

## APPLICATION ON NOTIFICATION – Category 2

<b>Applicant:</b>	MAB Park Pty Ltd
<b>Development Number:</b>	100/L077/18
<b>Nature of Development:</b>	The construction of a seven level multi-deck carpark (800 car parks) and 205 external at grade car park, to be constructed over three stages
<b>Development Type:</b>	Merit
<b>Subject Land:</b>	9L MAB Circuit, Tonsley (1284 South Road, Tonsley)
<b>Development Plan:</b>	City of Marion Council Development Plan
<b>Zone / Policy Area:</b>	Regional Activity Zone
<b>Contact Officer:</b>	Sara Zuidland
<b>Phone Number:</b>	7109 7069
<b>Consultation Start Date:</b>	26 February 2019
<b>Consultation Close Date:</b>	12 March 2019
<p><b>During the notification period, hard copies of the application documentation can be viewed at the Department of Planning, Transport and Infrastructure, Level 5, 50 Flinders St, Adelaide, during normal business hours. Application documentation may also be viewed during normal business hours at the local Council office (if identified on the public notice).</b></p>	

Written representations must be received by the close date (indicated above) and can either be posted, hand-delivered or emailed to the State Commission Assessment Panel.

**Any representations received after the close date will not be considered.**

Postal Address:

The Secretary  
State Commission Assessment Panel  
GPO Box 1815  
ADELAIDE SA 5001

Street Address:

Development Division  
Department of Planning, Transport and Infrastructure  
Level 5, 50 Flinders Street  
ADELAIDE

Email Address: [scapreps@sa.gov.au](mailto:scapreps@sa.gov.au)

**South Australian  
DEVELOPMENT ACT, 1993  
REPRESENTATION ON APPLICATION – CATEGORY 2**

**Applicant:** MAB Park Pty Ltd  
**Development Number:** 100/L077/18  
**Nature of Development:** The construction of a seven level multi-deck carpark (800 car parks) and 205 external at grade car park, to be constructed over three stages.  
**Development Type:** Merit  
**Zone / Policy Area:** Regional Activity Zone  
**Subject Land:** 9L MAB Circuit, Tonsley (1284 South Road, Tonsley)  
**Contact Officer:** Sara Zuidland  
**Phone Number:** 7109 7069  
**Close Date:** 12 March 2019

My Name: \_\_\_\_\_ My phone number: \_\_\_\_\_

**Primary method(s) of contact:** Email: \_\_\_\_\_  
Postal Address: \_\_\_\_\_ Postcode: \_\_\_\_\_

**You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.**

**My interests are:**  
(please tick one)

☐ owner of local property  
☐ occupier of local property  
☐ a representative of a company/other organisation affected by the proposal  
☐ a private citizen

**The address of the property affected is:**

\_\_\_\_\_  
Postcode: \_\_\_\_\_

**My interests are:**  
(please tick one)

☐ I support the development  
☐ I support the development with some concerns  
☐ I oppose the development

**The specific aspects of the application to which I make comment on are:** \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

**I:** ☐ wish to be heard in support of my submission  
(please tick one) ☐ do not wish to be heard in support of my submission  
(Please tick one)

**By:** ☐ appearing personally  
(please tick one) ☐ being represented by the following person  
(Please tick one)

**Signature:** \_\_\_\_\_

**Date:** \_\_\_\_\_

### **Why have I received this notice?**

The role of the State Commission Assessment Panel (SCAP) is to independently assess and determine specified kinds of development applications in South Australia in accordance with the *Development Act 1993*.

Some types of development application require public notification. This is determined by the relevant Development Plan and Schedule 9 of the *Development Regulations 2008*. Development applications fall into one of the following categories:

- Category 1: No public notification
- Category 2: Notice of the application to be given to an owner/occupier of adjacent land to where the development is proposed. A person contacted in this way has the right to make a written representation to the SCAP. Representations from those with a right to be heard must be taken into consideration by SCAP when assessing the development application.
- Category 3: Written notice of the application to be given to an owner/occupier of adjacent land to where the development is proposed and to any owner/occupier of land which the SCAP believes would be directly affected to a significant degree if the development were to proceed. Notice by newspaper advertisement to be given to the general public.

### **What is a valid representation?**

Your representation must be made within the public notification period as described upon the notice you have received. Pursuant to the *Development Act 1993*, this period is 10 business days from the date notice is given.

Your representation must be signed, dated, set out the reasons for the representation and include your full name and address contact details.

### **What can I comment on?**

It is important to be mindful that your representation should avoid raising matters that are not relevant to the planning assessment of the application. A planning assessment can only have regard to the relevant provisions of the Development Plan. A representation can raise issues both in support and in opposition to a development.

You can access the relevant Development Plan here: <https://www.sa.gov.au/topics/planning-and-property/development-plans>

### **What happens next?**

All valid representations received through either a Category 2 or Category 3 process are forwarded to the applicant for a response and taken into consideration by a Planning Officer from the Department of Planning, Transport and Infrastructure in preparing their assessment.

Pursuant to the *Freedom of Information Act 1991* and *Development Act 1993* any information provided may become part of a public document and may be published as an attachment to the Planning Officer's report.

If you have indicated that you wish to be heard you will receive an invitation to appear personally before the SCAP, or be represented by counsel, solicitor or agent. This invitation must give five (5) business days notice of the meeting but, dependent on other issues to be assessed, this meeting may not occur for an indefinite period of time after your representation is made. Unfortunately, the meeting time and date cannot be adjusted to accommodate all attendees.

If you have not indicated that you wish to be heard in support of your submission, you will not receive any further correspondence on this matter until a decision is made.

### **What is a SCAP meeting?**

SCAP meetings are generally held on the second and fourth Thursdays of each month in the Kardi Munaintya meeting room on the ground floor at 50 Flinders Street, Adelaide.

The SCAP will be assessing the development application against the relevant Council Development Plan. To assist, an assessment report will be prepared by a Planning Officer from the Department of Planning, Transport and Infrastructure. This report is publicly available from [https://www.saplanningcommission.sa.gov.au/scap/agendas\\_minutes](https://www.saplanningcommission.sa.gov.au/scap/agendas_minutes) on the Monday afternoon prior to the meeting. This report will include a copy of your representation.

Representors wishing to be heard will be given the opportunity to make a short (5 minute maximum) verbal presentation to the SCAP. Please note that Representors are only provided with the opportunity to make a verbal presentation at the initial hearing of an application. At this meeting, the SCAP may also hear comments from the applicant, relevant agencies, and Council.

### **How do I know what decision is made?**

You will be able to ascertain the outcome of the SCAP's deliberation when the meeting minutes are made available on the SCAP website on the afternoon of the day after a meeting.

Once a decision is made by the SCAP, valid representors will be sent a copy of the Decision Notification Form which includes any conditions relevant to the application.

*Note: Dependent on the assessment process for the application, and if no Representors indicate that they wish to be heard, a decision may be made by a Delegate of the SCAP without the application being heard at a SCAP meeting.*

### **Appeal rights**

If the proposal is a Category 3 application, then you can appeal a decision made by the SCAP if you have made a valid representation

Such an appeal must be lodged at the Environment, Resources and Development Court fifteen (15) business days from the date of decision. The Court is located in the Sir Samuel Way Building, Victoria Square, Adelaide (telephone number 8204 0300).

