development directly adjoining Sturt Street should not exceed 2 storeys in height to order to achieve a transition down to low scale at the interface with the adjoining Residential Character Zone.

### **7.4 Council Wide**

Council Wide provisions provide general and over-arching guidance to medium and high rise development (3 or more storeys), Crime Prevention Through Urban Design, energy efficiency, built form including height, bulk and scale, overshadowing, visual privacy, composition and proportion, materials, colours and finishes, active street frontages, and access and movement.

The policies which have guided this assessment are included in the attachments of this report.

### **7.5 Overlays**

#### **7.5.1 Affordable Housing**

The proposal is subject to the affordable housing overlay.

The application does not propose any affordable housing.

## **8. PLANNING ASSESSMENT**

The application has been assessed against the relevant provisions of the Holdfast Bay Council Development Plan with a consolidation date of 2 June 2016. These provisions are contained in the appendices of this report.

## 8.1 Quantitative Provisions

	Development Plan Guideline	Proposed	Guideline Achieved	Comment
<b>Building Height</b>	5 storeys (18.5 metre wall height)	7 storeys (18.25 metre wall height)	YES <input type="checkbox"/> NO <input type="checkbox"/> PARTIAL <input checked="" type="checkbox"/>	2 storeys over height (however achieves wall height provision)
<b>Land Use</b>	Residential flat building	Residential flat building and tourist accommodation	YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> PARTIAL <input type="checkbox"/>	Visitor accommodation listed in Desired Character of Policy Area
<b>Car Parking</b>	Studio/1/2 bed: 1 per dwelling plus 0.25 visitor 3/+ bed: 1.25 per dwelling plus 0.25 visitor Tourist: 1 per 4 bedrooms min, 1 per 2 bedrooms max.  Required: 22 residential, 10 visitor, and 6 visitor spaces. (38 combined)	40 car spaces combined	YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> PARTIAL <input type="checkbox"/>	
<b>Bicycle Parking</b>	No requirements for on-site bicycle parking	14 bicycles	YES <input type="checkbox"/> NO <input type="checkbox"/> N/A <input checked="" type="checkbox"/>	
<b>Private Open Space</b>	1 bedroom: 8 square metres 2 bedroom: 11 square metres 3/+ bedroom: 15 square metres	1 bedroom: 8 square metres 2 bedroom: 11 to 17 square metres 3: 17 square metres	YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> PARTIAL <input type="checkbox"/>	
<b>Affordable Housing</b>	15 percent	Zero	YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> PARTIAL <input type="checkbox"/>	

## 8.2 Land Use and Character

The Zone makes particular provision for residential redevelopment of a higher density, which includes being in the form of residential flat buildings. Residential flat buildings are further recognised as envisaged in the Policy Area, and also the Precinct.

Whilst tourist accommodation is not explicitly listed as an envisaged form of land use by Principle of Development Control, it is acknowledged in the Development Plan that the Policy Area will provide for a choice of accommodation in Holdfast Bay which lists visitor accommodation as one of these. Furthermore, the Desired Character of the Precinct lists tourist accommodation as a form that will feature in this Precinct.

Accordingly, the mix of residential flat and visitor accommodation are both suitable, and satisfy the Development Plan requirements of the land uses proposed.



### 8.3 Building Height

The Desired Character for the Policy Area envisages: *Roofs will be designed to be integrated into the overall façade and composition of buildings and provide enclosed places for the screening of plant and service equipment.*

Principle of Development Control 19 of the Policy Area provides Precinct-specific provisions, where it seeks development should not exceed an external wall height of 18.5 metres above natural ground level, which explicitly excludes lift service levels and 'gables' from this building height.

The proposed development has a wall height of 18.25 metres.

The architectural response is such that the built form will extend beyond this 18.25 metre wall height, however in the form of a mansard (rather than a 'gable', as the policy seeks or allows for); which acts as its roof form. Within this mansard houses one building level, which results in a seven storey building. It is acknowledged that the Precinct calls for buildings being a maximum height of five storeys, however provides a numeric value for this height – which would be considered a more appropriate measure of a building 'height'. This value is, in Precinct 4, stated at an 18.5 metre wall height (emphasis underlined). It should be noted here that the Desired Character of the Policy Area seeks the maximum building height of 18.5 metres – it does not specify that this to be an overall height or a wall height. It is accepted that the policy at the finest grain (in this case, the Precinct), should take precedence.

Whilst the proposed roof form is not strictly a 'gable' (with plain, triangular ends), the angled form of the mansard is considered such that it is the roof of the building; particularly as the mansard is angled as a roof would be, and has slopes on all sides of the building – where a gable would only have two.

It is considered that the mansard section of roof satisfies the 'gable' intent of the policy, and that the proposal satisfies the maximum building height, in that beyond the 18.25 metre point, the form of the building becomes a roof, rather than a wall, as a mansard.

Notwithstanding the proposed building satisfies the intent of the building height measure of the Development Plan, the Government Architect maintains her position of concern with regard to the building height.

### 8.4 Design and Appearance

The Precinct calls for development of the highest architectural standard, which contributes positively to the public realm through establishing clearly defined space between buildings on adjoining sites and building design that incorporates articulated facades and built form elements including balconies to create light and shadow.

The proposed building presents as a contemporary, architecturally designed building, and incorporates high quality materials including recycled sandstone, recycled timber, standing seam cladding, bronze metal, and solid brick. The building reads essentially in four segments – its ground level, with its spaces set back from the Canning Street edge; the two tourist accommodation levels which sit above this as a mostly brick-clad element; the three residential apartment levels above this with a mix of solid and glazed elements set behind bronze balustrade, and the dark coloured standing seam mansard element which contains a portion of the sixth level and all of the seventh level.

The proposal is considered to offer a fair degree of articulation, however this is generally restrained to the residential apartment levels where the building form is able to be articulated through the positioning of the balconies generally at the 'corners' of the building on its northern elevation.

In the Policy Area, roofs will be designed to be integrated into the overall façade and composition of buildings and provide enclosed places for screening of plant and service equipment in locations away from living areas that do not visually detract from the amenity of adjoining spaces. The proposed development incorporates the roof as a mansard, and further locates its plant on the rooftop, which is not accessible to occupants of the building. It is considered that this policy is satisfied.

Contextually, the positioning of the building, whilst not offering a great degree of setbacks on its own site, affords defined space between it and the Aquarius Tower, owing only to the generous setback of this existing tower. The building to the south, a two-storey dwelling, will be compromised by the proposed building; particularly with overshadowing and overlooking impacts. The applicant has addressed these, which are discussed further in this report.

The building offers very little in the way of setbacks to its Canning Street and Laycock Lane frontages, and will present as an imposing form at this corner. This is at odds with the Desired Character of the Policy Area which seeks that 'building form and setbacks will vary to provide large-scale articulation within the streetscape.' It is considered that the proposed setbacks do not satisfy this policy position, where the minor setbacks offered are not considered large-scale.

#### **8.4.1 Overlooking**

The Policy Area anticipates overlooking, where it states 'a higher degree of overshadowing and loss of privacy is expected in the Policy Area given the medium-to-high density nature of development (and heights)'.

Whilst general best-practice approach to direct overlooking should be an aspect of any development, the Development Plan provides little guidance to the way in which overlooking should be controlled in this location.

Further solidifying the position on overlooking, PDC3 of the Policy Area states that balconies should make a positive contribution to the internal and external amenity of buildings and should contribute to the safety and liveliness of the street by facilitating casual overlooking of public spaces.

It would be a difficult architectural outcome to introduce the overlooking of a public space, but prevent the overlooking of a private space; particularly where these two spaces may adjoin.

The applicant has provided justification of the overlooking impacts, which describes the provisions of the Development Plan that anticipate privacy loss, and the way in which the building will perform to mitigate any adverse or unnecessary overlooking, where angled louvre screens are proposed on the south of levels 1 and 2 to a height of 1.5 metres.

It is considered that the overlooking mitigation measures are appropriate, and that the building has been designed with Development Plan policy as its guidance.

#### **8.4.2 Overshadowing**

Similarly with overlooking, the Policy Area anticipates overshadowing, where it states 'a higher degree of overshadowing and loss of privacy is expected in the Policy Area given the medium-to-high density nature of development (and heights)'.

The applicant has provided overshadowing diagrams, which demonstrate impacts at 9am, midday, and 3pm during winter solstice. Principle of Development Control 5 of the Policy Area seeks that building design should minimise the impact of overlooking and overshadowing on existing lower density and scale development in adjoining zones/policy areas/precincts. It does not give policy direction on development within the same zone/policy area/precinct. It is considered, generally, that the overshadowing impacts are considered appropriate, particularly when the existing development in the locality – namely the Aquarius Tower – will shade the dwellings to the south and east of the subject site given its height.

The subject site would have, if it met policy, a building at no greater than 18.5 metres. If this were the case, the overshadowing impacts are said to be very similar to that of the proposed building; particularly on the adjoining site at 1 Laycock Lane.

### **8.4.3 Occupant Amenity**

Objective 2 of Medium and High Rise Development (3 or More Storeys) provisions of the General Section of the Development Plan seeks that residential development provides a high standard of amenity and adaptability for a variety of accommodation and living needs.

The proposed building has two levels of tourist accommodation rooms. Each of these rooms (19 per level) are arranged in a 'studio' style, and feature a double bed. Each room has direct access to light and ventilation, with 6 of the rooms having a terrace/balcony on level 1, and 4 rooms on level 2.

Levels 3, 4 and 5 each feature two 3-bedroom apartments and three 2-bedroom apartments. The northern apartments each feature a balcony that wraps around its front and side elevations. The remaining apartments, to the south, each have balconies that are all accessible directly from a living area.

Whilst the Development Plan is generally silent on this section, it is considered that the proposal will offer good amenity to its occupants. Large internal spaces; balconies with outlook and views from each apartment; vehicle parking spaces for each apartment; separate communal ground level areas for tourist accommodation occupants and residential apartment occupants; and a mix of dwelling types within the building are all positive aspects.

## **8.5 Traffic Impact, Access and Parking**

### **8.5.1 Site Access**

Vehicle access to the building is proposed via a crossover at the eastern edge of the site from Laycock Lane. This crossover facilitates the movements of private vehicles, and the collection of waste from the waste room adjacent the car park entry.

Pedestrian access to the building is from its main street frontage – off Canning Street. Two ramps will connect the Canning Street footpath to the tourist lobby and the apartment lobby respectively.

### **8.5.2 Vehicle Parking**

Table HoB/1B (Off street parking requirements for the Residential High Density Zone and for residential uses in the District Centre Zone Glenelg Policy Area 2) provides guidance for vehicle parking rates.

Residential flat buildings should provide 1 space per dwelling for studio, 1 or 2 bedroom dwellings, plus 0.25 visitor spaces. Dwellings with 3 or more bedrooms should provide 1.25 spaces per dwelling plus 0.25 visitor spaces.

Tourist accommodation should provide 1 space for every 4 bedrooms up to 100 bedrooms as a minimum, and a maximum of 1 space for every 2 bedrooms up to 100 bedrooms.

The proposed development should provide 22 residential, 10 visitor, and 6 visitor spaces (38 combined). The application proposes a total of 40 spaces, in the form of a car stacker. Given the stacking arrangement, it does not specify which of these spaces are for the use of visitors, and those for the use of residents.

The proposal satisfies the car parking requirements of the Development Plan.

### **8.5.3 Bicycle Parking**

The Development Plan gives no guidance for bicycle parking requirements.

14 bicycle parking spaces are provided on the site. These are located externally of the building, and are in the form of loops to which a bicycle is locked against.

The traffic report prepared by Infraplan notes that residents are likely to store their bicycles in their apartments, and that the lift is of a size which can facilitate this.

I am satisfied that the bicycle parking arrangement is suitable for the proposed development.

### **8.5.4 Visitor Parking**

As described above, the car stacker does not designate dedicated visitor parking spaces. It is therefore expected that residents or tourist users of the car parking will do so at their nominated rate, to allow for the required visitor car parking spaces to be available (and not taken up by residents or tourist accommodation).

Functionally, the Infraplan traffic report notes that visitors will utilise the car stacking system with the assistance of residents of the building. The report notes that visitors wishing to park in the car stacker will contact the resident via telecom system who will assist them to park, until they are familiar with the system. The report notes that a temporary visitor stopping area is marked in the aisle of the car park, where the visitor will wait until the resident assists.

The plans do not indicate where such a waiting area exists for these visitors, and it is therefore expected that either the car stacker will be ready at their arrival, or the visitor will need to wait in Laycock Lane or Canning Street – or informally in the aisle as commanded by the person who they are visiting.

It has not been demonstrated that the proposed visitor parking is sufficient, given the lack of 'waiting bay' line-marking proposed. With no dedicated 'waiting bay', there are likely impacts of a waiting vehicle being in the area where another vehicle may be entering/exiting. In addition, it is not clear whether the car stacker can be allocated with visitor-only spaces.

Council do not raise any objection to the car parking arrangement, however did note that further information regarding peak tourist numbers and management be provided to further consider the parking impacts on Canning Street. The

applicant has not provided this. A condition has been recommended that will require the applicant to provide detail to this end. This shall be to the satisfaction of the State Commission Assessment Panel, in consultation with Council.

### **8.5.5 Traffic Impact**

Vehicle trip rates have been provided in the Infraplan traffic report, which states that the proposed mix of land uses will result in a total of 101 trips per day.

The report considers the width and parking controls of Canning Street (11 metres wide, with parallel parking and no time restrictions), and Laycock Lane at 5.4 metres and a no-through road.

The report considers the traffic impact of the development as appropriate, in that the local street network – particularly that of Canning Street and Laycock Lane – are able to support the estimated trip generation and peak flows. It notes that there is space for a vehicle to wait in Laycock Lane if required while another exits the car park, or for another vehicle to overtake the waiting vehicle to access other properties further west (corr. South) in Laycock Lane.

## **8.6 Environmental Factors**

### **8.6.1 Crime Prevention**

Development should be designed to maximise surveillance of public spaces through the incorporation of clear lines of sight, appropriate and the use of visually permeable barriers wherever practicable. Buildings should be designed to overlook public and communal streets and public open space to allow casual surveillance.

Development should avoid pedestrian entrapment spots and movement predictors.

The proposed development maximises its opportunity for casual surveillance, through its balconies at each dwelling, and at a minimum, a window to every tourist accommodation room. These are designed in such a way that they promote casual surveillance, but do not unnecessarily allow for overlooking – as described in section 8.4.1 of this report.

The proposed development minimises its possibility for pedestrian entrapment. There is a small section of area beside the bicycle storage area that may present as problematic, however it is considered that this is appropriate given the illumination of the tourist apartment lobby behind this space, and the light spill that will naturally occur to this area.

The areas of private space – such as the laundry and store room, and the storage area at the south – are not accessible to the public through the use of gates. These are not considered to be points of entrapment for this reason.

### **8.6.2 Noise Emissions**

The application is accompanied by a letter prepared by Resonate. The letter considers the noise impacts of the car stacker. It does not detail any noise emissions from plant equipment.

The letter makes recommendations of acoustic treatment of the east and west facades of the ground floor to adhere to the noise goals of the Environment



Protection (Noise) Policy 2007. These have been adopted in the plans submitted with the application, and are considered appropriate.

As no details of the noise emissions of the roof plant have been provided, a condition requiring an acoustic report be prepared shall be attached to any consent given to this application to ensure building occupants and adjacent noise-sensitive uses are not unreasonably disturbed by noise generated through the operation of the development.

### **8.6.3 Waste Management**

The application is accompanied by a report prepared by Infraplan. The report details the estimated waste and recycling volumes, bin sizes and collection details, waste storage area, and collection requirements. The recommendations of the report align with the *SA Better Practice Guide – Waste Management in Residential or Mixed-Use Developments*.

The proposal is serviced by a dedicated waste area, located at the northern end of the car park access off Laycock Lane at east of the proposed building. The report recommends 660L waste bins be adopted for this site for efficiency and ease of handling, however then further summarises that thirteen 240-litre bins are required. It is unclear why the report would suggest different size bins in its conclusion. There is further confusion between the consultant report of Infraplan and that of URPS, who draw different conclusions.

The report mentions that the waste collection could occur from the car park (as demonstrated by illustration in their report), or from Laycock Lane, also supported by illustration. The report notes that the waste collection vehicle would only 'linger' for approximately 5 minutes, and that vehicles will be able to navigate past the parked waste collection vehicle. The report recommends that waste collection be outside of peak periods (7am to 9am, and 3pm to 6pm), to minimise impacts to surrounding properties and peak hour traffic. I concur with the recommended waste collection time restrictions.

Council administration has reviewed the proposed waste management arrangement, and request that a condition be included that requires waste collection be by a private contractor. I concur in applying this condition.

### **8.6.4 Energy Efficiency**

Development Plan policy encourages the conservation of energy and on-site power generation. The application proposes four substantial areas of roof mounted solar panels, however does not detail any outputs or otherwise. Notwithstanding, the inclusion of solar panels is supported.

All apartments will receive natural light, and can be ventilated naturally. The planning consultant report notes that insulation will be applied to the building, which further improves its energy performance. The intention for the proposed building is that it achieves a 7-star rating.

The report considers the energy use of the building, indoor environment quality, water consumption, materials, and emissions. The proposed landscaping utilises native and drought tolerant vegetation which reduces its consumption of water, features efficient water and electrical fittings, and is designed with high performance glass to further reduce energy consumption.

An Environmentally Sustainable Design report has been prepared by SUHO, and is included as an attachment to this report.

## 9. CONCLUSION

The proposed seven storey mixed-use development is consistent with the Zone's desire for high density dwellings, which envisage these being in the form of residential flat buildings which are designed to integrate with areas of open space. The land uses proposed further accord with the Policy Area objectives, where tourist accommodation is proposed along with private apartments within the residential flat building. This, in turn, provides for a diversity of residential offerings, which is also supported.

Whilst the proposal challenges a number of Development Plan policies – generally relating to height, setbacks, traffic movement, some with consequential issues, such as overshadowing, overlooking, and noise, no provision of affordable housing – the proposal achieves many others.

The Government Architect expresses clear concern with the height, massing and setbacks with view to respecting the existing residential context and interfaces with neighbouring buildings, the layout of apartment floors, and the demonstration of solar shading to the northern elevation to ensure delivery of the environmental intent of the development. Adjoining neighbours have also expressed concern with the proposed height and interface issues. Many of these are difficult to avoid and still achieve the desired uplift for the locality. The applicant has made an effort to minimise the impacts where possible.

The application is a merit form of development, and should therefore be assessed on the planning merits it presents. Whilst the proposal is at odds with some policy, it is considered that the proposed development satisfies the intent of the Desired Character for both the Zone and Policy Area, and meets many of the Council Wide provisions. Accordingly, the application warrants the granting of Development Plan Consent, however subject to conditions to manage the technical aspects of its shortfalls.

## 10. RECOMMENDATION

It is recommended that the State Commission Assessment Panel:

- 1) RESOLVE that the proposed development is NOT seriously at variance with the policies in the Development Plan.
- 2) RESOLVE that the State Commission Assessment Panel is satisfied that the proposal generally accords with the related Objectives and Principles of Development Control of the Holdfast Bay Council Development Plan.
- 3) RESOLVE to grant Development Plan Consent the proposal by Barrio Developments for demolition of existing building, and construction of a seven level residential flat building, comprising 20 apartments and 38 tourist accommodation rooms and ancillary car parking and landscaping at 2 Canning Street, Glenelg North, subject to the following conditions of consent.

## PLANNING CONDITIONS

1. That except where minor amendments may be required by other relevant Acts, or by conditions imposed by this application, the development shall be established in strict accordance with the details and following plans submitted in Development Application No 110/M004/19.

*Reason for condition: To ensure the development is undertaken in accordance with the approved plans.*

2. An acoustic impact report shall be prepared, with all recommendations implemented prior to occupation or use of the development, which considers the noise outputs from the plant and vehicle movements, to the satisfaction of the State Commission Assessment Panel.

*Reason for condition: To ensure the development does not cause undue impacts to the occupants of the building and to those in the locality.*

3. Waste collection shall occur outside of peak periods (being 7am to 9am, and 3pm to 6pm).

*Reason for condition: To ensure the waste collection from the development does not cause undue impacts to the occupants of the building and to those in the locality.*

4. Additional detail, by way of a Traffic Management Plan, shall be prepared to the satisfaction of the State Commission Assessment Panel, in consultation with the City of Holdfast Bay Council, prior to the granting of Development Approval, which considers peak tourist numbers and management of car parking within the site of the development to address any impacts to the surrounding street network.

*Reason for condition: To ensure the traffic impacts of the proposed development do not cause undue impacts to the locality.*

5. All vehicle car parks, driveways and vehicle entry and manoeuvring areas shall be designed and constructed in accordance with Australian Standards AS/NZS 2890.1:2004 and AS/NZS 2890.6:2009 to the reasonable satisfaction of the State Commission Assessment Panel prior to the occupation and use of the development.

*Reason for condition: To ensure safe operation of the development.*

6. Waste collection shall be undertaken by private contractor, unless otherwise agreed to by the City of Holdfast Bay Council.

*Reason for condition: Council will not undertake collection of waste for the proposed development.*

## ADVISORY NOTES

- a. This Development Plan Consent will expire after 12 months from the date of this Notification, unless final Development Approval from Council has been received within that period or this Consent has been extended by the State Commission Assessment Panel.
- b. The applicant is also advised that any act or work authorised or required by this Notification must be substantially commenced within 1 year of the final Development Approval issued by Council and substantially completed within 3 years of the date of final Development Approval issued by Council, unless that Development Approval is extended by the Council.
- c. The applicant has a right of appeal against the conditions which have been imposed on this Development Plan Consent. Such an appeal must be lodged at the Environment, Resources and Development Court within two months from the day of receiving this notice or such longer time as the Court may allow. The applicant is asked to contact the Court if wishing to appeal. The Court is located in the Sir Samuel Way Building, Victoria Square, Adelaide, (telephone number 8204 0289).
- d. The applicant is reminded of their obligations under the Local Nuisance and Litter Control Act 2016 and the Environment Protection Act 1993, with regard to the

appropriate management of environmental impacts and matters of local nuisance. For further information about appropriate management of construction sites, please contact the City of Holdfast Bay Council.

- e. Signage does not form part of this development application. No advertising display or signage shall be erected or displayed on the subject land without any required Development Approval being obtained first. This will be subject to a separation.
- f. The disused driveway crossover on Canning Street should be reinstated to kerb and footpath, where the proposed footpath is required to stand above the kerb, and that the design and material application for the footpath be consistent with the rest of the footpath in Canning Street.
- g. If a cooling tower is proposed for air conditioning of the building, there is a requirement to comply with the *South Australian Public Health Act 2001* and South Australian Public Health (Legionella) Regulations 2013.



**Will Gormly**  
**Senior Planning Officer**  
**DEVELOPMENT DIVISION**  
**DEPARTMENT OF PLANNING, TRANSPORT and INFRASTRUCTURE**



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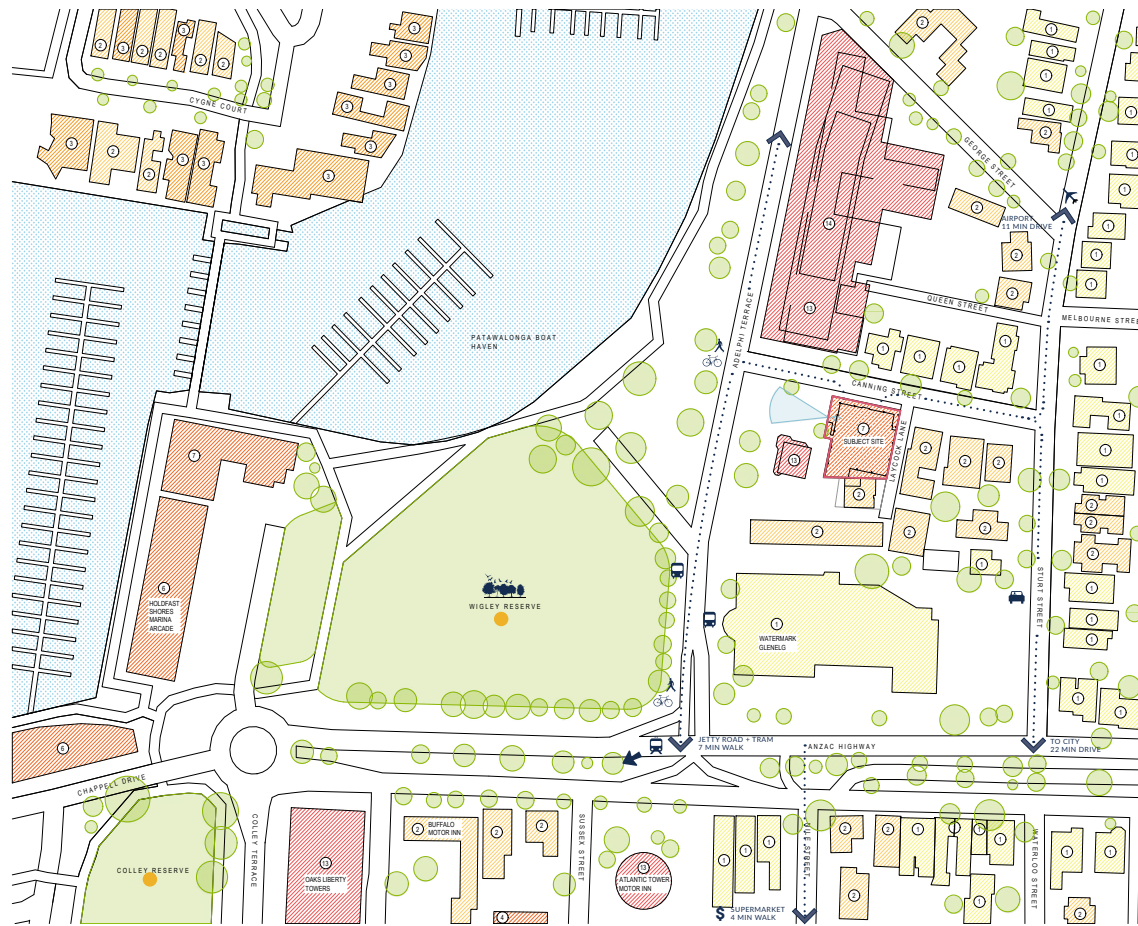
Contents

Page	Drawing Title	Revision
0	Cover Page	E
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2	Project Vision and Precedents	-
3	Perspectives	B
4	Long View Perspectives and Shadow Diagrams	B
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10	Overlooking Diagrams and Site Elevations	D
11	Sections and Site Context	D



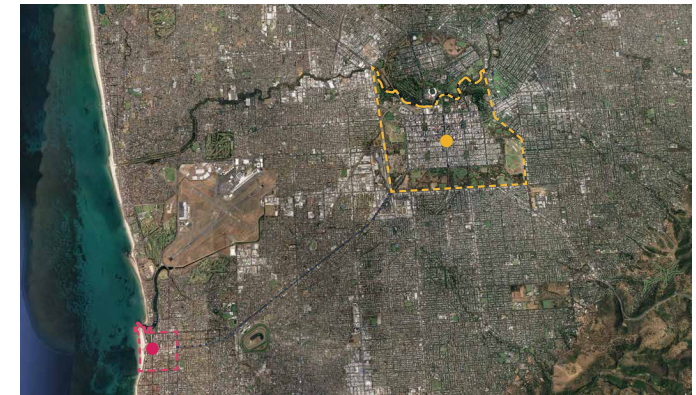
## 2 Canning Street, Glenelg North Context and Site Analysis

BROOK  
FALCONER



Location Plan  
Scale - 1:1000

- Legend**
- Number of Storeys
  - ▨ Progression from 1 - 14 Storeys
  - ▨ Subject Site
  - ▨ Key Views
  - ▨ Open Space
  - Community node
  - ... Thoroughfare



Context Plan - Greater Adelaide  
Scale - NTS

- Legend**
- Glenelg
  - Adelaide CBD



Context Plan - Glenelg  
Scale - NTS

- Legend**
- ▨ Subject Site
  - Marina
  - Wigley Reserve
  - Comfort Inn
  - Watermark
  - Anzac Highway
  - Glenelg Pier
  - Jetty Road
  - Colley Reserve
  - Moseley Square
  - Tapleys Hill Road

## 2 Canning Street, Glenelg North Project Vision and Precedents

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### Barrio Vision

## IMPROVING AND REGENERATING NEIGHBOURHOODS THROUGH SUSTAINABLE AND THOUGHTFUL DESIGNS

### Our mission

Our mission is to improve the way we live and commute plus have a low environmental footprint

### Our Vision

Building modern and sustainably liveable communities

### Our Focus

We specialise in identifying unique infill opportunities specifically located near high frequency public transportation.  
Locations like this enable us to build sustainable development projects which are highly regarded from an urban planning perspective.

### Project Vision

## GLENELG'S MOST LIVEABLE ADDRESS

### Beachside residential

Captures the essence of Glenelg community living.

### Location

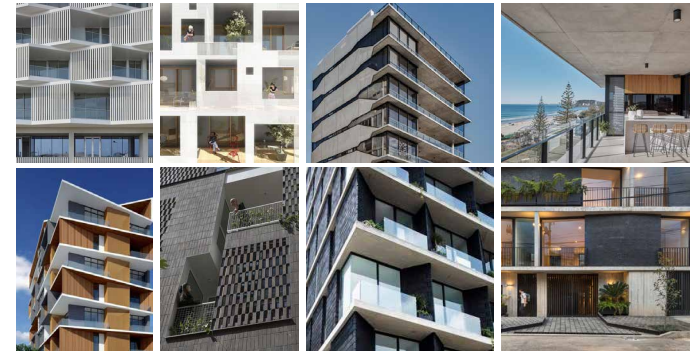
Engages with place. Context. Views. Abundant Amenity.

### Sustainable

Sustainable from first principles. Urban location. Supported by infrastructure. Embedded environmentally responsive design.

### Sustainability Initiatives

- Urban design**  
The building gives back to the public realm, pulling away from the site boundaries on the ground floor, integrating high quality materials and presenting an activated frontage to Canning Street. The tall, open space is fitted with reclaimed materials, and provides space for community engagement - a place where tourists and residents can gather.
- Transport connections**  
2 Canning Street is a short walk to the Bus and tram transport network. Close by are bikeways and facilities are provided to safely store bicycles.
- Renewable energy source**  
The roof is fitted with solar panels to harness renewable energy.
- Thermal performance**  
Sun-shading is built into the form of the building as opposed to being attached. A slab projects on the West and north elevations to shade the windows and walls of the floor below during the summer months. A balanced solid to glass ratio maximises ocean views while managing thermal loads.



Beachside Residential - Material / Texture + Horizontal Articulation



Location - Engagement with place and community



Sustainable - Embedded environmentally responsive design



2 Canning Street, Glenelg North  
Perspectives

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North Canning Streetscape



North-East Canning Streetscape



East Elevation



South Elevation



West Elevation



North-West corner

## 2 Canning Street, Glenelg North Long View Perspectives and Shadow Diagrams

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FALCONER



1 Sturt Street



2 Corner of Mary Street and Melbourne Street



3 Corner of Anzac Highway and Sturt Street



4 Wigley Reserve



5 River Edge - looking East



6 River Edge looking South East

### Shadow Diagrams Existing Context



21 Dec 9am



21 Dec 12pm



21 Dec 3pm



21 June 9am



21 June 12pm



21 June 3pm



23 Sept 9am



23 Sept 12pm



23 Sept 3pm

### Shadow Diagrams

Future Context - with approved 14 storey Hotel



21 Dec 9am



21 Dec 12pm



21 Dec 3pm



21 June 9am



21 June 12pm



21 June 3pm



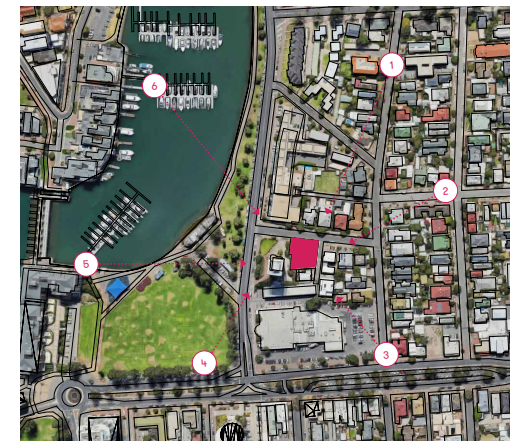
23 Sept 9am



23 Sept 12pm



23 Sept 3pm

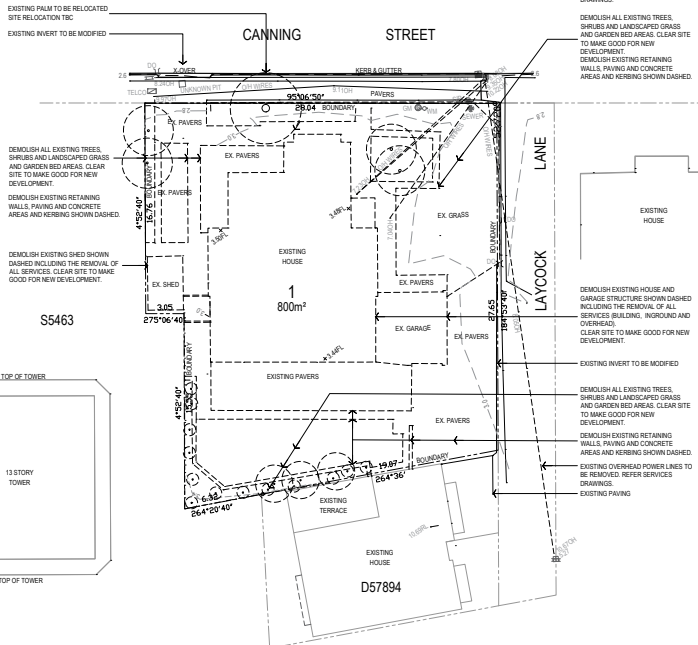




## 2 Canning Street, Glenelg North

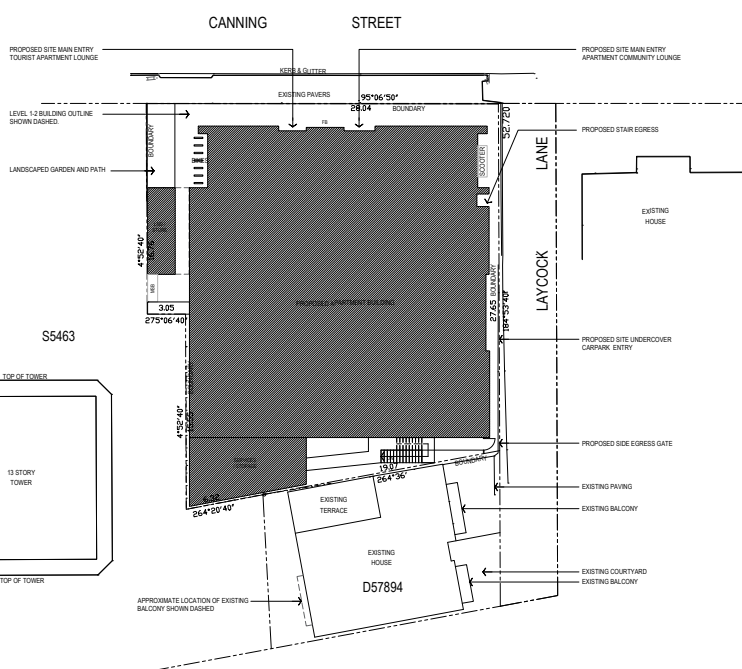
### Floor Plans

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Existing and Demolition Site Plan

Scale - 1:200



### Proposed Site Plan

Scale - 1:20



2 Canning Street, Glenelg North  
Floor Plans

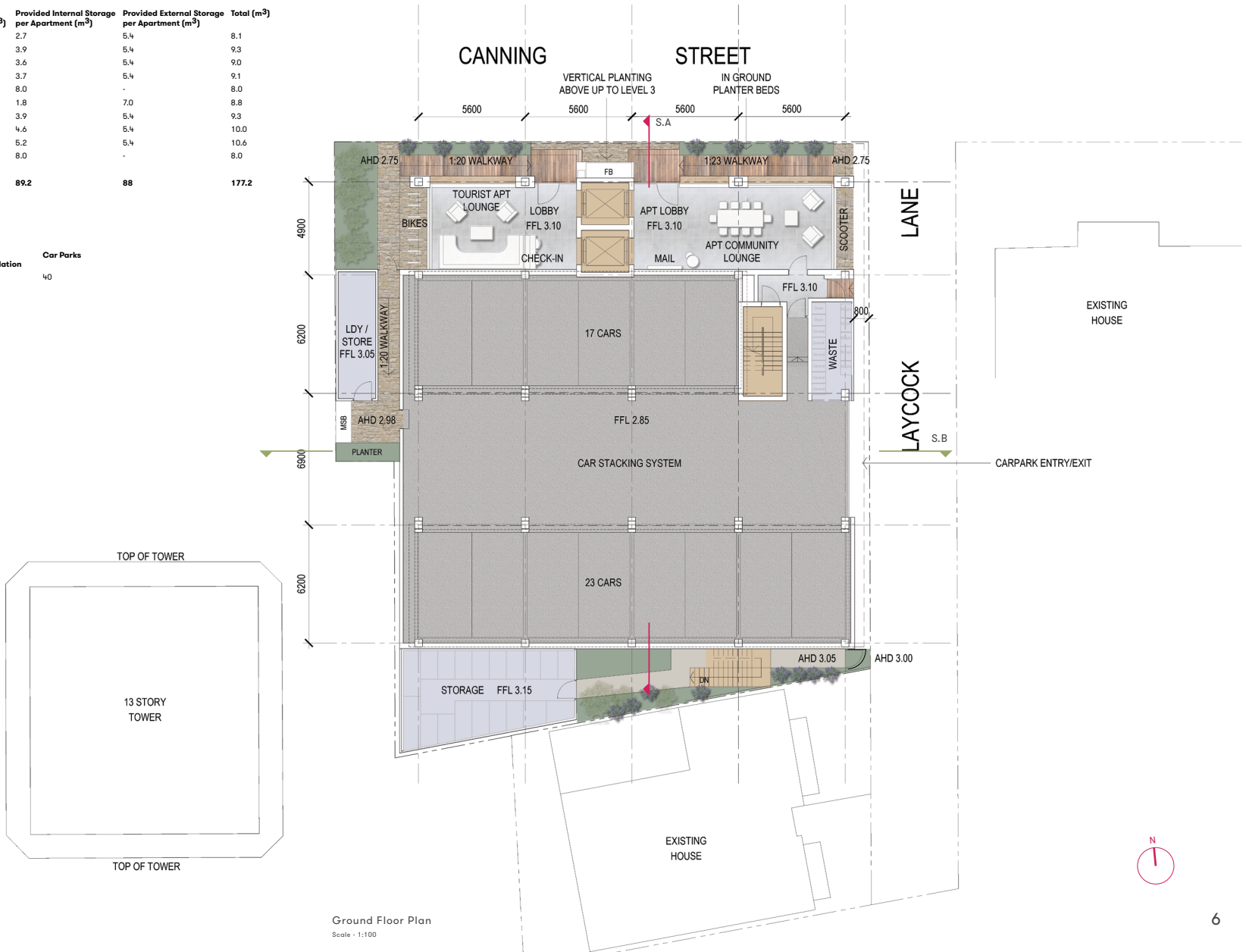
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Storage Provision

Apartment	Apartment Type	Number of Apartments	Required Storage per Apartment (m³)	Provided Internal Storage per Apartment (m³)	Provided External Storage per Apartment (m³)	Total (m³)
A	2 Bed	3	8	2.7	5.4	8.1
B	2 Bed	3	8	3.9	5.4	9.3
C	2 Bed	3	8	3.6	5.4	9.0
D	3 Bed	3	8	3.7	5.4	9.1
E	3 Bed	3	8	8.0	-	8.0
F	2 Bed	1	8	1.8	7.0	8.8
G	1 Bed	1	8	3.9	5.4	9.3
H	1 Bed	1	8	4.6	5.4	10.0
I	2 Bed	1	8	5.2	5.4	10.6
J	2 Bed	1	8	8.0	-	8.0
TOTAL		20	160	89.2	88	177.2

Yield

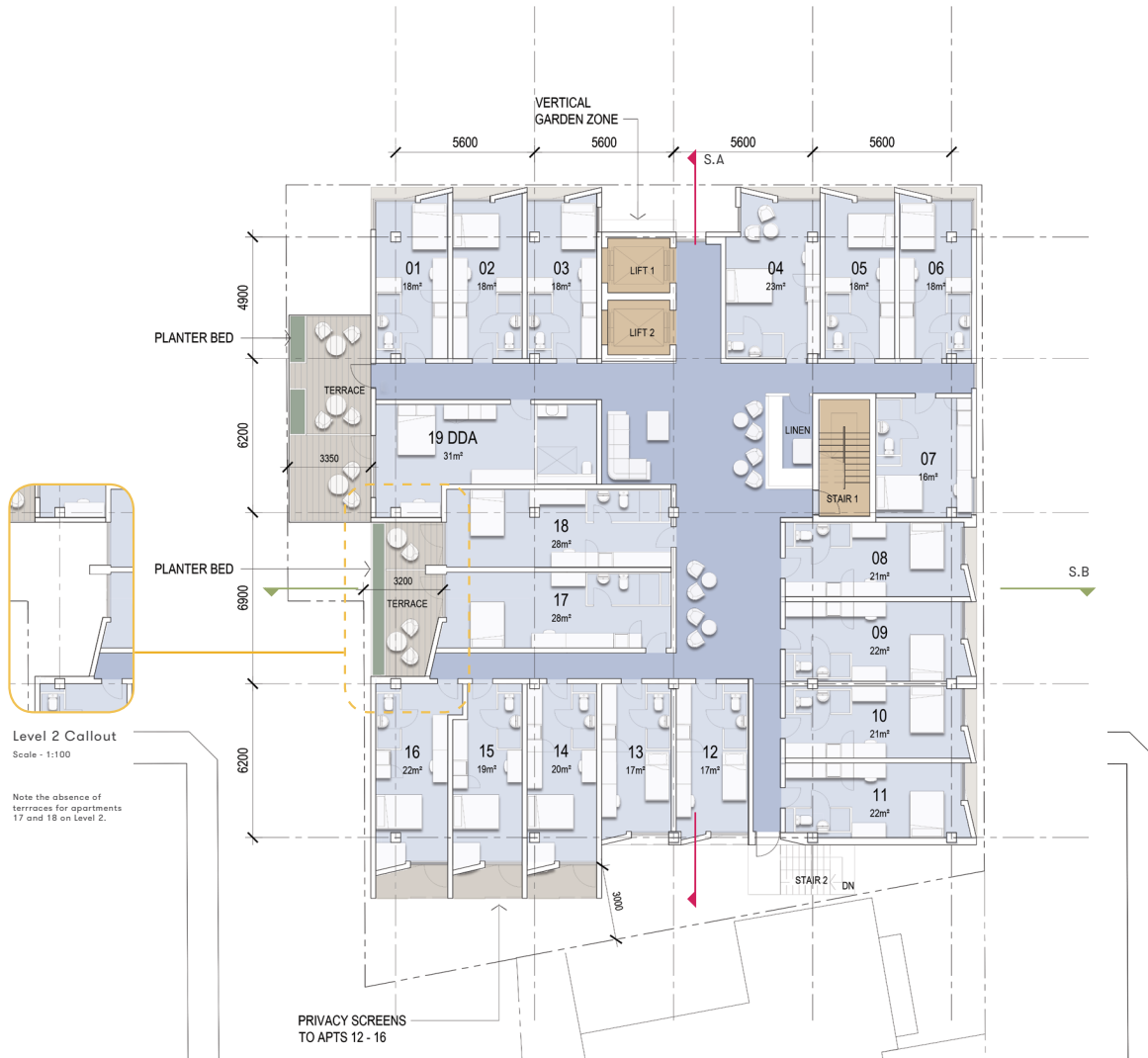
Level	1 Bed	2 Bed	3 Bed	Tourist Accommodation	Car Parks
L00	-	-	-	-	40
L01	-	-	-	19	-
L02	-	-	-	19	-
L03	-	3	2	-	-
L04	-	3	2	-	-
L05	-	3	2	-	-
L06	2	3	-	-	-
TOTAL	2	12	6	38	-