Representors do not have a right of appeal in relation to Category 2 development applications.

**For more information**

Contact the SCAP Secretariat on:

Telephone: 1800 752 664 (Select Option 4)

Direct: 7109 7061

E-mail: [scapadmin@sa.gov.au](mailto:scapadmin@sa.gov.au)

Postal: GPO Box 1815, Adelaide SA 5001

Street: Level 5, 50 Flinders Street, Adelaide SA 5000

Website: <https://www.saplanningcommission.sa.gov.au/scap>

# Development Application Form

## Completing this application

All sections must be completed, any missing sections may cause delays or Council will be unable to process the lodgement. It is recommended this form be completed online and printed prior to lodgement.

Print clearly using BLOCK LETTERS and place a TICK in appropriate boxes.



Development Application Number 100/ 1700 (this field is to be completed by Council)

### Section 1 – Correspondence Method please check "X" only one of the following boxes.

☐ I Accept all future correspondence by email OR

☒ I choose only to receive general assessment correspondence via email but to receive stamped Plans and Decision Notification Forms by hardcopy mail.

Send all future correspondence to the following e-mail address:

manny@technebuild.com.au

### Section 2 – Consent Sought

Select the type of consent you wish to apply for (please tick one box only):

☒ Development Plan Consent (Planning Consent Only)

☐ Building Rules Consent (Building Consent Only)

☐ Development Approval (Planning & Building)

☐ Schedule 1A \* development that requires Building Rules Consent only

\*Note – If you are unsure what type of consent is needed telephone Customer Service on (08) 8375 6685 or visit Council's website

CITY OF MARION  
DEVELOPMENT SERVICES

13 SEP 2013

RECEIVED

### Section 3 – Location of Proposed Development

Part of 1284	Allotment C39		6176 / 592
Street Number	Lot Number	Deposited/File/Strata Plan No	Ct Volume / Folio
South Road	Tonsley	SA	5042
Street Name	Suburb	State	Postcode

### Section 4 – Applicant Details \*All correspondence will be sent to the Applicant

Given Name	MAB Park Pty	Surname	
* Initials not accepted.			
Postal Address			
GPO BOX 1426	Adelaide	SA	5001
Street No.	Street Name	Suburb	State Postcode
Email	manny@technebuild.com.au	Mobile	0412 819 998 Phone

Acting on behalf of the Owner Yes ☐ No ☐

### Section 5 – Owner's Details of the Subject Land \*If same as Applicant, leave blank and move to Section 6

Given Name	MAB Park Pty Ltd	Surname	
* Initials not accepted.			
Postal Address			
GPO BOX 1426	Adelaide	SA	5001
Street No.	Street Name	Suburb	State Postcode
Email	manny@technebuild.com.au	Mobile	0412 819 998 Phone

### Section 6 – Further Information Contact \*Please note this section is to be completed if contact person is not the Applicant

Given Name	EMANUEL	Surname	PALYARIS
* Initials not accepted.			
Email	manny@technebuild.com.au	Mobile	0412 819 998 Phone

**Section 7 – Builders Details**

\*Note – Must be completed by the Applicant for Building Rules Consent and/or Development Approval if being sought otherwise leave blank

☐ Owner Builder OR ☒ Builder's Name (please complete below)

<b>Given Name</b>	Techne Build Pty Ltd	<b>Surname</b>	
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\* Initials not accepted.

**Postal Address**

	GPO Box 1426	Adelaide	SA	5001
<b>Street No.</b>	<b>Street Name</b>	<b>Suburb</b>	<b>State</b>	<b>Postcode</b>

<b>Email</b>	manny@technebuild.com.au	<b>Phone</b>		<b>Lic.No.</b>	BLD 269203
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**Section 8 – Description of Development & Associated Details**

**Description of Development** (example single-storey detached dwelling, domestic garage, office, tree removal)

Multi Deck Carpark - To be undertaken in 3 stages	<b>CITY OF MARION</b> DEVELOPMENT SERVICES 13 SEP 2018 RECEIVED	
<b>Existing use of the land</b>		Vacant Land

**Does the proposal affect a regulated or significant tree** No ☒ Yes ☐ (refer Page 5)

\*A regulated or significant tree may be on the adjoining land that may be affected (including damage to tree roots) by the proposed development. If unsure of what a regulated or significant tree is, contact Customer Service on 8375 6600. Please complete Page 5.

- ▲ Has a current copy of the Certificate of Title Been provided? Yes ☐ No ☐  
 If not, do you want Council to purchase a copy on your behalf? (additional fee) Yes ☐ No ☐ - Will provide own copy
  - ▲ Is/will there a brush fence within 3 metres of the proposed building work? Yes ☐ No ☒
  - ▲ Are there any easements on the Land? Yes ☒ No ☐
  - ▲ Is the site connected to Sewer (SA Water)? Yes ☒ No ☐ Septic System ☐
  - ▲ Has a CITB Levy Receipt Levy been provided (\$40,000 and above) Receipt No.
  - ▲ Has the Power Clearance Declaration Been provided (refer page 4)? Yes ☒
  - ▲ Has a copy of the Indemnity Insurance been provided? Yes ☐ No ☐ Not Required ☒
- (Required if development cost is \$12,000 or more, domestic construction is proposed and if Building Consent/Full Development Approval is sought)

\*Please note, assessment of the proposed will not occur until all the above information is provided.

**Section 9 – Development Cost & Floor Area** \*Council may require written confirmation to verify costs

<b>Total Cost of Proposal</b>	\$17,860,357 (All stages)	<b>Estimate Floor Area of work</b>	25,022	m <sup>2</sup>
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(inclusive GST, not including furnishing costs) - A Development Cost must be provided.

**Section 10 – Building Classification**

<b>Current Classification</b>		<b>Classification Sought</b>	
<b>If Class 5, 6, 7, 8 or 9 state number of employees</b>		<b>Male</b>	<b>Female</b>

**Section 11 - Declaration**

Council is required by the Development Act 1993 and the Regulations made under it to put on public display all documents and information lodged as part of an application for Category 2 or 3 developments and the public have the right (Regulation 34) to obtain copies of that material. You should assume any documents or information you lodge as part of an application which may be categorised in that way, will become public for all purposes. If you have any concerns over the confidentiality or security content of such documents or information, you should discuss these with a member of Council's Development & Regulatory staff prior to lodgement. If another person claims copyright on any material you lodge, you must obtain and provide to Council the express authority of that person for the display and copying of that material.

I declare the information that I have provided on this application form is correct to the best of my knowledge and that I have the authority of any copyright holder for the public display and copying of any material I lodge.

If you are using Council's Electronic Lodgement System the form does not need to be signed.