2 Canning Street, Glenelg North  
Floor Plans

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FALCONER



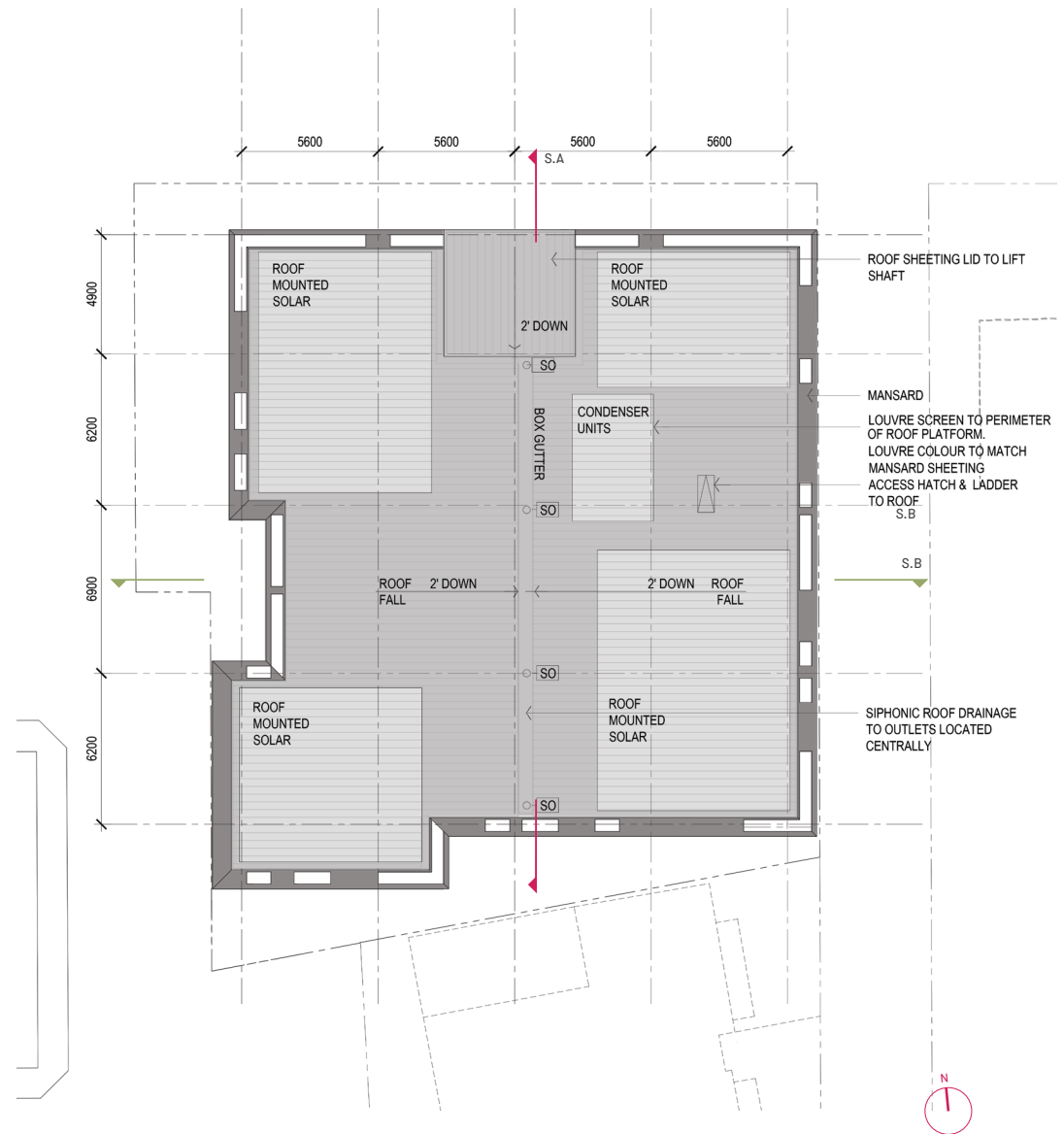
Level 1-2 Floor Plan  
Scale - 1:100



Level 3-5 Floor Plan  
Scale - 1:100



Level 6 Floor Plan  
Scale - 1:100



Roof Plan  
Scale - 1:100

## 2 Canning Street, Glenelg North Elevations

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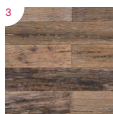
### Material Legend



1 Chalk brick colour



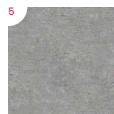
2 Re-use of sandstone



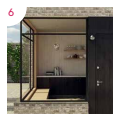
3 Recycled timber from Buffalo or similar recycled timber if Buffalo unsuitable



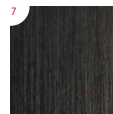
4 Standing seam metal roof sheeting in warm charcoal



5 'Brighton Lite' precast panels



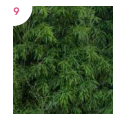
6 Bronze metalwork



7 Charcoal metalwork



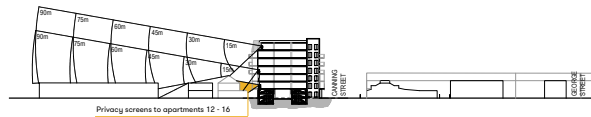
8 Glazing with bronze tint



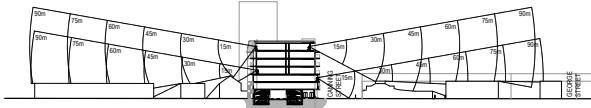
9 Landscape Refer LCS drawing package

## 2 Canning Street, Glenelg North Overlooking Diagrams and Site Elevations

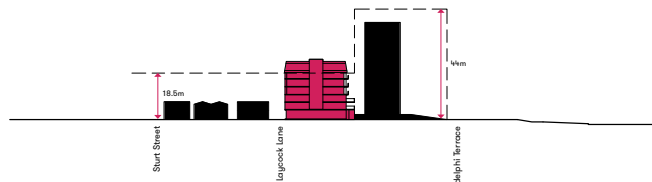
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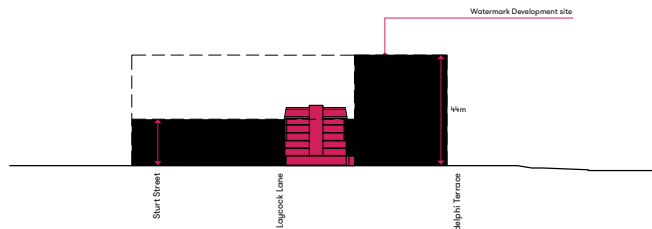
Overlooking Section 1  
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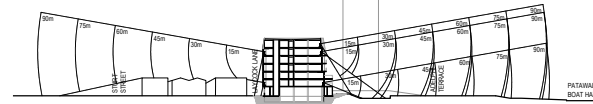
Overlooking Section 2  
Scale - 1:1000



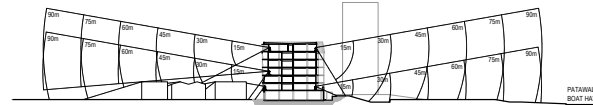
Current North Site Elevation  
Scale - 1:1000



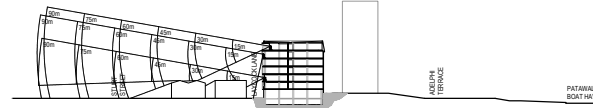
Future North Site Elevation  
Scale - 1:1000



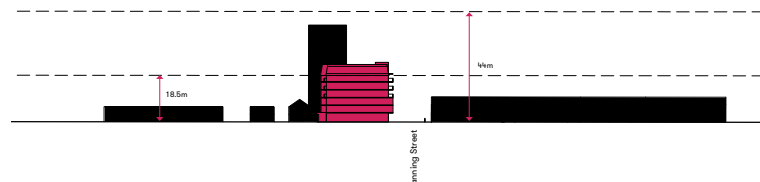
Overlooking Section 3  
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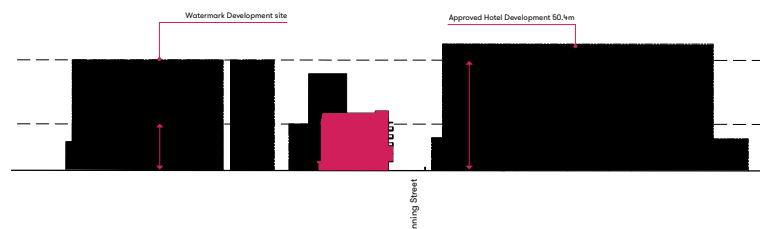
Overlooking Section 4  
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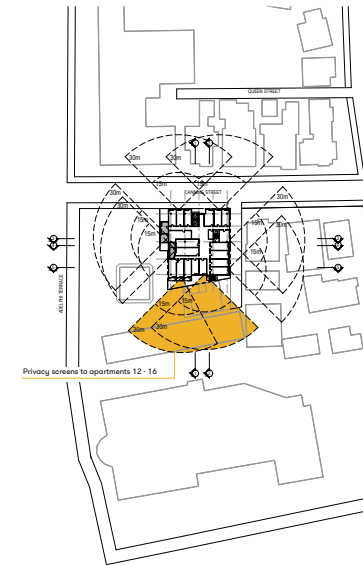
Overlooking Section 5  
Scale - 1:1000



Current East Site Elevation  
Scale - 1:1000



Future East Site Elevation  
Scale - 1:1000



Overlooking Plan  
Scale - 1:1000



On South neighbouring terrace looking North-West at tourist accommodation

Privacy screening - 1500 high and 35 deg angled NE-SW

Privacy screening - 1500 high and 35 deg angled NE-SW



On balcony of tourist accommodation looking South-East to neighbouring terrace

Privacy screening - 1500 high and 35 deg angled NE-SW

Privacy Screen Details

## 2 Canning Street, Glenelg North Sections and Site Context

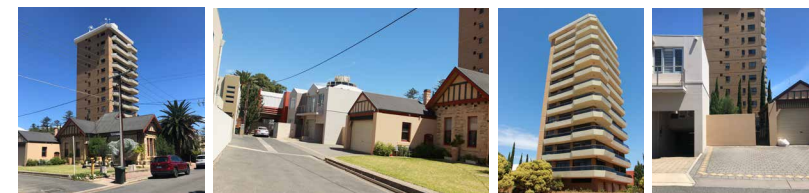
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Adelphi Terrace looking East



Canning Street looking West



2 Canning Street  
Residence

Laycock Lane  
Existing Residences

Adelphi Terrace  
Residential Tower

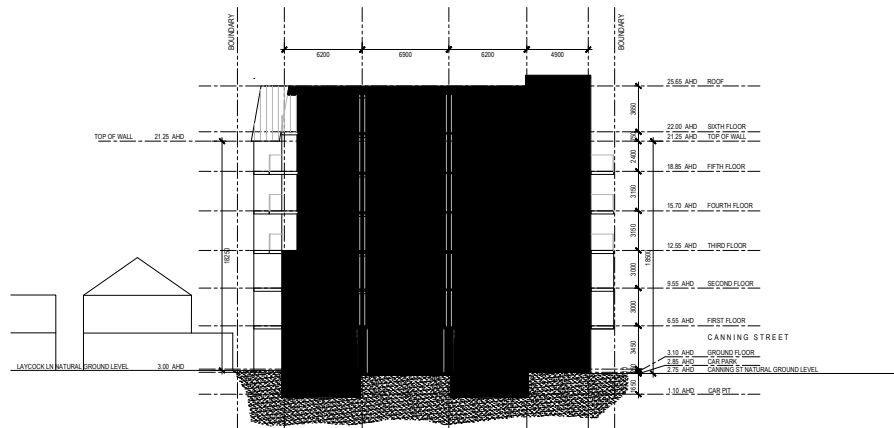
Laycock Lane  
South boundary



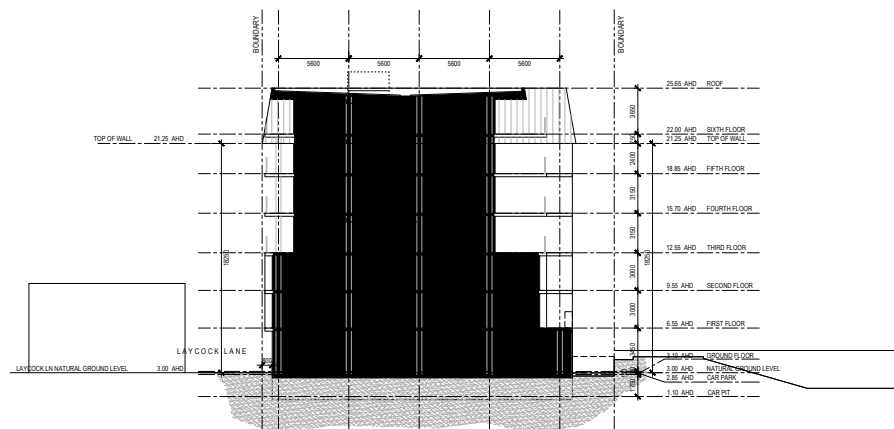
Unit 1/5 Adelphi Terrace  
Residence

1 Laycock Lane  
Residence

1/4 Canning Street  
Residence



Section A - North-South  
Scale - 1:200



Section B - East-West  
Scale - 1:200



CANNING STREET - GLENELG  
PROPOSED LANDSCAPE - 28.08.2019



LOCATION/CONTEXT PLAN  
NOT TO SCALE



PLANTING SCHEDULE

SPECIES	COMMON NAME	POT SIZE	INDICATIVE SIZE (Height x spread)
CLIMBERS/GROUND COVERS			
Hibbertia scandens	Guinea Flower	140mm	3m x 500mm
Tradescantia pallida	Purple heart	140mm	300mm x 3m
LOW PLANTING			
Dianella brevicaulis	Native flax lily	140mm	600mm x 600mm
Linopoe 'Evergreen Giant'	Turf Lily	140mm	600mm x 600mm
FEATURES			
Dracaena marginata	Madagascar palm	300mm	3m x 1m
Zamia furfuracea	Card board palm	140mm	600mm x 600mm
SCREENING			
Syzygium australe 'Pinnacle'	Lilly Pilly	300mm	5m x 1m
TREES			
Plumeria rubra	Frangipani	45L	5m x 5m

MOOD IMAGERY



28.08.2019 - For Development Application  
Revision:



Project:  
CANNING STREET GLENELG DEVELOPMENT

Drawing:  
PROPOSED LANDSCAPE

Scale: NTS  
Drawn: DF  
Checked: SK  
Date: 28.08.2019  
Dwg no: LS.030.19  
Sheet: 1 OF 5  
Rev:

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GROUND LEVEL GARDEN BEDS  
WITH HARDY FEATURE PLANTING  
• Dracaena marginata  
• Dianella brevicaulis

RECLAIMED SANDSTONE  
SEATING WALLS

RECLAIMED HARDWOOD  
RAMPED DECKING

DEEP SOIL ZONE WITH LOW  
PLANTING  
• Dianella brevicaulis  
• Zamia furfuracea

SMALL TREES  
• Plumeria rubra

GROUND LEVEL GARDEN BED  
WITH PLANTS TO SCREEN FENCE  
• Dracaena marginata

FIRE BOOSTER  
RAISED PLANTER ABOVE AS SHOWN  
ON LEVEL 1 PLAN SHEET 3

GROUND LEVEL GARDEN BEDS WITH  
HARDY FEATURE PLANTING  
• Dracaena marginata  
• Dianella brevicaulis

SMALL TREE IN GROUND LEVEL  
GARDEN BED  
• Plumeria rubra

RECLAIMED HARDWOOD RAMPED  
DECKING

RECLAIMED SANDSTONE SEATING  
WALLS

RAISED PLANTER WITH DRACAENA  
MARGINATA

EXPOSED AGGREGATE  
CONCRETE ACCESS PATH WITH  
BORDER PLANTING  
• Liriope Evergreen Giant  
• Planting UNDER STAIRCASE  
• Zamia furfuracea

SCREENING PLANTS TO  
BOUNDARY FENCE  
• Syzygium pinnate



PROPOSED LANDSCAPE - GROUND LEVEL  
SCALE 1:100 AT A1

28.08.2019 - For Development Application  
Revision:



Project:  
CANNING STREET GLENELG DEVELOPMENT

Drawing:  
PROPOSED LANDSCAPE

Scale: 1:100 AT A1  
Drawn: DF  
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REFER TO GROUND LEVEL  
LANDSCAPE PLAN ON PAGE 2

BALCONY ON LEVEL 2 & 3 WITH  
PROPOSED FURNITURE AND  
LIGHTWEIGHT POTS WITH  
FEATURE PLANTING  
• *Dracaena marginata*

TERRACE ON LEVEL 1 WITH  
BALLUSTRADE PLANTER WITH  
FEATURE AND UNDERSTORY  
PLANTING  
• *Dracaena marginata*  
• *Tradescantia palida*

RAISED PLANTER ABOVE FIRE  
BOOSTER WITH RECLAIMED  
SANDSTONE FACADE  
LOW CASCAIDING PLANTS  
• *Tradescantia palida*

RONSTAN STAINLESS STEEL MESH  
SCREEN ON LEVELS 1 & 2 WITH  
CLIMBING PLANTS FROM RAISED  
PLANTER  
• *Hibbertia scandens*



REFER TO GROUND LEVEL  
LANDSCAPE PLAN ON PAGE 2

PROPOSED LANDSCAPE - LEVELS 1 & 2  
SCALE 1:100 AT A1

28.08.2019 - For Development Application  
Revision:

**LCS LANDSCAPES**  
Defining Spaces

Project:

CANNING STREET GLENELG DEVELOPMENT

Drawing:

PROPOSED LANDSCAPE

Scale: 1:100 AT A1

Drawn: DF

Checked: SK

Date: 28.08.2019

Dwg no: LS.030.19

Sheet: 3 OF 5

Rev:

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**PROPOSED LANDSCAPE - LEVELS 3 TO 6**  
SCALE 1:100 AT A1

FEATURE PLANTING IN  
LIGHTWEIGHT POTS ON FRONT  
BALCONIES  
• *Dracaena marginata*

BALCONY ON LEVEL 2 & 3 WITH  
PROPOSED FURNITURE AND  
LIGHTWEIGHT POTS WITH  
FEATURE PLANTING  
• *Dracaena marginata*

FEATURE PLANTING IN  
LIGHTWEIGHT POTS ON  
SOUTHERN BALCONIES  
• *Dracaena marginata*



28.08.2019 - For Development Application  
Revision:

**LCS LANDSCAPES**  
Defining Spaces

Project:  
**CANNING STREET GLENELG DEVELOPMENT**

Drawing:  
**PROPOSED LANDSCAPE**

Scale: 1:100 AT A1  
Drawn: DF  
Checked: SK  
Date: 27.08.2019  
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# DEVELOPMENT APPLICATION FORM

PLEASE USE BLOCK LETTERS

COUNCIL: City of Holdfast Bay

APPLICANT: Barrio Developments

Postal Address: PO Box 3571

Rundle Mall SA 5000

Owner: Pasquale & Rosa Delfino

Postal Address: 2 Canning St

Glendy North SA 5045

BUILDER:                     

Postal Address:                     

                     Licence No:                     

## CONTACT PERSON FOR FURTHER INFORMATION

Name: Matt King

Telephone: 83337999 [work] 0417080596 [Ah]

Fax:                      [work]                      [Ah]

EXISTING USE: Dwelling

## FOR OFFICE USE

Development No:                     

Previous Development No:                     

Assessment No:                     

☐ Complying

☐ Non Complying

☐ Notification Cat 2

☐ Notification Cat 3

☐ Referrals/Concurrences

☐ DA Commission

Application forwarded to DA

Commission/Council on

/ /

Decision:                     

Type:                     

Date: / /

	Decision required	Fees	Receipt No	Date
Planning:	<u>                    </u>	<u>                    </u>	<u>                    </u>	<u>                    </u>
Building:	<u>                    </u>	<u>                    </u>	<u>                    </u>	<u>                    </u>
Land Division:	<u>                    </u>	<u>                    </u>	<u>                    </u>	<u>                    </u>
Additional:	<u>                    </u>	<u>                    </u>	<u>                    </u>	<u>                    </u>
Development Approval	<u>                    </u>	<u>                    </u>	<u>                    </u>	<u>                    </u>

DESCRIPTION OF PROPOSED DEVELOPMENT: Residential flat building.

LOCATION OF PROPOSED DEVELOPMENT: 2 Canning St Glendy North

House No: 2 Lot No:                      Street: Canning Town/Suburb: Glendy North

Section No [full/part]                      Hundred:                      Volume: 5855857 Folio: 966

Section No [full/part]                      Hundred:                      Volume:                      Folio:                     

## LAND DIVISION:

Site Area [m<sup>2</sup>]                      Reserve Area [m<sup>2</sup>]                      No of existing allotments                     

Number of additional allotments [excluding road and reserve]:                      Lease: YES ☐ NO ☐

BUILDING RULES CLASSIFICATION SOUGHT:                      Present classification:                     

If Class 5,6,78 or 9 classification is sought, state the proposed number of employees: Male:                      Female:                     

If Class 9a classification is sought, state the number o persons for whom accommodation is provided:                     

If Class 9b classification is sought, state the proposed number of occupants of the various spaces at the premises:                     

DOES EITHER SCHEDULE 21 OR 22 OF THE DEVELOPMENT REGULATIONS 2008 APPLY? YES ☐ NO ☒

HAS THE CONSTRUCTION INDUSTRY TRAINING FUND ACT 2008 LEVY BEEN PAID? YES ☐ NO ☒

DEVELOPMENT COST [do not include any fit-out costs]: \$ 5,390,009

I acknowledge that copies of this application and supporting documentation may be provided to interested persons in accordance with the Development Regulations 2008.

SIGNATURE: 

Dated: 6 / 6 / 19

DEVELOPMENT REGULATIONS 2008  
Form of Declaration (Schedule 5 clause 2A)



Government  
of South Australia

To: State Commission Assessment Panel and  
City of Holdfast Bay

From: Barro Developments

Date of Application: 6 / 6 / 19

Location of Proposed Development: 2 Canning St Glenside North

House No: 2 Lot No: 1 Street: Canning St

Town/Suburb: Glenside North

Section No (full/part): \_\_\_\_\_ Hundred: \_\_\_\_\_

Volume: 5837 Folio: 966

Nature of Proposed Development: Residential Flat Building

I Glen Vollebregt being the applicant/ a person acting on behalf of the applicant (delete the inapplicable statement) for the development described above declare that the proposed development will involve the construction of a building which would, if constructed in accordance with the plans submitted, not be contrary to the regulations prescribed for the purposes of section 86 of the Electricity Act 1996. I make this declaration under clause 2A(1) of Schedule 5 of the Development Regulations 2008.

Signed: [Signature] Date: 6 / 6 / 19





**Government  
of South Australia**

**Note 1**

This declaration is only relevant to those development applications seeking authorisation for a form of development that involves the construction of a building (there is a definition of 'building' contained in section 4(1) of the Development Act 1993), other than where the development is limited to –

- a) an internal alteration of a building; or
- b) an alteration to the walls of a building but not so as to alter the shape of the building.

**Note 2**

The requirements of section 86 of the Electricity Act 1996 do not apply in relation to:

- a) an aerial line and a fence, sign or notice that is less than 2.0 m in height and is not designed for a person to stand on; or
- b) a service line installed specifically to supply electricity to the building or structure by the operator of the transmission or distribution network from which the electricity is being supplied.

**Note 3**

Section 86 of the Electricity Act 1996 refers to the erection of buildings in proximity to powerlines. The regulations under this Act prescribe minimum safe clearance distances that must be complied with.

**Note 4**

The majority of applications will not have any powerline issues, as normal residential setbacks often cause the building to comply with the prescribed powerline clearance distances. Buildings/renovations located far away from powerlines, for example towards the back of properties, will usually also comply.

Particular care needs to be taken where high voltage powerlines exist; or where the development:

- is on a major road;
- commercial/industrial in nature; or
- built to the property boundary.

**Note 5**

An information brochure: 'Building Safely Near Powerlines' has been prepared by the Technical Regulator to assist applicants and other interested persons.

This brochure is available from council and the Office of the Technical Regulator. The brochure and other relevant information can also be found at [sa.gov.au/energy/powerlinesafety](http://sa.gov.au/energy/powerlinesafety)

**Note 6**

In cases where applicants have obtained a written approval from the Technical Regulator to build the development specified above in its current form within the prescribed clearance distances, the applicant is able to sign the form.

The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



Registrar-General

## Certificate of Title - Volume 5857 Folio 966

Parent Title(s) CT 5114/414  
Dealing(s) RTU 9158464  
Creating Title  
Title Issued 26/10/2001  
Edition 2  
Edition Issued 13/08/2003



## Estate Type

FEE SIMPLE

## Registered Proprietor

PASQUALE DELFINO  
ROSA DELFINO  
OF 2 CANNING STREET GLENELG NORTH SA 5045  
AS JOINT TENANTS

## Description of Land

ALLOTMENT 1 DEPOSITED PLAN 57894  
IN THE AREA NAMED GLENELG NORTH  
HUNDRED OF NOARLUNGA

## Easements

NIL

## Schedule of Dealings

Dealing Number	Description
9648149	MORTGAGE TO RAMS MORTGAGE CORPORATION LTD.

## Notations

### Dealings Affecting Title

NIL

### Priority Notices



NIL

### Notations on Plan

NIL

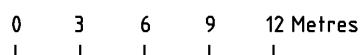
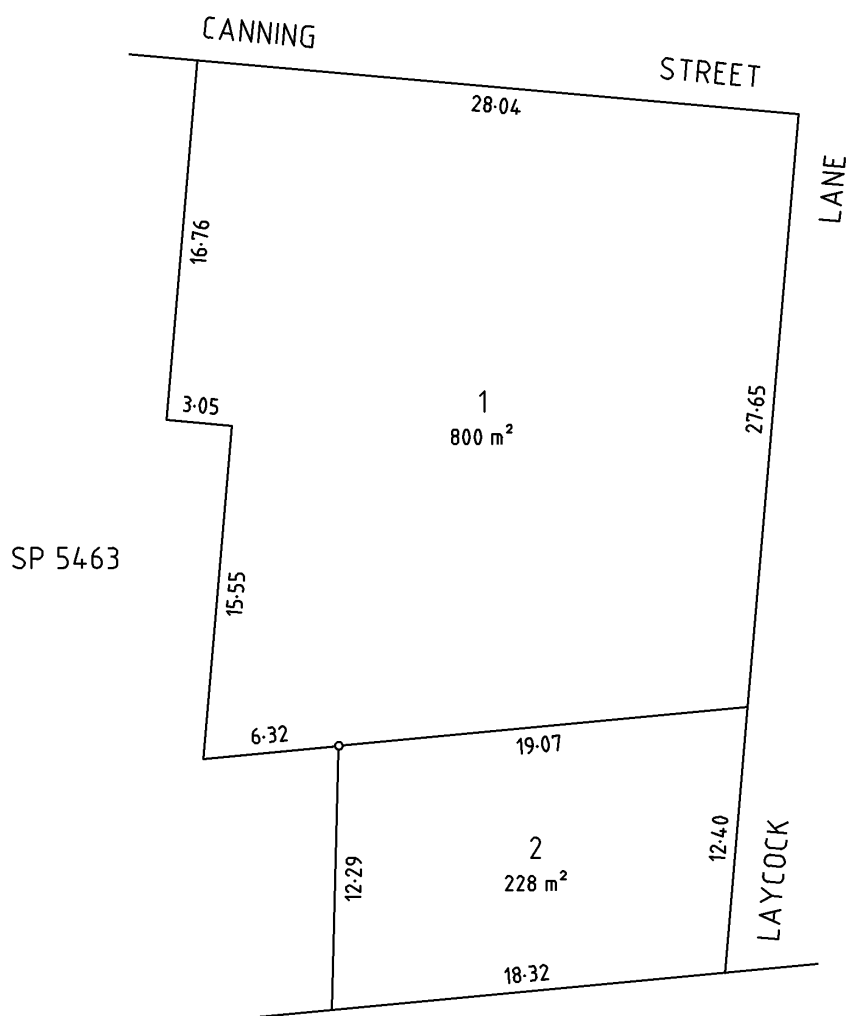
### Registrar-General's Notes

NIL

### Administrative Interests

NIL

\* Denotes the dealing has been re-lodged.





Barrio Developments  
2019-0070  
3 July 2019



RESIDENTIAL APARTMENTS AND TOURIST  
ACCOMODATION DEVELOPMENT AT  
2 CANNING STREET GLENELG  
PLANNING STATEMENT







# Residential Apartment Building

1 July 2019

<b>Lead consultant</b>	URPS
<b>In association with</b>	Brown Falconer Infraplan LCS Landscapes SuHo Botten Levinson Lawyers Pyper Leaker Surveying Services Structural Systems Resonate Consultants Strata Data Lucid Consulting
<b>Prepared for</b>	Barrio Developments
<b>Consultant Project Manager</b>	Matthew King, Director  Suite 12/154 Fullarton Road (cnr Alexandra Ave) Rose Park, SA 5067 Tel: (08) 8333 7999 Email: <a href="mailto:matthew@urps.com.au">matthew@urps.com.au</a>
<b>URPS Ref</b>	2019-0180

## ***Document history and status***

Revision	Date	Reviewed	Approved	Details
V1	18 June 2019	MK	MK	Draft V 1.0
V2	25 June 2019	MK	MK	Draft V 2.0
V3	3 July 2019	MK	MK	Final

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# 1.0 Introduction

URPS has been engaged by Barrio Developments, the applicant in this matter, to provide advice, liaise with the relevant government bodies during the pre-lodgment phase and prepare this planning report in respect of a proposed residential tourist accommodation development at 2 Canning Street Glenelg.

In accord with the pertinent planning objectives, the proposal presents an exciting opportunity to develop the land with high quality residential apartments and short-term accommodation afforded with views of the adjacent Patawalonga and access to public open space, recreational areas and the many shops/cafes and public transit services located at Jetty Road.

The proposal has been prepared by experienced architects, aided by an experienced team of consultants, and has been through a rigorous design review process with many iterations prepared before settling on the plan now submitted for formal assessment.

Discussions about the development of the land first commenced with the Department of Planning, Transport and Infrastructure in late 2017. The proposal was presented to design review and three pre-lodgment meetings and hence has evolved considerably since its early concept plans.

In our opinion, the submitted plan provides high quality urban design outcome that contributes to the objectives of the relevant zone, policy area and precinct all which seek additional compact housing, and tourist accommodation, and added vibrancy in the area, as described within this report.

In preparing this report we have undertaken a review of the following:

- Certificate of titles (**Appendix A**).
- Subject land and locality (refer URPS plan contained within **Appendix B**).
- Plans prepared by Brown Falconer (**Appendix C**).
- Traffic and parking assessment, and waste management plan, prepared by Infraplan (**Appendix D**).
- Landscaping plan prepared by LCS landscapes (**Appendix E**).
- Legal opinion prepared by Botten Levinson Lawyers (**Appendix F**).
- Sustainability report prepared by SuHo (**Appendix G**).
- Opinion from Pyper Leaker Surveying Services regarding the titling associated with the Stacker System (**Appendix H**).
- Stormwater management plan prepared by Structural Systems (**Appendix I**).
- Resonate Consultants acoustic assessment (**Appendix J**).
- Holdfast Bay City Development Plan, consolidated 2 June 2016.

A height policy analysis diagram (prepared by URPS) is also an appendix to this report (see **Appendix K**).

## 2.0 The Subject Land and Locality

The subject land comprises one allotment described as Allotment 1 in Deposited Plan 57894 in the area named Glenelg North Hundred of Noarlunga.

The land has a 28.04 metre frontage width to Canning Street and a 27.65 metre frontage width to Laycock Lane. The total area of the land is approximately 800m<sup>2</sup>.

The land is basically flat and contains a detached dwelling. That dwelling has a demolition approval that was separately sought with the City of Holdfast Bay (per Development Plan Consent 110/001071/19).

The locality surrounding the subject land comprises a mix of different kinds of residential development including housing between 1 and 2 storeys in height, varying in style, and a large residential flat building complex (comprising apartments) of some 13 storeys to the immediate west of the land.

Single storey housing in the locality is typically of early 1900's era and 2 storey housing is typically of more contemporary townhouse style development constructed within the past 10-15 years by my estimation.

In addition, the land on the corner Canning Street and Adelphi Terrace (to the north-west of the land) comprises a low-rise motel building. That site however has authorization to construct a 14 storey mixed use building comprising tourist accommodation and shops.

The land is located close to the Patawalonga River - a popular space for recreation due to its attractive views, green space and highly functional footpath.

The land also has excellent connectivity to Glenelg Beach and through to Colley Reserve and Moseley Square, all of which provide large open community spaces.





## 3.0 The Proposal

### 3.1 Overview

In summary, the development comprises the following elements:

- Construction of a multi-level residential apartment and tourist accommodation building comprising 20 apartments and 38 tourist accommodation rooms plus communal areas for travellers.
- The ground floor of the facility will comprise lobby and entrance areas as well as a small lounge area for each respective land use. The ground floor also comprises a services/storage room, waste room and laundry/store room.
- All car parking is to be situated off-street and at-grade where there will be capacity for 40 car parking spaces in a vertical stacker system. The car park is to be accessed from Laycock Lane.
- There is bike parking (14 spaces) provided at-grade toward the land's Canning Street/Laycock Lane boundary.
- Landscaping is proposed at-grade and upon the building.

### 3.2 Apartment Sizes and Private Open Space

There are five different apartment types situated within each of the floors designated to residential use.

In summary each floor will include:

- **Type A** - 2-bedroom apartment of 90m<sup>2</sup> with a 17m<sup>2</sup> balcony
- **Type B** - 2-bedroom apartment of 64m<sup>2</sup> with an 11m<sup>2</sup> balcony
- **Type C** - 2 bedroom apartment of 64m<sup>2</sup> with an 11m<sup>2</sup> balcony.
- **Type D** - 3-bedroom apartment of 110m<sup>2</sup> with a 17m<sup>2</sup> balcony
- **Type E** - 3-bedroom apartment of 127m<sup>2</sup> with a 17m<sup>2</sup> apartment

### 3.3 Storage

All apartments have storage provided both internally and externally which ranges in size between 9.5-10.1m<sup>3</sup>.

### 3.4 Mail Box

The mail box facility for the residential apartments is located within the apartment lobby and will be accessible during day time for Australia Post delivery services.

### 3.5 Design Philosophy

The proposed apartment building has been designed in close consultation with the DPTI/ODASA with the design consultation process commencing in late 2016, re-commencing in late 2018 and ultimately concluding in June 2019.

The project was presented to Design Review and three pre-lodgement panel meetings. Prior to commencing with any pre-lodgement consultation, verbal feedback provided from DPTI in late 2016 was that a project over height was a “reasonable proposition” at this site primarily given its adjacency to the adjoining Precinct 5 wherein 12 storey/43 metre tall development is contemplated and where there is an existing residential apartment building of some 13 storeys.

The explorative design process has seen the project’s design evolve and be responsive to the various planning constraints, whilst being cognisant of the proponent’s economic and market-based objectives.

The following factors underpin this design:

- A mix of apartment sizes but a higher proportion of 2-bedroom units to suit market trends as advised by Connekt Real Estate.
- Tourist accommodation that seeks to offer short term accommodation for travellers.
- A high level of amenity for apartment occupiers with regard to internal spaces, outlook and views, and orientation which maximises sunlight access and natural ventilation where able.
- A contemporary design approach that incorporates natural light and views, and articulated facades that adequately enhance appearance to the public realm.
- A material and colours palette that is suited to its coastal context being highly durable and low maintenance.

### 3.6 Materials and Colours

Materials and colours will include:

- Chalk coloured bricks.
- Re-use of the stone associated with the existing dwelling.
- Use of timber.
- Rib and pan wall cladding in a dark, warm grey color.
- White coloured concrete slab and columns.
- Bronze metal work.
- Glazing with bronze tint.

### 3.7 Landscaping and Surface Treatment

Landscaping will include:

- A mix of low plantings and small trees on the strip of land to the west of the building on the ground floor.
- Climbing plants that are to grow on a steel mesh element on Levels 1-3 (north elevation).
- Timber decking will be used for the surface treatment of external pedestrian areas except for the rear pathway which will be comprised of an exposed concrete aggregate material.



- Forward of the building along the Canning Street frontage will be garden beds set against the building. The garden beds will be constructed using reclaimed sandstone.
- Small gardens beds are proposed along Laycock Lane. A more sizeable strip of screen plantings is proposed along the rear boundary.

### 3.8 Titling

All car parking stacker areas will be nominated on the title as 'communal' with the specific allocation of car parking spaces nominated via the building's by-laws.

## 4.0 Procedural Matters

### 4.1 Relevant Authority

The development involves the construction of a building exceeding 4 storeys in height within the Residential High-Density Zone of the City of Holdfast Bay Development Plan as consolidated 2 June 2016.

Therefore, the proposal is to be assessed by the State Planning Commission pursuant to Clause 4C (1)(c) Schedule 10, the *Development Regulations 2008*.

### 4.2 Kind of Development

The subject land is located within the Residential High-Density Zone as depicted on Zone Map HoB/2.

The proposal comprises both a 'residential flat building' component and a 'motel or 'tourist accommodation' component. All of these uses are to be assessed on their merits in the Zone.

### 4.3 Public Notification and Referrals

Pursuant to the procedural matters section identified within the Zone any development with an overall height exceeding 11.5 metres (excluding gables) measured from the natural ground level is defined as Category 2 for public notification purposes.

The application will need to be referred to the City of Holdfast Bay and the Government Architect for formal comment.



## 5.0 Planning Assessment

The following assessment addresses what we consider to be the most pertinent Development Plan matters relevant to a development of this kind.

### 5.1 Orderly and Sustainable Development

The proposal will provide further positive investment into the City of Holdfast Bay to the tune of \$7.8 million (including fit-out).

The proposal seeks to establish a new, architecturally designed apartment and tourist accommodation building within a locality comprising residential and tourist accommodation uses, in a sought-after location with exceptional access to services, and views and access to attractive public space.

Residents will be provided secure and covered car parking along with secured pedestrian access via an internal lobby and lift to their respective apartments/rooms.

The development will be economic and targeted towards professionals looking for comfortable, pleasant yet low maintenance residential accommodation, as well as retirees looking to downsize to more compact accommodation.

The tourist accommodation component will likely be owned by a single operator and will provide short term accommodation in the area to travelers.

Given the above, Orderly and Sustainable Objective 1 is achieved.

In satisfaction of Objectives 2, 3, 4 and Principles 1, 2, 3, 4, 7 and 9 the development will:

- Be a compact multi-residential housing scheme that desirably make use of existing infrastructure and public services that are readily available.
- Not prejudice the present or future land use of any adjoining properties nor their ability to satisfy important provisions of the Development Plan.
- Expand the economic base of the region through increased local permanent and temporary population, creating demand local services and business.
- Undertake a development which is efficient and coordinated and which will make better use of the land which is presently put to a low-density residential use (i.e. a single dwelling).
- Add to the diversity of housing type and availability of tourist accommodation in the area.
- Be in accordance with the Structure Plan Map, by providing a residential land use in this locality.

### 5.2 Land Use

Zone Objective 1 states:

1. **A residential zone comprising a range of high-density dwellings, including a minimum of 15 per cent affordable housing, primarily in the form of row dwellings and residential flat buildings, designed to integrate with areas of open space, neighbouring centres or public transport nodes.**



Policy Area 15 Objective 1 states:

- 1 A policy area comprising tourist accommodation and a range of dwellings and residential flat buildings at medium to high densities.**

The proposal provides both high density dwellings within a residential flat building and a tourist accommodation use, therefore it satisfies the above land use objectives of the Zone and Policy Area.

Policy Area 15's Desired Character states:

**The policy area provides the Council's premier coastal medium and high-density living opportunities. It includes areas of Glenelg North around the foreshore and the Patawalonga, and within Glenelg and Glenelg South along the foreshore and extending into small parts of the suburban landscape, and along Colley Terrace.**

...

**The policy area is a premier location with excellent accessibility to views, beach, public spaces, centre services, facilities and public transport. The policy area adds to the choice of accommodation within Holdfast Bay and the wider metropolitan area by providing for a variety of medium and higher density dwelling types, including apartments for residential purposes and visitor accommodation.**

Zone Principle 7 defines High Density as follows:

- 7 High density development that achieves gross densities of more than 45 dwellings per hectare (which translates to net densities of more than 67 dwellings per hectare) should typically be in the form of over 4 storey buildings.**

(my underline)

The proposal features 20 dwellings on an about 800m<sup>2</sup> site meaning a net density equivalent to approximately 250 dwellings per hectare and within a building over 4 storeys in height.

Desirably this is a 'high density' form of development that occurs near the Patawalonga so as to afford occupants exceptional access to this quality recreational open space.

Given the above, the proposal satisfies the above provisions as well as Zone Objective 2 and Principle 2.

## 5.3 Form and Character

The Desired Character of the Policy Area states:

**Development will be of the highest architectural standard, contemporary in style and contribute positively to the quality of the public realm. Its built form will contrast with the open character of the adjacent foreshore and reserve public spaces. It will capitalise on the highly desirable location through significant scale, with built form between three and twelve stories in height. This development will demonstrate excellence in urban design. It will create design relationships between buildings at ground level and the street frontage that acknowledge and respect the existing context, ensuring that scale and the built form edge protects and enhances significant visual and movement corridors (including key vistas to the sea and views through to public spaces). Views into and out of development sites will also reinforce visual connectivity and way-finding within the policy area.**



Building form and setbacks will vary to provide large-scale articulation within the streetscape. Building form will also use light and shade through articulation, eaves, verandas, canopies and balconies, to provide architectural detail, summer shade and promote greater energy efficiency. Likewise, buildings will use a balanced approach to the use of solid materials and glazing so to provide an attractive backdrop to key public spaces and streets.

Basement or undercroft car parking is contemplated where site circumstances allow appropriate design and integration with the streetscape / built form. Where ventilation is required for basement car parks, vehicles should be screened and landscaped.

Roofs will be designed to be integrated into the overall façade and composition of buildings and provide enclosed places for the screening of plant and service equipment (if not provided in basements) in locations away from living areas that do not visually detract from the amenity of adjoining spaces.

Landscaping will contribute to the high quality of the adjacent public areas, open space and streetscapes. Car parking areas that are not visible from public spaces will be shared and consolidated. Commercial uses in residential developments will be restricted to those associated with the respective building function.

Public promenades will incorporate public art, which is easily identifiable and fully integrated into the public environment.

(my underline)

The proposal satisfies the relevant parts of the Desired Character of Policy Area 15 because:

- It provides high density living opportunities via apartment living thus providing greater choice of residential accommodation with a mix of 2 and 3 bedroom apartments.
- The proposal expresses a contemporary and pleasing high quality architectural form and contributes positively to the quality public realm through:
  - > An activated base with apartment and tourist accommodation entry points clearly visible to the street.
  - > A high level of activation on the building generally with 20 apartments/tourist accommodation rooms having direct views over Canning Street.
  - > A ground floor setback and space for pedestrians and space for landscaping, public realm style seating, and bike parking facilities.
  - > Use of textured and reclaimed materials that provide for a pleasant and interesting ground floor area for pedestrians and a connection to local building fabric and heritage.
  - > A concealed car parking area that is not visible to the land's primary road frontage.
  - > A concealed location for services and waste storage.



**Image 1: View of the Canning Street façade at street level (looking south-west)**



**Image 2: View of the Canning Street façade at street level (looking south)**

- The building height will desirably contrast with the nearby open space and be within the height range generally sought for the Zone being 7 storeys (and the Zone seeks development between 3 - 12 storeys in height).



- The proposal is acceptably respectful of its local context noting that surrounding built form is either comprised of high-density apartments, a motel use, or low scale residential but within the same Zone/Policy Area/Precinct wherein the same forms of development as proposed herein are contemplated.
- The proposal does not impede any notable vistas, nor will it impede the local road network from continuing to function in an orderly manner as confirmed by Infraplan.
- Regarding the more specific design related policies above the proposal will:
  - > Provide setbacks and wall lines that vary to provide meaningful articulation. Light and shade is created by the use of the projecting concrete slabs that act like eaves over the apartments below on levels 4-7.
  - > Via the provision of balconies on levels 4-7, the Canning Street elevation has a lighter feel above the heavier base associated with levels 1-3.
  - > Achieve a balance with respect to the use of both solid and glazed building materials such as concrete, brick and tinted glass all evident.
- The proposal does not feature any basement or undercroft parking, rather it is at-grade. However, it is designed in such a way that it does not detract from the adjacent public realm through its location and screening measures.
- Services are not located within enclosed roofs however are not visible to the street being sited at the rear of the land behind the building within a designated service yard area.
- High quality landscaping features are proposed including:
  - > Along the Canning Street frontage in planters and upon the main façade.
  - > Within a designated landscaping strip along the western boundary (toward the street).
  - > Within a designated landscaping strip along the southern boundary.

Further, the Desired Character of Precinct 4 also states:

**Development within Precinct 4 Five Storey will be predominantly in the form of residential flat buildings, serviced apartments and tourist accommodation of up to 5 storeys (or 18.5 metres) in height. Development may also include small scale non-residential uses such shops, restaurants and cafes, offices and consulting rooms at ground and first levels where site conditions permit.**

**Development will be of the highest architectural standard and contribute positively to the public realm through establishing clearly defined space between buildings on adjoining sites and building design that incorporates articulated facades and built form elements including balconies to create light and shadow. Building design will complement the scale, proportions, siting and materials of the existing heritage places in the locality.**

Development on land fronting the South Esplanade immediately adjacent Precinct 5 Twelve Storey may accommodate additional building height over 5 storeys to achieve a transition in scale from the taller building anticipated in Precinct 5, down to the 5 storey scale anticipated in Precinct 4, provided buildings are designed to minimise any impacts on adjoining land within Precinct 4 or adjoining residential zones.

Development directly adjoining Sturt Street should not exceed 2 storeys in height to order to achieve a transition down to low scale at the interface with the adjoining Residential Character Zone.

(my underline)

In response to this policy, I note:

- It is evident the policies call for the same kinds of uses proposed in this development.
- At 7 storeys at 23.5 metres, the proposal exceeds the 5 storey or 18.5 metre height limit. Such a variation is acceptable in my view given the conditions of the locality, the location at the 'edge' of the 12 storey/43 metre wall height area (i.e. Precinct 5) and the proximity of the land to the Residential Zone. Further justification is provided for this within a following section of this report.
- The nearby Patawalonga Reserve is a Local Heritage Place (including the Reserve and area occupied by the water body between Adelphi Terrace and the Patawalonga frontage). Development in the Zone ought to complement various aspects of built form of heritages places however, as this listing does not relate to build form and to open space, this provision is not directly applicable.

## 5.4 Design and Appearance

This section deals with the pertinent Design and Appearance provisions in the General Section as well as those contained within Urban Glenelg Policy Area 15.

### 5.4.1 Reflectivity of Materials

The proposal will satisfy Design and Appearance Principle 3 as there will not be any highly reflective materials or finishes upon external walls or roofs.

### 5.4.2 Building Entries

The building entrances to both uses will be:

- Orientated toward the street.
- Clearly visible/identifiable.
- Provided with shelter, personal address and transitional space in/around the entry to facilitate comfortable movement and space for pedestrians.

Policy Area 15 Principle 2 is satisfied.

### 5.4.3 Balcony Design/Privacy

In satisfaction of Policy Area 15 Principle 3:

- The balconies are highly functional in that they are all 2.0 metres in dimension.
- Only 1 of the 5 main balconies on each floor is not facing either north, east or west therefore solar access levels are high. The two tourist accommodation terraces also face west.
- The balconies are neatly integrated into the form and detail and architecture of the building.
- Several balconies face the street to contribute to passive surveillance and the activation of the public realm.





- Main balconies areas are located adjacent living areas of the dwellings.
- Given their transparent balustrades, the building features balconies which are designed, where able, to take advantage of views of the Patawalonga waterway.
- The location of the land means there are no privacy issues resulting from balconies upon low scale housing outside the Residential High-Density Zone.

## 5.5 Building Height

The pertinent height provisions are as follows:

### **Desired Character of Policy Area 15**

**Development within Precinct 4 Five Storey will be predominantly in the form of residential flat buildings, serviced apartments and tourist accommodation of up to 5 storeys (or 18.5 metres) in height. Development may also include small scale non-residential uses such shops, restaurants and cafes, offices and consulting rooms at ground and first levels where site conditions permit.**

**19 Development should not exceed an external wall height of 18.5 metres above natural ground level (excluding lift service levels and gables).**

The height of buildings should be treated like any numeric guideline with the Development Plan. Consideration should be given to its intent and planning purpose and to the planning consequences of the guideline not being met on the locality surrounding the land.

One can only speculate as to why there was a decision to have a 5 storey height limit imposed upon this part of the Residential High Density Zone. It may have been because of the location of this part of the zone relative to lower scale, low density housing areas.

The proposal was originally presented to the planning authority as an 11 storey building, then reduced to 8 storeys and it is now lodged at 7 storeys.

The building will have a total wall height in the order of 23.5 metres however this is measured to very top of the building. Given the design approach to have a roofing material at Level 7 one could argue the wall height is in fact 20.35m as above this height is a steel roofing material – the only difference is that it is vertically composed rather than having a slight pitch to it.

Had this level had a slight angel/pitch (aka like a mansard style roofing element) the wall height of the proposal would be 20.35 metres and only therefore 1.85 metres above the guideline. It was decided not to apply a mansard approach (even though the compliance with the development plan would have been improved) for aesthetic reasons.

The height is well below the Airport Building Heights maximum identified on Overlay Map HoB/2.

The proposal does exceed the wall height guideline of the Precinct 4 which is 18.5 metres (which excludes lift service levels and gables). However, in terms of broader and immediate context, we note that:

- Approval has been received for a 6-storey building at 22 Adelphi Terrace and we understand a proposal is going through assessment for a 7-storey scheme at 19-21 Adelphi Terrace – both sites fall into the same Precinct as this development.

- The land sits at the edge of a precinct wherein 12 storey and 43 metre wall heights are envisaged.
- There is an approval for a 14-storey mixed use development comprising hotel and shops on the immediately adjacent motel site to the north-west of the land.

The construction of a shorter building on the land has little planning benefit as we see it and would arguably mean a more sudden drop in height from the adjoining 13 storey development and any future 14 storey development on the adjacent motel site. Arguably, the design outcome is improved with a building taller than 5 storeys as the designed scheme provides a better transition to eventual and future circa 5 storey/18.5 metre high development that over time will occur to the east of the land within Precinct 4.

The building is visually attractive, articulated, and has incorporated domestic and durable materials and the 'top' level is clad in a darker roof-type steel material to provide contrast, be visually recessive and given the impression of a roof element.

The proposal incorporates ESD measures; the proposal incorporates good levels of ground level activation and spaces which integrates positively with the surrounding public realm; the proposal incorporates high quality landscaping features; and the apartments have high quality internal amenity with large internal spaces and balconies with outlook and views.

Further, the height of the development does not give rise to residential amenity impacts by way of overlooking or overshadowing upon low density housing outside of the zone.

Finally, and not critical in this argument, but in theory it would be possible to have a building that has an 18.5 metre wall and a gable roof on top and satisfy the policy – one can only speculate how tall a building of this kind would be however such a building may indeed not be dissimilar in pure height terms to this proposal at about 23.5 metres – see enclosed diagram within **Appendix K**.

For the above reasons, in my view, the height of the development is acceptable in the circumstances of the land and locality.

I also defer to the expert legal findings of Mr Tom Game who has provided an opinion on the suitability of the proposed height factoring in relevant case law and circumstances of the land and locality.

In summary, Mr Game holds the view that *“an exceedance of the height guidelines is likely to be appropriate and justified in the circumstances of the proposed development”*.

(underlining added)



## 5.6 Private Open Space

Principle 12 within the zone provides open space requirements as per Table 2 below:

**Table 2: Private Open Space Requirements**

Configuration	Open space requirement, other than for affordable housing
Studio (without separate bedroom)	No minimum requirement
One-bedroom	8 square metres
Two-bedroom	11 square metres
Three-bedroom or greater	15 square metres

The proposed area of private open space available to each dwelling is displayed in **Table 3** below:

**Table 3: Available Private Open Space**

Apartment Type	Number of Bedrooms	Open Space Requirement (square metres)	Available Private Open Space (square metres)
Type A	2	11	17
Type B	2	11	11
Type C	2	11	11
Type D	3	15	17
Type E	2	15	17

In addition, I note that each dwelling has excellent access to Glenelg Beach, the Patawalonga Frontage, Colley Reserve and Mosely Square all of which have large open public spaces.

## 5.7 Medium and High-Rise Development

Medium and High Rise Development Objective 1 is satisfied in that the proposal provides greater housing choice and short term employment opportunities in the construction industry and over the long term for a variety of businesses (waste contractors, cleaners and building maintenance professionals).

Objective 2 of this section is satisfied because the apartments will have a high standard of internal living amenity and because the building caters for a variety of accommodation needs given there is some diversity in dwelling typology to cater to different sections of the community.

Objective 4 of this section is satisfied because the building is designed and sited so as to be energy and water efficient as explained in more detail in a following section of this report.

### 5.7.1 Design and Appearance

In response to Principles 1, 2 and 3 of this section:

- The building achieves a human scale at street level through its 3 level podium in a brick material.
- It also provides shelter for pedestrians.
- The ground level provides for surveillance from public land to the inside of the building at night.
- The entry point is clearly visible to the street as previously noted in this report.

### 5.7.2 Building Separation and Outlook

In response to Principle 5 of this section:

- All dwellings and their balconies will have 'adequate separation' between habitable room windows and balconies of other buildings so as to provide visual and acoustic privacy for occupants and allow for infiltration of daylight into interior and outdoor spaces.
- The land has the benefit of two road frontages. As such the Canning Street and Laycock Lane sites of the buildings will not be 'boxed-in' by future redevelopment. In this regard:
  - > The land is some 16 metres from land to the north which may be developed with apartment building type development.
  - > The land is about 5.5 metres from land to the east which may also be developed with apartment building type development.
- Land to the south contains a two storey dwelling meaning the majority of the building is not enclosed. In the event future development of that land occurs, there remains reasonable separation provided along the southern boundary, with the main wall about 3 metres (on average) from this boundary.
- The 13 storey apartment building to the west is well setback from Canning Street and as such, its position has limited impact on outlook of the proposed building.

Further, the proposal forgoes ordinary residential zone type privacy screening measures in favor of providing apartments and rooms with high quality outlook and such an approach is considered acceptable on the basis that the proposal will not give rise to overlooking issues onto lower density housing in an adjoining zone/policy/precinct, which is the correct approach as per Policy Area 15 Principle 5:

- 5 Building design should minimise the impact of overlooking and overshadowing on existing lower density and scale development in adjoining zones/policy areas/precincts.**

(underling added)

### 5.7.3 Dwelling Configuration

In response to Principle 6, the proposal provides diversity of dwelling sizes as previously explained, with a mix of 2 and 3 bedrooms dwellings of varying sizes and shapes.



#### 5.7.4 Adaptability

In response to Principle 8, I note that there are no internal structural/load bearing walls meaning that the building has a flexible design that can provide for adaptation into more tourist accommodation rooms or more dwellings.

#### 5.7.5 Environmental

In response to Principles 9 and 11:

- The roof design can facilitate the easy installation of PV panels as per the roof plan.
- Rainwater harvesting and re-use will be integrated into the building to reduce mains water demand.
- The exterior of the building will not generate glare or reflect light so as to affect the temperature of any land in the locality.
- The building is not a sheer wall and the combination of balconies, protruding building elements and varied setbacks will assist to deflect wind flows and prevent excessive tunneling effects.

In addition to the above, other environmental considerations to be applied to the building are referenced within the ESD Summary Report prepared by Sustainability House, as per **Appendix G**.

#### 5.7.6 Site Facilities and Storage (including Waste)

Each apartment will have storage spaces within and outside their apartments. The areas available to each type of dwelling range between 9.5-10.1m<sup>3</sup> exceeding minimum requirements.

Furthermore:

- Specific storage is available at ground level for building infrastructure and other services. This space may contain electrical equipment or other infrastructure. This storage room will be screened from public and occupant view.
- Specific storage area is available at ground level for bins. This storage area will be situated in a locked room out of site from the public and building occupants.
- A waste management report has been prepared by Infraplan. Waste collection will occur in accordance with Infraplan's waste management plan. Infraplan advise:
  - > Five x 660 litre bins will be required with two collections per waste stream, per week.
  - > Swept path diagrams confirm that collection can occur on-site.
  - > Collections should occur outside of peak periods (7-9am, 3-6pm) to minimise impacts on surrounding properties and peak hour traffic. a condition may be imposed to this effect.



## 5.8 Energy Efficiency

The Development Plan encourages the conservation of energy and on-site power generation by way of photovoltaic cells and wind power. This can be achieved by

- Providing solar access to buildings and open space.
- Ensuring natural light to main activity areas.
- Public lighting should be designed to use renewable energy.

The proposal retains sufficient space for the provision of photovoltaic cells as shown on the roof plan.

All apartments will receive natural light. All apartments can be naturally ventilated and high-performance insulation will be applied to the building. It is intended that the building receives a 7-star rating with respect to environmentally sustainable design.

Other environmental considerations to be applied to the building are referenced within the ESD Summary Report prepared by Sustainability House, per **Appendix G**.

## 5.9 Car Parking

The applicant has engaged Infraplan to undertake an assessment of anticipated traffic and proposed car and bicycle parking. I note the following from this report:

- Based upon the relevant car parking rates of the Development Plan, the proposal has a theoretical parking demand of between 32-37 spaces.
- 40 access-controlled spaces are proposed in the car park off Laycock Lane which is 3 spaces more than the minimum requirement of the Development Plan.
- The removal of the driveway access at the subject site from Canning Street will result in two additional on-street car parking spaces.

## 5.10 Bike Parking

Infraplan says as follows regarding bike parking:

*“there are no specific requirements for bicycle parking at residential apartments in the zone. It is anticipated that residents will store their bicycles in their apartments, and therefore the lift has been designed to easily accommodate bicycles.*

*In addition, an area near the front entrance of the building provides convenient, well-lit parking for residential bike-share, and visitor bicycle parking. Seven parking racks are proposed, providing space for 14 x bikes”*



## 5.11 Access and Vehicle Trips

The proposal will use the 'WHORD Combilift 543' vertical stacker system which provides car parking on 3 levels. As noted in the Infraplan report, cars enter at ground level and then the system automatically stacks and shuffles the car as required. Details of the operation of the car park are provided in the 'car park layout' section of the Infraplan.

On trip generation, Infraplan advises as follows:

*"Canning Street is an 11m wide, residential street, with parallel parking (no time limit) on both sides. Traffic data has not been collected but observations indicate that Canning Street has more than sufficient capacity to carry the additional traffic generated from this development.*

*Laycock Lane is 5.4 metres wide and a no-through-road, providing rear access to 5 other properties. It is therefore considered appropriate for car park access to the subject site".*

## 5.12 Acoustic Assessment

Environmental noise emissions from the proposed development should comply with the *Environment Protection (Noise) Policy 2007* (Noise EPP).

Resonate Consultants were asked to consider the noise of the proposed car stacker system. Their assessment provided recommendations on amendments to the building to ensure the relevant policy is met.

Those recommendations from Resonate Consultants have been adopted such that the proposal complies with the *Environment Protection (Noise) Policy 2007* (Noise EPP).

## 6.0 Conclusion

The proposal is considered to be sufficiently in accordance with the Development Plan because the development will:

- Provide considerable investment into the area of some \$7.8 million (including fit out).
- Make use of existing infrastructure and services as are available.
- Not be prejudicial to the use and operation of any nearby uses.
- Delivers on the zone objectives with high density housing and tourist accommodation land uses.
- Provides a mix of 2 and 3 bedroom apartments.
- Provide a contemporary, attractive, architectural design response and a building that is articulated and uses high quality and durable materials and finishes that are not reflective to cause glare.
- Conceal its car parking, waste store and services.
- Provide a clear and identifiable entry point to each use.
- Provide functional balconies that provide casual surveillance of the street and views/outlook for occupants with reasonably good sunlight access for most main balconies.
- Exceed the building height but is not materially larger/bulkier than a building that could in theory comply with the numeric standard if it was built per the policy (see **Appendix K**) and provide:
  - > A wall height only 1.85 metres over the policy if excluding the level 7 element which is composed of a roof material and some 20 metres lower in height than that which is envisaged in the adjoining Precinct 5 area.
  - > A suitable transition to the adjoining 12 storey Precinct 5 area and an existing 13 storey building and potential future 14 storey building on the adjacent motel site.
  - > A building is visually attractive, articulated, and has incorporates domestic and durable materials that are well suited to its coastal context.
- Incorporate ESD measures.
- Incorporate good levels of ground level activation and transitional space for pedestrians.
- Incorporate landscaping features.
- Provide compliant levels of private open space and storage space.
- Provide compliant levels of car parking for residents, tourists and visitors.
- Provide adequate bike parking, car parking and safe and convenient access.
- Provide adequate on-site waste storage.
- Provide appropriate waste collection strategy that will not disrupt flow of traffic on Laycock Lane.
- Not cause undue noise impact through its car stacking system and will satisfy the *Environment Protection (Noise) Policy 2007* (Noise EPP).

For all of the above reasons, I consider the proposal to merit Development Plan Consent.



# Appendix A

## Certificate of Title







# Appendix B

Locality Plan prepared by URPS





# Appendix C

**Plans prepared by Brown Falconer**





# Appendix D

**Traffic and Parking Report, and WMP, prepared by Infraplan**







# Appendix E

Landscaping plan prepared by LCS





# Appendix F

**Legal opinion prepared by Botten Levinson Lawyers**





# Appendix G

**Sustainability report prepared by SuHo**







# Appendix H

**Opinion from Pyper Leaker Surveying Services regarding the titling associated with the Stacker System**





# Appendix I

**Stormwater management plan prepared by Structural Systems**





# Appendix J

## Resonate Consultants acoustic assessment



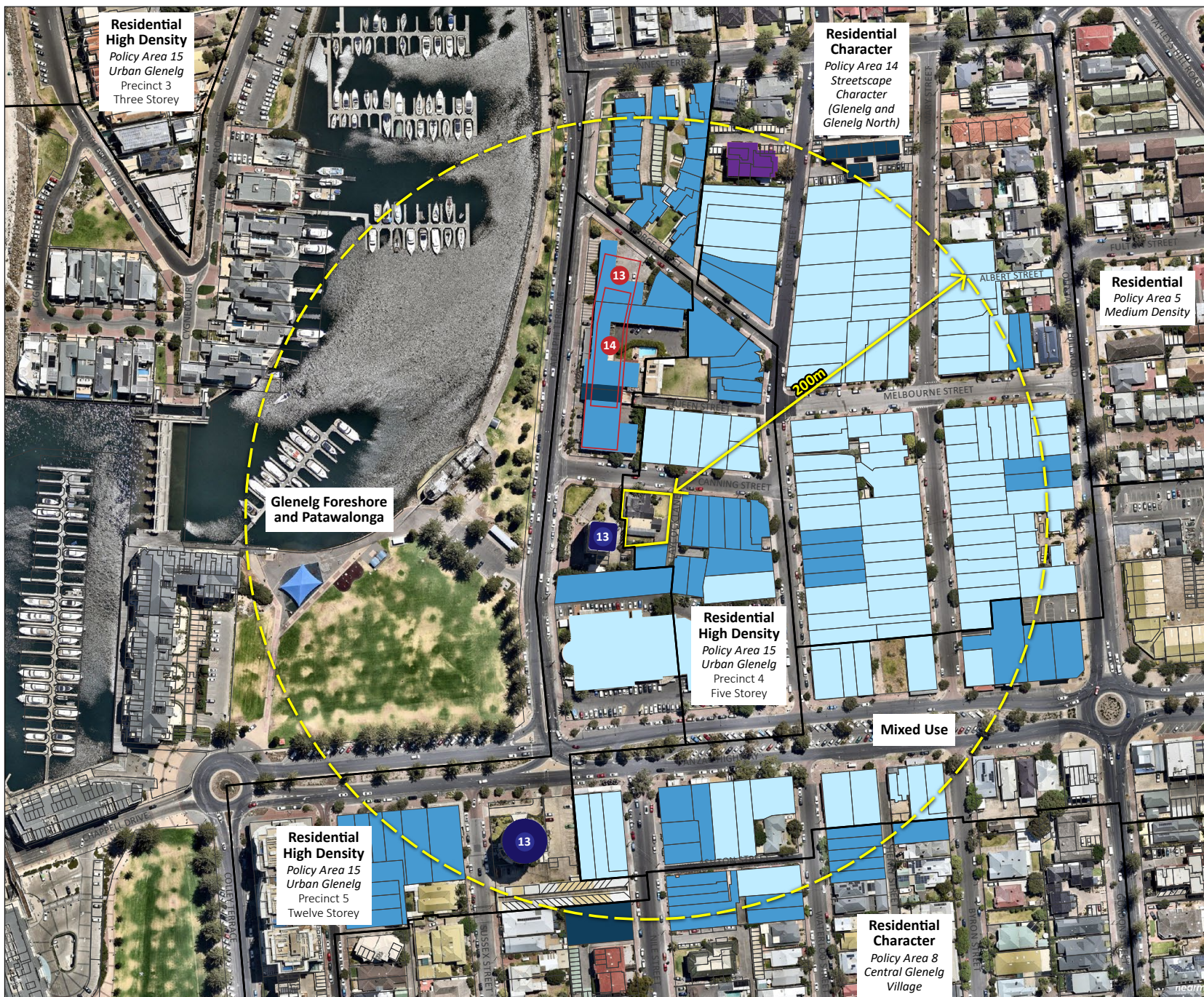




# Appendix K

Height analysis prepared by URPS





#### LEGEND

- SUBJECT SITE
- CADASTRE
- ZONE / POLICY AREA / PRECINCT BOUNDARY
- SINGLE STOREY
- TWO STOREY
- THREE STOREY
- FOUR STOREY
- OVER FOUR STOREYS
- PROPOSED BUILDING HEIGHT



0 20 40 80m  
SCALE: 1:2,000 @ A3

#### LOCALITY

**2 CANNING STREET,  
GLENELG NORTH**

JOB REF. 19ADL-0070  
PREPARED BY. MP  
DATE. 13.03.19  
REVISION. 1  
DATA SOURCE. Nearmap (27.01.19)  
data.sa.gov.au





Our ref: THG/219113

19 June 2019

Matthew King  
Urban & Regional Planning Solutions  
Suite 12  
154 Fullarton Road  
ROSE PARK SA 5067

**By email: [matthew@urps.com.au](mailto:matthew@urps.com.au)**

Dear Matt

**Approach to assessment of proposed development at 2 Canning Street, Glenelg North**

You have sought my advice on the relevant considerations and correct approach to assessing and determining a development application for a proposal that exceeds the relevant height guideline contained in the Holdfast Bay Council Development Plan consolidated 2 June 2016.

The proposed development involves a mixed use building (comprising residential dwellings and tourist accommodation) of 7 storeys (above-ground) with a total height in the order of 23.5 metres.

Having regard to the Development Plan and the factual context of the proposed development, I am of the view that the proposed exceedance of the height guideline in the Development Plan is likely to be justified in the circumstances.

**Approach to assessment generally**

As a starting point, it is important in assessing a development application to be mindful that no provision of a Development Plan should be read in isolation, and that the provisions of the Development Plan are not mandatory; rather they are guidelines which the statutory scheme requires assessment against (not strict compliance with)<sup>1</sup> and accommodates an acceptable degree of departure from<sup>2</sup>. They must also be applied having regard to the factual circumstances of the proposed development.

The provisions of Development Plans should not be applied mechanically.<sup>3</sup> In assessing the proposed development, and the height limit exceedance, it is necessary to consider the effect of any departures from the quantitative provisions in the context of the qualitative goals.<sup>4</sup>

The relevant principles and reasoning in the following cases are well established and bear rehearsing (**emphasis added**).

<sup>1</sup> *Development Act 1993*, s 33(1)(a).

<sup>2</sup> *Development Act 1993*, s 35(2).

<sup>3</sup> *Lakshmanan & Anor v City of Norwood, Payneham and St Peters & Anor* [2010] SASCFC 15 at [45] and [47].

<sup>4</sup> *Gibbs v City of Charles Sturt* [2010] SASC 26 at [22].

In *Development Assessment Commission v A&V Contractors Pty Ltd*, the Full Court of the Supreme Court stated:

*Objectives and principles are generally stated on a council wide and zone basis, by reference to particular classes of developments, and on occasion by reference to particular sites. Moreover, the objectives and principles are directed towards a wide range of planning objectives. Therefore, there will necessarily be a degree of tension between the provisions of development plans. Some principles and objectives may militate for a development and others militate against it. Nonetheless, a proposed development must be assessed against all of the provisions of a development plan which, on their terms, apply to that development.*<sup>5</sup>

Regard must also be had to the particular factual circumstances of an application:

*... planning authorities do not apply the objectives and principles of development plans in a vacuum. First, as I earlier observed, there will often be tension between those objectives and principles. Most of the objectives and principles, as a matter of construction, apply as general rules and not as inviolable prescriptions; they are guidelines within which an expert planning judgment must be made. Most obviously, the particular factual circumstances of a proposed development will inform that planning judgment, and, in particular, affect which of the principles and objectives will predominate.*<sup>6</sup>

On the task of exercising the planning judgment, the Full Court of the Supreme Court in *Lakshmanan & Anor v City of Norwood, Payneham and St Peters & Anor*<sup>7</sup> cited Justice Debelle in *City of Mitcham v Freckman*<sup>8</sup>:

*In cases such as this, where the proposed development is neither a complying or a non-complying development, that is to say, where the Plan neither permits nor prohibits the proposed development, the task of the planning authority is to weigh the benefits and detriments, in other words, to weigh "the pros and the cons" of the proposed development by reference to the Plan. Ultimately, the planning authority must make a judgment whether the proposal will be permitted. If it is to grant the application, it must be satisfied that there are sufficient reasons for that decision, reasons which are based on acceptable principles of planning and the relevant provisions of the Plan. That judgment will have regard to the factors mentioned above. Thus, a proposed development might be approved if it is conducive to the objectives and desired character of the zone in which it is to be sited. It will be relatively easy in some cases to decide that the proposal is quite compatible with the amenity of the locality. In other cases, that test will be more difficult. It will be a question of fact and degree in every case, after weighing all relevant considerations, whether the proposed development should be approved.*<sup>9</sup>

The combined operation of the above principles establish relevantly that SCAP must exercise a planning judgment which considers the merits of the proposal as a whole for which the Development Plan (and all of its relevant provisions) will be a guide.

---

<sup>5</sup> [2011] SASCF 21 at [72].

<sup>6</sup> Ibid, at [77].

<sup>7</sup> [2010] SASCF 15 at [45].

<sup>8</sup> [1999] SASC 234.

<sup>9</sup> Ibid at [19].

If SCAP were to refuse the proposal, based on a departure from a PDC only (in this case the height limit exceedance), without undertaking an assessment of, and making a planning judgement on, the merits of the proposal as a whole, SCAP could not be said to have properly exercised the "planning judgment" required of it by law.

**When will a departure from the provisions of a Development Plan be justified?**

The relevant height limit provisions in the Development Plan are contained in the desired character statement for "Precinct 4 Five Storey" which provides:

*Development within Precinct 4 Five Storey will be predominantly in the form of residential flat buildings, serviced apartments and tourist accommodation of up to 5 storeys (or 18.5 metres) in height.*

Also, PDC 19 in the precinct specific section of the Residential High Density Zone's Principles of Development Control provides:

*Development should not exceed an external wall height of 18.5 metres above natural ground level (excluding lift service levels and gables).*

As noted previously, the statutory scheme<sup>10</sup> accommodates departures from the provisions of a Development Plan. In *Town of Gawler v Impact Investments Corporation Pty Ltd*<sup>11</sup> the Full Court of the Supreme Court set out the following factors which form a guide for determining whether a proposal's departure from the provisions of a Development Plan is justified (footnotes in judgment omitted):

1. *The language of the principle or principles concerned – whether it is direct or contemplates some flexibility in approach;*
2. *Whether the relevant principle is in conflict with some other applicable planning principle. That is likely to happen only rarely, in which case the more specific principle may displace the more generally expressed principle;*
3. *The evident purpose and objective of the policy expressed in the principle or principles concerned;*
4. *The significance of the policy to this particular Development. The clearer the policy in its application to a particular development, the more compelling the reasons for departing from the policy will need to be;*
5. *Where the policy contemplates possible degrees of compliance, the extent of the Development's compliance with the policy;*
6. *Consistency of the Development with other objectives and purposes of the Zone;*
7. *Whether there is something unusual about the Development or the land on which it is to take place which makes the policy inapplicable or inappropriate;*
8. *Whether other events have happened since the Development Plan was adopted which make the policy redundant, either generally or in respect of this particular development;*

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<sup>10</sup> See *Development Act 1993*, ss 33(1)(a) and 35(2).

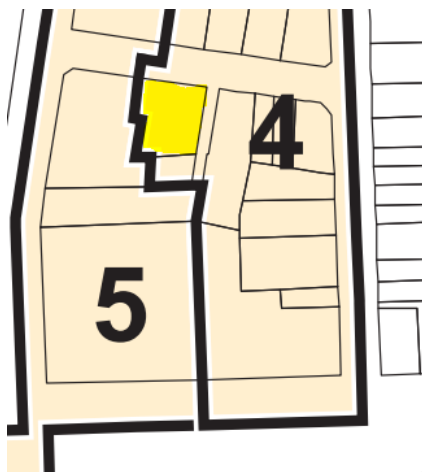
<sup>11</sup> [2007] SASC 326.



9. *The probable effect of non-compliance with the policy on the planning objectives of the Zone; and*
10. *Whether non-compliance with the policy in this case is likely to encourage other non-complying developments in the Zone.*<sup>12</sup>

It is prudent now to briefly consider each of the 10 factors in *Impact Investments* against the current proposal and the relevant Development Plan provisions. Adopting the numbering in *Impact Investments*, I consider:

1. The language in PDC 19 is inherently flexible in nature due to the use of the word "should" as opposed to, for example, "must" or some other term connoting that the requirement is mandatory or without flexibility.<sup>13</sup>
2. This factor is not applicable for present purposes.
3. The evident purpose of the relevant height limits is to regulate building height on a precinct-by-precinct basis throughout the Urban Glenelg Policy Area. Further, it is uncontroversial to suggest that the underlying purpose of the precinct-by-precinct height limit structure is to deliver a planned and co-ordinated distribution of high-density residential development, according to precinct-specific heights, throughout the Urban Glenelg Policy Area.
4. The height limit applies clearly to development within each precinct of the Urban Glenelg Policy Area. However, the proposal's unique location, in a pocket of Precinct 4 (5 storeys or 18.5m) surrounded on all sides (except one) by Precinct 5 (12 storeys or 43m), is a particularly compelling planning reason to depart from the prescribed height limit in order to deliver a co-ordinated transition in building heights within that pocket (**transition-pocket**). An extract from the Development Plan (Precinct Map HoB/4) showing the transition-pocket with the subject site marked is set out below.



5. While the numerical degree of exceedance of the height limits is not considered minor (i.e. a nominal exceedance), the unique factual scenario of the proposal's location should weigh heavily in SCAP's consideration of the numerical degree of exceedance and the weight it gives to that exceedance when exercising its "planning judgement" over the development as a whole.

<sup>12</sup> Ibid at [81]. See also *Yuile & Anor v The City of Unley & Anor* [2009] SAERDC 55 at [32].

<sup>13</sup> See Doyle CJ in *Town of Gawler v Impact Investments Corporation Pty Ltd* [2007] SASC 326 at [38].

6. But for the proposal's height, I understand that it will be generally consistent with all other applicable provisions of the Development Plan. Importantly I note that the proposal delivers precisely the type of development use that is envisaged in the Zone.
7. The fact of the proposal being sited in the transition-pocket diminishes the weight to be applied to the generic precinct-wide height limits. The transition-pocket, in my view, requires an assessment of building heights on a merits basis in order to achieve a sensible transition between the 2 precincts. Without a sensible transition, residential buildings in the transition-pocket will be surrounded on 2 or 3 sides by residential buildings that are potentially more than twice as tall (5 storeys versus 12 storeys).
8. This factor is not applicable for present purposes.
9. I understand that the proposed height exceedance would not have any adverse effects on the planning objectives of the Zone. For example, the additional height will not lead to unreasonable additional overlooking or overshadowing or result in an adverse impact on the streetscape. I understand you are of the view that the extent of the height exceedance achieves a sensible planning outcome in terms of a planned and co-ordinated stepping down of building height in the transition-pocket.
10. Plainly the height exceedance here, which is proposed based on its location in the transition-pocket, is not apt to be repeated carte blanche across the Zone and will not set a precedent for over-height developments. Every development will need to be assessed in the context of its particular factual circumstances.

For completeness, and further to the above considerations, I note that the Development Plan, in the precinct-specific provisions<sup>14</sup> and elsewhere<sup>15</sup>, envisages scenarios at the interface of zones and precincts where general application building controls are apt to change in order to create areas of transition. Such a transition area is explicitly established in Precinct 4, albeit to the south of the subject site. Nevertheless, the fact of the provisions establishing transition areas is evidence that the harmonious transition at the interface of zones and precincts is a compelling planning outcome implemented on occasion in the Development Plan where the locality context demands. It is plain, in my view, that the locality of this proposal warrants a sensible and more gradual transition between the 12 storey policy area and the 5 storey policy area.

### **Amenity impacts at the periphery of the Precinct**

As part of SCAP's assessment and the exercise of its planning judgment, it will consider the amenity impacts caused by the proposal. To the extent that the building's height (and the degree of exceedance) causes amenity impacts it should be noted that the courts have acknowledged that the integrity of a zone (and the impacts of development ordinarily anticipated in that zone) might be very different at the centre of the zone when compared to its periphery.

---

<sup>14</sup> Residential High Density Zone, Urban Glenelg Policy Area 15, Precinct 4 Five Storey PDC 20(c).

<sup>15</sup> Residential Zone, Institutional Policy Area 4, Objective 4 and Minda Incorporated Brighton Campus desired character.

In *Papadopoulos v Corporation of the City of Woodville* (1985) 39 SASR 569 at p 577, the Supreme Court said:

*... it must be remembered that zone boundaries are only lines on a map, and the residential integrity of a residential zone at its perimeter might be very different from its residential integrity elsewhere. Lines on a map cannot prevent noise or smoke or smells or the visual or other impact of non-residential development from escaping across the boundary between a non-residential and a residential zone...*

In *Lanzilli Holdings v City of Campbelltown* (1982) 38 SASR 81 at p 85, the Supreme Court said:

*... the amenity of the locality ... has to be judged by reference to the locality as a whole, and not by reference only to the houses located closest to the Industrial Zone ... the amenity of such a locality is not to be measured by the standards appropriate to a solely residential zone, and the amenity and convenience of those who choose to live on the very boundary of the Light Industrial Zone ought not necessarily to be regarded as the appropriate standard of amenity and convenience for the locality as a whole...*

In *Bond v City of Norwood, Payneham & St Peters* [2007] SAERDC 56 at [61], the ERD Court said:

*... Lanzilli Holdings and Papadopoulos are, in my view, authorities for the proposition that the amenity expectations of those who reside in zones within which commercial or residential activities are envisaged, or even on the periphery of a residential zone in close proximity to a commercial or industrial zone, cannot equate with those of residents in the heart of residential zones. Were that not to be the case, commercial and residential activities located in zones within which such uses were sanctioned could potentially be seriously restricted by the application of residential amenity standards having their origin in zones devoted solely to residential uses.*

Considering the logic of the above body of case law, it follows that the amenity expectations at the centre of Precinct 4 will not necessarily equate with expectations towards the periphery of Precinct 4 bordering the more intense Precinct 5.

### **Measure of height limits: storeys versus metres**

I note that there is a potential ambiguity under the relevant provision about whether the applicable height limit should be the measured in storeys or metres.<sup>16</sup> In my view, the 18.5m external wall height limit should be preferred to the 5 storey limit.

Determining height limits by reference to storeys is an imprecise and ambiguous method of measuring height. There is, of course, no agreed figure of how high a single storey might be and the height of a particular storey can vary depending upon the nature of the use.

The ambiguity of the terminology of “storeys in height” has frustrated the ERD Court on a number of occasions.

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<sup>16</sup> Noting that the relevant height provisions variously provide both storey and metre height limit.

For example, in *Frederick Snowden Pty Ltd v City of Unley*<sup>17</sup> the ERD Court observed:

*Provisions designating height limits in terms of 'storeys' are unsatisfactory, given that there is no fixed height for a storey, and there may be debate in relation to levels which are partly under natural ground level, or mezzanine levels, as to what constitutes a storey.*<sup>18</sup>

In *Mila Enterprises Pty Ltd v City of Holdfast Bay and Hutchens*<sup>19</sup> the ERD Court observed that "the use of the number of storeys as a means of defining the height of a building is imprecise and problematic"<sup>20</sup>.

Also of relevance, in *Pawmac (No 1) P/L v Corp. of City of Adelaide*<sup>21</sup> the ERD Court said of height limits:

*... It is important to keep in mind the purpose for which a building height limit is prescribed. It is sensible to assume that the height limit is to be measured from the perspective of a viewer of the proposed building, as it is a prescription limiting the mass of the built form...*<sup>22</sup>

Considering the above, as a general premise the height limit in metres should be applied in preference to the storeys limit where both are proffered. The relevant height guideline in this case is an external wall height of 18.5 metres. It is not a guideline for the total height of buildings.

The reference to "5 storeys" in the desired character statement should be considered as only a general and convenient descriptor of the scale of development expected in the policy area.

## Conclusion

Having regard to the above, an exceedance of the height guidelines is likely to be appropriate and justified in the circumstances of the proposed development. That is ultimately a matter to be determined in balance with an assessment of all other aspects of the development against all of the relevant provisions of the Development Plan.

I trust this advice assists.

Yours faithfully



**Tom Game**  
**BOTTEN LEVINSON**  
Mob: 0419 809 361  
Email: thg@bllawyers.com.au

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<sup>17</sup> [2003] SAERDC 96.

<sup>18</sup> Ibid at [19].

<sup>19</sup> [2005] SAERDC 34.

<sup>20</sup> Ibid at [31].

<sup>21</sup> [1998] SAERDC 539.

<sup>22</sup> Ibid at [3].

Barrio Developments  
Attn: Glen Vollebregt

13<sup>th</sup> June 2019

Hi Glen,

The cleanest and most efficient way of dealing with the car stacker is as I suggested in my earlier email. We would create 2 separate areas that can be referred to in the by-laws. The plan would label the areas as "Car Stacker". The Unit holders would have rights to use that particular area of at the exclusion of other Unit owners. I have attached an example C40885.

Regards,

David Pyper

**Licensed Surveyor**  
Pyper Leaker Surveying Services Pty. Ltd.

C40885

SHEET 2 OF 7

53844\_pland\_1\_V02\_Version\_4

BEARING DATUM: ① - ② 355°07'40"  
DERIVATION: D111330 ADOPTED

TOTAL AREA : 337m<sup>2</sup>

CIRCUIT

WATERFORD

EAST  
PARKWAY

FOUR LEVEL  
BUILDING

ELEVATION

BALCONY

BALCONY

GARAGE

CUNNINGHAM

LANE

355°07'40"  
19.35

40°07'40"  
8.70

83°07'40"  
6.85

27.50  
175°07'40"

310°07'40"  
2.83

VIDE

11.00  
285°07'40"

A7

A7

A7

D111330



SITE PLAN

0 1 2 3 4 5  
SCALE - METRES



C40885

SHEET 3 OF 7

53844\_pland\_2\_V02\_Version\_4

CIRCUIT

WATERFORD

EAST  
PARKWAY

D111330



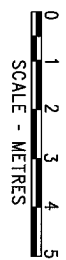
LIFT PIT

CAR STACKER  
PIT

CUNNINGHAM

LANE

BASEMENT LEVEL PLAN



C40885

SHEET 4 OF 7

53844\_pland\_3\_V02\_Version\_4

NOTE:  
PATIO IS COVERED

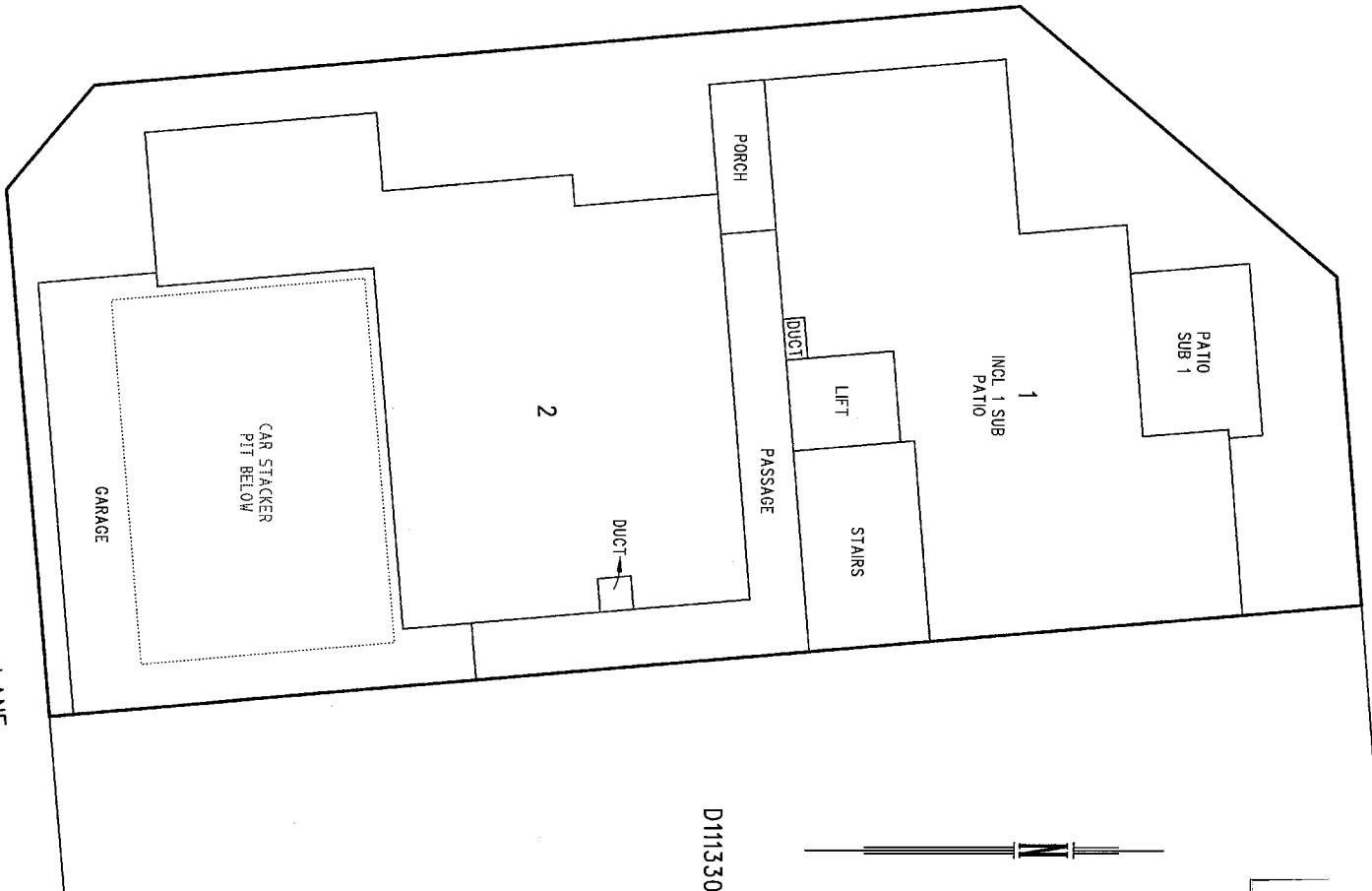
EAST PARKWAY

CIRCUIT

WATERFORD

CUNNINGHAM

LANE



D111330



GROUND FLOOR PLAN

0 1 2 3 4 5  
SCALE - METRES

C40885

SHEET 5 OF 7

53844\_pland\_4\_V02\_Version\_4

NOTE:  
BALCONIES ARE COVERED

EAST PARKWAY

CIRCUIT

WATERFORD

CUNNINGHAM

LANE

D111330



BALCONY  
SUB 3

3  
INCL. 1 SUB  
BALCONY

STAIRS

LIFT

DUCT  
PASSAGE

4  
INCL. 1 SUB  
BALCONY

DUCT

BALCONY  
SUB 4

GARAGE

FIRST FLOOR PLAN

0 1 2 3 4 5  
SCALE - METRES

C40885

SHEET 6 OF 7

53844\_pland\_5\_V02\_Version\_4

NOTE:  
BALCONIES ARE COVERED

EAST  
PARKWAY

CIRCUIT

WATERFORD

CUNNINGHAM

LANE

D111330



BALCONY  
SUB 5

5  
INCL. 1 SUB  
BALCONY

STAIRS

LIFT

PASSAGE

DUCT

6  
INCL. 1 SUB  
BALCONY

BALCONY  
SUB 6

DUCT

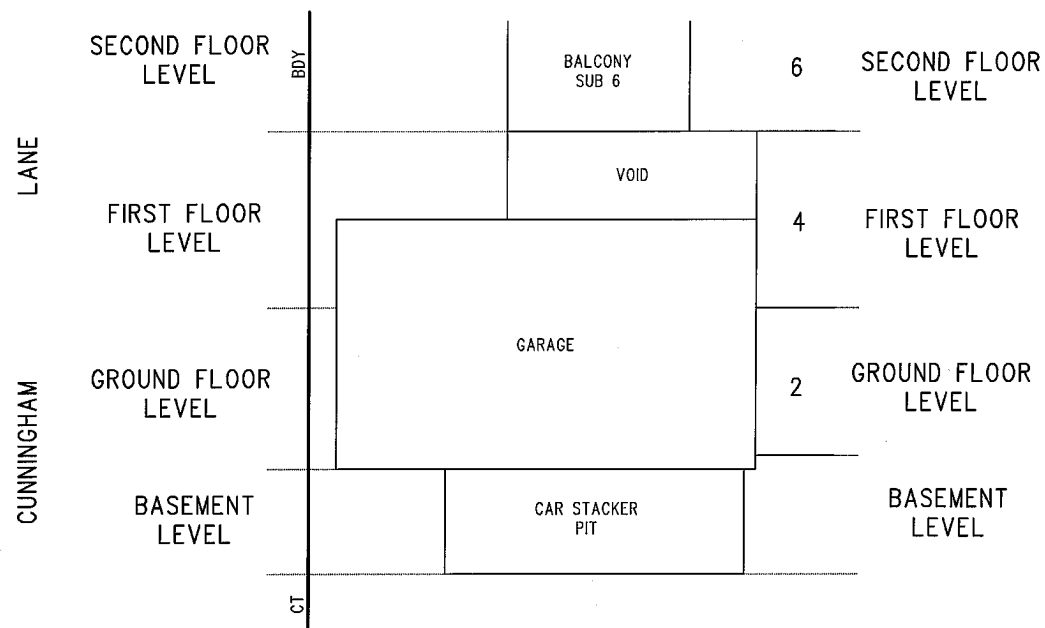
SECOND FLOOR PLAN

0 1 2 3 4 5  
SCALE - METRES

C40885

SHEET 7 OF 7

53844\_pland\_6\_V02\_Version\_4



ELEVATION A7  
NOT TO SCALE

# LOT ENTITLEMENT SHEET

COMMUNITY PLAN NUMBER

**C40885**

SHEET 1 OF 1

ACCEPTED

*[Signature]* 8/15/2017  
PRO REGISTRAR-GENERAL

DEV. NO. 040 : C101 : 16 :

APPLICATION: 12721503

## SCHEDULE OF LOT ENTITLEMENTS

LOT	LOT ENTITLEMENT	SUBDIVIDED
1	1,320	
2	1,360	
3	1,570	
4	1,710	
5	1,810	
6	2,230	
AGGREGATE	10,000	

CERTIFICATE OF LAND VALUER

I, JEFFREY WOOD

being a land valuer within the meaning of the  
Land Valuers Act 1994 certify that the  
schedule is correct for the purposes of the  
Community Titles Act 1998.

Dated the 7TH day of APRIL 2017

*[Signature]*  
Signature of Land Valuer



Development should not exceed an external wall height of 18.5 metres above natural ground level (excluding lift service levels and gables).

0 2 4 8m  
SCALE: 1:200 @ A4

## FRONT ELEVATION

### 2 CANNING STREET, GLENELG NORTH

JOB REF. 19ADL-0070

PREPARED BY. MP

DATE. 01.07.19

REVISION. 1





# NOTES:

- THIS DRAWING SHALL BE READ IN CONJUNCTION WITH ALL ASSOCIATED DRAWINGS/SPECIFICATIONS AND ANY DISCREPANCIES TO BE DIRECTED TO THE DESIGNER FOR CLARIFICATION.
- ALL WORK EXTERNAL TO SITE BOUNDARY TO BE CARRIED OUT TO COUNCIL REQUIREMENTS.
- USE FLEXIBLE CONNECTION FOR STORMWATER PIPES.
- THIS IS NOT A CADASTRAL PLAN AND SHOULD NOT BE USED IN DETERMINING PRECISE DIMENSIONS WITH RESPECT TO BOUNDARIES.
- ALL UP-V.C. PIPES LESS THAN 200mm BELOW THE SURFACE ON THE DRIVEWAY TO BE ENCASED IN 100mm CONCRETE.
- PIPES LESS THAN 300mm IN DEPTH (FROM TOP OF PIPE) MUST HAVE CONCRETE COVER.
- BUILDERS/ CONTRACTORS TO CHECK FOR ANY UNDERGROUND SERVICES PRIOR TO CONSTRUCTION.
- STORMWATER TANK TO BE PLUMBED TO LAUNDRY V.WC - REFER TO ARCHITECTURAL DRAWINGS, OWNER, BUILDER, DEVELOPER FOR DETAILS.
- IT IS THE RESPONSIBILITY OF THE OWNER/BUILDER TO ENSURE THAT FINISHED LEVELS AS PROPOSED BY ENGINEER BE ADEQUATE AS TO GET DESIRED FALL TO SEWERAGE INVERT. OWNER/BUILDER/PLUMBING CONSULTANT/PLUMBER MUST CHECK EXISTING SEWERAGE CONNECTION POINT INVERT TO ENSURE THAT PROPOSED FINISHED LEVELS ARE ADEQUATE PRIOR TO COMMENCEMENT OF ANY WORK.