<b>SIGNATURE</b>		<b>DATE</b>	11/9/18
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☒ Applicant ☐ Owner ☐ Authorised Agent

# Building Safely Near Powerlines

## Form of Declaration

Pursuant to Schedule 5 Clause 2A(1) of the Development Regulations 2008



TO CITY OF MARION - Development Services - Administration Staff  
PO BOX 21, OAKLANDS PARK SA 5046

### FROM

Given Name	MAB Park Pty Ltd	Surname	
Postal Address			
GPO Box 1426	Adelaide	SA	5001
Street No.	Street Name	Suburb	State Postcode
Email	manny@technebuild.com.au	Mobile	0412 819 998 Phone

Date of Application 11/9/18

### LOCATION OF PROPOSED DEVELOPMENT

Part of 1284	Allotment C39	6176 / 592
Street Number	Lot Number	Deposited/File/Strata Plan No Ct Volume / Folio
South Road	Tonsley	SA 5042
Street Name	Suburb	State Postcode

### DESCRIPTION OF DEVELOPMENT (example single-storey detached dwelling, domestic garage, office, tree removal)

Multi Deck Carpark



I (insert full name) Emanuel Palyaris

being the applicant / a person acting on behalf of the applicant (~~delete the inapplicable statement~~) for the development described above declare that the proposed development will involve the construction of a building which would, if constructed in accordance with the plans submitted, not be contrary to the regulations prescribed for the purposes of section 86 of the Electricity Act 1996. I make this declaration under Clause 2A (1) of Schedule 5 of the Development Regulations 2008.

If you are using Council's Electronic Lodgement System the form does not need to be signed.

SIGNATURE

DATE 11/9/18

**Note 1 - This declaration is only relevant to those development applications seeking authorisation for a form of development that involves the construction of a building** (there is a definition of 'building' contained in Section 4 (1) of the Development Act 1993), other than where the development is limited to -

- An internal alteration of a building; or
- An alteration to the walls of a building but not so as to alter the shape of the building.

**Note 2 -** The requirements of section 86 of the Electricity Act 1996 do not apply in relation to:

- an aerial line and a fence, sign or notice that is less than 2.0m in height and is not designed for a person to stand on; or
- a service line installed specifically to supply electricity to the building or structure by the operator of the transmission or distribution network from which the electricity is being supplied.

**Note 3 -** Minimum safe clearance distances between buildings or structures and powerlines are set out in the Electricity (General) Regulations 2012. These distances are legal requirements and must be maintained at all times.

**Note 4 -** The majority of applications will not have any powerline issues, as normal residential setbacks often cause the building to comply with the prescribed powerline clearance distances. Buildings/renovations located far away from powerlines, for example towards the back of properties, will usually also comply.

**Note 5 -** Particular care needs to be taken where high voltage powerlines exist; or where the development:

- is on a major road;
- Commercial/Industrial in nature; or
- Built to the property boundary.

**Note 6 -** Further information on powerlines and their sag and swing is available from SA Power Networks, the Office of the Technical Regulator or by visiting [sa.gov.au/energysafe](http://sa.gov.au/energysafe). Further information on working safely near overhead powerlines can be obtained from <https://www.sa.gov.au/topics/energy-and-environment/electrical-gas-and-plumbing-safety-and-technical-regulation/powerline-safety/working-safely-near-overhead-powerlines>

**Note 7 -** In cases where applicants have obtained a written approval from the Technical Regulator to build the development specified above in its current form within the prescribed clearance distances, the applicant is able to sign the form.

# Development Application Fees

(Financial year 2018/2019)

Fees are applied as per Schedule 6 of the Development Regulations 2008 (except where non statutory).



DESCRIPTION	FEE (inclusive of GST)	AMOUNT DUE
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## DEVELOPMENT PLAN CONSENT

Lodgement Fee OR Lodgement Fee (if assessment against Building Rules is required and development cost is > \$5,000, does not apply to applications consisting <u>solely</u> of a swimming pool).	\$64.00 (GST exempt) OR \$136.00 (GST exempt)
Swimming Pool Audit Inspection Fee	\$190.00 (GST exempt)
Development Authorisation (staged Consents) Fee	\$64.00 (GST exempt)
Planning Assessment Fee:	
• Development cost up to \$10 000	\$39.75 (GST exempt)
• Development cost > \$10 000 - \$100 000	\$109.00 (GST exempt)
• Development cost over \$100 000	Development cost x 0.125% (GST exempt)
Schedule 1A Application	\$53.00

The following fees may be payable, but will be determined following a preliminary assessment.

You will be advised in due course if these additional fees are required.

Referrals to government agencies (e.g Department for Transport, Planning & Infrastructure, EPA, Department for Environment etc)	\$227.00 per agency OR \$379.00 (If development cost exceeds 1 million)
Category 2 & 3 public notification	\$109.00 (GST exempt)
Category 3 advertisement fee	\$766.00

## BUILDING RULES CONSENT

Building Assessment Fee	Calculate fee using the area of the proposed building (in m <sup>2</sup> ) X the applicable rate. Note - a minimum fee of \$68.00 applies for all applications for Building Consent:		
	Construction	Reclassification	
Class 1, 2 & 4 (i.e single dwelling, units/flats)	\$3.08 x m <sup>2</sup>	\$2.39	x m <sup>2</sup>
Class 3, 5 & 6 (residential part of school or hotel, office, shop/restaurant)	\$4.10 x m <sup>2</sup>	\$3.19	x m <sup>2</sup>
Class 7 & 8 (warehouse, carpark, workshop/factory)	\$2.71 x m <sup>2</sup>	\$2.11	x m <sup>2</sup>
Class 9a & 9c (public building – healthcare eg hospital or laboratory)	\$4.65 x m <sup>2</sup>	\$3.62	x m <sup>2</sup>
Class 9b (public building – assembly eg church, school or community hall)	\$4.08 x m <sup>2</sup>	\$3.18	x m <sup>2</sup>
Class 10 (carport, verandah, garage, pool, fence, sign)	\$0.92 x m <sup>2</sup>	\$0.72	x m <sup>2</sup>
Demolition	Class 1, 2 and 4 - \$0.62 x m <sup>2</sup> Other Classes – relevant construction rate above x 0.2		
Certificate of Occupancy (Class 2-9)	\$45.75		
Building Rules Modification	\$159.00		
Essential Safety Provisions Schedule	\$98.00		
Certificate of Title Search	\$51.25		
(A copy of the current CT is required for all applications. council can purchase one on your behalf directly from the Land Titles Office for the scheduled fee)			
Application to extend any consent or Approval under Regulation 48	\$102.00		

**TOTAL**

**\$0.00**

7<sup>th</sup> August, 2018

Mr Mark Williams  
State Coordinator-General  
GPO Box 2343  
ADELAIDE SA 5000

CITY OF MARION  
DEVELOPMENT SERVICES

13 SEP 2018

RECEIVED

Dear Mark

**RE: PROPOSED MULTI DECK CARPARK / TONSLEY PRECINCT**

I act on behalf of MAB Tonsley Holdings Pty Ltd & MAB Park Pty Ltd in which I am also a Director.

We formally request consideration of our proposal of a multi deck carpark to be Developed in the Tonsley Precinct at Clovelly Park.

The land is currently is described in certificate of title volume 6176 Folio 592 being Allotment C39 when created. The site area is 10,058m<sup>2</sup>.

The proposed carpark development consists of 807 under cover carparks over 7 levels & 191 external at grade carparks. The building is predominately constructed in steel, concrete, precast walls & perforated metal screening.

We propose to develop the site in Stages. This may change subject to demand.

We currently have the site under contract from the Urban Renewal Authority with settlement later this year.