## LEGEND:

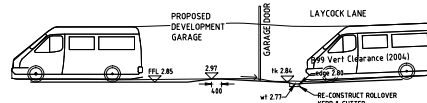
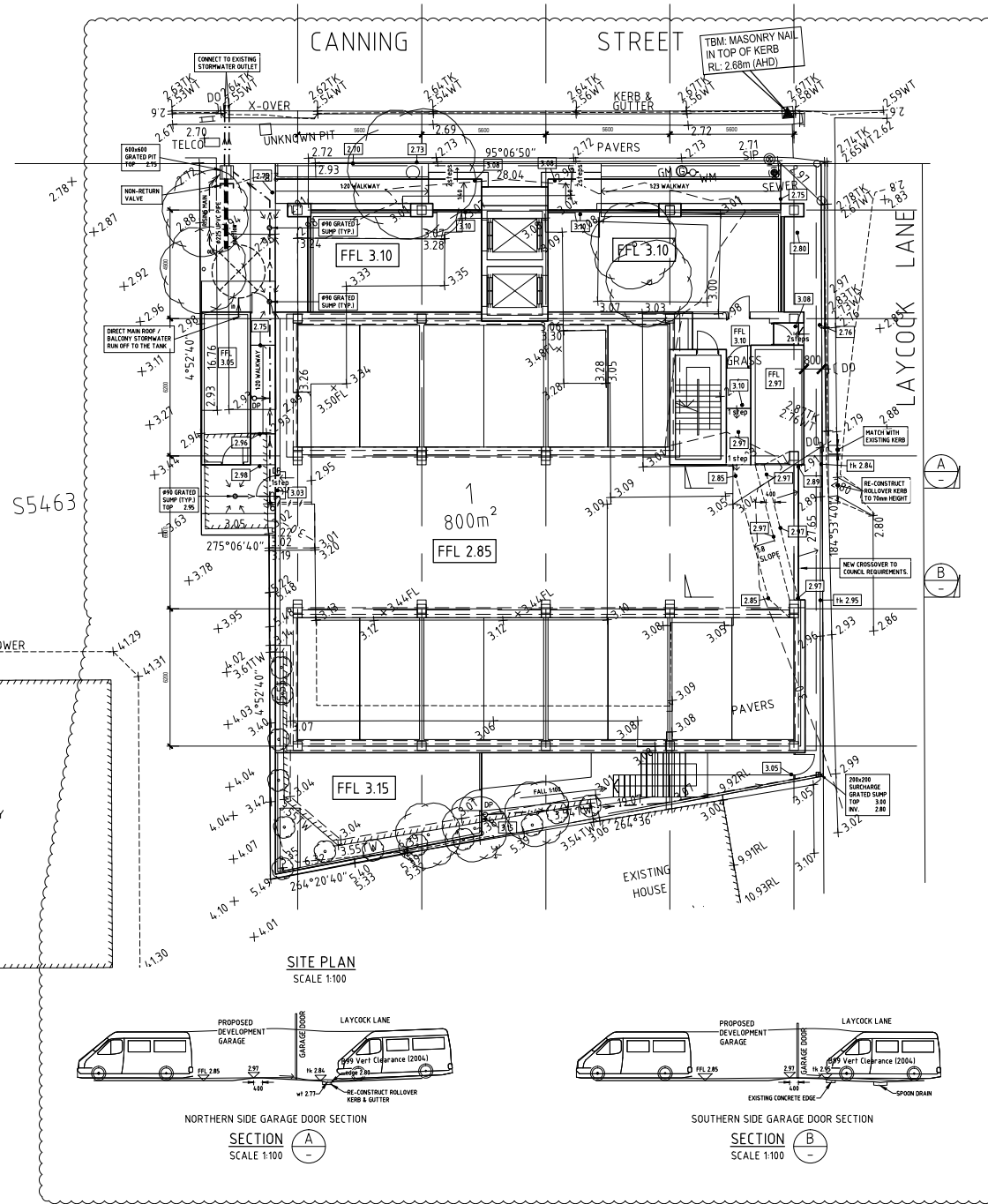
- STORMWATER ALIGNMENT #100 UPVC PIPE
- STORMWATER ALIGNMENT #225 UPVC PIPE
- UNDER GROUND SEALED SYSTEM #100 UPVC PIPE
- STORMWATER RISING MAIN DOWNPIPE
- #90 GRATED SUMP (TYP.)
- OPEN UNLINED SURFACE DRAIN AT MIN FALL 1 in 200
- EXISTING LEVEL
- DESIGN LEVEL
- DESIGN CONTOUR
- INSPECTION OPENING
- SEWER I.P.
- SUMP
- JUNCTION BOX
- CONCRETE SLEEPER RETAINING WALL

INDICATES 9000L CONCRETE UNDERGROUND DETENTION TANK WITH BUILT IN DUAL PUMP & ALARM PUMP OUT RATE 9L/S. PROVIDE OVERFLOW TO GRATED SUMP. DIRECT STORMWATER FROM UPPER ROOF & BALCONY TO TANK.

PROVIDE PERIMETER CONCRETE KERBING AS REQUIRED.

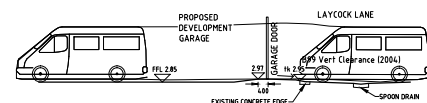
NOTE: BUILDER/CONTRACTOR TO ENSURE UNDERGROUND TANK ARE INSTALLED OUTSIDE EXISTING OR PROPOSED FOOTING INFLUENT ZONE. OUR OFFICE SHALL BE CONTACTED FOR ADVISE & RECOMMENDATION IF THERE IS POSSIBILITY OF DESTABILIZING EXISTING OR PROPOSED STRUCTURE AS THE RESULT OF UNDERGROUND TANK INSTALLATION.

NOTE: ALL UNDERGROUND TANK MUST BE DESIGNED & BUILT WITH ANTI-FLOATATION SYSTEM TO OVERCOME HYDROSTATIC PRESSURE (UPLIFT) ASSOCIATED WITH UNDERGROUND TANK. ALL UNDERGROUND TANK MUST BE DESIGNED & INSTALLED STRICTLY AS PER MANUFACTURER'S REQUIREMENTS / SPECIFICATIONS FOR ITS INTENDED USE. ALL SITE FACTORS SHALL BE CONSIDERED SUCH AS TRAFFIC LOAD, CORROSION, etc.



NORTHERN SIDE GARAGE DOOR SECTION

SECTION A  
SCALE 1:100



SOUTHERN SIDE GARAGE DOOR SECTION

SECTION B  
SCALE 1:100

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## PLANNING APPROVAL

PA.1	GARAGE LEVEL REVISED u_ARCH R: 02/09/19	05/09/19	KS
PA.0	ISSUED FOR PLANNING APPROVAL ARCH DRAWING RECEIVED: 14/06/19 LEVEL RECEIVED: 14/06/19	03/07/19	KS

DATE	DESCRIPTIONS	DATE	BY
11	LEVEL APARTMENT		

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2 CANNING STREET, GLENELG NORTH

DRAWING TITLE  
SITE PLAN

CLIENT  
BARRIO DEVELOPMENTS

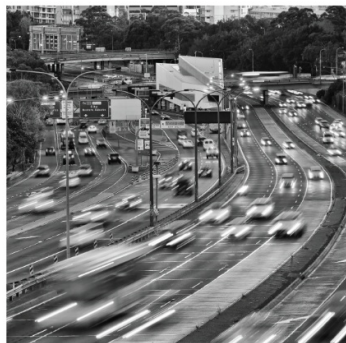


108 Wright Street, Adelaide SA 5000 Tel: (08) 8231 6000  
Fax: (08) 8231 3444 Email: csh@structuralsystems.com.au ABN 21 368 115 930

DRAWN	KS/LM	DESIGNED	NN
CHECKED		DATE REVISED	05/09/19
SCALE	1:100 UNO	PAPER SIZE	A1
ALL DIMENSIONS IN MM - DO NOT SCALE		DATE ISSUED	03/07/19
JOB No.	DT 190305	DRAWING No.	01
		STAGE	PA
		ISSUE	1

STAGE ABBREVIATION: P-PRELIMINARY, DS-ENGINEERING DESIGN STAGE, PA-FOR PLANNING APPROVAL, T-TENDER, BA-BUILDING APPROVAL, C-COR CONSTRUCTION

# infraPlan



## 2 Canning Street, Glenelg North: Proposed Development Car parking + Waste Report

August 2019



■ Urban and Regional Strategy + Consultation ■ Transport + Infrastructure Planning ■ Project Development + Design ■ Traffic Engineering + Movement Planning

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Project Name	Car parking + Waste Report - 2 Canning Street, Glenelg North: Proposed Development				
Client contact details	<div>Glen Vollebregt M1 Centre Level 4, 195 North Terrace PO Box 3571, Rundle Mall ADELAIDE SA 5000 p: 7070 2580 glen@barrio.com.au</div> <div></div>				
Consultant contact details	Gayle Buckby Director - InfraPlan (Aust) Pty Ltd Level 3, 66 Wyatt Street ADELAIDE SA 5000 p: 08 8227 0372 <a href="mailto:gayle@infraplan.com.au">gayle@infraplan.com.au</a>				

Document Control					
Name:	IP19.009 2 Canning Street, Glenelg North: Proposed Development				
Version	Notes	Author	Date	Reviewer	Date
0	Preliminary Review - DRAFT	GB	21-3-19	ES	21-3-19
1	Preliminary Review - DRAFT	GB	21-3-19	ES	21-3-19
2	DRAFT 2 Modifications to design	GB	13-5-19	ES	14-05-19
3	DRAFT 3 Modifications to design	GB	05-06-19	ES	05-06-19
4	Issue	GB	07-06-19		
5	Revised Issue	GB	27-08-19		

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# 1. Executive Summary

InfraPlan has been engaged by Barrio Developments to participate in design development for the traffic and waste management aspects of a proposed development at 2 Canning Street in Glenelg North. This development includes the following components relating to traffic, parking and waste:

- At total of 58 apartments or 82 bedrooms, comprising of:
  - 2 x 1-bedroom residential apartments,
  - 12 x 2-bed residential apartments,
  - 6 x 3-bed residential apartments,
  - 38 x 1 bed apartments for tourist accommodation,
- 40 x access-controlled undercover car parks,
- Bicycle parking, and
- Bin storage area for 13 x 240L waste, recycling and organic bins collected two times per weekly.

Key findings of this study are listed below and explored further in the report:

- Sustainable transport is nearby as it is a short walk to high-frequency bus stops, and less than a 10-minute walk to the Jetty Road Tram stops.
- Off-street car parking is in excess of the minimum required in the Development Plan and will sufficiently cater for the residential, tourists and visitor car parking demand.
- The underground car park is designed efficiently with a vertical stacking arrangement and is appropriately accessed from Laycock Lane.
- Canning Street and Laycock Lane will be improved for safety and amenity with the removal of four garages (1 on Canning Street and 3 on Laycock Lane). Vehicle access will be consolidated to one point only – from Laycock Lane.
- Canning Street has 24 on-street car parks with 4 of these directly in front of the subject site. The driveway crossover removal will enable two additional on-street car parking spaces to be installed.
- Canning Street is a local street and Laycock Lane is a no-through lane providing rear access to five other dwellings. These streets have sufficient capacity to accommodate the additional traffic that is likely to be generated by the development.
- There is sufficient space designated for waste, recycling and organic bins, which will be collected by a private contractor twice a week. The floor to ceiling height in the underground carpark is sufficient (greater than 4.2m) for refuse collection to be undertaken off-street.

As part of this study, we have reviewed:

- Brown Falconer Drawings dated August 23, 2019,
- Holdfast (City) Development Plan consolidated 2 June 2016,
- RMS Update to the RTA Guide to Trip Generating Developments, and
- Australian Standard AS2890.1, Off-Street Car Parking.

## 2. Existing Site

The subject site is a residential dwelling located at 2 Canning Street, Glenelg North within the City of Holdfast Bay. The site is bound by a rear access laneway to the east (Laycock Lane), and residential apartments and houses on other sides. Canning Street is a local street, and the nearby major road network includes Adelphi Terrace, Tapleys Hill Road and Anzac Highway. The site is within close vicinity of bus stops on Anzac Highway, and Adelphi Terrace, and tram stops on Jetty Road. Refer Figure 1.



Figure 1: Location Plan, subject site in orange (from Development Plan, Transport Overlay Map)

### 2.1 Planning Context

Under the City of Holdfast Bay Development Plan, the site is within a Residential High-Density Zone and the Urban Glenelg Policy Area 15. The primary transport related matters arising from these classifications include:

- *Service yards, car parking areas and facilities, service ducting and plant should be designed and located to ensure that the appearance of buildings and land viewed from all abutting roads is attractive*
- *Development should provide car parking within the zone in accordance with Table HoB/1B – Off Street Vehicle Parking Requirements for the Residential High Density Zone.*

### 2.2 Car parking

Two spot surveys were undertaken of Canning Street to understand the existing demand for on-street parking on Canning Street. There are 24 on-street spaces in total (between Adelphi Terrace and Sturt Street), with 4 of these directly in front of the subject site.

The surveys found:

- 18 spaces available on a typical weekday evening (Monday April 1 at 7:30pm):
- 1 space available on a busy weekend day (Sunday April 7 at 3pm). During this survey there was a 'Dream Cars' event at Wigley Reserve that resulted in a high demand for car parking throughout the entire precinct.

The existing dwelling has off-street parking in four garages (1 on Canning Street and 3 on Laycock Lane).

### 3. Proposed Development

The proposed high-density residential development includes the following elements that result in change to traffic, parking and waste:

- 2 x 1 bed residential apartments
- 12 x 2-bed residential apartments
- 6 x 3-bed residential apartments
- 38 x 1-bed apartments for tourist accommodation
- 40 x access-controlled undercover car parks
- Bicycle parking, and
- Bin storage to house 13x 240L litre waste, recycling and organic bins.

#### 3.1 Parking Requirements

The City of Holdfast Bay Development Plan provides guidance for Residential High-Density Zone car parking rates as listed in Table 1.

Dwelling type	Residential parking Required	Visitor parking required	Total spaces required per dwelling
Studio, 1 or 2 bed	1 per dwelling	0.25 per dwelling	1.25
3 x bed +	1.25 per dwelling	0.25 per dwelling	1.5
Tourist accommodation	1 per 4 bedrooms (min) or 1 per 2 bedrooms (max)	0	0.25 – 0.5

*Table 1: Development Plan rates*

In addition to this requirement, the City of Holdfast Bay Development Plan allows for a lesser parking rate if certain local circumstances are met. The circumstances that would apply at the Canning Street development are:

*(a) amalgamation of allotments occurs, or an agreement is formed to integrate and share adjoining parking areas, to create larger more functional and efficient parking areas, as follows:*

*(ii) side road frontage with two-way access provided*

*(iii) convenient flow through two-way accessibility created between side roads*

*(c) sites are located within 200 metres walking distance of a convenient and frequent service fixed public transport stop*

*(f) suitable arrangements are made for any parking shortfall to be met elsewhere or by other means*

*(g) generous on-street parking and/or public parking areas are available and in convenient proximity, other than where such parking may become limited or removed by future loss of access, restrictions, road modifications or widening.*



## 3.2 Parking provision

The parking rates are translated to this development in the table below, which shows that a minimum of 35 car parks are required.

Dwelling Type	No. apartments	Residential car parks required	Visitor parks required	Total spaces required
1 x bed	2	2	1	3
2 x bed	12	12	3	15
3 x bed	6	8	2	10
Tourist Accommodation	38	10 (min), 19 (max)	0	10 (min)
<b>Total</b>	<b>58</b>	<b>34 (min)</b>	<b>6</b>	<b>38 (min)</b>

Table 2: Car park requirement

38 parking spaces are required and 40 spaces are proposed in the car park off of Laycock Lane which will be designated as listed in Table 3.

Car park user	Spaces provided
Residential (22 spaces required)	24 spaces
Tourist (10 spaces required)	10 spaces
Visitor (6 spaces required)	6 spaces
<b>Total</b>	<b>40</b>

Table 3: Car space distribution options

### Visitor Parking

Visitors will utilise the car stacking system with the assistance of residents of the building. There is sufficient space inside the car park for visitors to wait while a resident explains the system or assists.

### On-street Car Parking

The removal of the driveway access to the subject site from Canning Street will result in space for two additional on-street car parks. Although this development does not rely on on-street parking, there would be potential for overflow parking on-street if required.

### Bicycle Parking

There are no specific requirements for bicycle parking at residential apartments in this zone. It is anticipated that residents will store their bicycles in their apartments, and therefore the lift has been designed to easily accommodate bicycles.

In addition, an area near the front entrance of the building provides convenient, well-lit parking for resident bike-share, and visitor bicycle parking. Seven parking rails are proposed, providing space for 14 x bikes.

### Loading and Deliveries

It is anticipated that delivery and/or service vehicles (particularly for the tourist accommodation, such as linen, cleaning etc) could utilise on-street car parking if available, or the visitor car parks during weekdays, when visitor demand would be at its lowest.

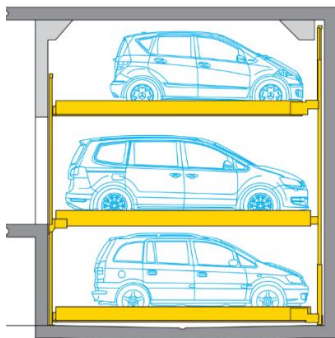
Waste collection can occur off-street in the car park or from the laneway (as discussed in Section 5).

### 3.3 Car Park Layout

The access controlled off-street car parks are provided in the form of a 'WOHR Combilift 543' vertical stacker and provides parking for 40 cars on 3 levels, refer images below. Vehicles enter from the ground level and the system automatically stacks and shuffle the cars as required.

The lift is operated using either a chip at an operating device, a hand-held transmitter in the vehicle, or by using a smartphone App. Vehicles are admitted into a designated single bay in each stacker unit. We have simulated the manoeuvring to check that vehicles can enter and exit the bay sufficiently, (refer to Appendix A for illustrations).

Visitors wishing to park in the secure car park will contact the resident via a telecom system who will assist them to park, until they are familiar with the system. It is proposed that a temporary visitor stopping area is marked in the aisle of the car park, where the visitor will wait until the resident assists.



The design has been assessed against Australian Standard AS2890.1 Off-Street Car Parking. This Standard is a resource for conventional car parking layouts and does not specifically apply to mechanical car parking, such as the Wohr Combilift.

The dimensions of the combilift, the column locations and the aisle width vary from the dimensions recommended in the Standard as illustrated in Figure 2. Therefore, we have reviewed the variations and made an assessment as to whether the proposed carpark functionally satisfies the requirements of the Standard. A summary of this assessment is listed in the table below. In addition, vehicle turn paths are illustrated in Appendix A.

Pedestrian movements within the car park are considered to be appropriate with a warning sign to be added for both pedestrians and vehicles at the pedestrian entrance to the car park to alert of potential conflict.

Item (residential car park)	Australian Standard AS2890.1	Proposed car park, variation to AS2890.1	Functional compliance Y/N?
Multiple vehicle garage with no internal walls. <i>Clause 5.4(b)</i>	Bays 2.4m wide. Spaces contiguous with the end spaces having a minimum width of between the centre line of the space and the end wall of 1.5m to allow clearance for door opening	Each of the bays are 2.8m wide and contiguous.	Y
Aisle Width <i>Table 1.1, Figure 2.2.</i>	5.8m wide minimum	7.7m total aisle width, with 6.1m clearance between columns.	Y
Blind Aisle <i>Fig. 2.3.</i>	Provide 1000mm extension to blind aisle to accommodate reversing manoeuvre.	There is an 800mm extension of the blind aisle. Given that the aisle width at the end of the blind aisle is 7.7m clear, the turn paths show that the reversing manoeuvre can be undertaken satisfactorily* (refer Appendix A)	Y
Column location and spacing. <i>Clause 5.2 Note</i>	Column should not be located at the edge of a parking aisle. The difficulty of manoeuvring into a parking space is increased by such a location.	Columns are located at the edge of the parking aisle, but the aisle width and parking bay width are wider than minimum and swept paths show that the manoeuvring is possible.	Y

**Table 4: Variations from AS2890.1**

\* AS2890 notes that, 'Drivers can manoeuvre vehicles within smaller spaces than swept turning paths would suggest. Wider parking spaces require slightly smaller aisle width'. This further indicates that the manoeuvre is satisfactory, given that our swept paths show that it is possible.

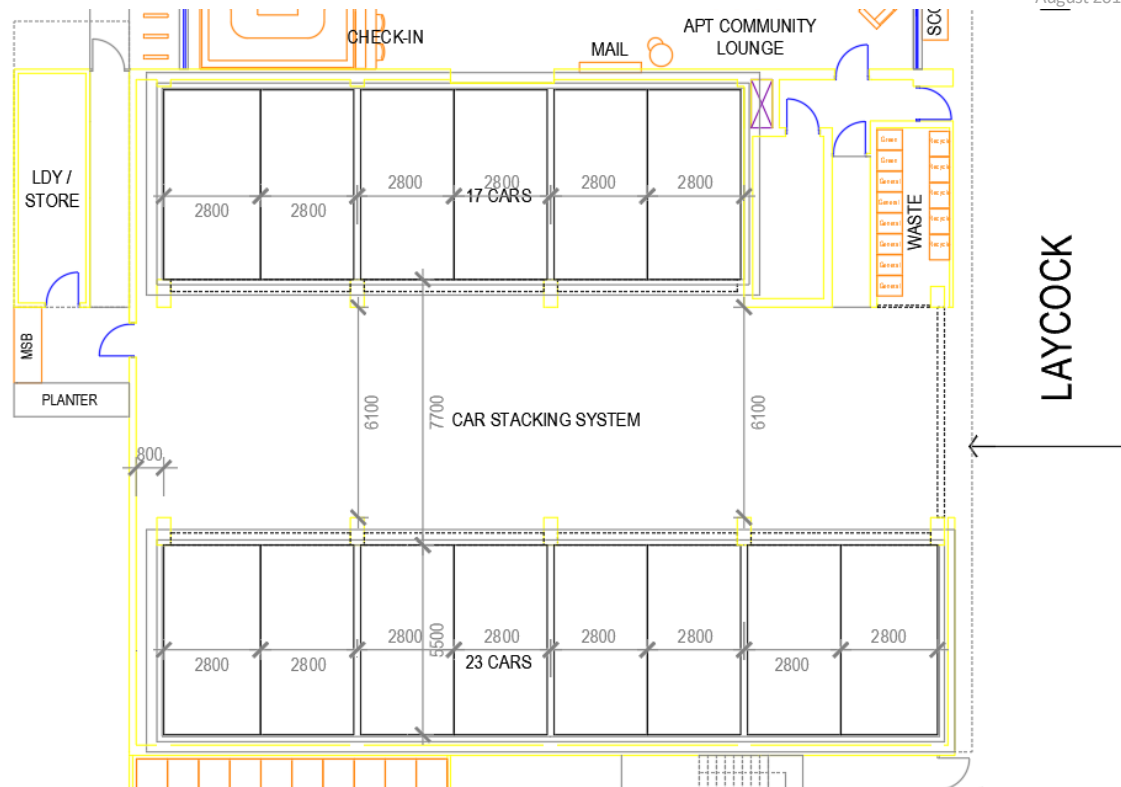


Figure 2: Car park layout and dimensions

### 3.4 Site Access

The proposed site access is entirely from Laycock Lane, and the driveway access from Canning Street will be removed. Laycock Lane is a no-through-road and its sole purpose is to provide rear access to car parking for the surrounding apartments and dwellings. There is space for a vehicle to wait in Laycock Lane if required while another exits the car park, or for another vehicle to overtake the waiting vehicle to access other properties further west in Laycock Lane.

As part of the proposal, the laneway will be effectively widened with the building setback to provide appropriate room. With a setback included, there will be improved visibility for exiting vehicles to see traffic on Laycock Lane. It is noted that the sightlines required by Australian Standards are only in relation to pedestrian movements which are expected to be minimal along Laycock Lane and in conjunction with low traffic movements, visibility when exiting is considered to be appropriate.

The setback of the property from Laycock Lane will not meaningfully impact on the existing configuration of the Laycock Lane and Canning Street Junction.

The consolidation of the existing four garages (Canning Street and Laycock Lane) to a single carpark access point off of Laycock Lane will result in improvement to both street frontages and pedestrian safety/amenity.

### 3.5 Future Proofing

The 3-level car lift results in a high floor to ceiling clearance of 4.6m, which provides future-proofing conversion opportunities if the car parks are not required due to change in travel modes.

## 4. Vehicle Trips

We have referred to the RMS Guide to Traffic Generating Developments (Updated traffic surveys 2013) for rates applicable to high-density residential developments. It is noted that these survey values are recorded for Sydney based properties. For applicability to Adelaide, these averages were multiplied by 2x which falls within the upper range of the survey data.

There is no guidance for typical trip generation for tourist accommodation because of considerable variance depending on location and transport alternatives. Therefore, we have used a first principles approach where we used the number of car parks designated to the tourist accommodation (assuming at 100% capacity) and assumed 4 trips per vehicle per day. These would be distributed throughout the day/night and not necessarily in the AM or PM peak as for the residential apartments.

Given the above, the assumed rates are considered conservative (on the higher end), and result in around 101 trips per day, as listed in Table 5.

RTA – Land Use	Weekday Daily	Weekday AM Peak	Weekday PM Peak	Daily Trips	AM Peak Hour Trips	PM Peak Hour Trips
	Rate: trips per apartment					
20 x apartments	3.04	0.38	0.3	61	7	6
38 x Tourist accommodation (10 car parks provided)	4 trips per vehicle			40		
Total				101		

*Table 5: Trip Generation Estimate*

Canning Street is an 11m wide, residential street, with parallel parking (no time limit) on both sides. Traffic data has not been collected but observations indicate that Canning Street has more than sufficient capacity to carry the additional traffic generated from this development.

Laycock Lane is 5.4 metres wide and a no-through-road, providing rear access to 5 other properties. It is therefore considered appropriate for car park access to the subject site.

Additionally, this is inline with Australian Standards in relation to accessway widths from a local road based on the number of car parks serviced.

## 5. Waste, recycling and organics

Zero Waste South Australia (ZWSA) have published a *Better Practice Guide* for waste management in South Australia that is used as a best practice guideline document when determining the waste needs of a development. This document bases waste generation on land use type, area and period of use and provides guidance on the systems, generation and collection methods of general, recycling and organic waste streams.

### 5.1 Waste management

General and recycling waste bins stored on each level for easy access as required by residents of the building. Separately a dedicated bin storage area is proposed that is conveniently located on the ground floor between the lift lobby and the car park. Residents will dispose of organic waste into the bins as they exit the building to either the car park, Canning Street or Laycock Lane. The general and recycling waste bins will be swapped twice weekly with bins on each level.

### 5.2 Waste generation

The ZWSA Guidelines identifies rates for waste generation based on number of beds for high density residential as listed in Table 6: Waste Generation per bedroom.

Land Use (rate)	General	Recycling	Organics
High Density Residential	30 Litres/bedroom/week	25 Litres/bedroom/week	10 Litres/bedroom/week

Table 6: Waste Generation per bedroom

### 5.3 Waste Storage

Knowing the total amount of waste generated, the number and size of bins can be assessed. Bins typically are sized in either 240L (standard kerbside collection), 660L or 1,100L. The 660L bin has been adopted for this site for efficiency and ease of handling. The 240L bins that will be used, and the bin storage layout are illustrated in Table 7.

Capacity	Dimensions		
240 Litres	730 L x 580 W x 1,060 H		
		General Waste	Co-mingled Recyclables    Food/Organics

*Table 7: Waste bin sizes and layout*

The waste generation, number of bins required, and collection frequency has been calculated using a total of 82 bedrooms as listed in Table 8.

Waste Stream: 78 bedrooms	General	Recyclable	Organic
Waste generated per week (Litres)	2,580	2,150	860
Collection frequency	Twice a week	Twice a week	Twice a week
Waste Capacity required (Litres)	1,290	1,075	430
No. of bins provided	6 x 240L	5 x 240L	2 x 240L
Capacity provided	1,440	1,200	480

*Table 8: ZWSA and adopted waste generation rate for high density residential*

In summary, thirteen x 240 litre bins are required as follows:

- 6 x 240 litre general waste bins collected twice a week,
- 5 x 240 litre recycling bins collected twice a week, and
- 2 x 240 litre organic bin collected twice a week.

## 5.4 Waste Collection

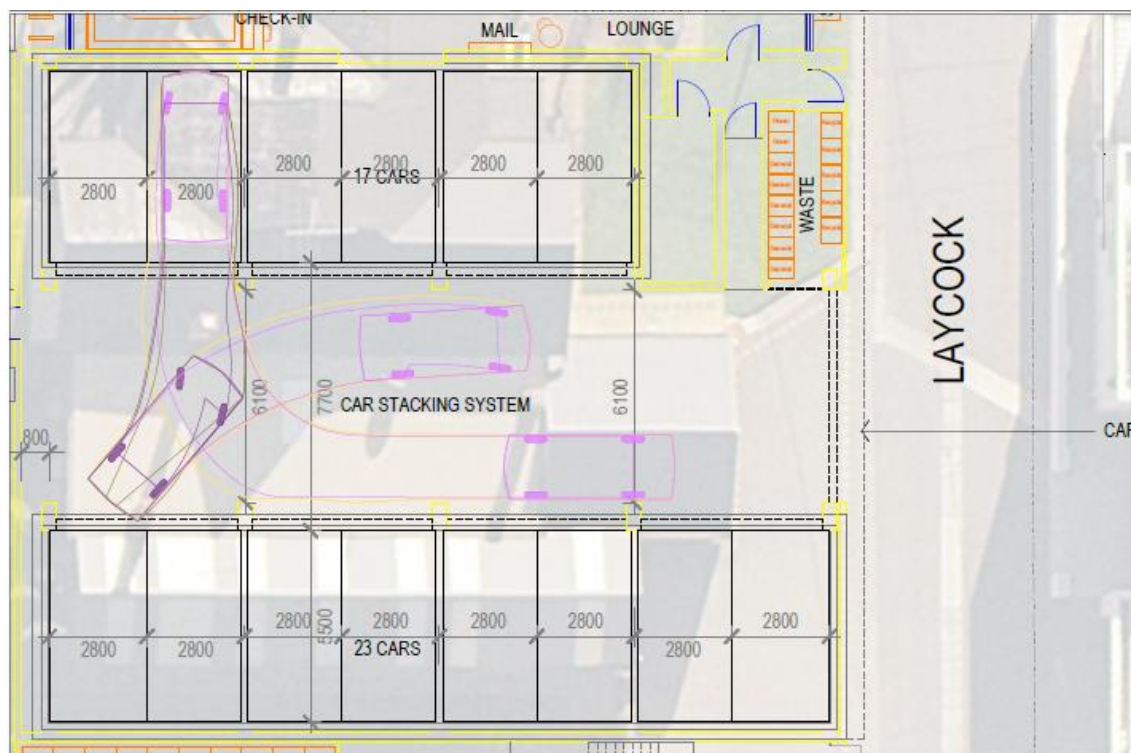
Subject to contractor availability, a small refuse truck (6.4m) operated by a private contractor will collect waste from the car park as illustrated in Figure 4. Alternatively, a typical refuse truck could be used with access from Laycock Lane as seen in Figure 5. While these vehicles will only linger for approximately 5 minutes, the parked waste vehicle will allow for a vehicle to pass along Laycock Lane if required.

It is recommended that waste collection should be conducted outside of peak periods (7-9am, 3-6pm) to minimise impacts to surrounding properties and peak hour traffic.



The site plan illustrates the proposed car stacking system at the Laycock site. The plan is divided into several key areas:

- Top Section:** A rectangular area containing a car stacking system with 17 cars. The width of this section is 2800 units, and the height is 6100 units. The total width of the top section is 2800 units.
- Central Section:** A large rectangular area labeled "CAR STACKING SYSTEM" with a height of 7700 units. The width of this section is 6100 units.
- Bottom Section:** A rectangular area containing a car stacking system with 23 cars. The width of this section is 2800 units, and the height is 6100 units. The total width of the bottom section is 2800 units.
- Waste Area:** A rectangular area labeled "WASTE" located to the right of the top section, with a width of 2800 units.
- Laycock Area:** A large rectangular area on the right side of the plan, labeled "LAYCOCK", with a width of 2800 units.
- Dimensions:** The plan includes various dimensions: 2800, 6100, 7700, and 800. The 800 unit dimension is located on the left side of the plan.
- Car Stacking System:** The car stacking system is shown as a series of rectangular blocks, with 17 cars in the top section and 23 cars in the bottom section.



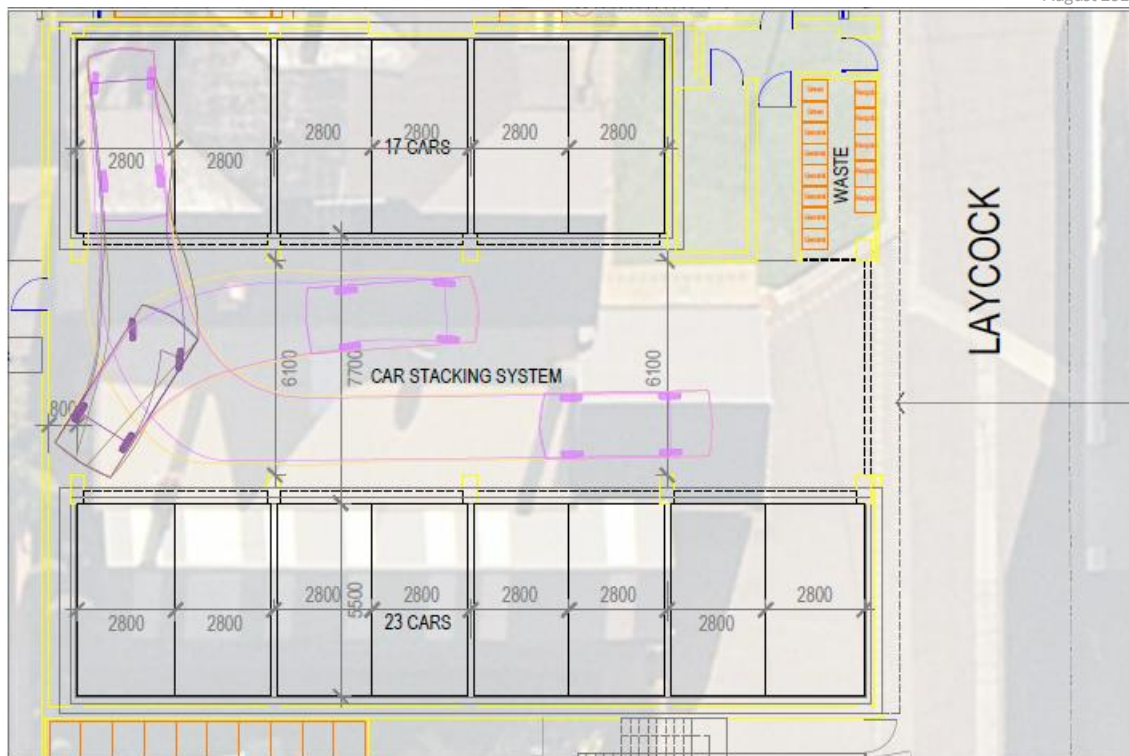


Figure 3: B85 Swept paths indicating sufficient manoeuvring

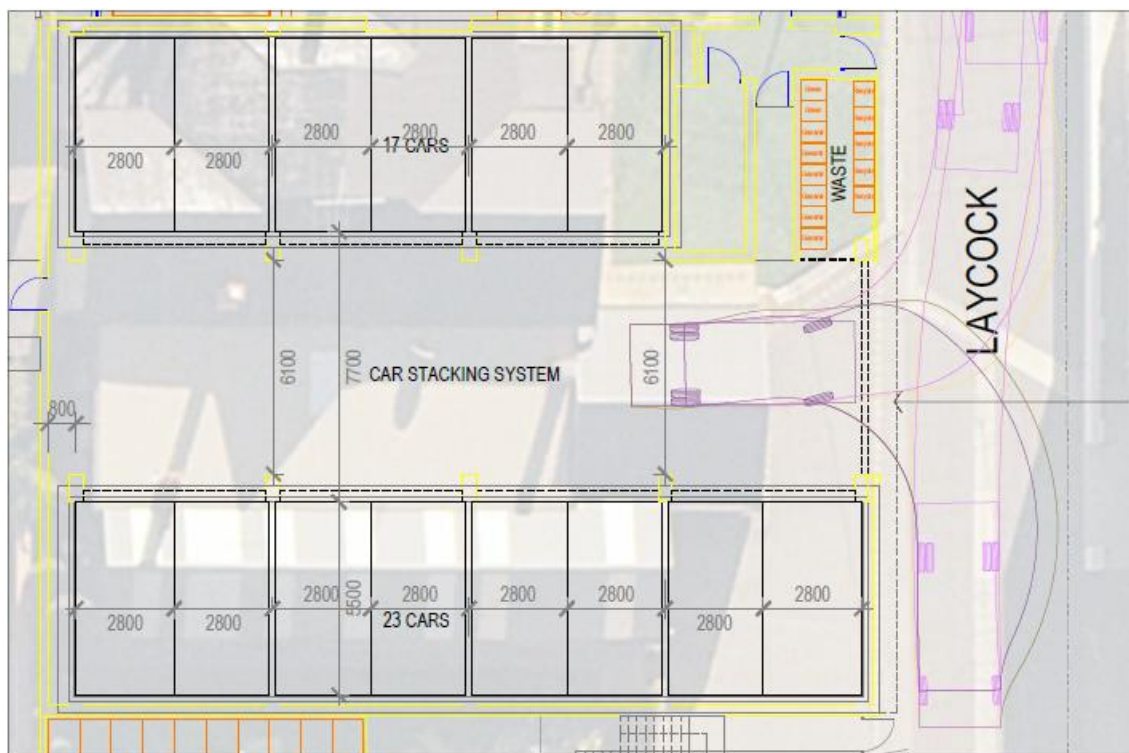


Figure 4: 6.4m long refuse vehicle collection

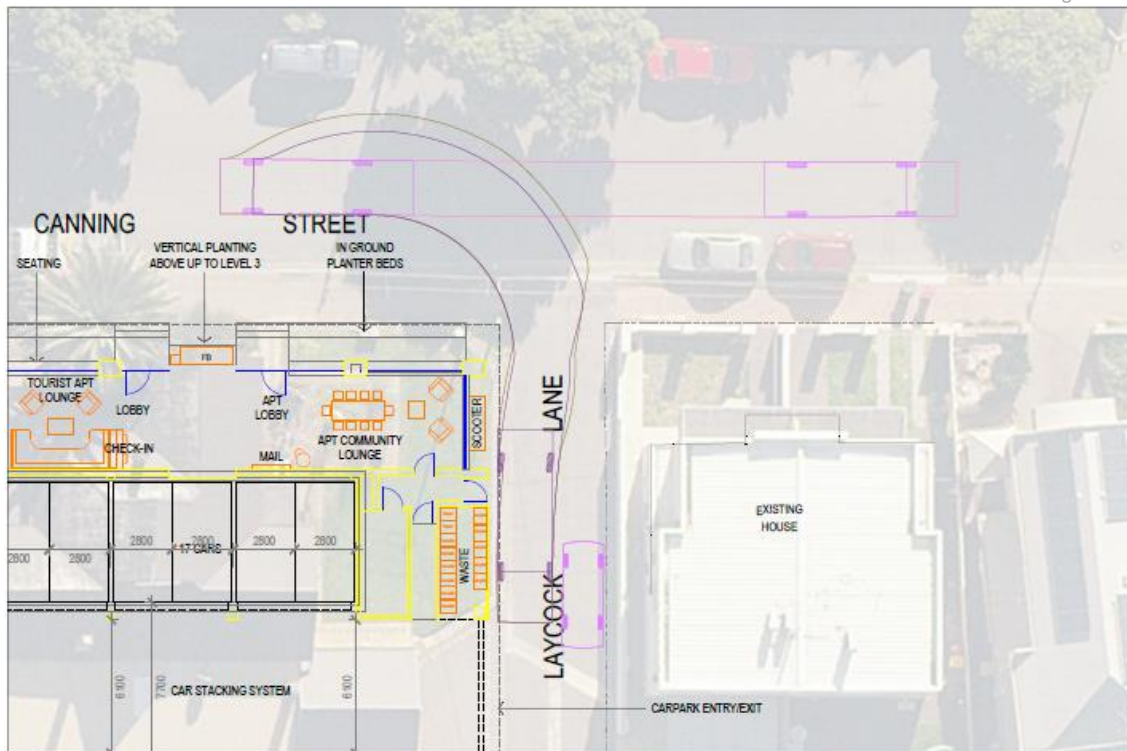


Figure 5: 8.8m long refuse vehicle collection (if required with vehicle passing)

Thursday, 20 June 2019

Project number: A190456  
Reference: A190456LT1

Glen Vollebregt  
Barrio Developments  
Suite 625, 38 Gawler Place  
PO Box 3571, Rundle Mall Adelaide SA 5000

Dear Glen,

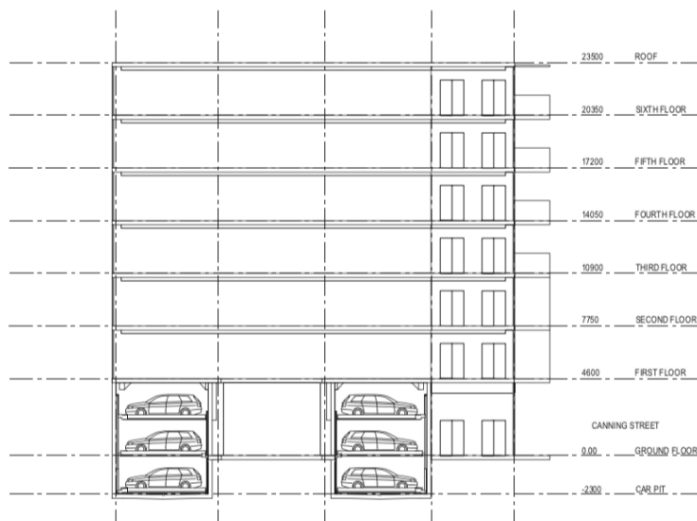
## 2 Canning Street - Car Stacker Noise Environmental Noise Assessment

This letter outlines the environmental noise assessment for the proposed residential development at 2 Canning Street, Glenelg North.

## 1 Proposed development

### 1.1 Noise source

As part of this development, a car stacker is to be used on the ground floor (with cars to be stacked one level above and one level below). A section through the building is shown in Figure 1, which demonstrates the layout of the car stacker.



Section - North South

Figure 1 Section showing car stacker

The car stacker system to be used is the Wohr Combilift 543 system.

## 1.2 Location

The site is located on the corner of Canning Street and Laycock Lane. The closest receptors are residential receptors across Laycock Lane to the east, and to the south west of the proposed site.

The site and residences to the north, east and south are located in a 'Residential High Density' zone, 'Urban Glenelg' Policy Area, 'Five Storey' Precinct. The adjacent land to the west is located in a 'Residential High Density' zone, 'Urban Glenelg' Policy Area, 'Twelve Storey' Precinct.

For the 'Urban Glenelg' Policy Area the following objectives are noted in the Development Plan:

### OBJECTIVES

1. A policy area comprising tourist accommodation and a range of dwellings and residential flat buildings at medium to high densities.
2. Accommodation that provides a diversity of dwelling sizes within residential flat buildings that cater for different household requirements.
3. Retention of the heritage character, especially along South Esplanade in Precinct 4 Five Storey and Precinct 5 Twelve Storey.
4. Development that contributes to the desired character of the policy area/precinct.

On this basis, we believe that the site and the closest receptors are located in an area that primarily promotes residential land uses.

We note that to the South of the site is the Watermark Glenelg hotel, which has the potential to generate noise from activities such as deliveries and forklifts, music noise, patron noise, car park and drive through activity.

## 2 Environmental noise criteria

Environmental noise emissions from the proposed development should comply with the *Environment Protection (Noise) Policy 2007* (Noise EPP).

The noise goals in the Noise EPP are based on the zoning of the development and the closest noise affected premises in the relevant development plan. The land uses primarily promoted by the zones are used to determine the environmental noise criteria with the indicative noise factors shown in Table 1.

Table 1 Excerpt from Noise EPP—Table 2(subclause(1)(b))

Land use category	Indicative noise factor dB(A)	
	Day (7 am to 10 pm)	Night (10 pm to 7 am)
Rural living	47	40
Residential	52	45
Rural industry	57	50
Light industry	57	50
Commercial	62	55
General industry	65	55
Special industry	70	60

As noted above, the development and the most affected noise sensitive premises are located in zones in which residential land uses are primarily promoted.



In accordance with Part 5 of the Noise EPP, the relevant criteria for this development will be the relevant indicative noise factors less 5 dB(A). The application of Part 5 results in the following environmental noise criteria:

- 47 dB(A) during the day, 7 am to 10 pm
- 40 dB(A) at night, 10 pm to 7 am.

In addition, as the receivers are located in a 'quiet locality' (being residential) a maximum noise criterion of  $L_{max}$  60 dB(A) is applicable.