Please feel free to contact me for any queries or additional information on 0412 819 998 or [manny@technebuild.com.au](mailto:manny@technebuild.com.au)

Yours sincerely



Emanuel Palyaris  
Director

# DEVELOPMENT COSTS

<b>STAGE 1</b>	Parks	Area	Value (exc GST)
External Carparks at Grade	191		\$1,710,689
<b>STAGE 2</b>			
Multi-deck Ground, 1 <sup>st</sup> & 2 <sup>nd</sup> floors	351	10,920.95	\$6,318,000
<b>STAGE 3</b>			
Multi-deck 3 <sup>rd</sup> , 4 <sup>th</sup> , 5 <sup>th</sup> , 6 <sup>th</sup>	456	14,101.60	\$8,208,000
Total Carparks	998		\$16,236,689



**From:** [Luci Ward](#)  
**To:** [Zuidland, Sara \(DPTI\)](#)  
**Cc:** [Anthony Gatti](#); [John Koutsoukou \(wpd@tpg.com.au\)](#); [Emanuel Palyaris](#); [Anthony Gatti](#)  
**Subject:** RE: Tonsley Multi-Deck Car Park – DA 100/L077/18 - RFI Response  
**Date:** Wednesday, 9 January 2019 12:00:12 PM  
**Attachments:** [image001.png](#)

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Hi Sara,

Further to my email below, it has come to my attention that the cover letter and traffic impact assessment incorrectly site the total number of car parking spaces in the proposed development as, 1,020. The correct number is sited on the parking schedule on the submitted architectural plans as a total of 1,005 car parking spaces: 800 within the multi-deck facility and 205 at-grade car parking spaces.

Therefore the revised scheme offers an additional 7 car parking spaces to that previously submitted.

The car parking numbers are inconsequential to the parking advice provided, however, please advise as to whether you require this report and cover letter amended and resubmitted in order to finalise your assessment or whether the advice in this email suffices.

Kind Regards,

**LUCI WARD**  
PLANNING ADVISOR  
(in office Monday, Wednesday and Thursday)

**INTRO**  
Activating human space

E [L.WARD@INTRO.COM.CO](mailto:L.WARD@INTRO.COM.CO)

T +61 (0)8 8410 0453  
M +61 406 603 123

INTRO ARCHITECTURE PTY LTD | LEVEL 11 | 44 WAYMOUTH STREET  
PO BOX 207 RUNDLE MALL PO | ADELAIDE | SA 5000

[INTRO.COM.CO](http://INTRO.COM.CO)

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**From:** Luci Ward  
**Sent:** Friday, 21 December 2018 12:51 PM  
**To:** [sara.zuidland@sa.gov.au](mailto:sara.zuidland@sa.gov.au)  
**Cc:** [Anthony Gatti <a.gatti@intro.com.co>](mailto:a.gatti@intro.com.co); [John Koutsoukou \(wpd@tpg.com.au\) <wpd@tpg.com.au>](mailto:wpd@tpg.com.au); [Emanuel Palyaris <manny@technebuild.com.au>](mailto:manny@technebuild.com.au)  
**Subject:** Tonsley Multi-Deck Car Park – DA 100/L077/18 - RFI Response

Hi Sara,

Please find a link below to the response to the request for further information for the

abovementioned development application – the Tonsley Multi-Deck Car Park.

<https://www.dropbox.com/sh/u1rnt35jb466kef/AACaQt4UAa6Jd-Ona59RH4TAa?dl=0>

Please don't hesitate to get in touch should you require any further clarification.

Merry Christmas and have a great break,

**LUCI WARD**

PLANNING ADVISOR

(in office Monday, Wednesday and Thursday)

# INTRO

Activating human space



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**INTRO ARCHITECTURE PTY LTD** | LEVEL 11 | 44 WAYMOUTH STREET  
PO BOX 207 RUNDLE MALL PO | ADELAIDE | SA 5000

[INTRO.COM.CO](http://INTRO.COM.CO)

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# INTRO

20 December 2018

Sara Zuidland  
Para Planner  
Planning & Development | Development Division  
Department of Planning Transport and Infrastructure

Via email: sara.zuidland@sa.gov.au

Intro Design Pty Ltd  
L11 44 Waymouth Street  
PO Box 207 Rundle Mall  
Adelaide SA 5000

T +61 (0)8 8410 0453  
info@intro.com.co

intro.com.co

Dear Sara,

**RE: Tonsley Multi-Deck Car Park – DA 100/L077/18  
Further Information**

Intro provide a response to the request for information received with respect to the proposed development on behalf of MAB Tonsley Holdings Pty Ltd & MAB Park Pty Ltd, the proponent.

This correspondence has been prepared in response to requests for further information from the Commissioner of Highways, the City of Marion and the Planning and Development Division in DPTI.

The following specialists have been engaged to inform the response to representations and refined plans:

Façade Architecture:	Enzo Caroscio Architecture + Design
Architecture and Engineering:	Tesseract International
Traffic and Car Parking:	GTA Consultants

The subject site is located on the corner of MAB Circuit and Charger Road within the Commercial Area of the Tonsley Regional Activity Zone. The site abuts the Core Area to the south. Across MAB Circuit 80 metres to the south is Flinders University's Tonsley Campus which will expand toward the site in the near future. The MAB comprises public open space and is to form the central community hub of Tonsley. A number of commercial multi-storey developments, a medical facility and a hotel are also earmarked for development within the immediate locality.

The subject development will provide centralised car parking for the precinct to free up further ground floor area within the precinct for active uses in preference to dispersed at-grade or undercroft car parking throughout.

I provide a response to the request for information (RFI) matters raised within your email dated, 16 November 2018 including comments from Marion Council in their referral capacity dated 8 November 2018. I respond to each item with reference to the relevant Marion Council Development Plan (Consolidated 20 February 2018) below.

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## CHANGES TO THE DEVELOPMENT

A number of amendments have been made to the plans in line with traffic engineering advice. The changes are depicted in the revised plans that form Attachment 01 to this correspondence. The revised proposed development includes the construction of a 6-level multi-deck car park with at-grade parking, providing an overall parking provision of 1,020 car parking spaces. 204 spaces are proposed within the at-grade parking (excluding multi-deck ground level) and 816 spaces are proposed within the multi-deck parking structure.

This results 22 car parking spaces in addition to that which was proposed in the plans initially lodged.

The ground floor/site layout proposed depicts a relocated car park access point further west along the southern boundary and now outside of the prohibited zone to ensure safe access and egress to the site.

The materials and finishes proposed for the façade remain unchanged.

## ITEM 01: LAND USE

Marion Council's referral response states that:

*"parking facilities are only explicitly listed as an envisaged primary use within the Core Area (Laffer's Triangle)."*

According to Zone PDC 1, is a parking facility a type of development desired to occur within the Zone overall, PDC 2(b) of the Regional Activity Zone also specifically envisages parking facilities within the Core Area (Tonsley).

The subject site is adjacent to the Core Area (Tonsley) and is well positioned to conveniently cater to a broad range of workers and visitors within the precinct. There is established demand for car parking within the precinct and the development of a multi-level facility as this location aligns with Renewal SA's masterplan for Tonsley and will avail additional land within the core area for higher value development and activated land uses.

## ITEM 02: EASEMENTS

- *A City of Marion easement runs through the subject land. Council's Development Engineer has reviewed the proposal and advised the supporting structure column and piers adjacent the easement should be, as a minimum, below stormwater pipe infrastructure. Additionally, it is sought that a City of Marion Indemnity from the developer and supporting insurance against any damage to the Council pipe caused by the building works is provided. This should also be supported by CCTV during construction.*

The proponent agrees to a condition being placed on the Development Plan Consent as requested by Marion Council with regards to the protection of their stormwater infrastructure.