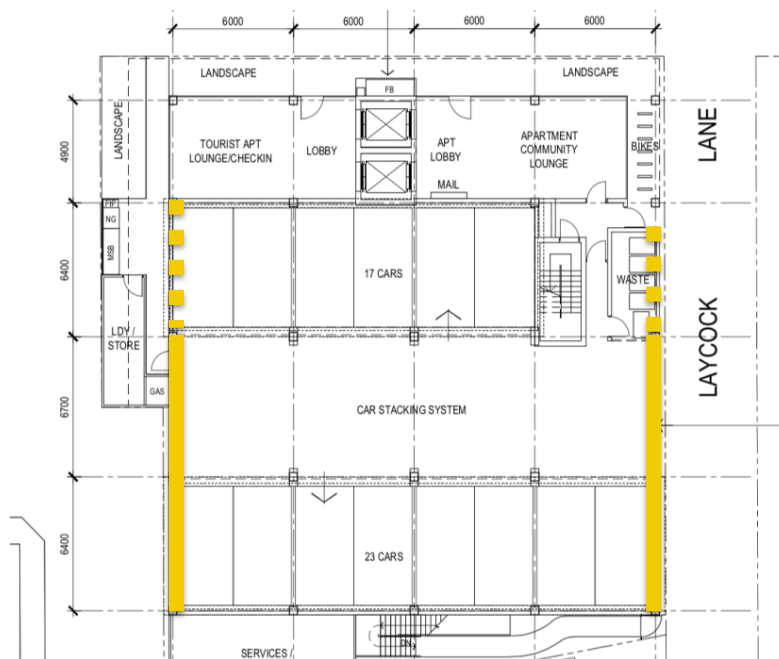
## 3 Assessment

Noise measurements of the Wohn Combilift 543 system was undertaken in Melbourne on Friday, 14 June 2019.

Our assessment is based on these measurements, and to achieve the more stringent night time criteria of  $L_{eq}$  40 dB(A) and  $L_{max}$  60 dB(A) we recommend the following treatments:

- To west facade:
  - Open brick section to the north west—breezeblock (36 % open) comprising approximately 50% of this section of wall.
  - Remainder of wall to west to be solid.
- To east facade:
  - Solid garage door.
  - Open brick section to the north east (open to the waste area)
  - Remainder of wall to be solid.

These recommendations are indicated on Figure 2.



**Figure 2 Indication of recommended facade treatments—dotted line is allowed to be partially open, solid line to be of solid construction**

The solid sections can be a mix of brick and glass bricks to maintain natural light if desired.

Please let me know if you have any questions.

Yours sincerely,

A handwritten signature in black ink, appearing to be 'Deb James', with a stylized, flowing script.

Deb James  
Managing Director  
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m+61 422 047 275  
deb.james@resonate-consultants.com



# NOTES:

- THIS DRAWING SHALL BE READ IN CONJUNCTION WITH ALL ASSOCIATED DRAWINGS/SPECIFICATIONS AND ANY DISCREPANCIES TO BE DIRECTED TO THE DESIGNER FOR CLARIFICATION.
- ALL WORK EXTERNAL TO SITE BOUNDARY TO BE CARRIED OUT TO COUNCIL REQUIREMENTS.
- USE FLEXIBLE CONNECTION FOR STORMWATER PIPES.
- THIS IS NOT A CADASTRAL PLAN AND SHOULD NOT BE USED IN DETERMINING PRECISE DIMENSIONS WITH RESPECT TO BOUNDARIES.
- ALL UP-V.C. PIPES LESS THAN 200mm BELOW THE SURFACE ON THE DRIVEWAY TO BE ENCASED IN 100mm CONCRETE.
- PIPES LESS THAN 300mm IN DEPTH (FROM TOP OF PIPE) MUST HAVE CONCRETE COVER.
- BUILDERS/ CONTRACTORS TO CHECK FOR ANY UNDERGROUND SERVICES PRIOR TO CONSTRUCTION.
- STORMWATER TANK TO BE PLUMBED TO LAUNDRY \ WC - REFER TO ARCHITECTURAL DRAWINGS, OWNER, BUILDER, DEVELOPER FOR DETAILS.
- IT IS THE RESPONSIBILITY OF THE OWNER/BUILDER TO ENSURE THAT FINISHED LEVELS AS PROPOSED BY ENGINEER BE ADEQUATE AS TO GET DESIRED FALL TO SEWERAGE INVERT. OWNER/BUILDER/PLUMBING CONSULTANT/PLUMBER MUST CHECK EXISTING SEWERAGE CONNECTION POINT INVERT TO ENSURE THAT PROPOSED FINISHED LEVELS ARE ADEQUATE PRIOR TO COMMENCEMENT OF ANY WORK.

## LEGEND:

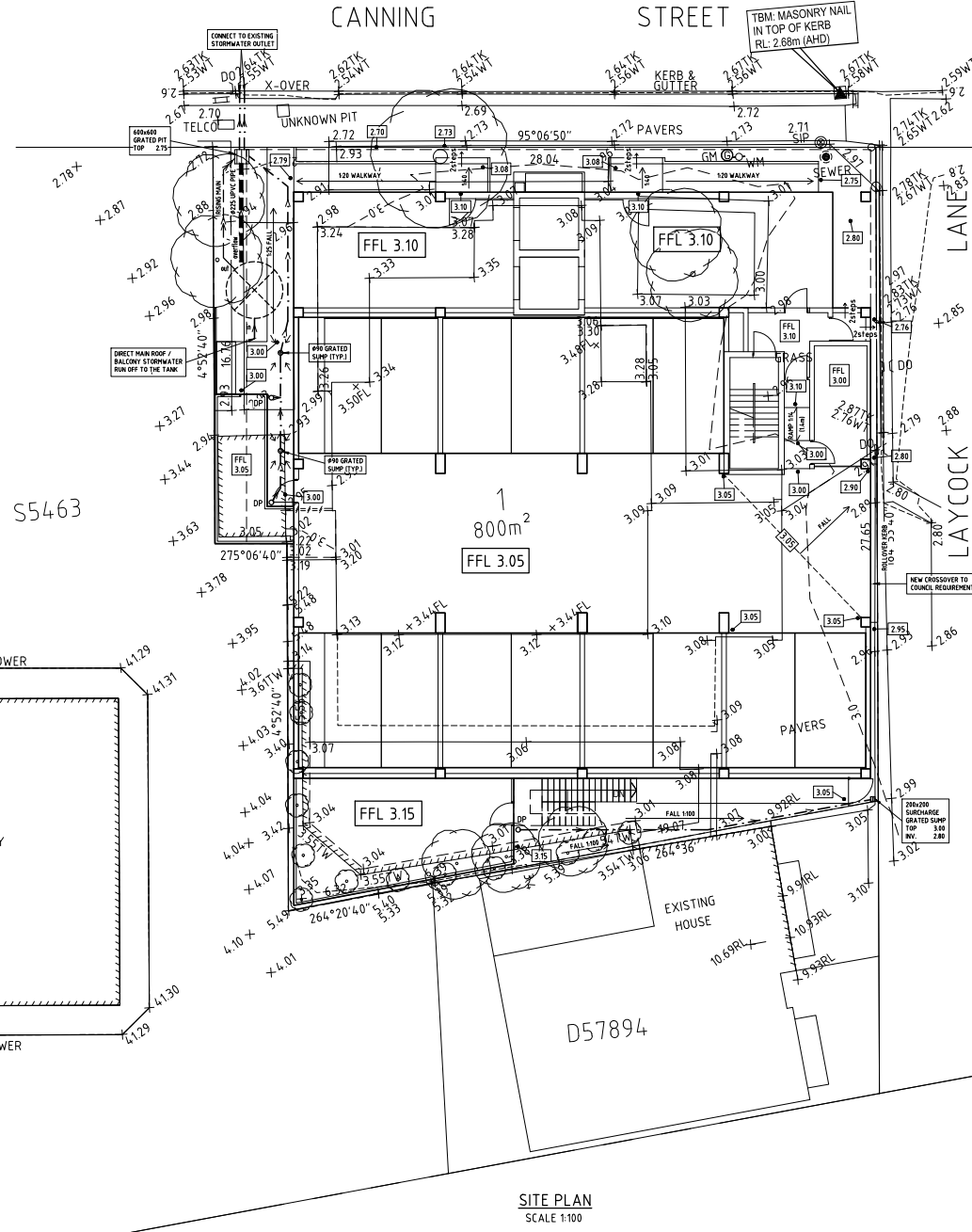
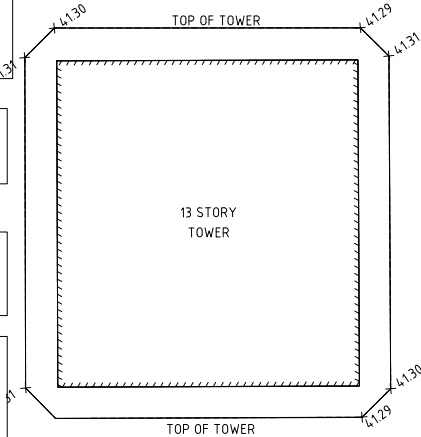
- STORMWATER ALIGNMENT #100 UPVC PIPE
- STORMWATER ALIGNMENT #225 UPVC PIPE
- UNDER GROUND SEALED SYSTEM #100 UPVC PIPE
- STORMWATER RISING MAIN DOWNPIPE
- #90 GRATED SUMP (TYP)
- OPEN UNLINED SURFACE DRAIN AT MIN FALL 1 in 200
- EXISTING LEVEL
- DESIGN LEVEL
- DESIGN CONTOUR
- INSPECTION OPENING
- SEWER I.P.
- SUMP
- JUNCTION BOX
- CONCRETE SLEEPER RETAINING WALL

INDICATES 9000L CONCRETE UNDERGROUND DETENTION TANK WITH BUILT IN DUAL PUMP & ALARM PUMP OUT RATE 9L/S. PROVIDE OVERFLOW TO GRATED SUMP. DIRECT STORMWATER FROM UPPER ROOF & BALCONY TO TANK.

PROVIDE PERIMETER CONCRETE KERBING AS REQUIRED.

NOTE: BUILDER/CONTRACTOR TO ENSURE UNDERGROUND TANK ARE INSTALLED OUTSIDE EXISTING OR PROPOSED FOOTING INFLUENT ZONE. OUR OFFICE SHALL BE CONTACTED FOR ADVISE & RECOMMENDATION IF THERE IS POSSIBILITY OF DESTABILIZING EXISTING OR PROPOSED STRUCTURE AS THE RESULT OF UNDERGROUND TANK INSTALLATION.

NOTE: ALL UNDERGROUND TANK MUST BE DESIGNED & BUILT WITH ANTI-FLOATATION SYSTEM TO MANUFACTURER'S SPECIFICATIONS TO OVERCOME HYDROSTATIC PRESSURE (UPLIFT) ASSOCIATED WITH UNDERGROUND TANK. ALL UNDERGROUND TANK MUST BE DESIGNED & INSTALLED STRICTLY AS PER MANUFACTURER'S REQUIREMENTS / SPECIFICATIONS FOR ITS INTENDED USE. ALL SITE FACTORS SHALL BE CONSIDERED SUCH AS TRAFFIC LOAD, CORROSION, etc.



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**NOTES**

- ENGINEER'S DRAWINGS ARE TO BE READ IN CONJUNCTION WITH THE SPECIFICATIONS AND GENERAL CONDITIONS OF THE CONTRACT. THE ASSOCIATED ARCHITECTURAL DRAWINGS, THE ENGINEER'S SOIL AND FOOTING CONSTRUCTION REPORT AND ANY OTHER DRAWINGS RELATING TO THIS PROJECT.
- ALL DIMENSIONS, LEVELS AND SETTING OUT SHALL BE VERIFIED WITH THE ARCHITECTURAL DRAWINGS AND CHECKED ON SITE PRIOR TO COMMENCING FABRICATION AND/OR CONSTRUCTION. THE ENGINEER'S DRAWINGS MUST NOT BE SCALED.
- ALL DIMENSIONS IN MM UNLESS OTHERWISE SPECIFIED.
- ADDITIONS AND SUBSTITUTIONS SHALL ONLY BE MADE WITH THE ENGINEER'S PRIOR KNOWLEDGE AND APPROVAL.
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**PLANNING APPROVAL**

PA.0	ISSUED FOR PLANNING APPROVAL	03/07/19	KS
	ARCH. DRAWING RECEIVED: 14/06/19		
	LEVEL RECEIVED: 14/06/19		

**PROJECT**  
11 - LEVEL APARTMENT

**ADDRESS**  
2 CANNING STREET, GLENELG NORTH

**DRAWING TITLE**  
SITE PLAN

**CLIENT**  
BARRIO DEVELOPMENTS

**STRUCTURAL SYSTEMS**  
consulting engineers

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Fax: (08) 8231 3444 Email: info@structuralsystems.com.au ABN 21 368 115 930

DRAWN	KS/LM	DESIGNED	NN
CHECKED		DATE REVISED	---
SCALE	1:100 UNO	PAPER SIZE	A1
ALL DIMENSIONS IN MM - DO NOT SCALE		DATE ISSUED	03/07/19
JOB No.	DT 190305	DRAWING No.	01
		STATUS	PA
		ISSUE	0

STAGE ABBREVIATION: P-PRELIMINARY, DS-ENGINEERING DESIGN STAGE, PA-FOR PLANNING APPROVAL, T-TENDER, BA-BUILDING APPROVAL, C-COR CONSTRUCTION



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ABN 21 366 115 939

Date Issued	Wednesday, 3 July 2019
Job No	DT 190305
Site	2 CANNING STREET, GLENELG NORTH
Client	BARRIO DEVELOPMENTS
Proposed	11 - LEVEL APARTMENT

# HYDROLOGICAL ANALYSIS

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Engineering Drawings

DT 190305.01.PA.0

Site Plan

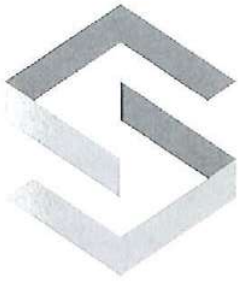
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Date Issued	Wednesday, 3 July 2019
Job No	DT 190305
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Proposed	11 - LEVEL APARTMENT
Eng	NN

Based on Council provide flood map & extract from "Marion & Adelaide Bay flood mapping 2013" estimate flood depth at the most critical point at North West corner is 0.2m - 0.4m

Based on detail survey, existing level at this location 2.53 AHD

→ Design flood level adopt  $2.53 + 0.4 = 2.93$  AHD

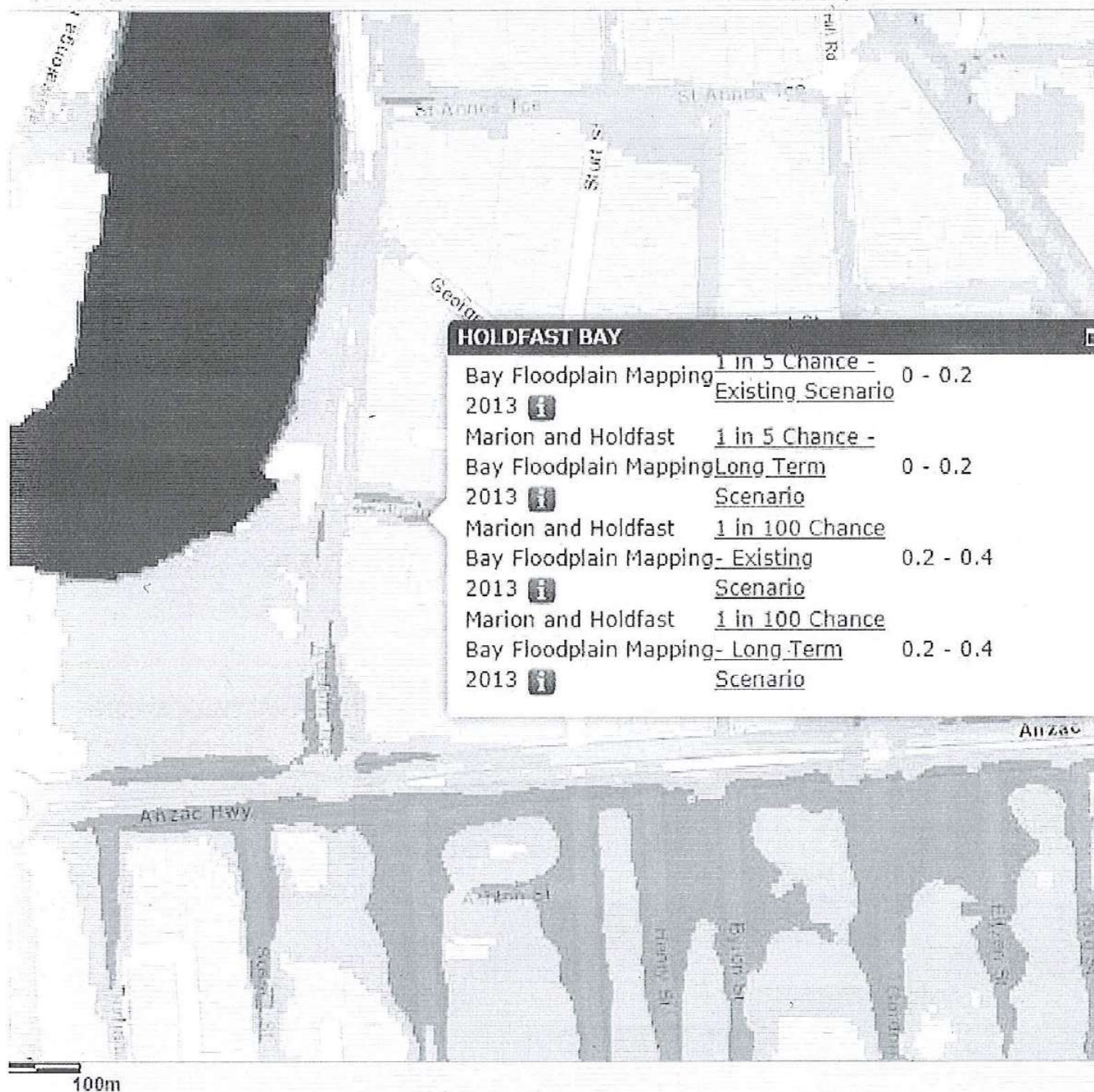
Adopt 0.15m freeboard → design FFL: 3.10 AHD

Based on available information, this proposed FFL will be sufficient protect the proposed property in 1 in 100 years ARI storm event.



CANNING STREET, GLENELG NORTH, SOUTH AUSTRALIA

Please be aware of the assumptions and limitations of flood maps. For further information refer to the FAQ link link on the website.

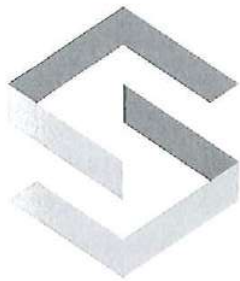


Flood map you can either:

Click on a study name in the right hand column and click on one of the layers for that study. When the Auto Zoom Map box will automatically zoom to the extent of the selected map layer, or

Use the search feature in the boxes directly above the map and either type in an address and selecting the correct address in box, choose a Suburb or choose a Council Area. Click on any point of the map and a text box will appear with flood depth.

Flood depth is available for some flood map layers by clicking on a location on the map.



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## Pre development:

The site consist of existing single storey house with extensive garden and parking.

Measured catchment areas:

$$A_{site} = 800 m^2$$

$$A_{roof} = 342 m^2$$

$$A_{pav} = 298 m^2$$

$$A_{garden} = 160 m^2$$

Estimate discharge rate to Canning street for  $t_c = 6 \text{ mins}$

$$Q_5 = 10.8 \text{ L/s}$$

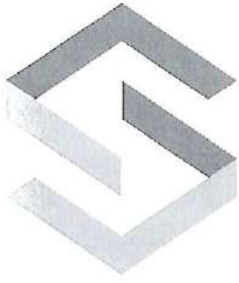
$$Q_{10} = 13.8 \text{ L/s}$$

$$Q_{100} = 30.2 \text{ L/s}$$

Council require post discharge rate not exceed predevelopment outflow rate.

Outlet discharge can be directed to Canning Street.  
With discharge rate for 1 in 5 years ARI storm not exceeding 10 L/s





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## Post development.

Proposed development is a multi storey building.

Area of main roof & balcony can be directed to detention tank

$$A_{\text{roof}_1} = 689 \text{ m}^2 \rightarrow \text{direct to detention tank}$$

Area of roof that not contribute to detention tank

$$A_{\text{roof}_2} = 15 \text{ m}^2 + 23 \text{ m}^2 = 38 \text{ m}^2$$

(laundry store)

Paving area that not cover by roof:

$$A_{\text{pav}} = 18 \text{ m}^2 + 5 \text{ m}^2 + 2 \text{ m}^2 = 25 \text{ m}^2$$

(rear paving) (Front side paving)

Landscape area

$$A_{\text{garden}} = 48 \text{ m}^2$$

Estimated discharge rate from roof, paving, pervious area that not contribute to detention tank:

$$Q_{5(1)} = 1.2 \text{ L/s}$$

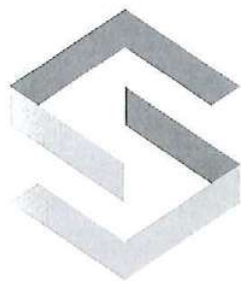
$$Q_{10(1)} = 1.5 \text{ L/s}$$

$$Q_{10(2)} = 3.2 \text{ L/s}$$

To satisfy Council requirement, allowable discharge rate from detention tank must not exceed.

$$Q_{5(2)} = Q_5 - Q_{5(1)} = 10.8 - 1.2 = 9.6 \text{ L/s}$$

$$Q_{10(2)} = Q_{10} - Q_{10(1)} = 13.8 - 1.5 = 12.3 \text{ L/s}$$



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Proposed

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Eng

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$$Q_{100(2)} = Q_{100} - Q_{100(1)} = 30.2 - 3.2 = 28 \text{ L/s}$$

Trial 7000 L under-ground tank for detention  
( $\phi 2.4 \text{ m} \times 2.4 \text{ m H}$ )

Refer Drain analysis output & input for estimated  
discharge outlet rate to Street WT.

For pump at 8-9 L/s pump out rate

Minor storm event 0.2 EY (1 in 5 years ARI)

$$Q_{out \max} = 8 \text{ L/s} < Q_{5(2)}$$

Major storm event 1% AEP (1 in 100 years ARI)

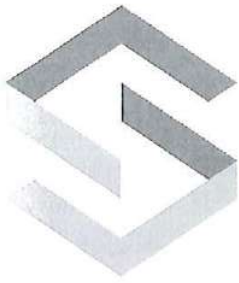
$$Q_{out \max} = 9 \text{ L/s} < Q_{100(2)}$$

PASS.

Maximum require detention volume is  $\approx 7000 \text{ L}$  at  
most-critical major storm event.

With contribution of detention tank, post-development flow rate  
less than predevelopment flow and less than 16 L/s  
in 1 in 5 years ARI critical storm event.





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Trial Ri industrial 9090L Under ground  
tank. Tank size  $\phi$  2.44m x 2.46m height

Internal tank size estimated

$\phi$  2.24m x 2.3m

Tank cover 150mm.

Tank height included cover :  $2.46 + 0.15 = 2.61m$

Adopt tank invert. 0.1m AHD

Date  
03.07.19

Job Number  
DT 190305 2 CANNING STREET, GLENELG NORTH

Engineer  
NN



DRAIN DIAGRAM - N.T.S

Date	Job Number	Site	Engineer
03.07.19	DT 190305	2 CANNING STREET, GLENELG NORTH	NN

#### PIT / NODE DETAILS

Name	Type	Family	Version 14 Size	Ponding Volume (cu.m)	Pressure Change Coeff. Ku	Surface Elev (m)	Max Pond Depth (m)	Base Inflow (cu m/s)	Blocking Factor	x	y	Bolt-down id	Part Full Shock Los	Inflow Hydrograph	Pit is	Internal Width (mm)	Inflow is Misaligned	Minor Safe Pond Depth (m)	Major Safe Pond Depth (m)
N1	Node					6.5		0		419		1			No				
OUT1	Node					2.75		0		833	-383 -374	7			No				

#### DETENTION BASIN DETAILS

Name	Elev	Surf. Area	Not Used	Outlet Typ	K	Dia(mm)	Centre RL	Pit Family	Pit Type	x	y	HED	Crest RL	Crest Leng	id
Detention	0.1	3.94		None							568	-389	No		6
	0.2	3.94													
	0.3	3.94													
	0.4	3.94													
	0.5	3.94													
	0.6	3.94													
	0.7	3.94													
	0.8	3.94													
	0.9	3.94													
	1	3.94													
	1.1	3.94													
	1.2	3.94													
	1.3	3.94													
	1.4	3.94													
	1.5	3.94													
	1.6	3.94													
	1.7	3.94													
	1.8	3.94													
	1.9	3.94													
	2	3.94													
	2.1	3.94													
	2.2	3.94													
	2.3	3.94													

#### SUB-CATCHMENT DETAILS

Name	Pit or Node	Total Area (ha)	Paved Area %	Grass Area %	Supp Area %	Paved Time (min)	Grass Time (min)	Supp Time (min)	Paved Length (m)	Grass Length (m)	Supp Length (m)	Paved Slope(%)	Grass Slope %	Supp Slope %	Paved Rough	Grass Rough	Supp Rough	Lag Time or Factor	Gutter Length (m)	Gutter Slope %	Gutter FlowFactor	Rainfall Multiplier
Roof and E N1		0.0569	100	0	0	6	10	0										0				1

#### PIPE DETAILS

Name	From	To	Length (m)	U/S IL (m)	D/S IL (m)	Slope (%)	Type	Dia (mm)	I.D. (mm)	Rough	Pipe Is	No. Pipes	Chg From	At Chg	Chg (m)	RI (m)	Chg (m)	RL (m)	etc (m)
Pipe 1	N1	Detention Tank	10	6	5.1	9	uPVC, not	150	154	0.03	NewFixed	1	Detention Tank	0					

This model has no pipes with non-return valves

Date	Job Number	Site	Engineer
03.07.19	DT 190305	2 CANNING STREET, GLENELG NORTH	NN

DRAINS results prepared from Version 2019.03

#### PIT / NODE DETAILS

Name	Max HGL	Max Pond HGL	Max Surface Flow (cu.m/s)	Max Pond Volume (cu.m)	Min Freeboard (m)	Overflow Constraint (cu.m/s)
N1	6.04		0.015			
OUT1	2.75		0			

#### SUB-CATCHMENT DETAILS

Name	Max Flow Q (cu.m/s)	Paved Max Q (cu.m/s)	Grassed Max Q (cu.m/s)	Paved Tc (min)	Grassed Tc (min)	Supp. Tc (min)	Due to Storm
Roof and E	0.013	0.013	0	6	10	0	0.2EY AEP, 10 min burst, Storm 8

#### PIPE DETAILS

Name	Max Q (cu.m/s)	Max V (m/s)	Max U/S HGL (m)	Max D/S HGL (m)	Due to Storm
Pipe 1	0.013	3.27	6.042	5.14	0.2EY AEP, 10 min burst, Storm 8

#### CHANNEL DETAILS

Name	Max Q (cu.m/s)	Max V (m/s)	Due to Storm
------	----------------	-------------	--------------

#### OVERFLOW ROUTE DETAILS

Name	Max Q U/S	Max Q D/S	Safe Q	Max D	Max DxV	Max Width	Max Due to Storm
Pump1	0.007	0.007					0.2EY AEP, 10 min burst, Storm 3

#### DETENTION BASIN DETAILS

Name	Max WL	MaxVol	Max Q Total	Max Q Low Level	Max Q High Level
Detention	0.48	1.5	0.007	0	0.007

Run Log for DT190305\_Prel detention cal.drn run at 12:13:15 on 14/6/2019

Date	Job Number	Site	Engineer
03.07.19	DT 190305	2 CANNING STREET, GLENELG NORTH	NN

DRAINS results prepared from Version 2019.03

#### PIT / NODE DETAILS

Name	Max HGL	Max Pond HGL	Version 8		Min Freeboard (m)	Overflow Constraint (cu.m/s)
			Max Surface Flow (cu.m/s)	Max Pond Volume (cu.m)		
N1	6.06		0.03			
OUT1	2.75		0			

#### SUB-CATCHMENT DETAILS

Name	Max Flow Q (cu.m/s)	Paved Max Q (cu.m/s)	Grassed Max Q (cu.m/s)	Paved Tc (min)	Grassed Tc (min)	Supp. Tc (min)	Due to Storm
Roof and Bal	0.027	0.027	0	6	10	0	1% AEP, 10 min burst, Storm 3

#### PIPE DETAILS

Name	Max Q (cu.m/s)	Max V (m/s)	Max U/S HGL (m)	Max D/S HGL (m)	Due to Storm
Pipe 1	0.027	3.85	6.063	5.163	1% AEP, 10 min burst, Storm 3

#### CHANNEL DETAILS

Name	Max Q (cu.m/s)	Max V (m/s)	Due to Storm
------	----------------	-------------	--------------

#### OVERFLOW ROUTE DETAILS

Name	Max Q U/S	Max Q D/S	Safe Q	Max D	Max DxV	Max Width	Max V	Due to Storm
Pump1	0.009	0.009						1% AEP, 25 min burst, Storm 1

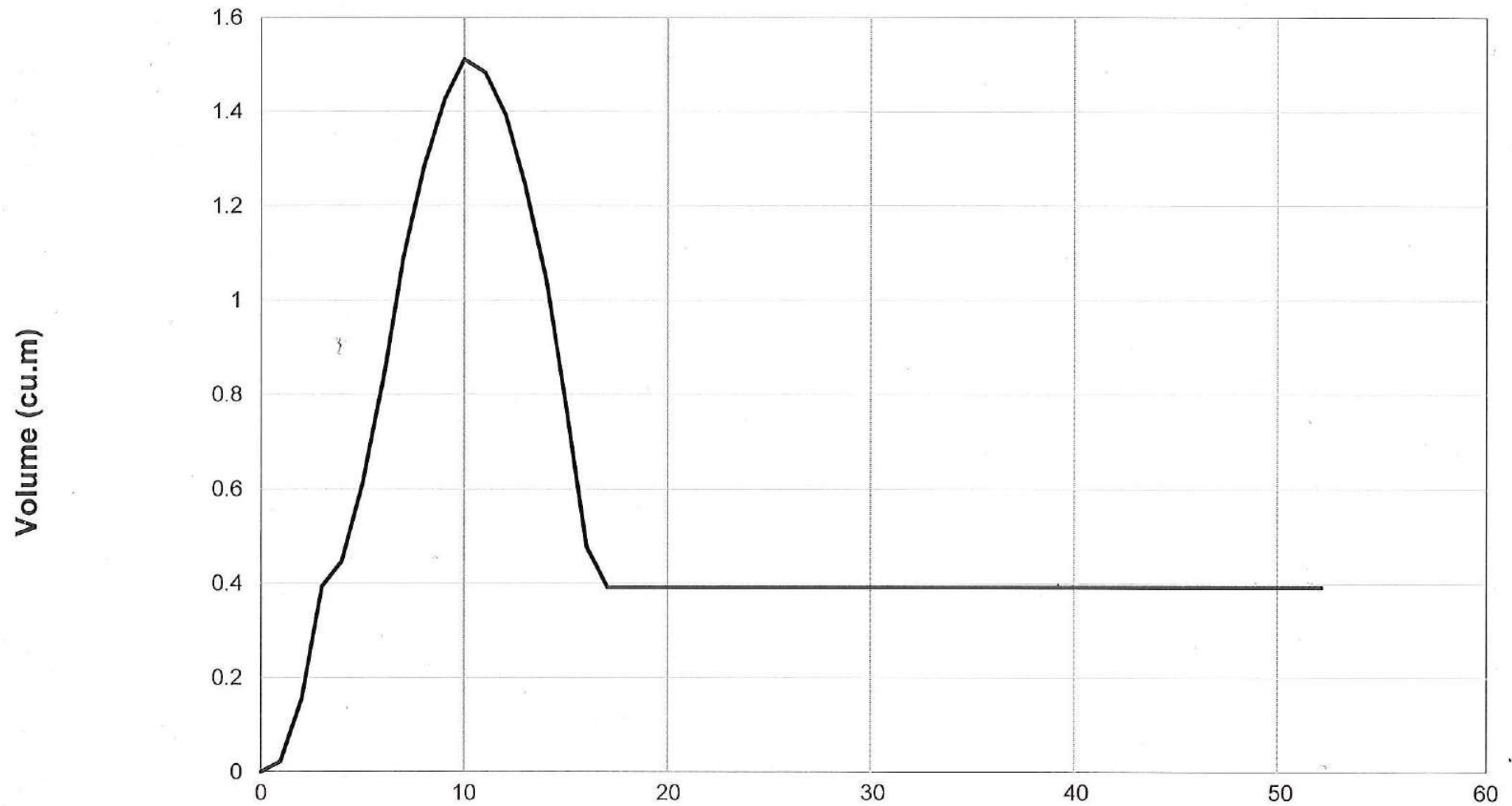
#### DETENTION BASIN DETAILS

Name	Max WL	Max Vol	Max Q Total	Max Q Low Level	Max Q High Level
Detention Tank	1.87	7	0.009	0	0.009

Run Log for DT190305\_Prel detention cal.drn run at 12:13:15 on 14/6/2019

(1 in 5 years ARI)

## Detention Tank Storage Volume - 0.2EY AEP, 10 min burst, Storm 3



Date  
03.07.19

Job Number  
DT 190305

Site  
2 CANNING STREET, GLENELG NORTH

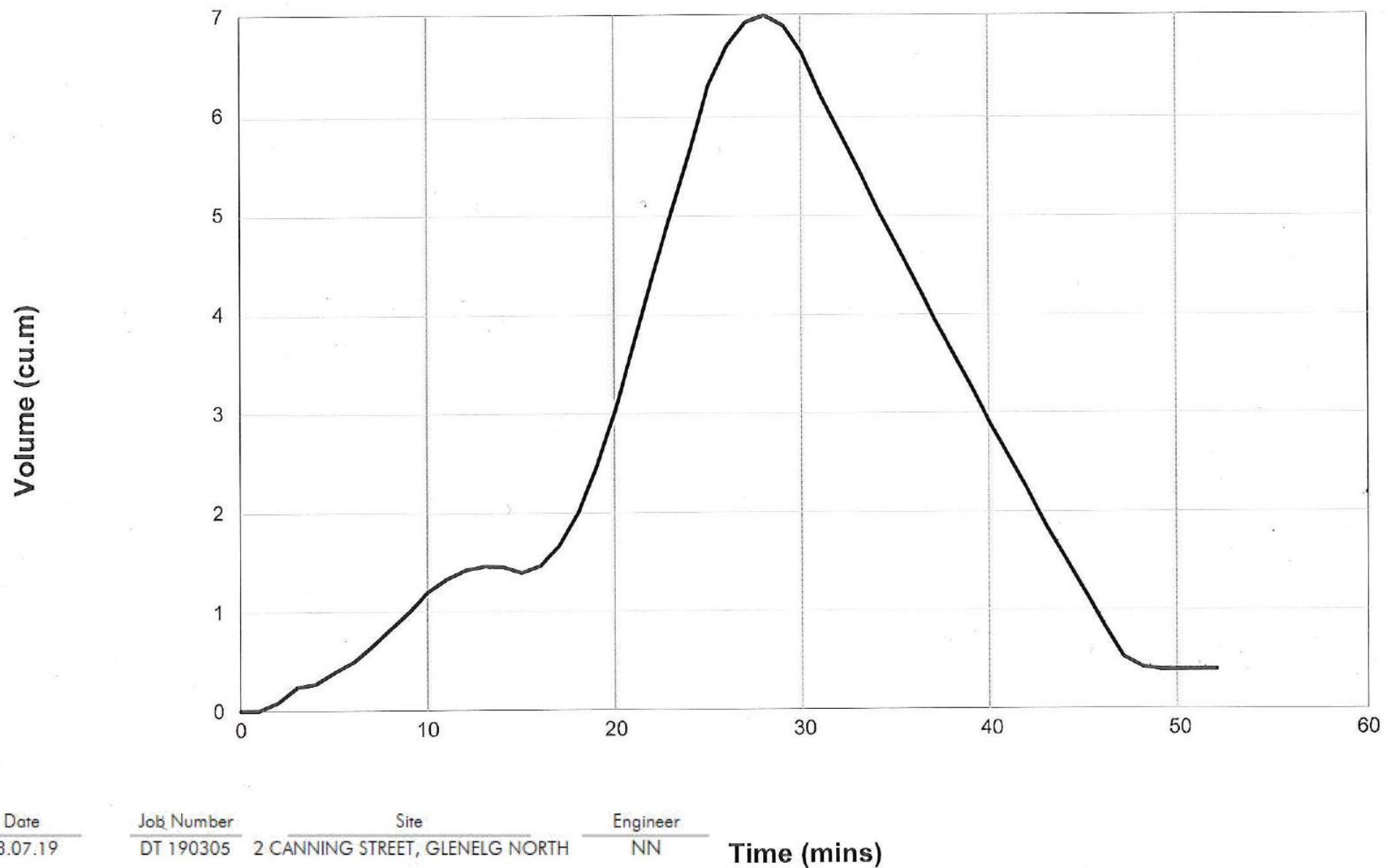
Engineer  
NN

Time (mins)



1 in 100 years ARI

## Detention Tank Storage Volume - 1% AEP, 25 min burst, Storm 1







## ESD Statement

2 Canning Street

**Reference:** SH110749

**Date:** 4 June 2019

**Assessment of:**

Proposed Residential Development

2 Canning Street, Glenelg, SA 5045

**Report commissioned by:**

Barrio Developments

**Responsible authority:**

City of Holdfast Bay

**Contact:**

Tom Symons

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## Document Control

<b>Job Title</b>	2 Canning Street	<b>SH Reference:</b>	SH110749
<b>Document Title</b>	ESD Statement	<b>File Reference:</b>	R:\...\SH110749\reports
<b>File Name</b>	ESD Statement_2 Canning Street_SH110749		

Version	Date	Prepared by	Checked by	Approved by	Comments
0	13/05/2019	TS	DN	JW	Draft Revision
1	4/06/2019	TS	LV	JW	Final

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## Executive Summary

SUHO has been engaged by Barrio Developments to provide an ESD Statement to support the Development Application for the proposed mixed-used residential development at 2 Canning Street, Glenelg, SA. This project is within the jurisdiction of the City of Holdfast Bay. The Council ESD requirements for this project have been outlined in this report, along with the project's design response. The purpose of this document is to demonstrate Council ESD requirements have been achieved. The key sustainable design strategies considered in the development include:

- High performance building fabric and glazing
- Solar PVs for onsite energy generation
- Energy efficient building services, including HVAC, lighting and DHW systems
- Water efficient fixtures and fittings
- Green landscaping and facades with preference for drought tolerant and/or native vegetation
- Good access to natural daylight and ventilation
- Adequate balconies to improve resident amenity and connection to the outdoors
- Car stacking system allowing for 1 bicycle/1 car per apartment to encourage active modes of transport for residents. Bicycle parking for visitors has also been provided off Laycock Lane.



# 1 Introduction

SUHO has been engaged by Barrio Developments to prepare an Environmentally Sustainable Design (ESD) Statement to support the Development Approval application of the proposed 2 Canning Street development.

## 1.1 Purpose

The purpose of this document is to demonstrate the client's commitment to ESD initiatives in the 2 Canning Street development and outline Development Plan compliance.

## 1.2 Project Overview

The 2 Canning Street development is a 7-storey apartment building. There are 20 residential two and three bedroom residential apartments, with an additional 38 tourist accommodation one bedroom serviced apartments located on the first and second floors. The project also includes a 40 car stacking facility.

## 1.3 Planning Requirements

The local planning requirements generally relate to managing solar exposure, managing waste, water sensitive urban design and minimising energy consumption. These requirements and objectives are outlined in Section 3 of this report, along with the design response. Barrio Developments is committed to delivering a project that exceeds Council requirements, for a market that has growing expectations for ESD.

## 1.4 Basis of Assessment

This document and all related assessments have been based on the following:

- Project discussions and email correspondence with Brown Falconer Architects and Barrio Developments.
- The architectural concept design package set from Brown Falconer Architects. Received at the start of June 2019.

## 1.5 Sustainability Categories

This Statement categorises the proposed ESD initiatives into 8 broad sustainability categories. These categories align with Green Star; a widely recognised and applied rating tool across the industry. These categories are summarised below:

- Management
- Indoor Environment Quality
- Energy
- Transport
- Water
- Materials
- Land Use & Ecology
- Emissions



## 2 ESD Features

The following is a summary of the ESD initiatives included in each of the categories.

### 2.1 Management

The following describes items relating to Management included in this project.

#	Initiative	Description	Responsibility
2.1.A	Environmental Management Plan	The preferred contractor will develop a site-specific Environmental Management Plan prior to construction commencement. ISO 14001 Environmental Management System accreditation will also be highly regarded when considering tenderers.	Contractor
2.1.B	Waste Management Plan	Waste will be collected privately and adequate bin space will be provided for the landfill and recycling waste. The residential apartments will have waste and recycling collection points on each floor. FOGO (Food and Garden Organics) waste will be managed separately by each tenant. Allowance for future shared FOGO waste management will be considered if necessary. The ground floor has been designed to allow for trucks to pull in to collect the waste off the street.	Waste Consultant
2.1.C	Building Information	The building will incorporate a number of smart technologies. The client is currently exploring the option of potentially including a BMS Lite technology with the idea of creating a share economy between the residential apartments on top of some building operational efficiencies.	BMS Contractor

### 2.2 Indoor Environment Quality

The following describes items relating to Indoor Environment Quality (IEQ) included in this project.

#	Initiative	Description	Responsibility
2.2.A	Natural Ventilation	All of the apartments and tourist accommodation will have openable windows improving the liveability of the bedrooms and living areas. While this will reduce energy consumption, it will also improve the connection to the outdoors and allow for fresh air into these spaces.	Architect
2.2.B	Daylight	Windows have been provided to the corridors on each floor, allowing good levels of natural light into these spaces that would otherwise rely on 100% artificial lighting.	Architect



		All bedrooms and living rooms have been provided with large windows to allow for abundant natural light. The majority of bathrooms also have windows. This design initiative will allow for high indoor environment quality, while also minimising the need for lighting thus reducing energy consumption.	
--	--	--	--

### 2.3 Energy

The following describes items relating to Energy included in this project.

#	Initiative	Description	Responsibility
2.3.A	Building Fabric	Where appropriate, the building will include additional insulation that significantly improves upon the NCC reference case. Actual building fabric system performance values will be confirmed following detailed energy modelling.	ESD Consultant / Architect
2.3.B	Glazing	High performance double glazing, most likely with a warm grey tint will be provided throughout the development to provide good performance and reduce solar heat gain. Actual glazing thermal performance will be confirmed following detailed energy modelling.	ESD Consultant / Architect
2.3.C	Solar PV	The project includes an indicative area of 260m <sup>2</sup> for the inclusion of solar photovoltaic (PV) panel. This area can accommodate a total system size of 30kWp, which can generate approximately 40MWh per annum with a 10° inclination.  This energy will be used for common area services and the feasibility for future battery storage will be explored at a later stage. Detailed modelling may be undertaken at a later date to optimise the solar layout.	ESD Consultant / Contractor
2.3.D	HVAC	The building's heating and cooling will be provided by efficient VRF / VRV systems. These systems generally have a coefficient of performance (COP) of at least 3.5, but can achieve COPs of greater than 5.0 depending on the system configuration and environmental conditions.	Mechanical Designer / Contractor
2.3.E	Lighting	The project will generally include energy-efficient LEDs throughout. This initiative will enable the development to achieve an overall lighting power density of no more than 4W/m <sup>2</sup> .	Lighting Designer / Contractor
2.3.F	Lighting Control	Common area lighting, excluding safety lighting, will be provided with daylight/motion sensors where applicable.	Lighting Designer / Contractor





2.3.G	Domestic Hot Water System	The project proposes a bulk hot water storage system to reduce overall energy consumption and greenhouse gas emissions when compared with having individual units for each apartment. There is the potential to incorporate a solar boosted option, this will be confirmed at a later date once further analysis has been conducted.	Hydraulic Consultant / Contractor
2.3.H	Appliances	Whitegoods that are included in the development, such as dishwashers, will have a minimum 4 Star Energy Rating.	Architect / Contractor

## 2.4 Transport

The following describes items relating to Transport included in this project.

#	Initiative	Description	Responsibility
2.4.A	Bicycle Parking Facilities	The proposed car stacker has been selected with the ability to park both a car and bicycle in each apartment's parking space. Visitor car parking spaces have also been provided off Laycock Lane. The care stacker will be accessible at grade to allow for ease of access and avoid cyclists having to unnecessarily lift their bikes.	Architect / Contractor
2.4.B	Walk Score	Based on the project's location, the Walk Score for the site is 87. This means that the project is within close proximity of key businesses and shops allowing for most errands can be accomplished on foot.	Architect

## 2.5 Water

The following describes items relating to Water included in this project.

#	Initiative	Description	Responsibility
2.5.A	Efficient Fixtures and Fittings	Through the use of water-efficient fixtures and fittings the development intends to reduce its potable water consumption wherever possible. As a guide selections for fittings and fixtures would include WELS 6 Star taps, 4 Star toilets, and 3 Star showers.	Architect / Contractor
2.5.C	Landscape Irrigation	It is intended that if required, landscape irrigation will be efficient and most likely delivered via water saving sub-surface drip systems.	Landscape Designer / Contractor



## 2.6 Materials

The following describes items relating to Materials included in this project.

#	Initiative	Description	Responsibility
2.6.A	Construction & Demolition Waste	During demolition and construction phases, the contractor is to ensure as much material is recycled as practical. Individual bins to separate waste streams will improve recycling rates on site. Sandstone from the existing dwelling may also be recycled (See Section 2.6C).	Contractor
2.6.B	Sustainable Timber	All major timber in this development will be sustainably sourced and hold either FSC or PEFC/AFS certification.	Architect / Contractor
2.6C	Reuse of Materials	To both incorporate recycled materials and tie the building to Glenelg's Heritage the building may boast the reuse of local materials. This could include the recycling and re-use of the demolished dwellings sandstone and the potential inclusion of recycled timber from the Buffalo.	Architect / Designer / Contractor

## 2.7 Land Use & Ecology

The following describes items relating to Land Use & Ecology included in this project.

#	Initiative	Description	Responsibility
2.7.A	Large Balconies	Each residence is provided with large balconies with floor waste traps to encourage outdoor living and urban-scale gardening.	Architect / Contractor
2.7.B	Deep Soil Zones	Buffer zones have been provided at the rear and side of the site. These have been incorporated where possible between the neighbouring homes and the building.	Landscape Designer / Contractor

## 2.8 Emissions

The following describes items relating to Emissions included in this project.