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As noted in the attached plans PL02 and PL03, there are to be no footings over the easement. The supporting structure column and piers adjacent the easement are to be, as a minimum, below the depth of the stormwater pipe infrastructure.

## ITEM 03: TRAFFIC + ACCESS

The proposed site access off MAB Circuit aligns with Renewal SA's approved access plans.

As requested, GTA consultants were engaged to undertake a Site Layout Assessment of the subject car park. Their Assessment forms Attachment 02. This process resulted in a number of changes being made to the proposed car park. The attached revised Architectural Plans in Attachment 01, reflect these key amendments:

- minor changes to the car parking layout to preclude columns from parking design envelopes and swept paths;
  - relocation of the access and egress to outside of the prohibited zone and the provision of 4 access lanes with an entry queue capacity of 4 vehicles per lane to prevent undue disruption to traffic flows along MAB Circuit;
  - the provision of 22 additional car parking spaces; and
  - the provision of 28 bicycle parking spaces at the ground level.
- 
- *Justification for access via a primary access route and traffic impacts along MAB Circuit, particularly as a result of queuing*

GTA Consultants advise that the development will not present any unacceptable traffic impacts on MAB Circuit for the following reasons:

- typically, with most gated access control system car parks the customer interface with the equipment is between 3-6 seconds. Most of the Adelaide CBD car parks have queuing space for approximately 3-4 vehicles per lane. At peak times it is rare for customer vehicles to spill onto roads. This includes car parks with single entries that facilitate upwards of 900 vehicle movements per day;
- the demographic of the Tonsley Site means that usage will be more evenly spread across the day. (i.e. 9-5 works in addition to students who will rotate throughout the day). This will reduce the peak entry and exit pressures as demand is spread across a greater period.
- the planned operation of the car park involves a smart technology. At this stage there will be no need for customers to interface with car park equipment whilst egressing in or out of the site. This will further reduce queuing (particularly on entry) to the site;
- as depicted in Concepts Plan Map Mar/8 of the Development Plan, the pedestrian and cycle link on MAB Circuit aligns itself along the

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southern or MAB side of the circuit and as such, the vehicular access and egress will not present unreasonable conflicts with pedestrian or cyclist activity.

- *The proposal will, overall, result in an increase in traffic movements throughout the local road network*

GTA advise that the site is expected to experience up to 610 vehicle movements in the peak hour. Given the nature of the proposed development as a car parking facility, the proposed development (excluding future building) is not anticipated to generate any additional traffic movements to the precinct that would not have been forecast as part of the broader site planning and masterplan.

- *Provide a traffic management study, vehicle manoeuvring profiles, access ramp gradients and clearances to ascertain that the layout conforms with Australian design standards*

GTA's Site Layout Assessment confirms that the proposed parking layout is generally consistent with the dimensional requirements as set out in the Australian/New Zealand Standards for Off Street Car Parking (AS/NZS2890.1:2004 and AS/NZS2890.6:2009). The required changes identified within GTA's Site Layout Assessment can and will be reviewed within detailed design phase of the project.

- *Council suggests that a condition of approval be attached that the car park's management (including parking fees) be coordinated with the "Precinct Parking Plan" which is a recommendation by the FTPITS*

The proponent agrees to a condition being attached regarding to the management of the car park's management pending further details of such a condition be provided.

## ITEM 04: CRIME PREVENTION THROUGH ENVIRONMENTAL DESIGN + OPERATIONS

- *Response to CW Crime Prevention PDC 5*

### COUNCIL WIDE CRIME PREVENTION

**PDC 5**                      **Development, including car park facilities should incorporate signage and lighting that indicate the entrances and pathways to, from and within sites.**

The main entrance to the lift core is clearly legible from the street front by way of the pre-cast concrete column in contrast to the perforated steel mesh skin surrounding the remainder of the façade. Pedestrian paths link the lift foyer with the public footpath and clear lines of sight are retained along those paths with low-lying planting flanking the paths. There is clear flow within the car parking areas and areas of entrapment are avoided.

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Landscaping incorporates planting with tree canopies raised above head height and with an understorey of low-lying rushes, grasses and ground covers near footpaths to avoid opportunities for concealment.

Lighting will be provided throughout the car parking area and adjacent paths in line with Tonsley Design Guidelines and in line with Australian Standards. A lighting plan will be developed during the detailed design phase and will consider energy efficient luminaires. The provision of the lighting plan can form a condition of Development Plan Consent.

Internal wayfinding signage will indicate the lift locations to those inside the upper car park levels and other signage as required to comply with the National Construction Code.

Should additional signage be required, it will form part of a separate development application.

The possibility can be explored for the blank eastern façade to be painted with a mural that references the former creek line running through the site of historic and/or burgeoning culture of Tonsley and will deter vandalism to this concrete face. This mural can be commissioned by the proponent in consultation with Renewal SA in line with the protocols prepared for procurement of public art at Tonsley. Further, the mural can be washed with light outside of daylight hours to remain a feature.

## ITEM 05: PRESENCE TO THE URBAN REALM + DESIGN STRATEGY

- *The proposal is not considered to result in a high-quality design and does not incorporate an appearance that responds to and reinforces aspects of the local environment and building form.*

*Council considers it necessary that further consideration to a higher quality design, especially for elements of the built form which will be highly visible from the public realm, is required. This, in Council's opinion can be achieved through the incorporation of a greater range of materials and colours, articulation to the façade and use of different / innovative design materials.*

*Given no development application has been lodged for adjacent land to the east, the built form will present significant uninterrupted walling facing the public realm and result in considerable visual impacts. Whilst Council accepts the built form can abut the site boundary, further design elements should be integrated into this façade to reduce the bulk, scale and create additional visual interest to what is otherwise an extensive area of uninterrupted walling.*

## REGIONAL ACTIVITY ZONE DESIRED CHARACTER

..... Development will be of high-quality urban design and will contribute to the provision of a coherent public realm by framing streets. Buildings at the interface of the zone will create an

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appropriate transition of development in terms of design, scale, massing and intensity of land use

Overlooking, overshadowing and noise impacts will be moderated through good design and noise attenuation techniques, transition of building heights, design and siting of windows and balconies, and use of landscaping. Buildings will also be designed to maximise solar access.

Building entrances will be oriented to the street, large parking areas will be minimised and on-site parking areas will be located behind buildings. Landscaping will unify the public realm and encourage outdoor activities and support ecological biodiversity.

Sharing of facilities, including communal open space, parking areas and access ways, is encouraged.

In response to feedback from the Government Architect, the proponent engaged Enzo Caroscio to work with Tesseract to further develop the façade design for the car park structure.

The architectural intent is to create a translucent sculptural form in the streetscape in polite conversation with MAB that Enzo Caruscio conceived of in his time at Woods Bagot. The proposed car park building wears a veil of perforated mesh with expressed frames referencing the skin of the MAB which employs a similar structural approach to mounting the polycarbonate skin of its parapet.

The proposed form's mass has been broken down into bands of perforated, powder coated steel mesh panels with expressed frames rhythmically wrapping the skin of the car park. The bands decreasing in detail as the building ascends. The horizontality bestowed by the panels is countered by carefully proportioned and expressed precast concrete cores in play with both horizontal and vertical open negative spaces.