#	Initiative	Description	Responsibility
2.8.A	Waterless Heat Rejection	The building does not utilise any heat-rejection water. This is achieved through the adoption of VRF / VRV heating and cooling.	Mechanical Designer / Contractor
2.8.B	Light Pollution	All external lighting that is not required for pedestrian safety will be positioned to avoid direct light pollution to the night sky.	Lighting Designer / Contractor



### 3 Council ESD Requirements and Objectives

The following table provides a summary of the overall design response in relation to Council requirements and objectives.

#	Objective	Design Response
Holdfast Bay Council Development Plan, Page 10.	<p>Council Strategic Setting:</p> <p><i>"The Council is committed to building a strong community, creating a sustainable environment, delivering economic prosperity, and enhancing city design and function along with investments in bike paths and delivery of the shared-use metropolitan Coast Park through the Council area."</i></p>	<p>The development's design approach was to capture the essence of Glenelg community living, engage with the location and to be sustainable through the embedded environmentally responsive design.</p> <p>While the addition of the 38 tourist accommodation apartments will offer additional employment opportunities for local residents of the Glenelg community.</p>
Holdfast Bay Council Development Plan, Page 34.	<p>Overshadowing</p> <p><i>"10 The design and location of buildings should enable direct winter sunlight into adjacent dwellings and private open space and minimise the overshadowing .."</i></p>	<p>During the design process overshadowing has been taken into consideration, based on both existing and the approved addition of a 14 story hotel, which is in close proximity.</p>
Holdfast Bay Council Development Plan, Page 36.	<p>PRINCIPLES OF DEVELOPMENT CONTROL</p> <p><i>"1 Development should provide for efficient solar access to buildings and open space all year around.</i></p> <p><i>2 Buildings should be sited and designed:</i></p> <p><i>(a) to ensure adequate natural light and winter sunlight is available to the main activity areas of adjacent buildings</i></p> <p><i>(b) so that open spaces associated with the main activity areas face north for exposure to winter sun</i></p> <p><i>(c) to promote energy conservation by maintaining adequate access to winter sunlight to the main ground level of living areas of existing dwellings on adjoining land.</i></p> <p><i>3 Except for buildings that take advantage of coastal views, development should promote the efficient consumption of energy through the use of larger but appropriately shaded windows on the north and east building surfaces and</i></p>	<p>The building has been designed with solar access and natural light as a key design principle.</p> <p>The development has been designed to provide appropriate shading windows to the north and west of the building.</p> <p>Western windows have also been limited at approximately a 50/50 wall to glazing ratio combined with high performance glazing this will allow for adequate solar access while also minimising solar heat gain during summer from the western sun.</p>



	<i>smaller windows on the south and west building surfaces."</i>	
Holdfast Bay Council Development Plan, Page 36.	<p><i>On-site Energy Generation</i></p> <p><i>"4 Development should facilitate the efficient use of photovoltaic cells and solar hot water systems by:</i></p> <p><i>(a) taking into account overshadowing from neighbouring buildings</i></p> <p><i>(b) designing roof orientation and pitches to maximise exposure to direct sunlight.</i></p> <p><i>5 Public infrastructure and lighting, should be designed to generate and use renewable energy."</i></p>	<p>The development includes allowance for a potential 30kWp onsite energy generation system. The system will be on the roof of the building which will not be impacted by neighbouring overshadowing or vegetation for much of the year. However the proposed 14 Storey Hotel will most likely render the Solar PV somewhat redundant during the middle of winter.</p> <p>The relatively flat roof will allow for a maximum number of panels to be installed on the roof with minimal self-shading.</p> <p>It is the intent of the developer to use the Solar PV system to directly supply where possible and if not offset the buildings facilities.</p>
Holdfast Bay Council Development Plan, Page 64.	<p><i>Water Sensitive urban design</i></p> <p><i>"5. Development should be designed to maximise conservation, minimise consumption and encourage reuse of water resources."</i></p>	<p>The building will features water saving features throughout to minimise water usage wherever possible. This includes high efficiency taps, shower heads, toilets and appliances.</p>
Holdfast Bay Council Development Plan, Page 66.	<p><i>Biodiversity and Native Vegetation</i></p> <p><i>"26 Development should retain existing areas of native vegetation and where possible contribute to revegetation using locally indigenous plant species."</i></p>	<p>Minimal native vegetation is currently on the existing site, with the existing dwelling having a relatively sparse garden. The proposed development will incorporate native and drought tolerant vegetation.</p>
Holdfast Bay Council Development Plan, Page 86.	<p><i>Site Facilities and Storage</i></p> <p><i>"47 Site facilities for group dwellings, residential parks and residential flat buildings and should include:</i></p> <p><i>(b) bicycle parking for residents and visitors</i></p> <p><i>(c) household waste and recyclable material storage areas away from dwellings"</i></p>	<p>Adequate bicycle facilities will be available for both visitor and residential spaces available. Visitor facilities will be located outside the front of the building. While the residents will each have a space in the car stacker that allows sufficient space for both a car and a bicycle to be stored.</p> <p>As discussed on in the next row waste and recyclable storage areas will be located in a designated area on the ground floor. Sufficiently away from the dwellings on the floors above.</p>
Holdfast Bay Council Development	<p><i>Waste</i></p> <p><i>"Development that, in order of priority,</i></p>	<p>The waste disposal area for the development will be located in the ground floor basement area. It should be screened</p>



Plan, Page 105.	<i>avoids the production of waste, minimises the production of waste, reuses waste, recycles waste for reuse, treats waste and disposes of waste in an environmentally sound manner."</i>	<p>and separated from adjoining areas, designed and have an impervious surface to ensure that wastes cannot contaminate storm water.</p> <p>There will be a serviced waste collection point on each of the residential apartment levels.</p> <p>Waste will be collected privately and adequate bin space will be provided for the landfill and recycling waste.</p> <p>The ground floor has been designed to allow for trucks to pull in collecting the waste off of the street.</p>
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## 4 Conclusion

Based on the above inclusions and the client's commitment to ESD, the project satisfies council requirements for a development of this nature. The development has strived to achieve this through the design approach with its three main principles of capturing the essence of Glenelg's community living, engaging with the location all with a sustainable approach.



File No:  
2014/11235/01

31 July 2019

Ref No:  
14308122

Mr Will Gormly  
Senior Planning Officer – City & Inner Metro  
Development Assessment  
Planning and Land Use Services  
Department of Planning, Transport and Infrastructure  
Level 5, 50 Flinders Street  
Adelaide SA 5000

[will.gormly@sa.gov.au](mailto:will.gormly@sa.gov.au)

For the attention of the State Commission Assessment Panel

## 2 Canning Street, Glenelg North

Further to the referral DA 110/M004/19 received 8 July 2019 pertaining to the development application at the above address and in my capacity as a statutory referral in the State Commission Assessment Panel, I am pleased to provide the following comments informed by the Design Review process for your consideration.

The proposal was presented to the Design Review panel on one occasion.

This proposal presents a significant opportunity due to the site's key location in proximity to the beach, Wigley Reserve and Jetty Road. I strongly support the aspiration to deliver a residential development in this part of Glenelg. I am of the opinion that the proposed development should capitalise on the potential of the unique and varied context, and deliver a high benchmark for residential design quality. Fulfilling this responsibility will be contingent on achieving a high quality outcome particularly in terms of scale, massing, residential amenity and the relationship of the built form to its context. While the proposal affords apartments with good residential amenity, I am of the view that other envisaged outcomes are yet to be successfully demonstrated.

The subject site is located on the north east corner of Canning Street and Laycock Lane. The current built form context of Canning Street is predominantly low scale buildings of varying character, including a two storey Comfort Inn with primary frontage to Adelphi Terrace, single storey sandstone and bluestone cottages to the north of the street, and two storey contemporary terraces to the south of the street. The site currently contains a single storey sandstone residence to be demolished as part of the development application. Laycock Lane is a cul-de-sac that provides vehicular access to a number of recently constructed two storey residences. To the immediate south of the subject site is a two storey residence. Private open space for this residence is located to the east on adjacent Laycock Lane in the form of a ground floor enclosed courtyard and level one balconies, and an additional private open space is located to the north of the residence, adjacent the site boundary of the 2 Canning Street site. The adjoining property to the west of the subject site is a 13 storey building that comprises one apartment per level.

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of South Australia

File No:  
2014/11235/01

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14308122

This adjoining site has a large landscaped setback from Canning Street and affords the subject site clear views to the marina and ocean from the upper levels of the development. The site of the existing two storey Comfort Inn at 6-10 Adelphi Terrace has a development approval for a 14 storey hotel and apartments that was granted development authorisation as a Major Development.

The proposal is for a seven storey apartment building comprising two ground floor entrance foyers, car parking stackers, two levels of serviced apartments and five levels of residential apartments. I support the proposed mix of uses. I also support the project team's aspirations to promote liveability through high quality amenity, contribution to neighbourhood and place, and a broader contribution through sustainability initiatives. However, I am yet to be convinced that the proposal has fully explored the opportunities to incorporate sustainability measures, and fulfil the aspirations.

The proposed building height is seven storeys and 23.85 metres to the top of the roof, excluding the rooftop plant and lift overruns. This height exceeds the five storey (18.5 metre) height limit envisaged by the Development Plan for this site. I acknowledge the site adjoins a policy area that allows for development up to 12 storeys to the west, and the precedent for taller buildings set by existing and approved developments in the locality. However, my support for any additional height in this location is contingent on mitigating visual, physical and amenity impacts, providing a high quality design and amenity outcome, and achieving a significant contribution to the public realm. In my view, sufficient merit to justify support for a development with a significant overheight element is yet to be demonstrated. The design approach is for four elements that express the different uses of the building through materiality and architectural expression. The overall built form predominantly extends to all site boundaries with the exclusion of the west boundary adjacent the tower site. In my view, greater differentiation between the building elements should be achieved through increased setbacks, rather than relying on changes in materiality and architectural expression, to mitigate the mass and bulk of the development in the residential context. I recommend holistic review of the height, massing and setbacks with the view to respecting the existing residential context, and management of the interfaces with neighbouring buildings in particular the private open space to the existing residence to the south. I support the consolidated rooftop plant area for air conditioning condensers, however I recommend confirmation is provided of the screening strategy for the units and overall height.

The ground floor level is expressed as a glazed element to Canning Street, with the intent to provide engagement with the street through materiality and provision of landscape and external seating areas. The east elevation with frontage to Laycock Lane has a minimal setback to the majority of the facade, and a recessed area at the north east corner for external bicycle parking. The balance of the site along the west and south boundaries provide for landscape edges and ancillary spaces for the serviced apartments to the west, and a consolidated residential storage area, stair egress and landscape edge to the south. The Canning Street frontage comprises separated entrance foyers and lounge spaces for the serviced accommodation and residential apartments. A waste collection area is proposed adjacent the Laycock Lane boundary, to provide access for residents and waste collection via the rear car park. I support the separation of serviced apartment and residential entrances to provide a sense of address for all users and improve activation of Canning Street. I also support the provision of bicycle parking spaces for residential bike share and visitor bicycle parking, however I recommend further details of the bicycle parking recessed area and existing footpath to demonstrate safe manouvering and minimal impact on pedestrian and vehicle movements. I also recommend consideration of a strategy for management of access to the rear/south area that ensures security and

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convenience for the private residential storage units while maintaining egress from the level one and two serviced apartments, and ensuring a safe environment for all users, informed by Crime Prevention Through Environmental Design (CPTED) principles.

I support the proposed material and colour selections, and use of authentic materials including full bricks, re-used sandstone and bronze metalwork. I also support the residential expression of the building elements above the ground floor level. However, I am concerned by the composition of the ground floor glazing and columns which in my opinion is inconsistent with the overall architectural expression. I recommend exploration of opportunities to extend aspects of the residential expression through to the base element to anchor the design at ground level and provide a finer grain articulation.

Above the ground floor, the serviced apartments on levels one and two include cantilevered balconies to the west and concrete slab edge extensions providing solar shading. Levels three to six follow a similar building footprint to the lower levels, with the concrete slab extensions providing solar shading and deep balconies. Additional balconies are proposed to the south of the development. The horizontal expression of the apartment levels and roof intends to reflect the residential nature of the development which I support. I also support the design team's consideration of resident amenity and framing of views. In principle, I support the expression of the floor plates and horizontal articulation of the development and acknowledge their contribution in fulfilling the sustainability aspirations of the project. However, I have concerns regarding the depth of solar shading to the north elevation for levels one and two, and the lack of solar shading to the central corridor spaces to levels one to five and full height glazing sections to the west elevation. I recommend demonstration of effective solar shading to these elements with the view to ensuring delivery of the environmental intent for each elevation and all levels of the development.

The proposal includes two levels of serviced apartments that are separated from the four levels of private residential apartments above, which I support. Each residential apartment level comprises two three-bedroom apartments and three two-bedroom apartments. I support the mix of residential apartments. I also support the provision of natural light to the central circulation space. Since the Design Review session, some of the apartment layouts have been rationalised and an additional dwelling has been proposed on each level, resulting in a comparable built form. I remain concerned regarding the extensive internal areas that have affected the overall bulk and mass of the building. In my view, an opportunity exists to review the apartment floor configuration, including the number of apartments, with the view to reducing the building footprint, mass and bulk, address interface issues with the neighbouring properties and better respond to the existing residential context. I support the ambition to provide a high quality residential offering for this significant site, and recommend consideration of opportunities to further support the ambition including increased external private open space and provision of waste chutes. I remain concerned by overlooking of the existing two storey residence to the south as demonstrated in the overlooking diagrams and recommend further review of the southern setback to mitigate this impact.

The proposal includes a number of landscape elements, which I support in principle. I also support the early engagement of the landscape architect and recommend ongoing discussions to ensure an integrated approach, species selection appropriate to the environment and successful delivery of the design intent.

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Vehicle access to the car stacker is via a dual lane driveway to Laycock Lane. I recommend ongoing engagement with the traffic consultant to ensure potential impacts on the locality including lines of sight are mitigated. I anticipate the suitability of car stackers for serviced apartment use has been duly considered by the project team, including the anticipated frequency of use and complex operational requirements. Access from the car stackers is proposed at the east portion of the site between the stair core and waste collection area. I recommend review of the circulation strategy from the car park to the lift lobby to ensure a sense of address commensurate with the high quality offering envisaged.

The submitted material includes the Sustainability Report that lists proposed sustainable measures such as high performance glazing, space allowance for potential photovoltaic panel installation, and energy efficient fittings and appliances which I support. However, I am yet to be convinced the proposal has fully explored the opportunities for integrated Ecologically Sustainable Design (ESD) outcomes consistent with the project team's ambition to deliver a high quality outcome underpinned by a strong project vision with sustainability initiatives. In my view the opportunity exists to further strengthen the development's response to the project vision and achieve a high quality outcome commensurate with the significant site.

To ensure the most successful design outcome is achieved the State Commission Assessment Panel may like to consider particular aspects of the project that require further consideration or which would benefit from protection as part of the planning permission, such as:

- Review of the height, massing and setbacks with the view to respecting the existing residential context, and management of the interfaces with neighbouring buildings in particular the private open space to the existing residence to the south.
- Confirmation of the screening strategy for the rooftop condenser units and overall height.
- Details of the bicycle parking recessed area and existing footpath to demonstrate safe manoeuvring and minimal impact on pedestrian and vehicle movements.
- Demonstration of effective solar shading to the north elevation for levels one and two, central corridor spaces to levels one to five, and full height glazing sections to the west elevation, to ensure delivery of the environmental intent for each elevation and all levels of the development.
- Review of the apartment floor configuration, including the number of apartments with the view to reducing the building footprint, mass and bulk, address interface issues with the neighbouring properties and better respond to the existing residential context.
- Further exploration and incorporation of ESD principles to deliver the project's sustainability initiatives.
- A high quality of external materials supported by a materials sample board.

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Yours sincerely



Kirsteen Mackay  
South Australian Government Architect



Government  
of South Australia

cc: Belinda Chan

ODASA

belinda.chan@sa.gov.au

## Gormly, Will (DPTI)

---

**From:** Chan, Belinda (DPTI)  
**Sent:** Tuesday, 17 September 2019 5:01 PM  
**To:** Gormly, Will (DPTI)  
**Cc:** Chard, Rose (DPTI)  
**Subject:** Re: Canning Street - URPS Letter

Dear Will,

This email is in response to amended drawings and the '2 Canning Street Glenelg North – Response to Representations' document received 12 September 2019.

We acknowledge and support the following amendments:

- The raised floor level and interface conditions of the Canning Street lobbies in response to Council stormwater requirements.
- 800mm setback to the ground floor eastern boundary to improve sight lines of vehicles exiting to Laycock Lane
- Inclusion of solid window panels and sills to the tourist accommodation levels with the intent to improve privacy for the occupants.
- Increase in depth of the balcony to the west of the site at levels one and two. As these balconies are for the tourist accommodation levels, we recommend a management strategy is considered to minimise any acoustic impacts on the neighbouring apartments and properties.

In addition, we acknowledge the following amendments, however request further clarification:

- The level one floor slab/soffit is noted as Brightonlite precast panels. Confirmation is requested of the finish and colour of the other floor slabs (previously noted to be white coloured concrete) to ensure consistency of finish and overall architectural expression.

We have concerns regarding the following amendments:

- The full height walls to the west and south elevations are proposed as Brightonlite precast concrete with the panel joints aligned with the top of each floor slab. In our view, the change in material from the previously proposed face brick to these areas reduces the coherence of the overall architectural expression of the building.
- We acknowledge the intent of the proposed mansard style roof to reduce the visible height of the development. However, in our view, the inclusion of a mansard style roof further emphasises the upper level element of the building instead of treating the overheight element in a recessive manner to minimise visual impact.

We remain of the views expressed in the original referral letter that the following elements of the proposal require further consideration:

- We acknowledge the new screening strategy proposed to the southern balconies of the tourist accommodation levels. However we remain concerned by the proposed building height, massing and setbacks with regard to respecting the existing residential context, and management of the interfaces with neighbouring buildings in particular the private open space to the existing residence to the south.
- We support the consolidated rooftop plant area for the air conditioning condensers, however recommend the plant area is demonstrated on the elevations and sections for clarity.
- Demonstration of effective solar shading to the north elevation for levels one and two, central corridor spaces to levels one to five, and full height glazing sections to the west elevation, to ensure delivery of the environmental intent for each elevation and all levels of the development.
- While we support the review of apartment types and layouts for level 6 to accommodate the mansard style roof, we remain of the view that further review of the apartment floor configuration, including the number of apartments should be considered with the view to reducing the building footprint, mass and bulk, address interface issues with the neighbouring properties and better respond to the existing residential context.

- Further exploration and incorporation of ESD principles to deliver the project's sustainability initiatives.
- A high quality of external materials supported by a materials sample board.

Kind regards,

**Belinda Chan** on behalf of Kirsteen Mackay, South Australian Government Architect  
 Team Leader, Design  
 Office for Design and Architecture SA  
 Planning and Land Use Services  
 Department of Planning, Transport and Infrastructure  
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**From:** Matthew King [[mailto:matthew@urps.com.au](mailto:mailto:matthew@urps.com.au)]  
**Sent:** Thursday, 12 September 2019 8:22 AM  
**To:** Anthony Marroncelli <[AMarroncelli@holdfast.sa.gov.au](mailto:AMarroncelli@holdfast.sa.gov.au)>; Chan, Belinda (DPTI) <[Belinda.Chan@sa.gov.au](mailto:Belinda.Chan@sa.gov.au)>  
**Cc:** Glen Vollebregt <[glen@barrio.com.au](mailto:glen@barrio.com.au)>; Gormly, Will (DPTI) <[Will.Gormly@sa.gov.au](mailto:Will.Gormly@sa.gov.au)>; Philip Harnett <[philip@urps.com.au](mailto:philip@urps.com.au)>  
**Subject:** FW: Canning Street - URPS Letter

Hi Anthony & Belinda

The below Drop Box link contains all final documents – note, a minor change was made to privacy screening on the southern elevation.

It is critical for our client this matter gets to the 26/9 SCAP hearing so a swift response from you both would be very much appreciated!

Please call as needs. Note, I am in Sydney tomorrow and unavailable this afternoon, but Philip Harnett from URPS can assist as needs during this period.

Kind Regards

**Matthew King**  
 Managing Director



# URPS

shaping great communities

ADELAIDE | MELBOURNE

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**From:** Matthew King

**Sent:** Thursday, 12 September 2019 8:17 AM

**To:** 'Gormly, Will (DPTI)' <[Will.Gormly@sa.gov.au](mailto:Will.Gormly@sa.gov.au)>

**Cc:** Glen Vollebregt <[glen@barrio.com.au](mailto:glen@barrio.com.au)>; Philip Harnett <[philip@urps.com.au](mailto:philip@urps.com.au)>; Anthony Marroncelli <[AMarroncelli@holdfast.sa.gov.au](mailto:AMarroncelli@holdfast.sa.gov.au)>; Christie Bailey <[c.bailey@brownfalconer.com.au](mailto:c.bailey@brownfalconer.com.au)>; Tom Game <[thg@bllawyers.com.au](mailto:thg@bllawyers.com.au)>

**Subject:** RE: Canning Street - URPS Letter

Hi Will – updated documents here:

[https://www.dropbox.com/sh/9l6rkv3jqbn0m5q/AADIAKb9L7soyW3nMbbf\\_1sLa?dl=0](https://www.dropbox.com/sh/9l6rkv3jqbn0m5q/AADIAKb9L7soyW3nMbbf_1sLa?dl=0)

Critical for Glen we get to 26/9 SCAP please.

If you need anything, give me a call.

Cheers,

Matthew King  
Managing Director



# URPS

shaping great communities

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TO: **STATE COMMISSION ASSESSMENT PANEL**  
 DATE: **12 SEPTEMBER 2019**  
 SUBJECT: **DEVELOPMENT ASSESSMENT REPORT**  
 AUTHOR: **CRAIG WATSON**  
**TEAM LEADER DEVELOPMENT ASSESSMENT**

---

DA NO.	:	110/M004/19
APPLICANT	:	BARRIO DEVELOPMENTS
LOCATION	:	2 CANNING STREET, GLENELG NORTH
DEVELOPMENT PLAN	:	CONSOILDATED 2 JUNE 2016
ZONE AND POLICY AREA	:	RESIDENTIAL HIGH DENSITY, POLICY AREA 15, PRECINCT 4
NATURE OF DEVELOPMENT:		MERIT
PROPOSAL	:	CONSTRUCTION OF A 7 LEVEL RESIDENTIAL APARTMENT BUILDING, COMPRISING 20 APARTMENTS AND 38 TOURIST ACCOMMODATION ROOMS AND ANCILLARY CAR PARKING AND LANDSCAPING
EXISTING USE	:	DETACHED DWELLING

---

## 1. Introduction

This report does not include a full assessment of the application. It considers local impact, public realm improvements, storm water, waste removal and traffic management. It highlights some areas of concern, suggests possible improvements and includes recommended conditions if approved.

## 2. Assessment

### Built Form

The development is at variance with the Desired Character for Precinct 4, which states that development will be predominantly residential flat buildings up to 5 storeys or 18.5 metres in height. Further clarification is provided by Policy Area 15 Principle 19, which states that development should not exceed an external wall height of 18.5 metres above natural ground level excluding lift service levels and gables. The proposed building comprises 7 levels with a wall height of 21.25 metres and an overall roof height of 25.65 metres. The Development Plan does not specifically offer any height dispensations to the site unlike other portions of the Zone e.g. properties facing South Esplanade within the Residential High Density Zone Precinct 4, which allow a transition to adjacent taller buildings in Precinct 5. Nevertheless it is acknowledged that the site immediately adjoins Precinct 5 to the west and south where 12 storey (43 metre high) buildings are envisaged and which comprises a 13 storey apartment building on the western adjoining site. In this context some increased height may be appropriate to achieve a transition in height between the precincts. This might be achieved by stepping down the eastern half of the building to 5 storeys. The incorporation of a mansard-type design assists with a visual transition down to the existing and anticipated lower scales within the remainder of Precinct 4 to the east. Notwithstanding, a physical downward transition would provide a better scale relationship to the existing single and two storey buildings and possible future 5 storey buildings to the east and north while also reducing the opportunity for precedent to justify continuing the 7 storey built form further into Precinct 4.



The development will have significant impact on the adjoining southern property through visual, shading and overlooking impacts, notwithstanding the improved screening measures provided in the latest variation plans. Given its small size that site may also be difficult to develop in accordance with the Precinct expectations. Ideally the two sites should be amalgamated in accordance with Principle 8 of the Residential High Density Zone to achieve better design outcomes for both sites.

Some activation at ground level is provided to Canning Street although more meaningful landscaping might be established if setback from Canning Street were increased. While a podium effect is provided due to different design treatments to the first three levels and those above, the scale minimisation and street impact would be improved with greater street setbacks at all levels.

### **Local Heritage**

There are no buildings with heritage status within the locality however the Patawalonga Reserve including the water body and banks between Adelphi Terrace and the Patawalonga Frontage are listed as a Local Heritage Place within the Development Plan. Council is satisfied that the development will have no direct impact on the Heritage Place and is consistent with Heritage Places Principle 11.

### **Access and Parking**

Council has not engaged a traffic engineer to examine access, parking and traffic however it acknowledges on-site parking compliance with Development Plan requirements, the removal of a number of existing driveways providing for additional on-street parking and on-site collection of refuse by a private contractor. It is recommended that if approved a condition require the on-site refuse collection by a private contractor. The increased setback to Laycock Lane is acknowledged as a positive measure for providing improved safety conditions for road users and pedestrians. Council is not seeking to formally take ownership of this land.

It is not clear how the refuse contractor will access the secured on-site car park or more particularly how tourist parking will be managed. It is assumed tourists will park in the street on arrival before being directed to the parking area. Further information regarding peak tourist numbers and management would be useful to further consider the parking impacts in Canning Street.

### **Storm Water Management/other**

Council's City Asset's Department advise that:

- Storm water disposal system should cater for a 10 year rainfall event.
- Post development storm water flows should not exceed pre-development flows or not exceed 10 litres per second with any excess to be detained on site.

Council's Environmental Health section advise that if a cooling tower is proposed to be installed for air conditioning it should comply with the South Australian Public Health Act 2001 and South Australian Public Health (Legionella) Regulations 2013.

## **3. Conclusion**

While the building is well articulated and designed, with improvements made to the overall appearance in the latest amendments, it is at variance with height provisions of the Development Plan. The supporting argument of an upward transition to the higher building heights anticipated within the adjoining Precinct 5 to the west and south is acknowledged. Notwithstanding the

reduction in overall height and the inclusion of a visual transition through the use of a mansard-type design, further consideration however should be given to scaling down portions of the building and increasing setbacks from Canning Street to improve public realm aspects and provide a more suitable downward scale transition to the remainder of the Precinct 4 to the north and east.

It is also recommended that additional information regarding tourist parking numbers and management be obtained to further consider parking impacts on Canning Street.

If approved Council requests that the following notes be applied to the decision notification:

- If a cooling tower is proposed for air conditioning of the building, that there is a requirement to comply with the South Australian Public Health Act 2001 and South Australian Public Health (Legionella) Regulations 2013.
- That the disused driveway crossover on Canning Street be reinstated to kerb and footpath, where the proposed footpath is required to stand above the kerb, and that the design and material application for the footpath be consistent with the rest of the footpath in Canning Street.

**CRAIG WATSON**  
**TEAM LEADER DEVELOPMENT ASSESSMENT**



## APPLICATION ON NOTIFICATION – Category 2

<b>Applicant:</b>	Barrio Developments
<b>Development Number:</b>	110/M004/19
<b>Nature of Development:</b>	Demolition of existing building, and construction of a seven level residential flat building, comprising 20 apartments and 38 tourist accommodation rooms and ancillary car parking and landscaping.
<b>Development Type:</b>	Merit
<b>Subject Land:</b>	2 Canning Street, Glenelg North
<b>Development Plan:</b>	Holdfast Bay Council, consolidated 2 June 2016
<b>Zone / Policy Area:</b>	Residential High Density Zone / Urban Glenelg Policy Area 15 / Precinct 4 Five Storey
<b>Contact Officer:</b>	Will Gormly
<b>Phone Number:</b>	08 7109 7370
<b>Consultation Start Date:</b>	16 July 2019
<b>Consultation Close Date:</b>	29 July 2019
<p><b>During the notification period, hard copies of the application documentation can be viewed at the Department of Planning, Transport and Infrastructure, Level 5, 50 Flinders St, Adelaide, during normal business hours. Application documentation may also be viewed during normal business hours at the local Council office (if identified on the public notice).</b></p>	

Written representations must be received by the close date (indicated above) and can either be posted, hand-delivered or emailed to the State Commission Assessment Panel.

**Any representations received after the close date will not be considered.**

Postal Address:

The Secretary  
State Commission Assessment Panel  
GPO Box 1815  
ADELAIDE SA 5001

Street Address:

Development Division  
Department of Planning, Transport and Infrastructure  
Level 5, 50 Flinders Street  
ADELAIDE

Email Address: [scapreps@sa.gov.au](mailto:scapreps@sa.gov.au)

Fax Number: (08) 8303 0753

**South Australian  
DEVELOPMENT ACT 1993  
REPRESENTATION ON APPLICATION – CATEGORY 2**

**Applicant:** Barrio Developments  
**Development Number:** 110/M004/19  
**Nature of Development:** Demolition of existing building, and construction of a seven level residential flat building, comprising 20 apartments and 38 tourist accommodation rooms and ancillary car parking and landscaping.  
**Development Type:** Merit  
**Zone / Policy Area:** Residential High Density Zone / Urban Glenelg Policy Area 15 / Five Storey Precinct 4  
**Subject Land:** 2 Canning Street, Glenelg North  
**Contact Officer:** Will Gormly  
**Phone Number:** 08 7109 7370  
**Close Date:** 29 July 2019

My Name: Paul Peterson My phone number: 0424412474

**Primary method(s) of contact:** Email: Pato20098@gmail.com  
Postal Address: 1 Canning St Postcode: 5045  
Glenelg North

You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.

**My interests are:**  
(please tick one)

☒ owner of local property  
☐ occupier of local property  
☐ a representative of a company/other organisation affected by the proposal  
☐ a private citizen

**The address of the property affected is:**

1 Canning St Glenelg North Postcode 5045

My interests are:  
(please tick one)

☐ I support the development  
☐ I support the development with some concerns  
☒ I oppose the development

**The specific aspects of the application to which I make comment on are:** The building is 2 stories above the 5 Storey Zone, 40 car park for 58 apartments/rooms is not enough. Car Parking IS ALREADY extremely exhausted. Building height is above the 18.5m allowed limit. The Privacy of 1 Canning St Glenelg North is severely affected with the balconies looking directly into the main living areas of my house

I: ☒ wish to be heard in support of my submission  
(please tick one) ☐ do not wish to be heard in support of my submission  
(Please tick one)

By: ☒ appearing personally  
(please tick one) ☐ being represented by the following person  
(Please tick one)

Signature: [Signature]  
Date: 29-07-2019

**Return Address:** The Secretary, State Commission Assessment Panel, GPO Box 1815, Adelaide, SA 5001 /or  
**Email:** [scapreps@sa.gov.au](mailto:scapreps@sa.gov.au)

RECEIVED 30 JUL 2019

South Australian  
DEVELOPMENT ACT 1993  
REPRESENTATION ON APPLICATION – CATEGORY 2

Applicant: Barrio Developments  
Development Number: 110/M004/19  
Nature of Development: Demolition of existing building, and construction of a seven level residential flat building, comprising 20 apartments and 38 tourist accommodation rooms and ancillary car parking and landscaping.  
Development Type: Merit  
Zone / Policy Area: Residential High Density Zone / Urban Glenelg Policy Area 15 / Five Storey Precinct 4  
Subject Land: 2 Canning Street, Glenelg North  
Contact Officer: Will Gormly  
Phone Number: 08 7109 7370  
Close Date: 29 July 2019

My Name: Lee Melville  
Steve Church My phone number: 0417866563  
0418821610

Primary method(s) of contact: Email: greenhatthing@gmail.com  
lelmelville@gmail.com  
Postal Address: 3 Canning Street Postcode: 5045  
Glenelg Nth

You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.

My interests are:  
(please tick one)

- ☒ owner of local property  
☐ occupier of local property  
☐ a representative of a company/other organisation affected by the proposal  
☐ a private citizen

The address of the property affected is:

3 Canning Street, Glenelg Nth Postcode 5045

My interests are:  
(please tick one)

- ☐ I support the development  
☐ I support the development with some concerns  
☒ I oppose the development

The specific aspects of the application to which I make comment on are:

PLEASE SEE ATTACHED LETTER

I:

(please tick one)

- ☒ wish to be heard in support of my submission  
☐ do not wish to be heard in support of my submission  
(Please tick one)

By:

(please tick one)

- ☐ appearing personally  
☐ being represented by the following person  
(Please tick one)

Signature:

Date:

[Signature]  
23  
07 19

Steve Church & Lee Melville  
3 Canning Street  
Glenelg North SA 5045

23<sup>rd</sup> July 2019

RE: DEVELOPMENT APPLICATION 110/M004/19

Dear Mr Gormly,

In reference to the above development application we would like to make the following comments in support of our concerns over such advancement in our area.

Canning Street is a small residential side road; it is bordered by a 12 story apartment building to the south west, and a proposed 12 story hotel development to the west, this combined with the inevitable future redevelopment of the watermark hotel makes this current development proposal even more inappropriate.

These concerns are not restricted to this particular development, but to the larger picture, that should this project be allowed it will set a precedence for future "private entrepreneurs" to reshape this long established residential environment, overtime (literally) becoming overshadowed by multiple commercial and semi commercial encroachment. This development would not be considered appropriate in say, Mary Street, Melbourne Street nor Sturt Street. Canning Street is no different it is a quiet, small residential street that lends itself to 2 story building at best. Developments such as that now being proposed should be restricted to main roadways which offer extended views and a far more balanced ratio of height to street scape perspective such as Adelphi Tce and Anzac Hwy.

The impact of this and other developments within such close proximity will not only create a confining and overshadowing environment for the existing residents, but is completely out of proportion with the surrounding houses.

Yours Sincerely

Steve Church and Lee Melville.



South Australian  
DEVELOPMENT ACT 1993  
REPRESENTATION ON APPLICATION – CATEGORY 2

**Applicant:** Barrio Developments  
**Development Number:** 110/M004/19  
**Nature of Development:** Demolition of existing building, and construction of a seven level residential flat building, comprising 20 apartments and 38 tourist accommodation rooms and ancillary car parking and landscaping.  
**Development Type:** Merit  
**Zone / Policy Area:** Residential High Density Zone / Urban Glenelg Policy Area 15 / Five Storey Precinct 4  
**Subject Land:** 2 Canning Street, Glenelg North  
**Contact Officer:** Will Gormly  
**Phone Number:** 08 7109 7370  
**Close Date:** 29 July 2019

My Name: Amy DANGERFIELD My phone number: 0415 253 660

Primary method(s) of contact: Email: amy.dangerfield@gmail.com  
Postal Address: \_\_\_\_\_ Postcode: \_\_\_\_\_

**You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.**

**My interests are:**  
(please tick one)

- ☒ owner of local property  
☐ occupier of local property  
☐ a representative of a company/other organisation affected by the proposal  
☐ a private citizen

**The address of the property affected is:**

4 Canning St, Glenelg North Postcode 5045

**My interests are:**  
(please tick one)

- ☒ I support the development  
☐ I support the development with some concerns  
☒ I oppose the development

**The specific aspects of the application to which I make comment on are:**

The traffic congestion in Layton lane is already of concern and your proposal would not cope with extra volume of traffic

**I:** ☒ wish to be heard in support of my submission  
(please tick one) ☐ do not wish to be heard in support of my submission  
(Please tick one)

**By:** ☐ appearing personally  
(please tick one) ☐ being represented by the following person  
(Please tick one)

Signature: [Signature]  
Date: 29/7/19



South Australian  
DEVELOPMENT ACT 1993  
REPRESENTATION ON APPLICATION – CATEGORY 2

**Applicant:** Barrio Developments  
**Development Number:** 110/M004/19  
**Nature of Development:** Demolition of existing building, and construction of a seven level residential flat building, comprising 20 apartments and 38 tourist accommodation rooms and ancillary car parking and landscaping.  
**Development Type:** Merit  
**Zone / Policy Area:** Residential High Density Zone / Urban Glenelg Policy Area 15 / Five Storey Precinct 4  
**Subject Land:** 2 Canning Street, Glenelg North  
**Contact Officer:** Will Gormly  
**Phone Number:** 08 7109 7370  
**Close Date:** 29 July 2019

My Name: Danny Colangelo My phone number: 0414 855 446

Primary method(s) of contact: Email: danny@magilldemolition.com.au  
Postal Address: PO Box 2601 – Regency Park - SA Postcode: 5942

You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.

My interests are:  
(please tick one)

☒ owner of local property  
☐ occupier of local property  
☐ a representative of a company/other organisation affected by the proposal  
☐ a private citizen

The address of the property affected is:

4A Canning Street – Glenelg North – SA Postcode 5045

My interests are:  
(please tick one)

☐ I support the development  
☐ I support the development with some concerns  
☒ I oppose the development

The specific aspects of the application to which I make comment on are:  
PARKING ISSUES & OVERDEVELOPMENT OF THE EXISTING SITE  
\_\_\_\_\_  
\_\_\_\_\_

I: ☒ wish to be heard in support of my submission  
(please tick one) ☐ do not wish to be heard in support of my submission  
(Please tick one)

By: ☒ appearing personally  
(please tick one) ☐ being represented by the following person  
(Please tick one)

Signature: D. Colangelo  
Date: 29<sup>th</sup> July 2019

**South Australian  
DEVELOPMENT ACT 1993  
REPRESENTATION ON APPLICATION – CATEGORY 2**

**Applicant:** Barrio Developments  
**Development Number:** 110/M004/19  
**Nature of Development:** Demolition of existing building, and construction of a seven level residential flat building, comprising 20 apartments and 38 tourist accommodation rooms and ancillary car parking and landscaping.  
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**Zone / Policy Area:** Residential High Density Zone / Urban Glenelg Policy Area 15 / Five Storey Precinct 4  
**Subject Land:** 2 Canning Street, Glenelg North  
**Contact Officer:** Will Gormly  
**Phone Number:** 08 7109 7370  
**Close Date:** 29 July 2019

My Name: Gerry, Tonia Russo & family My phone number: 0414697790

**Primary method(s) of contact:** Email: slppl@internode.on.net  
Postal Address: 4b Canning St, Glenelg North SA Postcode: 5045

**You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.**

**My interests are:**  
(please tick one)

☒ owner of local property  
☒ occupier of local property  
☐ a representative of a company/other organisation affected by the proposal  
☐ a private citizen

**The address of the property affected is:**

4b Canning St, Glenelg North SA Postcode 5045

My interests are:  
(please tick one)

☐ I support the development  
☐ I support the development with some concerns  
☒ I oppose the development

**The specific aspects of the application to which I make comment on are:** \_\_\_\_\_  
Please refer attachment.

I: ☒ wish to be heard in support of my submission  
(please tick one) ☐ do not wish to be heard in support of my submission  
(Please tick one)

By: ☐ appearing personally  
(please tick one) ☒ being represented by the following person  
(Please tick one)

**Signature:** 

**Date:** 26.07.19

Representation on application – category 2

G. & T. Russo  
4b Canning Street, Glenelg North SA 5045

ATT: The secretary / Will Gormly  
State Commission Assessment Panel  
scapreps@sa.gov.au

<b>Applicant:</b>	Barrio Developments
<b>Development Number:</b>	110/M004/19
<b>Nature of Development:</b>	Demolition of existing building, and construction of a seven level residential flat building, comprising 20 apartments and 38 tourist accommodation rooms and ancillary car parking and landscaping.
<b>Development Type:</b>	Merit
<b>Subject Land:</b>	2 Canning Street, Glenelg North
<b>Development Plan:</b>	Holdfast Bay Council, consolidated 2 June 2016
<b>Zone / Policy Area:</b>	Residential High Density Zone / Urban Glenelg Policy Area 15 / Precinct 4 Five Storey
<b>Contact Officer:</b>	Will Gormly
<b>Phone Number:</b>	08 7109 7370
<b>Consultation Start Date:</b>	16 July 2019
<b>Consultation Close Date:</b>	29 July 2019

Dear Sir / Madam / Will,

Thank you for the opportunity to voice my concerns and recommendations for the proposed development.

I have lived at the current address for over 10 years, my wife and 2 young children also reside at this address.

An overall review of the proposal seems to push the boundaries of the development plan asking for a 7-storey building / 23.85m+ height in an area where the policy asks for 5-storeys maximum / 18.5m height.

The developer should not take privilege of using the development at 6-10 Adelphi Tce (corner Canning Street) as it doesn't physically exist, it only holds an extension to planning approval and the site may never be developed as per its proposal and does not form part of this development.

The building as stated in the proposal 3.1, *comprises of 20 apartments and 38 tourist accommodation rooms plus areas for travellers*. I am not sure what "areas for travellers" actually means, it is believed the developer specialises in "student accommodation", I am hoping this is not the case for this development considering the 38 tourist accommodation rooms.

We are seeking the tourist accommodation rooms are not separately titled.

The building seems to lack articulation and form with the exception of balconies recessed in from the building, extremely minimal articulation also has been considered in levels 6 and 7, given they shouldn't even exist. The building is a box of 24m X 24.4 (plus front balconies) X 23.85m high (plus roof condenser units). Landscaping is also quite minimal and should be further considered.

## Representation on application – category 2

Our main concerns for the proposal is;

- the carpark entry / exit is from Laycock lane, the lane is only 5.5m wide. This is of great and major concern as stated in the traffic report, the movements will increase to 101 per day from 6 current residences which use approximately 20 trips per day.

During our development of 4 canning street a portion of land being of 930mm wide was added to Laycock Lane to increase in the vehicle manoeuvrability (please refer attachment). As per INFRAPLAN report 2.2 it states 18 on street carpark spaces are available during a typical weekday evening, taking a survey on a Monday night will deliver a positive result for the developers argument only. I can confirm during the summer or anytime of the year Wednesdays to Sundays on street carparking is a struggle. The survey taken on Sunday is quite typical of any weekend, events at Wigley Reserve and general increase of traffic is of a common event.

3.2 states 37 carparks required and 40 are provided, I don't understand how visitors are first meant to enter a closed garage door, then try to figure out how stackable visitor carparking works. It has been stated they will contact the residence, where is the allocated / temporary visitor stopping area, where will the vehicle wait until the residence gets downstairs, checks and accommodates the stacker? I believe this is a poor attempt at visitor parking and serious consideration should be taken to improve this. This will also increase loitering in the Lane Way.

Loading and deliveries will be a huge problem, especially because of food (the tourist accommodation will have no kitchen), parcel delivers, taxis, Ubers, they will require parking bays to resolve these matter as no dedicated carparks are available for these vehicles and deliveries. Loading and deliveries will also affect the Linen drop off and collection, cleaners and staff carparking, it be likely trucks will be 'medium rigid vehicles' and manoeuvrability will not accommodate the linen trucks.

3.4 states *"there is space for another vehicle to wait in Laycock lane if required while another exits the carpark, or for another vehicle to overtake the waiting vehicle to access another property further west in Laycock Lane"* I'm not sure what this means or how it works. It is a public road, not a waiting area or where actually is it?

Figure 4 confirms a vehicle must cross onto the opposite side of the lane to be able to exit increasing the chance of accidents.

Figure 5 shows only a *small rigid vehicle* can only enter and exit the carpark, crossing the entire width of the lane, this vehicle will block access to the users of the carpark while the waste collection is being undertaken causing dangerous circumstances and vehicle banking for Laycock Lane and Canning Street.

A bank up of vehicles stuck in Laycock Lane will be what this plan and traffic report has proposed.

Should access from Laycock Lane be required I recommend 1.5m of land be given to Laycock lane to assist in vehicle manoeuvrability and ease of overtaking stationary vehicles. A better form of development will be to have all access for carpark from Canning Street.

## Representation on application – category 2

- Over shadowing is of concern as I don't have a verandah in our private open space for the reason of obtaining as much natural light into my property as possible. I will lose this privilege with the 7-storey building.  
SUHO ESD Statement item 3 *"during the design process overshadowing has been taken into consideration, based on both existing and approved addition of a 14-storey hotel, which is in close proximity"*

Please refer attachment indicating current and proposed circumstance. It is quite clear absolutely no attempt has been made to assist in overshadowing, a 24m X 24.4 X 23.85H box has been designed with no articulation to assist in this.

- Overlooking is our great concern, we have a young family and from the second level of the development there will be 42 windows and 12 balconies facing east with extreme and guaranteed overlooking into our private open space and lower level windows and glass doors, this is quite a breach of personal privacy where my children play on their trampoline and use an inflatable pool during the warmer periods as well as the privacy for myself and my wife.

The Aquarius apartment building just to the west was built many years ago with a lot more consideration for this, as only utility areas face east.

I recommend further design consideration be given to the overlooking issue or accommodate a form of screening to 1.5m high to all east and south facing windows and balconies.

In conclusion, should a 5-storey apartment in articulated built form with access from Canning street only and a design team doing the best they can to avoid overlooking would be alleviate many concerns for not only myself but many of the local residences.

I hope my concerns and recommendations are taken seriously by the developer and SCAP.

Please do not hesitate to contact myself in the meantime should you wish to liaise or have any queries.

Gerry Russo

SHADOW DIAGRAMS (CURRENT)

SHADOW DIAGRAMS (PROPOSED)



POS = PRIVATE OPEN SPACE

# COMPILED PLAN

ALLOTMENTS 25 TO 36 INCL. AND 139 IN F.P. 1437 OF PORTION OF SECTION 1495  
HUNDRED OF NOARLUNGA IN THE AREA NAMED GLENELG NORTH



FILED PLAN NUMBER  
**FP 10792**

ACCEPTED FOR FILING

*Stables*  
per Registrar-General  
12/15/81

Reference Map No. 6628-50-1

COUNCIL

CITY OF GLENELG

NARRATIVE FILE NO. 4535941

\* Redefinition of boundaries of Lots  
in accordance with F.P. 15802  
P.O. 1/81

Pt. Lot 145 now Canning (Public) St  
vide S.2. 5463 p. 24-28

MICROFILMED  
**4-3-88**

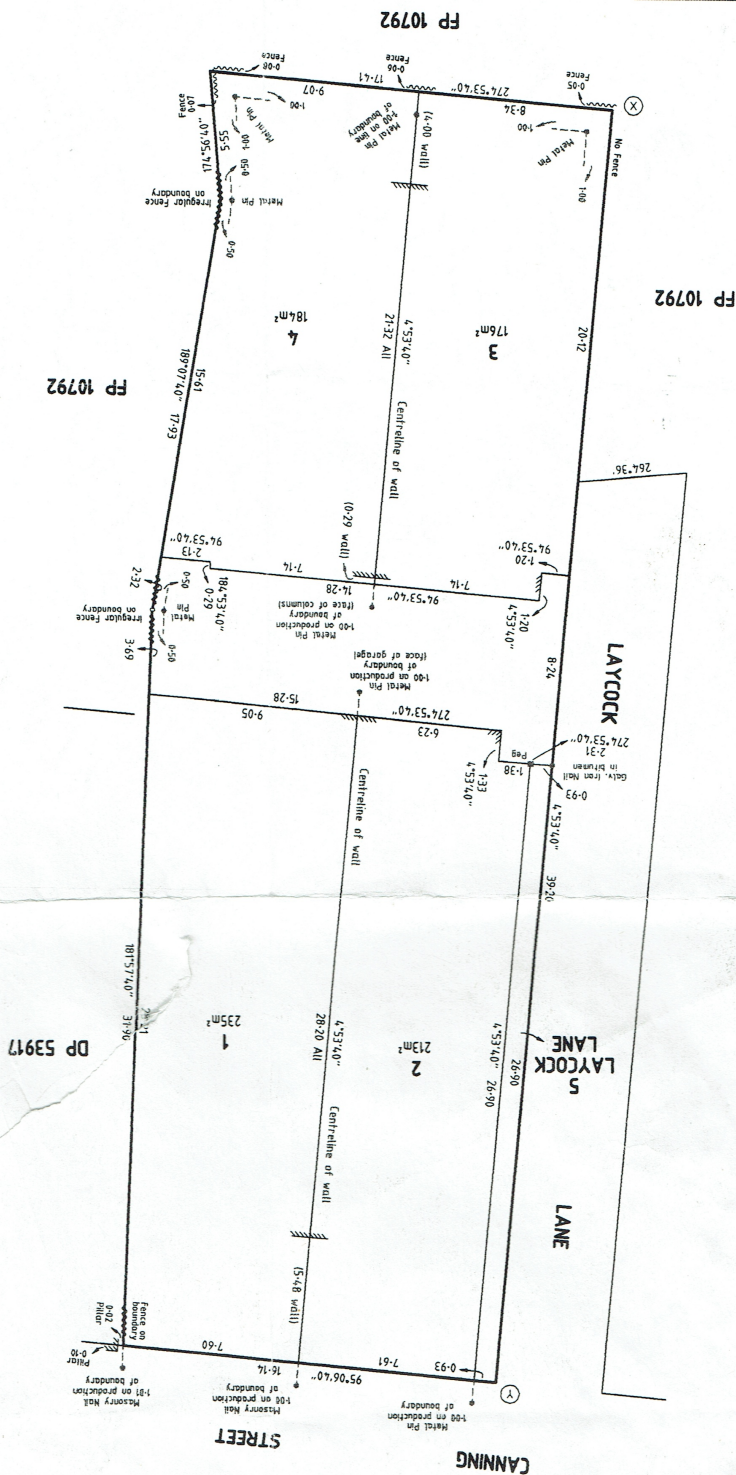
THIS PLAN WAS EXAMINED IN THE  
DRAFTING PROJECTS BRANCH  
ON 12/15/81  
DRAWN M.P. EXAMINED A.G.



ALL MARKS HAVE BEEN SET OUT IN ACCORDANCE WITH PLANS SUPPLIED TO STATE SURVEYS BY THE CONTRACTOR. THE CONTRACTOR IS TO VERIFY ALL DIMENSIONS AND SET OUT POINTS PRIOR TO COMMENCEMENT OF CONSTRUCTION.

STATE SURVEYS  
INCORPORATING CAVALLIO FENEST & ASSOCIATES  
465/B SOUTH ROAD, KESWICK, S. A. 5035  
PHONE (08) 8793 7939 FAX (08) 8793 2949  
EMAIL INFO@STATESURVEYS.COM.AU  
REF: 04/371 PEPPLAN DMS VERSION: 31/8/05

LOCATION PLAN



PRINT SHOWING PLACEMENT OF SURVEY MARKS

CP

COORDINATE PLAN NUMBER

PLAN TYPE	PRIMARY	SHEET	2
TITLE SHEET	1	OF	2
DESIGNED TO	1 / 1		
PRO. RESUBMITTAL HERE			
CLOSURE CHECKED	PLAN EXAMINED	PLAN APPROVED	PLAN APPROVED
TITLE REFERENCE CT 5487/870			
LAND DESCRIPTION ALLOTMENT 140 IN FP 80792			
RELATIONSHIP HARBORED DIVISION AREA NOARUNGA GLENELG NORTH CITY OF HOLDBAST BAY 46428-50-J			
QA. FP	TOTAL AREA		
DRIVE No.	110/COM/05		
SCALE	0	1-25	25 75 10 METERS
ANNOTATIONS THE SERVICE INFRASTRUCTURE WAS NOT IN PLACE AS AT 24/8/2005			
PRINT SHOWING PLACEMENT OF SURVEY MARKS ** 24 / 8 / 2005 **			
STREET HISTORY NO. 1 HISTORY NO. 2 HISTORY NO. 3 HISTORY NO. 4 HISTORY NO. 5 HISTORY NO. 6 HISTORY NO. 7 HISTORY NO. 8 HISTORY NO. 9 HISTORY NO. 10 HISTORY NO. 11 HISTORY NO. 12 HISTORY NO. 13 HISTORY NO. 14 HISTORY NO. 15 HISTORY NO. 16 HISTORY NO. 17 HISTORY NO. 18 HISTORY NO. 19 HISTORY NO. 20 HISTORY NO. 21 HISTORY NO. 22 HISTORY NO. 23 HISTORY NO. 24 HISTORY NO. 25 HISTORY NO. 26 HISTORY NO. 27 HISTORY NO. 28 HISTORY NO. 29 HISTORY NO. 30 HISTORY NO. 31 HISTORY NO. 32 HISTORY NO. 33 HISTORY NO. 34 HISTORY NO. 35 HISTORY NO. 36 HISTORY NO. 37 HISTORY NO. 38 HISTORY NO. 39 HISTORY NO. 40 HISTORY NO. 41 HISTORY NO. 42 HISTORY NO. 43 HISTORY NO. 44 HISTORY NO. 45 HISTORY NO. 46 HISTORY NO. 47 HISTORY NO. 48 HISTORY NO. 49 HISTORY NO. 50 HISTORY NO. 51 HISTORY NO. 52 HISTORY NO. 53 HISTORY NO. 54 HISTORY NO. 55 HISTORY NO. 56 HISTORY NO. 57 HISTORY NO. 58 HISTORY NO. 59 HISTORY NO. 60 HISTORY NO. 61 HISTORY NO. 62 HISTORY NO. 63 HISTORY NO. 64 HISTORY NO. 65 HISTORY NO. 66 HISTORY NO. 67 HISTORY NO. 68 HISTORY NO. 69 HISTORY NO. 70 HISTORY NO. 71 HISTORY NO. 72 HISTORY NO. 73 HISTORY NO. 74 HISTORY NO. 75 HISTORY NO. 76 HISTORY NO. 77 HISTORY NO. 78 HISTORY NO. 79 HISTORY NO. 80 HISTORY NO. 81 HISTORY NO. 82 HISTORY NO. 83 HISTORY NO. 84 HISTORY NO. 85 HISTORY NO. 86 HISTORY NO. 87 HISTORY NO. 88 HISTORY NO. 89 HISTORY NO. 90 HISTORY NO. 91 HISTORY NO. 92 HISTORY NO. 93 HISTORY NO. 94 HISTORY NO. 95 HISTORY NO. 96 HISTORY NO. 97 HISTORY NO. 98 HISTORY NO. 99 HISTORY NO. 100 HISTORY NO. 101 HISTORY NO. 102 HISTORY NO. 103 HISTORY NO. 104 HISTORY NO. 105 HISTORY NO. 106 HISTORY NO. 107 HISTORY NO. 108 HISTORY NO. 109 HISTORY NO. 110 HISTORY NO. 111 HISTORY NO. 112 HISTORY NO. 113			

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South Australian  
DEVELOPMENT ACT 1993  
REPRESENTATION ON APPLICATION – CATEGORY 2

**Applicant:** Barrio Developments  
**Development Number:** 110/M004/19  
**Nature of Development:** Demolition of existing building, and construction of a seven level residential flat building, comprising 20 apartments and 38 tourist accommodation rooms and ancillary car parking and landscaping.  
**Development Type:** Merit  
**Zone / Policy Area:** Residential High Density Zone / Urban Glenelg Policy Area 15 / Five Storey Precinct 4  
**Subject Land:** 2 Canning Street, Glenelg North  
**Contact Officer:** Will Gormly  
**Phone Number:** 08 7109 7370  
**Close Date:** 29 July 2019

**My Name:** DAVID RAYMOND **My phone number:** 0412 970 441

**Primary method(s) of contact:** **Email:** DAVID.RAYMOND@Y7MAIL.COM  
**Postal Address:** 4C CANNING ST **Postcode:** 5045  
GLENELG NORTH

You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.

**My interests are:**  
(please tick one)

☒ owner of local property  
☐ occupier of local property  
☐ a representative of a company/other organisation affected by the proposal  
☐ a private citizen

**The address of the property affected is:**

4C CANNING ST GLENELG NORTH **Postcode** 5045

**My interests are:**  
(please tick one)

☐ I support the development  
☐ I support the development with some concerns  
☒ I oppose the development

**The specific aspects of the application to which I make comment on are:** ACCESS TO MY PROPERTY, NOISE, PARKING, LOSING NATURAL LIGHT, TAXI/UBER ACCESS, WASTE DISPOSAL

**I:** ☒ wish to be heard in support of my submission  
(please tick one) ☐ do not wish to be heard in support of my submission  
(Please tick one)

**By:** ☒ appearing personally  
(please tick one) ☐ being represented by the following person  
(Please tick one)

**Signature:** [Signature]

**Date:** 22/7/19



South Australian  
DEVELOPMENT ACT 1993  
REPRESENTATION ON APPLICATION – CATEGORY 2

Applicant: Barrio Developments  
Development Number: 110/M004/19  
Nature of Development: Demolition of existing building, and construction of a seven level residential flat building, comprising 20 apartments and 38 tourist accommodation rooms and ancillary car parking and landscaping.  
Development Type: Merit  
Zone / Policy Area: Residential High Density Zone / Urban Glenelg Policy Area 15 / Five Storey Precinct 4  
Subject Land: 2 Canning Street, Glenelg North  
Contact Officer: Will Gormly  
Phone Number: 08 7109 7370  
Close Date: 29 July 2019

My Name: DANIEL CARETT My phone number: 0417831931

Primary method(s) of contact: Email: ADELAIDEBUILDINGGROUP@GMAIL.COM  
Postal Address: 40 CANNING ST Postcode: 5045  
GLENELG NORTH

You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.

My interests are:  
(please tick one)

- ☒ owner of local property  
☐ occupier of local property  
☐ a representative of a company/other organisation affected by the proposal  
☐ a private citizen

The address of the property affected is:

Postcode

My interests are:  
(please tick one)

- ☐ I support the development  
☐ I support the development with some concerns  
☒ I oppose the development

The specific aspects of the application to which I make comment on are:

NOISE TRAFFIC,  
WASTE DISPOSAL, SIZE OF DEVELOPMENT DOES  
NOT FIT THE ZONE IN A SIDE STREET.

I: ☒ wish to be heard in support of my submission  
(please tick one) ☐ do not wish to be heard in support of my submission  
(Please tick one)

By: ☐ appearing personally  
(please tick one) ☐ being represented by the following person  
(Please tick one)

Signature:

Date:

Return Address: The Secretary, State Commission Assessment Panel, GPO Box 1815, Adelaide, SA 5001 /or  
Email: [scapreps@sa.gov.au](mailto:scapreps@sa.gov.au)

## Gormly, Will (DPTI)

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**From:** Colin Elmer <colinelmer@mail.com>  
**Sent:** Monday, 29 July 2019 7:46 PM  
**To:** Gormly, Will (DPTI)  
**Subject:** Re: 2 Canning St Glenelg North 110/M004/19  
**Attachments:** 2 Canning Street Glenelg North - Plans.pdf; ATT00001.htm

Hi Will

thanks for your email reply.

I would appreciate you granting an extension until midnight tonight, Adelaide time.

Please note I have worked and travelled o/s for extended periods of time and there are many organisations both Govt, Local Govt and private who often do not even realise that the firewalls they have in place prevent access for Australians to certain parts of their sites and in some cases even emails from being received.

Therefore I am not able to access your link from Budapest-Hungary so cannot speak directly to it.

Anthony Maroncelli, Manager of Development Services at the city of Holdfast Bay has been kind enough to send across the information he has on file so I will address that and attach it here for your reference.

Our concerns are not limited to the below however are given as a starting point in response to this proposal:

1. The proposed building exceeds the height allowed in that area, being proposed as 7 stories and 23.5m whereby the height allowed for this site is 5 stories and 18.5m high.

This creates a building not in keeping with the street scape of the existing 1 and 2 story dwellings in Canning St.

The proposal for 38 units plus parking for 40 cars, is not to scale for what is required or in any way keeping with the built environment of Canning St.

The block size of 785sqm does not support this level of density in this area.

2. Car parking proposed is for 40 cars but is insufficient both in number of and the size of car parks according to the drawings.

The plans show 14 car parks at random and differing sizes, not being uniform and no heights given for a car stacker.

The access door shown on the East elevation is too small to allow two vehicles turning to enter and leave, to pass each other, there are no measurements given on these drawings.

3. The width of Laycock Lane is significantly smaller than Canning St and may not be wide enough to allow two vehicles to pass each other freely, again no measurements are given.

No provision has been made for the access or parking of service vehicles.

No provision has been made for staff parking.

The extra traffic created by this proposal will not be able to be accommodated on Laycock Ave and parking is not available on Canning St.

4. I can see no proposal for a dis-abled carpark with appropriate widths and sizings which will impact upon the number of other parks available.

Thanks for accepting our representation, in general terms we oppose this development and can expand upon the above points and other issues with more time given to respond.

Regards  
Colin Elmer



South Australian  
DEVELOPMENT ACT 1993  
REPRESENTATION ON APPLICATION – CATEGORY 2



**Applicant:** Barrio Developments  
**Development Number:** 110/M004/19  
**Nature of Development:** Demolition of existing building, and construction of a seven level residential flat building, comprising 20 apartments and 38 tourist accommodation rooms and ancillary car parking and landscaping.  
**Development Type:** Merit  
**Zone / Policy Area:** Residential High Density Zone / Urban Glenelg Policy Area 15 / Five Storey Precinct 4  
**Subject Land:** 2 Canning Street, Glenelg North  
**Contact Officer:** Will Gormly  
**Phone Number:** 08 7109 7370  
**Close Date:** 29 July 2019

My Name: KEN TREMBATH My phone number: 0411 108 778

**Primary method(s) of contact:** Email: Sydney@hilditchlawyers.com  
Postal: GPO Box 11010  
Address: ADELAIDE SA Postcode: 5001

You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.

**My interests are:**  
(please tick one)

☒ owner of local property  
☒ occupier of local property  
☐ a representative of a company/other organisation affected by the proposal  
☐ a private citizen

**The address of the property affected is:**

1 LAYCOCK LANE, GLENELG NORTH, SA Postcode 5045

**My interests are:**  
(please tick one)

☐ I support the development  
☐ I support the development with some concerns  
☒ I oppose the development

**The specific aspects of the application to which I make comment on are:** PLEASE SEE ATTACHED LETTER FROM HILDITCH LAWYERS TOGETHER WITH THE LETTER FROM MELISSA MELLEN OF MFF TRAFFIC ENGINEERS.

**I:** ☒ wish to be heard in support of my submission  
(please tick one) ☐ do not wish to be heard in support of my submission  
(Please tick one)

**By:** ☐ appearing personally  
(please tick one) ☒ being represented by the following person SYDNEY McDONALD OF HILDITCH LAWYERS  
(Please tick one)

**Signature:** [Signature] SYDNEY McDONALD FOR AND ON BEHALF OF KEN TREMBATH  
**Date:** 26/07/2019



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HILDITCH LAWYERS

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Adelaide SA 5000

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Fax 08 8231 8323  
[lawyers@hilditchlawyers.com](mailto:lawyers@hilditchlawyers.com)

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26 July 2019

**The Secretary**  
**State Commission Assessment Panel**  
GPO Box 1815  
ADELAIDE SA 5001  
[scapreps@sa.gov.a](mailto:scapreps@sa.gov.a)

Dear Secretary

**2 Canning Street, Glenelg North (DA No. 110/M004/19) – Statement of representation**

We act for Mr Ken Trembath, being an owner and occupier of 1 Laycock Lane, Glenelg North ("**our client's land**").

We are instructed to make a written representation on behalf of our client in relation to DA No. 110/M004/19 which proposes a development described by the SCAP as "*Demolition of existing building, and construction of a seven level residential flat building, comprising 20 apartments and 38 tourist accommodation rooms and ancillary car parking and landscaping*" ("**the proposed development**") on land known as 2 Canning Street, Glenelg North ("**the subject land**").

Our client is opposed to the proposed development and submits that the SCAP should refuse planning consent.

**Our client's land**

The northern boundary of our client's land adjoins the majority of the southern boundary of the subject land.

Our client's land comprises a contemporary two storey dwelling. The majority of the private open space associated with our client's dwelling is in the form of a ground level courtyard and first level balcony to the northern side of his dwelling with open access to northern light and views to the Patawalonga.

We are instructed that our client has resided at 1 Laycock Lane with his wife since around 2004.

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Liability limited by a scheme approved under Professional Standards Legislation.

Hilditch Lawyers Pty Ltd ACN 145 516 276



### The Development Plan

The subject land is located in the Residential High Density Zone ("the Zone") and, more specifically, in Urban Glenelg Policy Area 15 ("the Policy Area") and Precinct 4 Five Storey ("Precinct 4").

### Height of proposed development

The relevant provisions for Precinct 4 deal with building height as follows:

*"Development within Precinct 4 Five Storey will be ... of up to 5 storeys (or 18.5 metres) in height".*

The proposed development clearly exceeds the relevant maximum height guideline in Precinct 4 by two storeys and by at least 5 metres (the plans available to us are not clear on the precise height of the building. They show a height of 23.5 metres to a point described as "roof" however the building (possibly a parapet and infrastructure) project above this level). It will be 7 storeys in height and clearly over 23.5 metres in height. We agree with the applicant that this is not a "nominal exceedance".

The assessment process involves a consideration of whether the Development Plan speaks for or against the proposed development (eg. see *City of Mitcham v Freckman & Ors* [1999] SASC 234 at [18]). The Supreme Court of South Australia has identified the proper approach to the assessment of development proposals against quantitative provisions on numerous occasions, including in the matter of *City of Port Adelaide Enfield v Moseley* [2008] SASC 88. We respectfully encourage the SCAP to have regard to the following remarks of the Supreme Court when considering the exceedance of building height guidelines in the present circumstances:

*"It is appropriate to add that, while Principle 12 is not mandatory in its operation, **due weight must be given to it. If that is not so, the purpose of the Development Plan is seriously undermined.** Although it was decided in South Australian Housing Trust v Development Assessment Commission (1994) 63 SASR 35 that provisions of a Development Plan are not mandatory in their operation, that decision does not negate the fact that **due weight must be given to them.** The Objectives and Principles of Development Control are important and a planning authority should have regard to them. As King CJ said in South Australian Housing Trust v Development Assessment Commission at 38, **in many cases non-compliance with a particular principle may be decisive** in a planning authority's consideration of an application. The Objectives and Principles of Development Control are advisory in the sense of expressing goals and guiding principles: per Prior J at 41. **While not mandatory, the provisions of the Development Plan are directory and persuasive and one would normally expect a planning authority to apply them unless, as a matter of planning judgment, there is good reason to depart from them.** District Council of Angaston v Hamilton (1995) 64 SASR 110 at 117 to 118 approved in Town of Gawler v Impact Investment Corporation Pty Ltd [2007] SASC 356; (2007) 99 SASR 115 at [22] per Doyle CJ and at [79] per Bleby J. **If that is not so, the Development Plan becomes a relatively meaningless and ineffectual***

**document.** *In addition, it would tend to negate the injunction in s 33(1)(a) of the Development Act that a development is to be assessed against the relevant provisions of the Development Plan.” (our emphasis)*

The applicant's legal representative has made a number of comments in relation to the manner in which the assessment of the proposed development should be approached. We generally agree with the observations made beneath the heading “Approach to assessment generally” in the Botten Levinson letter dated 19 June 2019. The letter is simply referring to some well-established principles. However we respectfully disagree with the response provided to the “10 factors” outlined in the *Impact Investments* case.

We cannot agree with the suggestion that the language in PDC 19 is “*inherently flexible in nature due to the use of the word “should” as opposed to, for example, “must” or some other term connoting that the requirement is mandatory or without flexibility*”. The Environment, Resources and Development Court arrived at a very different conclusion in the matter of *Angove v District Council of the Copper Coast* [2009] SAERDC 18. In that matter the Court made the following remarks at paragraph 40 of its judgment:

**“In the present matter I find the language of Principle 21(e) to be direct and limited in its flexibility – “... should have a floor area no greater than... 75 square metres”. ...”** (our emphasis).

The Court had no difficulty concluding that a provision using the word “should” rather than “must” was direct and limited in its flexibility.

We respectfully suggest that the SCAP should approach its assessment on the basis that the height limit outlined in the Development Plan is expressed in a way that is clear, direct and limited in its flexibility. The exceedance of the applicable building height limitation is undeniable and obvious. It is material and cannot be ignored. It should be the focus of careful consideration and assessment noting the careful arrangement of heights and precinct boundaries within the relevant Policy Area. In our submission there is no good reason to depart from the clearly stated provisions.

There is nothing “unique” about the location of the subject land. It is simply not located in a position which lends itself to development of greater than 5 storeys. That much has been made crystal clear as the authors of the Development Plan have obviously taken great care in identifying the boundaries demarking the relevant precincts. The only conclusion which can be reasonably reached is that the subject land has been deliberately excluded from Precinct 5 and, conversely, that it has been deliberately included within Precinct 4 so that the Precinct 4 height limits apply to it. It could have readily been included within Precinct 5 (or specifically identified by the Development Plan provisions as being capable of accommodating a transition) had the authors of the Plan envisaged that it should accommodate development of more than 5 storeys in height. However when looking at the spatial arrangement of the 3 separate precincts it is unsurprising that it has been deliberately excluded. In any event, it is not for the applicant or for the relevant planning authority to attempt to “second guess” the authors of the Plan or to embark on some kind of de facto rezoning on the basis of an unfounded observation that the subject land is “unique” and represents an opportunity to



depart from the clear provisions in the Development Plan. If the SCAP agrees that the proposed development is not being sited in a "transition pocket" (see paragraph number 7 of Botten Levinson's letter at page 5) then it will be unable to accept the argument advanced on behalf of the applicant that this "*diminishes the weight to be applied to the generic precinct wide height limits*". Indeed, many of the arguments advanced on behalf of the applicant collapse in the event the SCAP properly concludes that the subject land is not a "transition-pocket". It is difficult to see how the SCAP could accept the argument where there is no support for it in the clearly worded policies applying within the relevant precincts. Furthermore, we disagree that the height limits should be regarded in any way as being broadly applicable within the Development Plan or "generic". The relevant precincts are clearly and tightly demarked and the height limits are applicable in focused pockets of the Council's area.

The subject land is not in fact "*surrounded on all sides (except one) by Precinct 5 (12 storeys or 43m)*" as suggested by Botten Levinson. The land to the south is our client's land which is located in Precinct 4. The land to the north and east also comprises land located in Precinct 4.

The Policy Area is broken down into three distinct precincts, being "Precinct 3 Three Storey", "Precinct 4 Five Storey" and "Precinct 5 Twelve Storey".

There is a significant difference in the maximum height limit envisaged within each precinct. Each precinct is intended to achieve its own unique outcomes. There is a significant difference in the development potential of land within each of these precincts. Precinct boundaries are apparently drawn with reference to street and allotment boundaries. All precincts extend to the north and south of Jetty Road, Glenelg.

The boundaries of each precinct are clearly defined. Each precinct takes the form of a series of "strips" which are generally oriented in a north-south direction. Many of the strips which comprise Precinct 5 are a single allotment or land holding in depth (see Precinct Maps HoB/2 and HoB/4). From a planning perspective, this enables the taller buildings within Precinct 5 to capitalise on views available in more than one direction. So, for example, on the single allotments which comprise Precinct 5 between Anzac Highway and George Street, multi-storey buildings can be constructed so as to take advantage of views to the east and west. This of course enables proponents to design buildings which can capitalise on obvious opportunities to achieve high levels of residential amenity whilst also avoiding the risk that such views and residential amenity will be compromised if another tall building suddenly rises next door to the east.

These obvious observations bring us to the assertions made on behalf of the applicant that some kind of "co-ordinated transition" should be permitted to occur on an allotment which sits outside Precinct 5 but which abuts the boundary to Precinct 5 (eg. the subject land). For convenience, we **enclose** a composite map which combines Precinct Maps HoB/2 and HoB/4 to show the full extent of the relevant precincts on one document. The Development Plan does recognise the fact that there is a particular opportunity within the southernmost portion of Precinct 4 along South Esplanade (which abuts the southernmost boundary of Precinct 5 on South Esplanade) for a co-ordinated transition

in building heights to occur from north to south (see the desired character for Precinct 4 which states "*Development on land fronting the South Esplanade immediately adjacent Precinct 5 Twelve Storey may accommodate additional building height over 5 storeys to achieve a transition in scale from the taller building anticipated in Precinct 5, down to the 5 storey scale anticipated in Precinct 4 ...*"). With reference to Precinct Map HoB/4, it is important to note that such a transition can happily occur in this unique location because it will occur on South Esplanade to the immediate south of Precinct 5. This presents an obvious opportunity for transition in a way which will still enable occupants to enjoy views to the east and west and happily co-exist in a typical waterfront built form arrangement where buildings sit side by side one another with primary views to the west (and with some views also enjoyed to the east depending on a variety of factors). The opportunity for this kind of comfortable and typical built form relationship obviously does not arise on the subject land. The situation on the subject land is completely different and presumably this is the reason why the authors of the Development Plan did not take the opportunity to identify it as presenting an opportunity to achieve some kind of transition. Indeed, the specific acknowledgement of an opportunity of a transition at the South Esplanade and the absence of such an acknowledgement with respect to the subject land speaks volumes in relation to the intent and purpose of the Development Plan provisions. We respectfully suggest that the SCAP should be very cautious about endorsing a 7 storey transition on the subject land in circumstances where the Development Plan clearly does not envisage one here and only envisages this occurrence along a confined portion of the South Esplanade. We fail to see how it is possible to call such a policy approach in support of the proposed development. It is a plain and obvious indication that a transition is not envisaged on the subject land by the authors of the Development Plan and is only envisaged in one very unique situation along the South Esplanade.

It is not at all difficult to identify logical reasons why the Development Plan does not envisage, encourage or contemplate a "transition" in building height between 5 storeys and 12 storeys occurring on the subject land. For example, the taller the building on the subject land within Precinct 4, the greater the impact it will have on the future development potential of the Precinct 5 land.

A building which exceeds the height limit envisaged in Precinct 4 will begin to block potential views to the east of multi-storey development envisaged in Precinct 5 in an unwelcome way. The development potential of the valuable Precinct 5 land will then be unnecessarily compromised. A building which exceeds the height limit of a building envisaged in Precinct 4 will unnecessarily create unwelcome privacy and acoustic impacts (to identify some of the potential impacts) between the occupants of a Precinct 5 building and the occupants of the upper levels of a Precinct 4 building which is too tall.

Quite apart from the above, the proposed development would also bring the impacts of taller, multi-level development closer to dwellings adjoining Sturt Street in Precinct 4 which should not exceed two storeys in height having regard to the desired character for Precinct 4. There is just no justification or logical reason for the SCAP to contemplate or encourage development which encroaches above the carefully identified height limits for each precinct.



Another consequence of the proposed development exceeding the maximum height limit by two storeys is that it will result in a building which is capable of accommodating more people. The sixth and seventh storeys will, for example, accommodate 24 beds. This, in turn, will, for example, result in more pedestrian and vehicle movements, the creation of more waste and the requirement for more visitor and resident car parks. From our client's perspective it will obviously be an extremely dominant and over-bearing building which will result in significant overshadowing to the south and a significant loss of privacy and amenity.

With respect to the obvious privacy concerns, the proposal does not appear to have incorporated any measures to prevent overlooking from internal living spaces or balconies into our client's property, including his private open space and habitable rooms.

### **Traffic, parking and waste management**

Our client, along with a number of others, gain access to their residential properties via Laycock Lane. Given our client's experience with using this lane, and noting that vehicular access to the proposed development is solely via the Lane, our client has obtained advice from a qualified and experienced traffic engineer, Melissa Mellen of MFY, in relation to traffic, parking and waste management issues.

We **enclose** a report prepared by Ms Mellen accordingly.

Ms Mellen has raised a number of safety concerns for users of the Lane, including occupants and visitors to the proposed development. The ERD Court has recognised that *"Safety is a basic concern of town planning and one about which the benefit of the doubt should be given rarely, if at all."* These concerns cannot be overlooked by the relevant planning authority which has a responsibility to comprehensively consider safety issues in the public interest.

One of the great difficulties with the proposed design is that the eastern wall of the proposal and the eastern edge of the car park entry is all to be constructed on the eastern property boundary with no setback, driveway or opportunity for improved manoeuvring and sight lines within the subject land. Vehicles of all kinds will exit the car park straight onto Laycock Lane.

The reality is that all of the concerns expressed by Ms Mellen are magnified when a proponent seeks to provide more extensive accommodation over more levels than are envisaged within the relevant precinct. The more people accommodated by the proposed building, and the more people using the car parks and lane, the greater the problem. This is another issue directly associated with the unacceptable exceedance of maximum building height and maximum building levels. Once again, there can be no justification for the approval of a proposal which exceeds relevant and material quantitative standards to such an extent. This is fatal to the current proposal in our client's opinion. The concern is clearly supported by Ms Mellen.

We wish to be heard by the State Commission Assessment Panel in relation to our client's representation and would be grateful if you would advise us of the date and time of the relevant meeting.

Please contact me if you have any queries.

Yours Faithfully,



Sydney McDonald

[sydney@hilditchlawyers.com](mailto:sydney@hilditchlawyers.com)

Our Ref: JRH:000683

Land Not Within a Council Area (Metro)

MAP HoB/1 Adjoins

Coast 3 nautical miles seaward of low water mark

SUBJECT LAND

Holdfast Bay

Development Plan Boundary is the Low Water Mark

KEN TREMBATH'S LAND

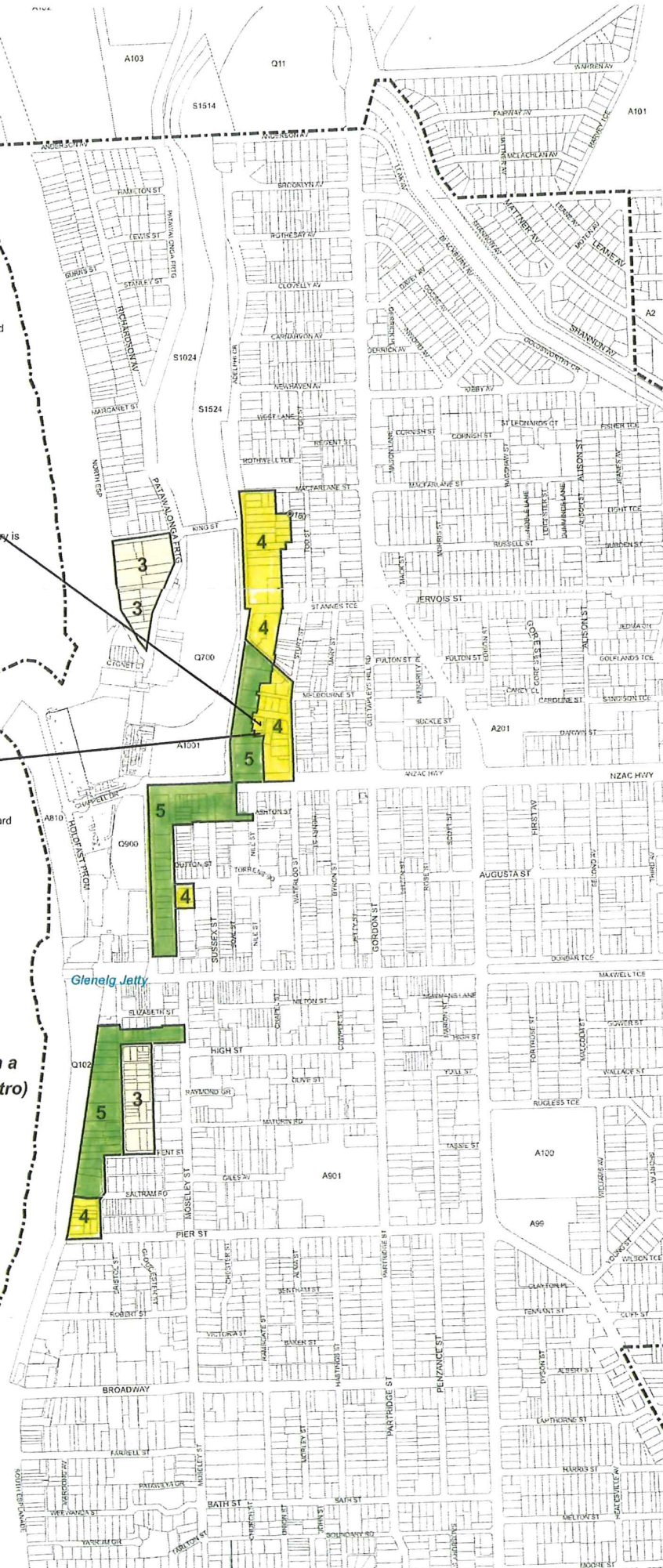
Coast 3 nautical miles seaward of low water mark

S495

MAP HoB/1 Adjoins

Land Not Within a Council Area (Metro)

Development Plan Boundary is the Low Water Mark



MAP HoB/6 Adjoins





MLM/19-0170

26 July 2019

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Hilditch Lawyers  
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MFY Pty Ltd

ABN 79 102 630 759

Dear Mr McDonald,

**DA 110/M004/19 - 2 CANNING STREET, GLENELG NORTH**

I refer to the proposed development for a multi-level apartment building at 2 Canning Street, Glenelg North. The proposal, which will include a mix of residential and serviced apartments, would be accessed via Laycock Lane.

As requested, I have completed a review of the proposal as it relates to traffic, parking and waste collection matters. In undertaking this review, I have considered the proposed plans prepared by Brown Falconer and the report prepared by InfraPlan.

The subject site is currently occupied by a single dwelling. Access to the existing garages at the site are via Laycock Lane but are set-back from the edge of the lane to increase manoeuvrability and sight lines to and from the garages.

Laycock Lane is reported in the InfraPlan report to be 5.4 m in width. The effective width of the lane, however, is 5.1 m at the northern end (and 4.8 m at the southern end) due to the solid fence(s) at the boundary of the lane. A driver using a trafficable area requires a minimal off-set to any solid obstruction. Relevant Australian Standards and Guidelines recommend this clearance be at least 300mm. This is normally achieved through the provision of a footpath or verge but in the case of Laycock Lane, the trafficable width extends to the boundary. The lack of clearance to the fence results in an effective reduction in width of 300 mm adjacent the solid fence(s). That is, drivers position their vehicle at least 300mm within the lane.

Access to adjacent existing residential properties is via Laycock Lane. All existing access points are recessed into their allotments which, in my view, is to provide for adequate turning and sight lines for drivers entering and exiting their properties.

The proposal seeks to develop the following:

- 12 three bedroom residential apartments;
- 8 two bedroom residential apartments;
- 38 single bedrooms serviced apartments: and
- parking in a stacker system.

Having reviewed the plans and traffic assessment, I consider that there are a number of deficiencies with the proposal from a traffic and parking perspective, namely:

- While the traffic assessment has reviewed turn paths of vehicles accessing the proposed parking spaces, it has not considered that the proposed stacker system has a structure on the outside of each stacking system. Figure 1 illustrates the structure of the WOHR Combilift 543 270 stacking system (which is the premium version of the system specified in the InfraPlan report which maximises the width of the spaces) and demonstrates that drivers will not be able to readily enter or exit the spaces to access the stacker system;



**Figure 1: Vehicle paths to/from the stacker system**

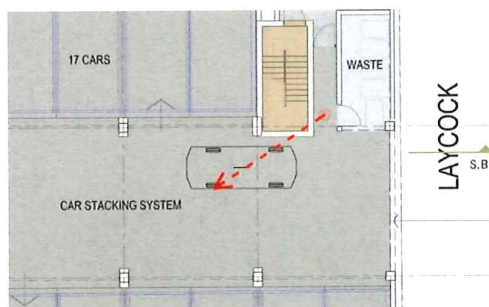
- the car park aisle width adjacent the internal stairs is only 5.9 m wide. Even if it was to be accepted that dimensional requirements associated with a stacker system can be assessed on merit as they are not detailed in Australian/New Zealand Standard, *Parking Facilities Part 1: Off-street car parking (AS/NZS 2890.1–2004)*, this standard still applies to other design requirements within the car park. The Standard provides minimum aisle dimensions for two-way traffic. Allowing for the 300 mm clearance requirement to solid obstructions, the minimum width of the aisle should be 6.1 m at the point between the columns and the stair wall. This is not met by the proposal;
- the access to the site is proposed on the boundary of the lane. This will result in very limited sightlines to drivers approaching the access via Laycock Lane, as illustrated in Figure 2.



**Figure 2: Inadequate Development Sightlines at existing car park**

The lack of sightlines for drivers will result in a significant and unacceptable safety risk for drivers exiting the site and on Laycock Lane. While existing volumes will be low, this does not obviate the need for minimum sightlines to be met. The existing properties have provided for sightlines by provision of a set-back at all vehicular access points. The subject proposal, despite being a much larger traffic generator than the existing dwellings, does not propose to include any provision for sightlines for exiting drivers;

- the pedestrian access within the car park is adjacent the egress lane, with no sightlines for pedestrians prior to stepping into the travel path of the vehicle, as illustrated in Figure 3.



**Figure 3: Pedestrian access within the car park**

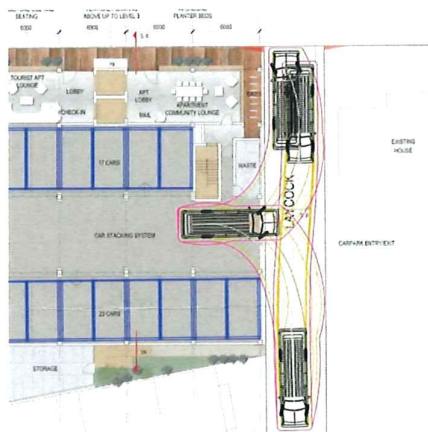
The above situation will result in an unsafe situation for pedestrians when entering the car park to access their vehicle. I am of the view that this will result in an unacceptable risk for pedestrians;

- the refuse collection is proposed using a small refuse vehicle and 600L bins, with potential restrictions on the time during which refuse can be collected. When assessing waste collection vehicle requirements, consideration needs to be given to waste, recycling and green waste requirements. The proposal does not include any details in respect to recycling or green waste collection requirements. Further, no detail has been provided as to whether the small refuse vehicle is capable of handling the larger bins or how long the waste vehicle would obstruct the access while servicing the building.

In addition, the number of small refuse vehicles is limited and it should be confirmed whether the specific contractor has availability to service the site, particularly given the suggested time



restrictions for waste collection. Further, there are no waste collection companies which collect recycling using a small refuse vehicle. A typical recycling vehicle would not be able to access the site, as illustrated in Figure 4;

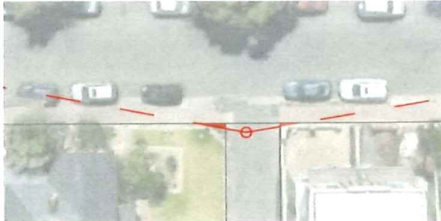


**Figure 4: A typical waste/recycling vehicle unable to access the site**

The above figure illustrates that the recycling vehicle would not be able to access the site, even if a small refuse vehicle could be used for waste. Accordingly, the proposal has not demonstrated that it can cater for the recycling vehicle requirements or green waste collection;

- the parking assessment has considered the minimum rate should be applied to the serviced apartments and relied on clauses in the Development Plan to justify the lower rate. I do not consider that the clause relating to shared use of parking on adjacent sites to be relevant (this clause relates to sharing parking provided for adjacent developments, not constructing parking on one site for a single development) and I would question whether one space per four serviced apartments would be adequate on the subject location because the apartments are all single bedroom;
- even if the minimum parking rate was to be considered appropriate by Council, visitor parking will not readily be able to be accommodated within the stacker system. Visitor parking should be accommodated independently of this system. The InfraPlan report did review on-street parking. While I consider on-street parking to be appropriate for visitors, the studies completed by InfraPlan identified that inadequate spaces were available on the weekend to meet the anticipated demand. It will be important to demonstrate that there is adequate parking on-street to cater for the visitor parking demand;
- the proposal will result in an increase in traffic volume on Laycock Lane. While this lane was originally designed to provide for rear access to residential properties, it does not meet the minimum standard for two-way traffic. The effective width of the lane will be reduced to 4.8 m. Two-way domestic vehicles could (just) pass in such a width, but the higher volume generated by the proposal (an increase from less than 10 vehicles per day(vpd) to over 100 vpd being generated by the proposed development) will increase the potential risk of conflict and two vehicles requiring to pass. This will decrease the safety in Laycock Lane;

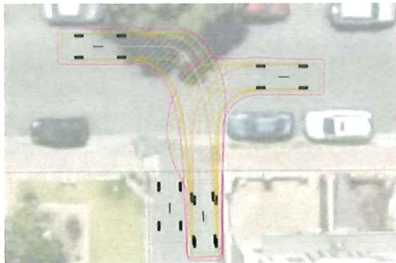
- the sightlines at the intersection of Laycock Lane and Canning Street will be compromised by the proposal, as illustrated in Figure 5.



**Figure 5: Proposed development will obstruct sightlines**

While the proposal is confined to within the subject site, it is proposed to extend to the existing boundary. The volumes exiting at this location will be substantially increased when compared to the existing very low volumes, thus increasing the potential risk at the intersection. Further, while the drivers view does extend across the corner of the site, in reality sightlines are currently met due to the set-back of the existing dwelling; and

- there are existing turning constraints at the Laycock Lane/Canning Street intersection created by the narrow lane width, solid fence and stobie pole. Figure 6 illustrates this constraint.



**Figure 6: Turn constraints at the intersection**

The above issue will be exacerbated by an increase in turning movements at the intersection.

In summary, there are a number of design deficiencies with the proposal. Of particular concern is the increase risk to drivers in Laycock Lane and substandard access to the site and stacker system. Laycock Lane is not of adequate width to cater for the access as currently proposed and there will be a decrease in safety for existing users of the lane, together with a risk for drivers exiting the subject site.

Further, the proposal will result in additional parking on the street and this issue, together with the requirements for waste collection, have not been resolved for the proposal.

Yours sincerely,  
**MFY PTY LTD**



**MELISSA MELLEN**  
Director

South Australian  
DEVELOPMENT ACT 1993  
REPRESENTATION ON APPLICATION – CATEGORY 2

RECEIVED 18 JUL 2019

**Applicant:** Barrio Developments  
**Development Number:** 110/M004/19  
**Nature of Development:** Demolition of existing building, and construction of a seven level residential flat building, comprising 20 apartments and 38 tourist accommodation rooms and ancillary car parking and landscaping.  
**Development Type:** Merit  
**Zone / Policy Area:** Residential High Density Zone / Urban Glenelg Policy Area 15 / Five Storey Precinct 4  
**Subject Land:** 2 Canning Street, Glenelg North  
**Contact Officer:** Will Gormly  
**Phone Number:** 08 7109 7370  
**Close Date:** 29 July 2019

My Name: AIME BOWERS My phone number: 08 83764381

**Primary method(s) of contact:** Email: AIME.BOWERS@BIGPOND.COM  
Postal Address: 7/4 ADELPHI TCE GLENELG NORTH SA Postcode: 5043

You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.

**My interests are:**  
(please tick one)

☒ owner of local property  
☒ occupier of local property  
☐ a representative of a company/other organisation affected by the proposal  
☒ a private citizen

The address of the property affected is:

7/4 ADELPHI TCE GLENELG NORTH Postcode 5043

**My interests are:**  
(please tick one)

☐ I support the development  
☐ I support the development with some concerns  
☒ I oppose the development

The specific aspects of the application to which I make comment on are: 5 storey Precinct  
CRAMMING 58X2 people into an area which accommodated one family  
is totally unacceptable based on adjoining street & traffic which is already under  
pressure. Overshadowing & loss of privacy issues are a concern

**I:** ☒ wish to be heard in support of my submission  
(please tick one) ☐ do not wish to be heard in support of my submission  
(Please tick one)

**By:** ☒ appearing personally  
(please tick one) ☐ being represented by the following person  
(Please tick one)

**Signature:** Aime Bowers  
**Date:** 15.07.2019



South Australian  
DEVELOPMENT ACT 1993  
REPRESENTATION ON APPLICATION – CATEGORY 2

RECEIVED 29 JUL 2019

**Applicant:** Barrio Developments  
**Development Number:** 110/M004/19  
**Nature of Development:** Demolition of existing building, and construction of a seven level residential flat building, comprising 20 apartments and 38 tourist accommodation rooms and ancillary car parking and landscaping.  
**Development Type:** Merit  
**Zone / Policy Area:** Residential High Density Zone / Urban Glenelg Policy Area 15 / Five Storey Precinct 4  
**Subject Land:** 2 Canning Street, Glenelg North  
**Contact Officer:** Will Gormly  
**Phone Number:** 08 7109 7370  
**Close Date:** 29 July 2019

**My Name:** BRIAN WOOD **My phone number:** 0414 824 167

**Primary method(s) of contact:** **Email:** brgawood@senet.com.au  
**Postal Address:** 514 ADELPHI TERRACE **Postcode:** 5045  
GLENELG NORTH

You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.