The palette of materials was kept unapologetically minimal in order to harmonise with the MAB's industrial aesthetic and forms a natural utilitarian element in its own right.

By contrast, the applicant is investigating treatment to the eastern façade to enliven it with a public art mural commissioned by the applicant in consultation with Renewal SA and the City of Marion. This would be a prominent feature to vehicles entering the precinct until such a time as the adjacent site is developed.

The minimalistic architectural approach to the façade design is supported by Renewal SA as evidenced in their letter that forms Attachment 03 of this correspondence.

## ITEM 06: LANDSCAPING AND WATER SENSITIVE URBAN DESIGN

- *Landscaping plan including species types*

### REGIONAL ACTIVITY ZONE DESIRED CHARACTER

... A high quality public realm will be incorporated with a pedestrian and cycle network that delivers a cohesive, amenable and safe pedestrian environment through landscaping, surface treatments, street furniture, wayfinding and public art and building design. Colonnades, courtyards, awnings and street furniture will be encouraged, together with wide footpaths and

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street trees to shade the footpath and soften the built form. Consistency in the use of these elements will visually connect different areas within the zone....

....Water Sensitive Urban Design systems, including the harvest, treatment, storage and reuse of storm water, will be integrated throughout the zone. Harvested storm water will improve the aesthetic and functional value of open spaces, including public access ways and greenways.....

..... The integration of vegetation and water into the design of developments is encouraged to reduce the urban heat island effect, including landscaping (particularly if actively or passively irrigated), living architecture (green roofs and walls), and water bodies or features....

The Landscaping Plan continues the consistency of planting establishing across the precinct. Deciduous trees are dispersed throughout the at grade car park area offering shade to parked cars and underplanted with local native groundcovers, grasses, rushes and shrubs. The proposed landscaping will provide a human scale and soft green counterpoint to the otherwise crisp building and exposed at grade car park.

All landscaping on site will be actively irrigated through subsurface drip irrigation attached to an automatic watering system which will mitigate the urban heat island effect in the summer months.

The suggestion for vertical landscaping is duly noted, however, the maintenance and water consumption required for such an initiative in this exposed environment are considered cost prohibitive for a car parking operation and will also dilute the minimalist design intent for the building.

## ITEM 07: WATER SENSITIVE URBAN DESIGN AND STORMWATER MANAGEMENT

- *Water sensitive Urban Design systems, including the harvest, treatment, storage and reuse of stormwater*

### REGIONAL ACTIVITY ZONE DESIRED CHARACTER

....Water Sensitive Urban Design systems, including the harvest, treatment, storage and reuse of storm water, will be integrated throughout the zone. Harvested storm water will improve the aesthetic and functional value of open spaces, including public access ways and greenways.....

In line with advice from Council's Civil Engineering Designer, we ask that it is conditioned that a comprehensive Engineered Site Drainage Plan be designed and provided to Council's reasonable satisfaction. As the subject application will not result in an increase in sealed surface on the subject site, it is thought that this can comfortably be dealt with during the detailed design phase.

## ITEM 08: SIGNAGE

- *Size and colouring of any signage proposed*

Car park directional signage will be designed in line with Australian Standards and will be defined in the detailed design phase.

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Any additional signage to the façade will form the subject of a separate development application.

## ITEM 09: 'FUTURE BUILDING' ON THE SOUTH WEST OF SITE

- *Confirmation of what the 'future building' shown on the site plan is likely to be*

The area marked 'Future Building' on the site plan provides the indicative footprint for a subsequent commercial development to be sited on the south western corner of the site. This building is proposed to be commercial in use and is to comply with the Tonsley Master Plan and design guidelines. This building will form the subject of a future development application.

## CONCLUSION

Changes to the plans have resulted in a safe, convenient and efficient parking facility that will support the commercial, institutional, medical and residential land uses proposed within the precinct. The building effectively results in the addition of a balanced, transparent and minimalist object in-keeping within the MAB design language. The simple form can, in the short-term be invigorated by a mural or other community art display to the eastern façade that supports the identity of Tonsley. The landscaping proposed will complement landscaping throughout the precinct.

I trust that the response is sufficient for you to finalise your assessment. Should you require further information, please do not hesitate to contact the undersigned on 0406 603 123.

Yours sincerely



Luci Ward  
**Planning Advisor**

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## APPENDIX 01 – PROPOSAL PLANS

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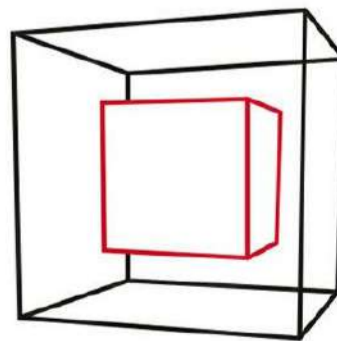
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# MULTI STOREY CARPARK WITH ASSOCIATED LANDSCAPING AND CARPARKING FOR TECHNEBUILD

TONSLEY CARPARK, CLOVELLY  
PARK  
SOUTH AUSTRALIA  
PROJECT: 17-205

ARCHITECTURAL DOCUMENTATION		
SHT NO.	DRAWING LIST	
PL00	COVER SHEET	
PL01	EXISTING SITE PLAN / LOCALITY PLAN	1:250
PL02	PROPOSED SITE PLAN	1:250
PL03	PROPOSED LANDSCAPE PLAN	
PL04	TYPICAL FLOOR PLAN LEVEL 1 & 2	1:200
PL05	TYPICAL FLOOR PLAN LEVEL 3, 4, 5	1:200
PL06	PARKING LEVEL 6	1:200
PL07	ELEVATIONS - SHEET 1 OF 2	1:200
PL08	ELEVATIONS - SHEET 2 OF 2	1:200
PL09	3D PERSPECTIVE - SHEET 1 OF 3	
PL10	3D PERSPECTIVE - SHEET 2 OF 3	
PL11	3D PERSPECTIVE - SHEET 3 OF 3	



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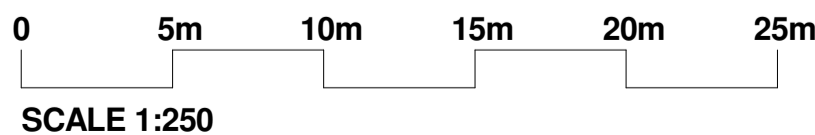
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## EXISTING SITE PLAN