**My interests are:**  
(please tick one)

☐ owner of local property  
☒ occupier of local property  
☐ a representative of a company/other organisation affected by the proposal  
☐ a private citizen

**The address of the property affected is:**

AS ABOVE **Postcode** \_\_\_\_\_

**My interests are:**  
(please tick one)

☐ I support the development  
☐ I support the development with some concerns  
☒ I oppose the development

**The specific aspects of the application to which I make comment on are:** 7 levels are 2 levels too high - the land area and infrastructure do not cope with the proposed number of apartments and rooms especially parking, traffic, noise levels and overlooking of nearby properties.

**I:**  
(please tick one)

☐ wish to be heard in support of my submission  
☒ do not wish to be heard in support of my submission  
(Please tick one)

**By:**  
(please tick one)

☐ appearing personally  
☐ being represented by the following person  
(Please tick one)

**Signature:** [Signature]  
**Date:** 29.07.2019

29 July 2019

James Rusk  
Strata Corporation 5463 Inc.  
Aquarius Apartments  
4-5 Adelphi Terrace, Glenelg North SA 5045  
Tel: +61 411 778 163 Email: [jimmyrask@gmail.com](mailto:jimmyrask@gmail.com)

The Secretary  
State Commission Assessment Panel  
[scapreps@sa.gov.au](mailto:scapreps@sa.gov.au)

**RE: Development Application 110/M004/19 - 2 Canning Street, Glenelg North**

On behalf of Strata Corporation 5463 Inc. (The Strata) which represents all land owner occupiers from 4-5 Adelphi Terrace, Glenelg North, we hereby submit a representation opposing the application for the following Category 2 development which adjoins our property.

Applicant	Barrio Developments
Application Number	110/ M004/ 19
Proposed Development	Demolition of existing building, and construction of seven level residential flat building, comprising 20 apartments and 38 tourist accommodation rooms and ancillary car parking and landscaping.
Subject Land	2 Canning Street, Glenelg North

The Strata has completed the representation template (enclosed) but given the space limitations of the form it has also provided comment on the specific aspects of the application which form the basis of its opposition to the application in its current form. The Strata has made references to various aspects of the planning statement prepared by URPS Planning Consultants.

**REPRESENTATION SUMMARY**

The proposed development is significantly at odds with a number of provisions of the Development Plan – and should therefore be REFUSED.

Our planning concerns relate to the following:

- Overall height and scale of the development – significantly exceeding the maximum building height and maximum number of storeys for the Policy Area
- Insufficient on-site car parking to meet the needs of the development
- Significant overshadowing and loss of residential amenity, particularly to the existing residential properties to the east of the subject land
- Interface and poor urban form, particularly to the Canning Street and Laycock Lane boundaries (i.e. no building setback).

### **Height of Development**

The proposed development site is located in Urban Glenelg Policy Area 15 of the Residential High Density Zone Precinct 4 - *Five (5) Storeys – should not exceed 18.5m in height.*

Section 5.3 of the planning statement states:

*“At 7 storeys at 23.5 metres, the proposal exceeds the 5 storey or 18.5 metre height limit.”*

The URPS planning consultant then states *“Such a variation is acceptable in my view given the conditions of the locality, the location at the ‘edge’ of the 12 storey/43 metre wall height area (i.e. Precinct 5) and the proximity of the land to the Residential Zone.”*

Section 5.5 of the planning statement states:

*“The proposal was originally presented to the planning authority as an 11 storey building, then reduced to 8 storeys and it is now lodged at 7 storeys”; and*

*“The land sits at the edge of a precinct wherein 12 storey and 43 metre wall heights are envisaged.*

Botten Levinson Lawyers were commissioned by the applicant to support the application to exceed the height limitations. Mr Tom Game states that “an exceedance of the height guidelines is likely to be appropriate and justified in the circumstances of the proposed development.”

The Strata considers that inclusion of legal opinion and case law in the application to be inappropriate, as SCAP and not a lawyer acting on behalf of the applicant is best placed to consider whether or not there is merit for the proposal/ development to be approved given departures from the provisions of the Development Plan.

The current application still exceeds the height limitations of the precinct by 5 metres. This represents a 27% increase on the 18.5 metre limit. Based on the height analysis (Appendix K of the planning statement), to keep to the 18.5 metre external wall height limitations of Precinct 4 of the Development Plan, the proposed development should not exceed 5 storeys in height including ground floor hotel reception and car parking.

The planning statement does not present any technical argument as to the basis of why it should be approved given the significant departure to the height limitations of the Precinct (e.g. how despite its height it does not unduly impact on the character of the local area and amenity of other residents), rather relying solely on the fact that the development site is adjacent the boundary of the 12 storey/ 43 metre wall height area of Precinct 5.

In 2018 SCAP rejected a similar application by the applicant for an apartment development nearby at 22-23 Adelphi Terrace on the basis that its height exceeded the height limitations of Precinct 4. The Strata understands that SCAP subsequently approved a six storey development for the site within the 18.5 m height limitations.

The Strata suggests that in submitting this development application, the applicant continues to not have any regard for the height limitations of the Precinct 4, nor the precedence of the decision surrounding similar developments submitted by itself and others in the local area.

Section 3.5 of the planning statement notes that *“prior to commencing with any pre-lodgement consultation, verbal feedback provided from DPTI in late 2016 was that a project over height was a*

*“reasonable proposition” at this site primarily given its adjacency to the adjoining Precinct 5 wherein 12 storey/43 metre tall development is contemplated and where there is an existing residential apartment building of some 13 storeys.”*

The Strata objects to the use of unreferenced verbal communication being used to support the application.

### **Car parking**

Section 3.8 of the planning statement mentions that *“all car parking stacker areas will be nominated on the title as ‘communal’ with the specific allocation of car parking spaces nominated via the building’s by-laws”*. The applicant provides no details as to how many apartments and tourist accommodation rooms will be allocated with car parking spaces.

The Infraplan traffic and car parking report provided in Appendix D of the planning statement indicates that the 40 off-street car parking spaces (only possible due to a vertical car stacker system) marginally meet the minimum number of required off-street carparks required by the City of Holdfast Bay Development Plan based on the number of apartment dwelling types and tourist accommodation rooms. Infraplan also undertook limited (two) spot surveys of the availability of the 24 on-street car parking spaces along Canning Street.

The Strata suggests that Infraplan has not presented a robust assessment as to whether any on-site car parking shortfalls can be supplemented by on-street car parking. No allowances for on-site car parking for staff servicing the on-site tourist rooms have been included in the calculations. Infraplan has also not considered other approved development in the area that may result in such on-street car parking becoming more limited.

The approved (but yet to be developed) large-scale 14-storey integrated apartment, hotel and conference centre development at 6-10 Adelphi Terrace (diagonally north-west from the subject development) made a mere allowance for 70 on-site carparks based either on minimum requirements of the Development Plan and in some instances “trends in parking demand”. The on-site carpark numbers did not meet minimum requirements of the Development Plan. Further, this development did not make any allowance for on-site car-parking for the 65 full-time equivalent hotel staff, visitors to the hotel and retail shops. The development application also did not provide any surveys of on-street car parking numbers nor availability in the area.

Although Infraplan suggests (without data) that Canning Street has more than sufficient capacity to carry the additional traffic generated from this development, it has failed to consider that if on-street car parking demand from the subject development and/or the nearby 14-storey integrated development is not met then there will be greater traffic circulation in the local streets in this neighbourhood.

The Strata also notes that in future, Council may extend timed parking restrictions that exist on the adjacent Adelphi Terrace to include Canning Street, thus further limiting the ability for on-street car parking to cater for times when demand is exceeded.

### **Overlooking and Overshadowing**

There are numerous residential premises directly south, east and west of the property that are seriously affected by the significant overshadowing impacts arising from the proposal.

Section 5.5 of the planning statement notes *“Further, the height of the development does not give rise to residential amenity impacts by way of overlooking or overshadowing upon low density housing outside of the zone.”* The Strata notes that no technical basis for such a statement is provided.

Section 5.7.2 of the planning statement refers to, but does not demonstrate, how it has met Policy Area 15 Principle 5: *Building design should minimise the impact of overlooking and overshadowing on existing lower density and scale development in adjoining zones/policy areas/precincts.*

### **Setback Distances**

The proposed development does not include any setback from the footpath of Canning Street. This is evident within the photomontage provided by Brown Falconer, “Site Context – Canning Street looking west” (Appendix C of the planning statement). The Strata notes that this is not in keeping with the visual amenity of Canning Street in which all other properties have building setback distances of around three metres.

### **Visual Impact of Building Design**

The concept design drawings (Appendix C of planning statement) indicate that eastern facing windows of the building occupied by residents of the Strata will primarily overlook a pre-fabricated concrete wall. The Strata considers that the applicant and its designer have not any regard for the visual amenity of the existing residents in this aspect of the design.

### **Noise from Vertical Car Stacker**

Resonate Acoustics undertook an acoustic impact assessment of the vertical car stacker system (refer Appendix J of planning statement). Noise measurements of an installed Wohr Combilift 543 system were reportedly undertaken by Resonate Acoustics but details were not outlined in their report. On the basis of these unreported measurements, acoustic treatments were recommended. Without the measurement data, the Strata is unable to consider whether acoustic treatment was needed to meet the environmental noise criteria and whether it should seek an independent assessment of whether the acoustic mitigation/ treatment recommendations are likely to be reasonable.

### **Solar Panels on Roof**

No information on the tilt and angle of the proposed solar PV panels on the roof were provided in the application. The applicant has not considered the potential for glare resulting from the solar PV panel installations to impact on residents of the Strata.

### **Engagement with Stakeholders**

The Strata considers itself a key stakeholder regarding the application and elements of the design of the development. Section 3.5 of the URPS planning statement notes that *“the proposed apartment building has been designed in close consultation with the DPTI/ODASA with the design consultation process commencing in late 2016, re-commencing in late 2018 and ultimately concluding in June 2019”*.

The Strata has not been engaged by the applicant or its representatives during the ~3-year design process that the applicant has undertaken.

## **Closure**

The proposal is significantly at odds with a number of provisions of the Development Plan – and should therefore be REFUSED.

As noted on the enclosed form, the Strata wishes to be notified of and be given the opportunity to speak at the SCAP hearing regarding a decision on the development. The Strata may nominate an alternative representative to speak at the hearing. Further, given the limited time provided to respond to the notification, the Strata would like the opportunity to clarify aspects of the comments outlined above and to provide additional comment on specific aspects of the application.

Strata respectfully requests that it be given adequate notice of any SCAP hearing relating to this application in order to obtain professional advice and prepare for a submission.

Sincerely,



James Rusk  
Committee Member  
Strata Corporation 5463 Inc.

Enc: Representation on Application Form

Cc Will Gormy, Senior Planning Officer, delegate of SCAP [will.gormly@sa.gov.au](mailto:will.gormly@sa.gov.au)  
Steve Officer, President, Strata Corporation 5463 Inc. [steveoff@adam.com.au](mailto:steveoff@adam.com.au)  
David Chapman, Whittles Strata Management [David.Chapman@whittles.com.au](mailto:David.Chapman@whittles.com.au)



**South Australian  
DEVELOPMENT ACT 1993  
REPRESENTATION ON APPLICATION – CATEGORY 2**

**Applicant:** Barrio Developments  
**Development Number:** 110/M004/19  
**Nature of Development:** Demolition of existing building, and construction of a seven level residential flat building, comprising 20 apartments and 38 tourist accommodation rooms and ancillary car parking and landscaping.  
**Development Type:** Merit  
**Zone / Policy Area:** Residential High Density Zone / Urban Glenelg Policy Area 15 / Five Storey Precinct 4  
**Subject Land:** 2 Canning Street, Glenelg North  
**Contact Officer:** Will Gormly  
**Phone Number:** 08 7109 7370  
**Close Date:** 29 July 2019

**My Name:** James Rusk **My phone number:** 0411778163

**Primary method(s) of contact:** **Email:** jimmyrask@gmail.com  
**Postal Address:** 8/4 ADELPHI TERRACE **Postcode:** 5045  
GLENELG NORTH

**You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.**

**My interests are:**  
(please tick one)

- ☒ owner of local property + representative of Strata Corporation 5463 Inc,  
☐ occupier of local property land owner occupiers of 4-5 Adelphi Terrace  
☐ a representative of a company/other organisation affected by the proposal  
☐ a private citizen

**The address of the property affected is:**

4-5 Adelphi Terrace, Glenelg North

**Postcode** 5045

**My interests are:**  
(please tick one)

- ☐ I support the development  
☐ I support the development with some concerns  
☒ I oppose the development

**The specific aspects of the application to which I make comment on are:**

please refer to accompanying letter for details. Our planning concerns relate to (1) overall height and scale of the development exceeding existing heights and storeys for the Policy Area, (2) insufficient carparking, (3) significant overshadowing, (4) interface and poor urban form (ie no building setback)

**I:** ☒ wish to be heard in support of my submission  
(please tick one) ☐ do not wish to be heard in support of my submission  
(Please tick one)

**By:** ☒ appearing personally  
(please tick one) ☐ being represented by the following person  
(Please tick one)

**Signature:** [Signature]

**Date:** 29/07/2019

Refer attached letter

Ref: 19ADL-0070

10 September 2019

Mr Will Gormly  
Senior Planner  
DPTI  
Level 5, 50 Flinders Street  
**ADELAIDE SA 5000**



**URPS**

Suite 12  
154 Fullarton Road  
ROSE PARK SA 5067

08 8333 7999  
[www.urps.com.au](http://www.urps.com.au)  
ABN 55 640 546 010

Dear Will

## **2 Canning Street Glenelg North – Response to Representations**

### **Introduction**

This letter provides our response to the representations and is to be read in conjunction with the following:

- Amended proposal plans prepared by Brown Falconer.
- Amended landscaping plans prepared by LCS Landscapes.
- Amended Levels/Stormwater Management Plan prepared by Structural Systems.
- Updated traffic, parking and waste assessment prepared by Infraplan.
- Written response to MFY representation prepared by Infraplan.

### **Amended Plans**

The proposal has been amended, in essence, as follows:

#### **Height**

The overall building height has been reduced by 1.1 metres by:

- Reducing the floor-to-ceiling height of the two levels of tourist accommodation (i.e. levels 1 and 2) by 150mm therein reducing it down to 2550mm i.e. there is a total reduction here of 300mm.
- Optimising the height of the stacking system used which reduces the overall height by a further 600mm (there has been a change from 'Premium' to 'Standard' type combilift 543).
- A 200mm car park floor level set down (i.e. below natural ground level)

#### **Setbacks to Laycock Lane**

A ground level setback on the eastern portion of the site of 800mm has been incorporated to effectively widen the carriageway of Laycock Lane to improve trafficability and sight lines. Although it

is strictly unnecessary the applicant is willing to cede that land to the Council permanently by merging it with the Laycock Lane road reserve, and would abide a condition to that effect.

### ***Bulk, Scale and Design***

The external wall height has been reduced from 23.85 metres to 18.5 metres by incorporating a proper mansard style roof design. In turn, this has reduced the bulk and scale of the development as the upper-most level now features angled elements which serve to reduce the verticality and 'heaviness' of the top floor.

As result of the mansard roof, instead of having 2 x 3 bedroom and 3 x 2 bedroom apartments the top floor comprises smaller 3 x 2-bedroom apartments and 2 x 1-bedroom apartments – a total reduction therefore in 4 bedrooms at the top floor of **105.5m<sup>2</sup>** has resulted.

The angled mansard roof design is represented in image 1 below.

**Image 1: 3D visualisation of the amended proposal**



## Representations

Representations have been received from the following:

- |                                |                                   |
|--------------------------------|-----------------------------------|
| • Paul Patterson               | 1 Canning Street Glenelg North    |
| • Lee Melville/Steve Church    | 3 Canning Street Glenelg North    |
| • Amy Dangerfield              | 4 Canning Street Glenelg North    |
| • Danny Colangelo              | 4A Canning Street Glenelg North   |
| • Gerry and Tonia Russo        | 4B Canning Street Glenelg North   |
| • David Ramond                 | 4C Canning Street Glenelg North   |
| • Daniel Caretti               | 4D Canning Street Glenelg North   |
| • Colin Elmer and Lee Widdison | 5 Canning Street Glenelg North    |
| • James Rusk                   | 4-5 Canning Street Glenelg North  |
| • Ken Trembarth                | 1 Laycock Lane Glenelg North      |
| • Aime Bowers                  | 7/4 Adelphi Terrace Glenelg North |
| • Brian Wood                   | 5/4 Adelphi Terrace Glenelg North |

## Summary of Concerns

The concerns of the presentations can be summarised as follows:

- Height
- Density
- Privacy
- Overshadowing
- Access and Car parking

## Response

### Height

A number of concerns have been expressed regarding the height of the proposed development.

In my experiences with multi-residential/mixed use projects it is common for surrounding residents residing within low scale housing to have concerns about the heights of new development upon recently re-zoned land.

In circumstances such as the subject proposal and locality/wider-locality which is going through significant transformation, I can appreciate the concern residents surrounding the land may have with the substantial change such a development as this provides to the character of the area.

As reflected below, it is important for the planning authority to remember however the intent and purpose of the Zone/Policy Area 15 – that being, in this instance, to provide for increased housing choice in high density residential buildings of significant scale that integrate with and contrasts nearby open space.

The Development Plan expresses a clear ambition for increased height and density in this locality, objectives which this development supports.

The most pertinent provisions state:

**Objective 1:**

**A residential zone comprising a range of high-density dwellings, including a minimum of 15 per cent affordable housing, primarily in the form of row dwellings and residential flat buildings, designed to integrate with areas of open space, neighbouring centres or public transport nodes.**

**Desired Character:** **Its built form will contrast with the open character of the adjacent foreshore and reserve public spaces. It will capitalise on the highly desirable location through significant scale, with built form between three and twelve stories in height.**

A number of assertions are made within the Hilditch Lawyers submission about the proposed building height being too tall for the subject land primarily on the basis that the provisions of the Development Plan are clear in their desire for 5 storey development upon land.

I do not dispute the provisions which suggest development should not exceed 5 storeys Precinct 4 of Policy Area 15 however the assessment should not end there and to adopt such an approach is far too simplistic in my view.

The height provision in Precinct 4 is a guideline and development which exceeds such is not a non-complying form of development rather it is to be considered on its planning merits.

For reasons I have previously detailed in my planning report dated 3 July 2019 there are a number of important factors to suggest that the site is in fact a very good candidate for a building taller than the height guidelines that are applicable.

Issues of height have been carefully considered in the design and planning of the development following several months of design development and consultation with the planning authority and ODASA.

Drawing on the advice Tom Game provided to you, it is important in this respect to also remember that the Development Plan is a practical code for practical application and the provisions of the Plan are not mandatory laws and are rather guidelines.

Contrary to the submission from Hilditch Lawyers, there is nothing in the Development Plan which suggests that the height of buildings in Precinct 4 should be limited in order to preserve views east from buildings in Precinct 5. Indeed, the existing 13 storey Aquarius apartment building on the adjacent site effectively "turns its back" to the east, presenting a largely blank wall to the subject land.

Precinct 4's Desired Character refers to up to 5 storeys (or 18.5 metres) in height. PDC 19 is more instructive and states...."*Development should not exceed an external wall height of 18.5 metres above natural ground level (excluding lift service levels and gables)*". The Development Plan clearly acknowledges that there may be roof elements (such as pitched rooves) which project above 18.5m.

All the above said, the proposal has been amended since it was lodged/notified such that it now satisfies the Development Plan's height provisions because:

1. It has an 18.5 metre wall height reducing down to 18.2 metre wall height at its southern end with its upper-most level located within a mansard style roof.

2. While a mansard is not technically a 'gable' the outcome is in effect the same albeit this is simply a different form of pitched roof.

To the extent that the proposal can be said to exceed the height guideline there are very few if any planning consequences that flow from the exceedance, as discussed further below.

### **Density**

Concern is expressed with the density of the development.

Zone Principle 7 defines High Density as follows:

- 7 High density development that achieves gross densities of more than 45 dwellings per hectare (which translates to net densities of more than 67 dwellings per hectare) should typically be in the form of over 4 storey buildings.**

The proposal features 20 dwellings on an about 800 square metre site meaning a net density equivalent to approximately 250 dwellings per hectare and within a building over 4 storeys in height.

The relevant density policies are satisfied. Desirably this is a 'high density' form of development that occurs near the Patawalonga so as to afford occupants exceptional access to this quality recreational open space.

This location and the broader locality have been strategically targeted for high density infill development such as the proposal. While residents might be concerned about the population in the locality increasing substantially, that is exactly what the Development Plan is seeking.

The density of the development does not of itself give rise to a reason to refuse the development unless it is shown that it will lead to unacceptable planning consequences.

### **Privacy**

Concerns are expressed about privacy however the Zone states "*....higher degree of overshadowing and loss of privacy is expected in the policy area given the medium-to-high density nature of development (and heights).*"

It would be difficult to conceive a situation where privacy screening is incorporated on the external windows and balconies upon a development of this kind in a Zone which expressly calls for high density residential apartment buildings and where the general provisions of the Development Plan for Medium and High-Rise Development favour dwellings have an outlook and views:

- Principle 5: Residential buildings (or the residential floors of mixed use buildings) should: (a) have adequate separation between habitable room windows and balconies from other buildings to provide visual and acoustic privacy for dwelling occupants and allow the infiltration of daylight into interior and outdoor spaces (b) ensure living rooms have, at a minimum, a satisfactory short range visual outlook to public or communal space.**

All the above said, the proposal has sought to reduce the impacts of overlooking on the land to the immediate south (i.e. the only low scale site which has a boundary with the subject land) through the provision of privacy screens (angled louvre screens) on levels 1-2 to a height of 1.5m.



This property falls within the Residential High-Density Zone where reduced levels of privacy are contemplated and where the internal amenity and provision of an outlook from the subject development is an important planning policy per PDC 5 above.

Residents will typically have lower expectations of privacy, as an acknowledged trade-off for the other lifestyle attractions afforded by the locality. This locality is Adelaide's equivalent of Bondi Beach.

### **Overshadowing**

Various representators have expressed concerns about overshadowing.

As I have noted the land falls within a Zone wherein buildings of up to 18.5 metres tall (plus gable roof elements on top) are contemplated. In this context, therefore, it is inevitable there will be overshadowing cast by such developments. This statement is reinforced by the Desired Character of PA15 states:

*"...higher degree of overshadowing and loss of privacy is expected in the policy area given the medium-to-high density nature of development (and heights).*

Furthermore, it is clear that the impacts of overshadowing on nearby low sale housing are to be protected when such housing occurs in adjoining zones/policy areas/precincts, i.e. PDC 5 states:

*"Building design should minimise the impact of overlooking and overshadowing on existing lower density and scale development in adjoining zones/policy areas/precincts.*

I have reviewed the overshadowing diagrams and note that at 9am, midday and 3pm during winter solstice there is no impact on any properties within the adjoining zone/policy area/precinct. Therefore, the proposal satisfies Policy Area 15 PDC 5 as quoted above.

The overshadowing impacts to the property to the south at 1 Laycock Lane are unavoidable and would not be materially different even if the upper level were removed.

### **Access and Car Parking**

Please refer to the attached opinion prepared by Infraplan which provides a direct response to the MFY comments. In addition, Infraplan has prepared an updated assessment report on the traffic, parking and waste collection aspects of this matter.

### **Conclusion**

The proposal has been amended in response to the representations and also concerns expressed by ODASA regarding the bulk and scale of the development. In particular, the total height has been lowered and the bulk and scale also reduced through various changes as summarised above.

This response also addresses the concerns expressed regarding density, privacy, overshadowing, and access and car parking. In my view, the proposal in its amended form provides greater alignment with the Development Plan than the as-lodged version. Development Plan Consent is warranted

I confirm I will be available to present in support of this project at the relevant SCAP hearing. Should you have questions I can be contacted on 8333 7999.

Yours sincerely

A handwritten signature in black ink, appearing to be 'MK' followed by a long horizontal stroke.

Matthew King RPIA  
**Managing Director**

## **Development Plan Provisions**

Extracted from  
**Adelaide (City) Development Plan**  
**Consolidated 2 June 2016**

**2 Canning Street, Glenelg North**

## **Council Wide**

## **Crime Prevention**

### **OBJECTIVES**

- 1 A safe, secure, crime resistant environment where land uses are integrated and designed to facilitate community surveillance.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

- 1 Development should be designed to maximise surveillance of public spaces through the incorporation of clear lines of sight, appropriate lighting and the use of visible permeable barriers wherever practicable.
- 2 Buildings should be designed to overlook public and communal streets and public open space to allow casual surveillance.
- 3 Development should provide a robust environment that is resistant to vandalism and graffiti.

## **Design and Appearance**

### **OBJECTIVES**

- 1 Development of a high design standard and appearance that responds to and reinforces positive aspects of the local environment and built form.
- 2 Roads, open spaces, paths, buildings and land uses laid out and linked so that they are easy to understand and navigate.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

- 1 Buildings should reflect the desired character of the locality while incorporating contemporary designs that have regard to the following:
  - (a) building height, mass and proportion
  - (b) external materials, patterns, colours and decorative elements

- (c) roof form and pitch
  - (d) façade articulation and detailing
  - (e) verandas, eaves, parapets and window screens.
- 2 Where a building is sited on or close to a side boundary, the side boundary wall should be sited and limited in length and height to minimise:
    - (a) the visual impact of the building as viewed from adjoining properties
    - (b) overshadowing of adjoining properties and allow adequate sun light to neighbouring buildings.
  - 3 The external walls and roofs of buildings should not incorporate highly reflective materials which will result in glare to neighbouring properties or drivers.
  - 4 Structures located on the roofs of buildings to house plant and equipment should form an integral part of the building design in relation to external finishes, shaping and colours.
  - 5 Balconies should:
    - (a) be integrated with the overall form and detail of the building and make a positive contribution to the external and external amenity of residential buildings
    - (b) be sited adjacent to the main living areas, such as the living room, dining room or kitchen to extend the dwelling's living space
    - (c) include balustrade detailing that enables line of sight to the street
    - (d) be recessed where wind would otherwise make the space unusable
    - (e) have a minimum dimension of 2 metres for upper level balconies or terraces.

## **Energy Efficiency**

### **OBJECTIVES**

- 1 Development designed and sited to conserve energy.
- 2 Development that provides for on-site power generation including photovoltaic cells and wind power.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

- 1 Development should provide for efficient solar access to buildings and open space all year around.
- 2 Buildings should be sited and designed:
  - (a) to ensure adequate natural light and winter sunlight is available to the main activity areas of adjacent buildings
  - (b) so that open spaces associated with the main activity areas face north for exposure to winter sun
  - (c) to promote energy conservation by maintaining adequate access to winter sunlight to the main ground level of living areas of existing dwellings on adjoining land.

### **On-site Energy Generation**

- 3 Development should facilitate the efficient use of photovoltaic cells and solar hot water systems by:
  - (a) taking into account overshadowing from neighbouring buildings
  - (b) designing roof orientation and pitches to maximise exposure to direct sunlight.

### **Medium and High Rise Development (3 or More Storeys)**

#### **OBJECTIVES**

- 1 Medium and high rise development that provides housing choice and employment opportunities.
- 2 Residential development that provides a high standard of amenity and adaptability for a variety of accommodation and living needs.
- 3 Commercial, office and retail development that is designed to create a strong visual connection to the public realm and that contributes to the vitality of the locality.
- 4 Buildings designed and sited to be energy and water efficient.

#### **PRINCIPLES OF DEVELOPMENT CONTROL**

##### **Design and Appearance**

- 1 Buildings should:
  - (a) achieve a human scale at ground level through the use of elements such as canopies, verandas or building projections
  - (b) provide shelter over the footpath where minimal setbacks are desirable
  - (c) ensure walls on the boundary that are visible from public land include visually interesting treatments to break up large blank facades.
- 2 The ground floor level of buildings (including the foyer areas of residential buildings) should be designed to enable surveillance from public land to the inside of the building at night.
- 3 Entrances to multi-storey buildings should:
  - (a) be oriented towards the street
  - (b) be clearly identifiable
  - (c) provide shelter, a sense of personal address and transitional space around the entry
  - (d) provide separate access for residential and non-residential land uses.

##### **Visual Privacy**

- 4 The visual privacy of ground floor dwellings within multi-storey buildings should be protected through the use of design features such as the elevation of ground floors above street level, setbacks from street and the location of verandas, windows porticos or the like.

### **Building Separation and Outlook**

- 5 Residential buildings (or the residential floors of mixed use buildings) should:
- (a) have adequate separation between habitable room windows and balconies from other buildings to provide visual and acoustic privacy for dwelling occupants and allow the infiltration of daylight into interior and outdoor spaces
  - (b) ensure living rooms have, at a minimum, a satisfactory short range visual outlook to public or communal space.

### **Dwelling Configuration**

- 6 Buildings comprising more than 20 dwellings should provide a variety of dwelling sizes and a range in the number of bedrooms per dwelling.

### **Environmental**

- 7 Multi-storey buildings should:
- (a) minimise detrimental micro-climatic and solar access impacts on adjacent land or buildings, including effects of patterns of wind, temperature, daylight, sunlight, glare and shadow
  - (b) incorporate roof designs that enable the provision of rain water tanks (where they are not provided elsewhere), photovoltaic cells and other features that enhance sustainability.

### **Residential Development**

### **OBJECTIVES**

- 1 Safe, convenient, pleasant and healthy-living environments that meet the full range of needs and preferences of the community.
- 2 A diverse range of dwelling types and sizes available to cater for changing demographics, particularly smaller household sizes and supported accommodation.
- 3 Medium and high-density residential development in areas close to activity centres, public and community transport and public open spaces.

### **Transportation and Access**

- 4 Development that:
- (a) provides safe and efficient movement for all transport modes
  - (b) ensures access for vehicles including emergency services, public infrastructure maintenance and commercial vehicles
  - (c) provides off-street parking
  - (d) is appropriately located so that it supports and makes best use of existing transport facilities and networks
  - (e) provides convenient and safe access to public transport stops.



### **Vehicle Parking**

5 Development should provide off-street vehicle parking and specifically marked accessible car parking places to meet anticipated demand in accordance with [Table HoB/1 – Off Street Vehicle Parking Requirements](#)

## **Residential High Density Zone**

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

### **OBJECTIVES**

- 1 A residential zone comprising a range of high density dwellings, including a minimum of 15 per cent affordable housing, primarily in the form of row dwellings and residential flat buildings, designed to integrate with areas of open space, neighbouring centres or public transport nodes.
- 2 Development that supports the viability of community services and infrastructure.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

#### **Land Use**

- 1 The following forms of development are envisaged in the zone:
  - affordable housing
  - residential flat building
  - row dwelling
  - small scale non-residential use that serves the local community, for example:
    - child care facility
    - open space
    - recreation area
    - shop, office or consulting room
  - supported accommodation.
- 2 Development listed as non-complying is generally inappropriate.
- 3 Non-residential development such as shops, schools and consulting rooms should be of a nature and scale that:
  - (a) serves the local community
  - (b) is consistent with the character of the locality
  - (c) does not detrimentally impact on the amenity of nearby residents.
- 4 The use and placement of outbuildings should be ancillary to and in association with a dwelling or dwellings.

#### **Form and Character**

- 5 A residential flat building should provide a variety of dwelling sizes (e.g. bed-sit, one, two and three bedrooms) particularly in larger complexes.
- 6 Medium density development that achieves gross densities of between 23 and 45 dwellings per hectare (which translates to net densities of between 40 and 67 dwellings per hectare) should typically be in the form of 2 to 4 storey buildings.
- 7 High density development that achieves gross densities of more than 45 dwellings per hectare (which translates to net densities of more than 67 dwellings per hectare) should typically be in the form of over 4 storey buildings.

- 8 Development should result in high-quality aesthetic and urban design outcomes, and where possible, allotments should be amalgamated to assist the achievement of this.
- 9 Ground floor dwellings and accommodation should contribute to the desired streetscape of a locality and, where applicable, create active, safe streets by incorporating either or both of the following:
  - (a) front landscaping or terraces that contribute to the spatial and visual structure of the street while maintaining adequate privacy for apartment occupants
  - (b) individual entries for ground floor accommodation.
- 10 All residential development should be designed to ensure the living rooms have an external outlook (an outlook being a short range prospect, as distinct from a view which is more extensive and long range to particular objects or geographic features). Living rooms should not have an outlook only through high level windows and/or a skylight.
- 11 Garages and carports that face and take direct access from the primary street should be avoided. Where there is no practical alternative to their location, garages and carports facing the street (other than an access lane way) should be designed with a maximum width of 6 metres or 50 per cent of the allotment or building site frontage width, whichever is the lesser distance.
- 12 The development of a residential flat building or of group dwellings should include minimum private open space of at least the area shown in the following table:

Configuration	Open space requirement, other than for affordable housing
Studio (without separate bedroom)	No minimum requirement
One-bedroom	8 square metres
Two-bedroom	11 square metres
Three-bedroom or greater	15 square metres

- 13 Service yards, car parking areas and facilities, service ducting and plant should be designed and located to ensure that the appearance of buildings and land viewed from all abutting roads is attractive.
- 14 Driveway cross-overs in prominent pedestrian footpath areas should maintain the footpath level and incorporate measures to maximise pedestrian safety.
- 15 Development should provide car parking within the zone in accordance with [Table HoB/1B – Off Street Vehicle Parking Requirements for the Residential High Density Zone or for Residential Uses in the District Centre Zone Glenelg Policy Area 2](#).

## Urban Glenelg Policy Area 15

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 A policy area comprising tourist accommodation and a range of dwellings and residential flat buildings at medium to high densities.
- 2 Accommodation that provides a diversity of dwelling sizes within residential flat buildings that cater for different household requirements.
- 3 Retention of the heritage character, especially along South Esplanade in **Precinct 4 Five Storey** and **Precinct 5 Twelve Storey**.
- 4 Development that contributes to the desired character of the policy area/precinct.

### DESIRED CHARACTER

The policy area provides the Council's premier coastal medium and high density living opportunities. It includes areas of Glenelg North around the foreshore and the Patawalonga, and within Glenelg and Glenelg South along the foreshore and extending into small parts of the suburban landscape, and along Colley Terrace.

The Glenelg District Centre and Glenelg's major foreshore reserves provide important features adjacent to the policy area (including Wigley and Colley Reserves, the beach and Glenelg foreshore and the Anzac Highway streetscape) and entertainment, retail, residential and visitor apartment accommodation provided nearby in the Holdfast Shores site to the western side of Colley and Wigley Reserve.

The policy area is a premier location with excellent accessibility to views, beach, public spaces, centre services, facilities and public transport. The policy area adds to the choice of accommodation within Holdfast Bay and the wider metropolitan area by providing for a variety of medium and higher density dwelling types, including apartments for residential purposes and visitor accommodation.

Small scale non-residential land uses including shops offices and consulting rooms will be developed in appropriate areas to support residents and the local community.

Retail development will be small in scale (and not exceed in the order of 1500 square metres in gross leasable floor area) and will primarily comprise cafes, restaurants, convenience stores and other tenancies designed to service local community requirements. Retail development will also be of a scale that supports an active mixed use environment.

Development will be of the highest architectural standard, contemporary in style and contribute positively to the quality of the public realm. Its built form will contrast with the open character of the adjacent foreshore and reserve public spaces. It will capitalise on the highly desirable location through significant scale, with built form between three and twelve stories in height. This development will demonstrate excellence in urban design. It will create design relationships between buildings at ground level and the street frontage that acknowledge and respect the existing context, ensuring that scale and the built form edge protects and enhances significant visual and movement corridors (including key vistas to the sea and views through to public spaces). Views into and out of development sites will also reinforce visual connectivity and way-finding within the policy area.

Building form and setbacks will vary to provide large-scale articulation within the streetscape. Building form will also use light and shade through articulation, eaves, verandas, canopies and balconies, to provide architectural detail, summer shade and promote greater energy efficiency. Likewise, buildings will use a balanced approach to the use of solid materials and glazing so to provide an attractive backdrop to key public spaces and streets.

The policy area is well provisioned with quality public open spaces and accessible by public transport (in the form of buses and tram). Accordingly, there is a recognised reduced need for provision of private car parking and private open space (when compared to suburban localities in other zones and policy areas). Similarly, a higher degree of overshadowing and loss of privacy is expected in the policy area given the medium-to-high density nature of development (and heights).

Basement or undercroft car parking is contemplated where site circumstances allow appropriate design and integration with the streetscape / built form. Where ventilation is required for basement car parks, vehicles should be screened and landscaped.

Roofs will be designed to be integrated into the overall façade and composition of buildings and provide enclosed places for the screening of plant and service equipment (if not provided in basements) in locations away from living areas that do not visually detract from the amenity of adjoining spaces.

Landscaping will contribute to the high quality of the adjacent public areas, open space and streetscapes. Car parking areas that are not visible from public spaces will be shared and consolidated. Commercial uses in residential developments will be restricted to those associated with the respective building function.

Public promenades will incorporate public art, which is easily identifiable and fully integrated into the public environment.

### **Precinct 3 Three Storey**

**Precinct 3 Three Storey** will be developed to comprise a mix of residential development including row dwellings, residential flat buildings and tourist accommodation, with buildings in the order of 3 storeys (or 11.5 metres) in height. Development may also include small scale non-residential uses such shops, restaurants and cafes, offices and consulting rooms at ground and first levels where site conditions permit.

### **Precinct 4 Five Storey**

Development within **Precinct 4 Five Storey** will be predominantly in the form of residential flat buildings, serviced apartments and tourist accommodation of up to 5 storeys (or 18.5 metres) in height. Development may also include small scale non-residential uses such shops, restaurants and cafes, offices and consulting rooms at ground and first levels where site conditions permit.

Development will be of the highest architectural standard and contribute positively to the public realm through establishing clearly defined space between buildings on adjoining sites and building design that incorporates articulated facades and built form elements including balconies to create light and shadow. Building design will complement the scale, proportions, siting and materials of the existing heritage places in the locality.

Development on land fronting the South Esplanade immediately adjacent **Precinct 5 Twelve Storey** may accommodate additional building height over 5 storeys to achieve a transition in scale from the taller building anticipated in Precinct 5, down to the 5 storey scale anticipated in Precinct 4, provided buildings are designed to minimise any impacts on adjoining land within Precinct 4 or adjoining residential zones.

Development directly adjoining Sturt Street should not exceed 2 storeys in height to order to achieve a transition down to low scale at the interface with the adjoining **Residential Character Zone**.

### **Precinct 5 Twelve Storey**

Development within **Precinct 5 Twelve Storey** will be predominantly in the form of residential flat buildings, serviced apartments and tourist accommodation of up to 12 storeys (or 43 metres) in height. Development may also include small scale non-residential uses such shops, restaurants and cafes, offices and consulting rooms at ground and first levels where site conditions permit.

Development will be of the highest architectural standard and contribute positively to the public realm through establishing clearly defined space between buildings, incorporating surface articulation using a balanced approach to the use of solid materials and glazed areas and adopting a building design that incorporates design elements that relate to the surrounding buildings, streetscape and public open space.

Building design will complement the scale, proportions, siting and materials of the existing heritage places in the locality.

In the section of this precinct that adjoins the **Coastal Open Space Zone**, development will be designed to provide spaces between adjacent buildings and accommodate pedestrian walkways and visual connections between the **Coastal Open Space Zone** and the developed areas to the east.

## **PRINCIPLES OF DEVELOPMENT CONTROL**

### **Land Use**

- 1 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 2 Building entrances should satisfy all of the following:
  - (a) be oriented towards the primary street
  - (b) be visible and easily identifiable from the street
  - (c) provide shelter, a sense of personal address and transitional space around the entry.
- 3 Balconies should make a positive contribution to the internal and external amenity of buildings and should:
  - (a) be functional and responsive to the environment
  - (b) be located to predominantly face north, east or west to provide solar access
  - (c) be integrated into the overall architectural form and detail of the building
  - (d) contribute to the safety and liveliness of the street by facilitating casual overlooking of public spaces
  - (e) be located adjacent to the main living areas, such as the living room, dining room or kitchen to extend the dwelling's living space
  - (f) be of a minimum depth of 2 metres in order to be functional and promote indoor/outdoor living
  - (g) be designed to provide residential flat buildings/tourist accommodation with private open space, thereby promoting the enjoyment of outdoor living
  - (h) incorporate balustrades designed to allow views and casual surveillance of the street and public open space while providing for safety and visual privacy through detailing that incorporates a



proportion of solid to transparent materials to promote a balance of privacy and casual surveillance and public interaction.

- 4 Fencing and external walls should facilitate the use of private open space abutting the street.
- 5 Building design should minimise the impact of overlooking and overshadowing on existing lower density and scale development in adjoining zones/policy areas/precincts.
- 6 To minimise building massing at the interface with directly abutting residential development outside the zone (ie where not separated by a public street or laneway), new buildings should provide a transition in height and bulk down to a two storey scale at the interface.
- 7 Private open space may be reduced by a maximum of 10 per cent if the equivalent amount of space provided the annexed portion is appended to communal open space accessible to all occupants of the development.
- 8 The incorporation of roof top gardens should only occur on multi storey buildings.
- 9 Where access to the site is available from a side, rear or private road, or via a right of way, development should not incorporate vehicle access to the Esplanade.
- 10 For development along St John's Row, within **Precinct 3 Three Storey** and/or **Precinct 5 Twelve Storey**:
  - (a) space between buildings on adjoining sites should be created or, where existing, maintained
  - (b) the walls of buildings nearest to the boundary of that road should be parallel to and setback a minimum distance of 4 metres from the boundary of that road
  - (c) building elements in excess of 3 storeys should be setback from the external walls below to create a 'podium' effect.
- 11 For development along Colley Terrace:
  - (a) building elements in excess of 3 storeys (or 11.5 metres) should be set-back from the external walls below to create a 'podium' effect
  - (b) the facade of a building or part of a building up to three-storeys should have predominantly horizontal proportions and architectural features, such as banding
  - (c) the facade of buildings should be of masonry construction with a high proportion of solids to voids
  - (d) space between buildings on adjoining sites should be created or, where existing, maintained
  - (e) the facades of buildings should be parallel to Colley Terrace and set-back at least 4 metres from Colley Terrace
  - (f) the site of a building should have a minimum frontage of 25 metres where that building has more than 5 storeys (external wall height greater than 18.5 metres) above natural ground level.
- 12 For development along Anzac Highway within **Precinct 4 Five Storey** and/or **Precinct 5 Twelve Storey**:
  - (a) the facade of a building or part of a building with an external wall height up to 11.5 metres above natural ground level should have predominantly horizontal proportions and architectural features such as banding to reinforce the horizontal emphasis

- (b) building elements in excess of 3 storeys (or 11.5 metres) should be set-back from the external walls of the lower levels to create a 'podium' effect
  - (c) buildings should be constructed close to the Anzac Highway frontage and present a continuous facade to Anzac Highway with minimal breaks between buildings on adjoining sites
  - (d) vehicular access onto Anzac Highway should be minimised.
- 13 Development in the form of a battleaxe/hammerhead configuration should not be undertaken within the policy area.
- 14 Development above 5 storeys (or 18.5 metres) in height should incorporate spaces between buildings or other design techniques that enable sunlight access and avoid wide continuous building walls.
- 15 Shops should be of a local scale and not exceed in the order of 1500 square metres gross leasable floor area.

## PRECINCT SPECIFIC PROVISIONS

Refer to the [Map Reference Tables](#) for a list of the maps that relate to the following precincts.

### Precinct 3 Three Storey

- 16 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 17 Development should not exceed an external wall height of 11.5 metres above natural ground level (excluding lift service levels and gables).

### Precinct 4 Five Storey

- 18 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 19 Development should not exceed an external wall height of 18.5 metres above natural ground level (excluding lift service levels and gables).
- 20 For development along South Esplanade:
- (a) space between buildings on adjoining sites should be created or, where existing, maintained
  - (b) the modulated form of buildings along South Esplanade should be retained by continuing the predominant, regular, building alignment with stepped facades
  - (c) immediately adjacent **Precinct 5 Twelve Storey**, additional building height above 5 storeys in height is appropriate in order to achieve a transition in scale from the taller building anticipated in **Precinct 5 Twelve Storey**, down to the 5 storey scale anticipated in **Precinct 4 Five Storey**, provided buildings are designed to minimise any impacts on adjoining land within **Precinct 4 Five Storey** or adjoining residential zones.
- 21 Development directly adjoining Sturt Street should not exceed 2 storeys in height to order to achieve a transition down to low scale at the interface with the adjoining **Residential Character Zone**.

**Precinct 5 Twelve Storey**

- 22 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 23 Development should not exceed an external wall height of 43 metres above natural ground level (excluding lift service levels and gables).
- 24 Buildings should be setback on a podium that is designed to be a maximum height of 11.5 metres above natural ground level.

## **PROCEDURAL MATTERS**

### **Complying Development**

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

### **Non-complying Development**

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of Development	Exceptions
Advertisement and/or advertising hoarding	
Amusement machine centre	
Community centre	
Crematorium	
Dairy	
Detached dwelling	Except for additions and alterations to the existing building where located within <b>Precinct 3 Three Storey</b> .
Entertainment venue	
Farming	
Fuel depot	
Group dwelling	
Horse keeping	
Horticulture	
Hospital	
Hotel	
Indoor recreation centre	
Industry	
Intensive animal keeping	
Motor repair station	
Petrol filling station	
Public service depot	
Road transport terminal	
Semi detached dwelling	Except for additions and alterations to an existing building where located within <b>Precinct 3 Three Storey</b> .
Service trade premises	
Stock sales yard	
Stock slaughter works	

Form of Development	Exceptions
Store	
Telecommunications facility	
Warehouse	
Waste reception, storage, treatment or disposal	
Wrecking yard	

### **Public Notification**

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development (except where the development is classified as non-complying) are designated:

Category 1	Category 2
Development with an overall height of 11.5 metres or less (excluding gables) measured from the natural ground level.	Development with an overall height exceeding 11.5 metres (excluding gables) measured from the natural ground level.