1 : 250 @ A1

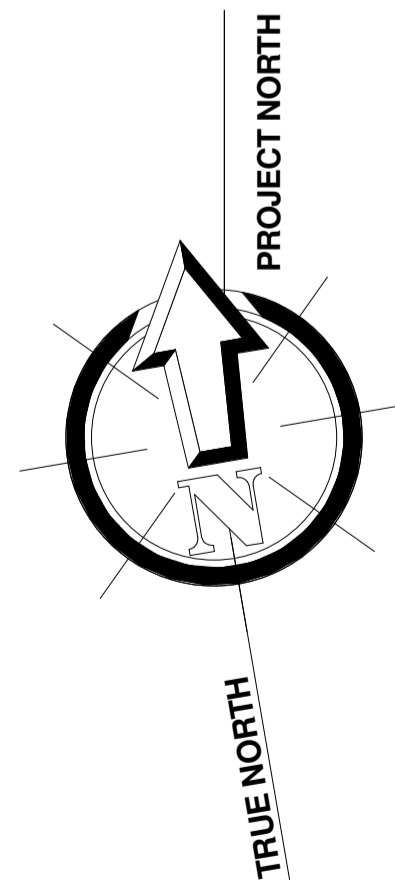


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## LOCALITY PLAN

1 : 5000 @ A1



### DEVELOPMENT AREA SCHEDULE

AREA	SQM
EXISTING SITE TOTAL	10058.15m <sup>2</sup>
ALLOTMENT 1 AREA	6264.80m <sup>2</sup>
ALLOTMENT 2 AREA	3793.36m <sup>2</sup>
<b>BUILDING</b>	
GROUND FLOOR	3416.00m <sup>2</sup>
LEVEL 1	3564.40m <sup>2</sup>
LEVEL 2	3564.40m <sup>2</sup>
LEVEL 3	3564.40m <sup>2</sup>
LEVEL 4	3564.40m <sup>2</sup>
LEVEL 5	3564.40m <sup>2</sup>
LEVEL 6	3564.40m <sup>2</sup>
TOTAL BUILDING	24802.4m <sup>2</sup>
<b>LANDSCAPING</b>	
PROPOSED LANDSCAPING	773.4m <sup>2</sup>
<b>PAVEMENT</b>	
CONCRETE PAVEMENT	770.300m <sup>2</sup>
BITUMEN PAVEMENT	5981.8m <sup>2</sup>

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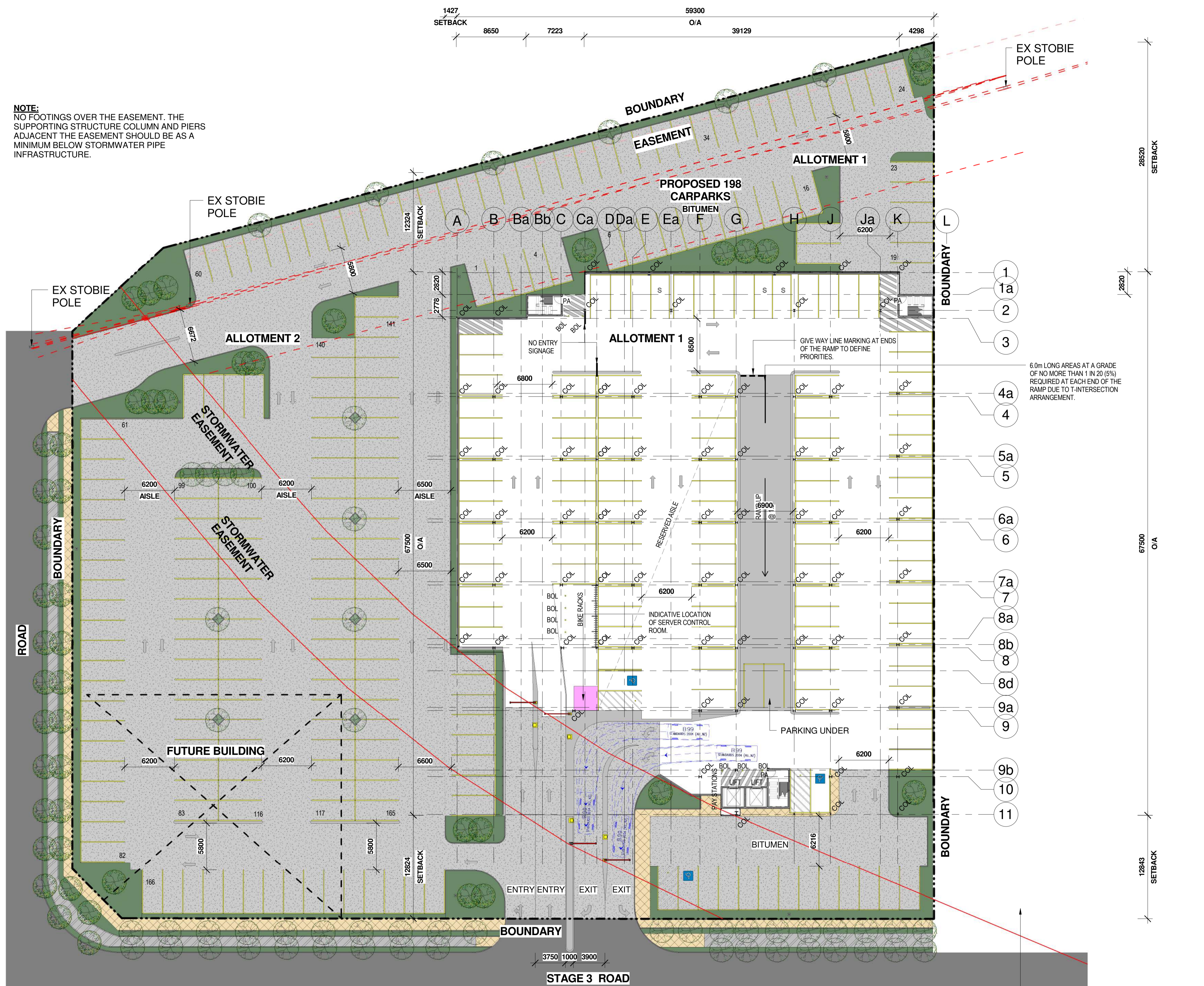


TITLE EXISTING SITE PLAN / LOCALITY PLAN  
ADDRESS TONSLEY CARPARK, CLOVELLY PARK SA 5042

CLIENT **TECHNEBUILD**  
PROJECT MULTI STOREY CARPARK

DESCRIPTION	DATE	BY	REV.
ISSUED TO CLIENT	18/12/2018	ST	E.
ISSUED TO CLIENT	22/06/2018	NKH	D.
ISSUED TO CLIENT	03/05/2018	NKH	C.
ISSUED TO CLIENT	30/04/2018	NKH	B.
ISSUED TO CLIENT	26/04/2018	NKH	A.
Unit 3 1 66 Rundle St. Kent Town SA 5067 T+618 8362 8860 I F+618 8362 8890 I E enquiries@tesseract.com.au W www.tesseract.com.au			
JOB #	17-205		
SHEET #	PL01	REV.	E.

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SUPPORTING STRUCTURE COLUMN AND PIERS  
ADJACENT THE EASEMENT SHOULD BE AS A  
MINIMUM BELOW STORMWATER PIPE  
INFRASTRUCTURE.



#### PARKING SCHEDULE

##### NUMBER OF STANDARD CARPARKS PER LEVEL

GROUND FLOOR	105 SPACES
LEVEL 1	100 SPACES
LEVEL 2	100 SPACES
LEVEL 3	100 SPACES
LEVEL 4	100 SPACES
LEVEL 5	100 SPACES
LEVEL 6	116 SPACES
TOTAL CARPARK SPACES	721 SPACES

##### NUMBER OF SMALL CARPARKS PER LEVEL

GROUND FLOOR	3 SPACES
LEVEL 1	10 SPACES
LEVEL 2	10 SPACES
LEVEL 3	10 SPACES
LEVEL 4	10 SPACES
LEVEL 5	10 SPACES
LEVEL 6	- SPACES
TOTAL SMALL CARPARK SPACES	53 SPACES

##### NUMBER OF DISABLED CARPARKS PER LEVEL

GROUND FLOOR	2 SPACES
LEVEL 1	4 SPACES
LEVEL 2	4 SPACES
LEVEL 3	4 SPACES
LEVEL 4	4 SPACES
LEVEL 5	4 SPACES
LEVEL 6	4 SPACES
TOTAL DIS CARPARK SPACES	26 SPACES

##### TOTAL CARPARK SPACES

GROUND FLOOR EXTERNAL CARPARKS	204 SPACES
GROUND FLOOR EXTERNAL DIS CARPARKS	1 SPACES
BIKE RACKS	28 SPACES

#### DEVELOPMENT AREA SCHEDULE

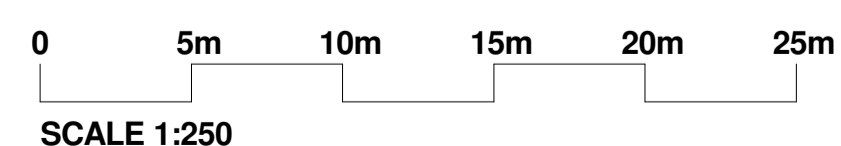
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<b>PAVEMENT</b>	
CONCRETE PAVEMENT	770.300m <sup>2</sup>
BITUMEN PAVEMENT	5981.8m <sup>2</sup>

#### SITE LEGEND

	DENOTES NEW CONCRETE PAVEMENT EXTENTS. TO CIVIL ENGINEER'S DESIGN.
	DENOTES NEW BITUMEN PAVEMENT EXTENTS. TO CIVIL ENGINEER'S DESIGN.
	DENOTES NEW BOLLARDS. CONCRETE FILLED. TO CIVIL ENGINEER'S DESIGN.
	DENOTES NEW LANDSCAPING EXTENTS. TO CIVIL ENGINEER'S DESIGN.

#### PROPOSED SITE PLAN

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DATE 21/08/2017  
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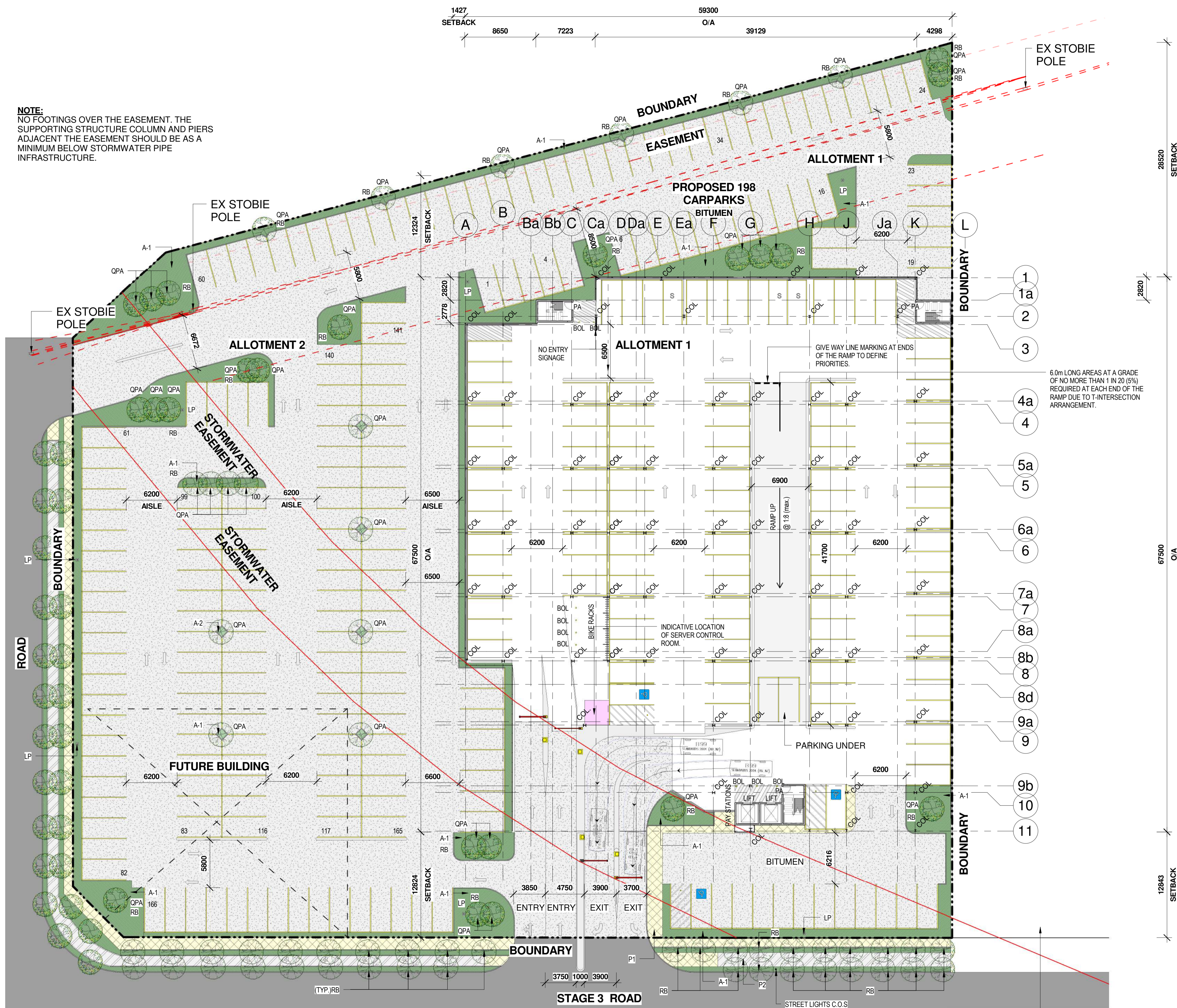
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SHEET # **PL02** REV. I..

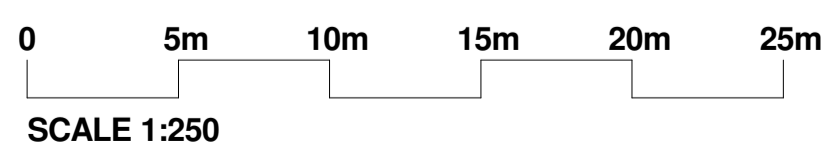
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## PROPOSED LANDSCAPE PLAN

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SCALE/S As indicated  
DATE 21/08/2017  
DRAWN ST  
CHECKED NK

TITLE PROPOSED LANDSCAPE PLAN  
ADDRESS TONSLEY CARPARK, CLOVELLY PARK SA 5042

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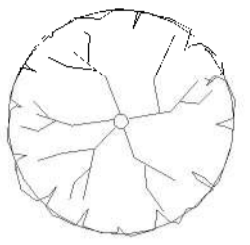
CLIENT **TECHNEBUILD**  
PROJECT MULTI STOREY CARPARK

ISSUED TO CLIENT 18/12/2018 ST A  
DESCRIPTION DATE BY REV.  
Unit 3 1 66 Rundle St. Kent  
Town SA 5067  
T+618 8362 8860 I  
F+618 8362 8890 I  
E enquiries@tesseract.com.au  
W www.tesseract.com.au  
JOB # **17-205**  
SHEET # **PL03** REV. A.

## LANDSCAPING

### WATER WISE PLANT SCHEDULE

THIS IS A PLANT LAYOUT PLAN ONLY. PLANT QUANTITIES, POT SIZES AND  
IRRIGATION DESIGN DO NOT FORM PART OF THIS PLAN.



TREES SUITABLE FOR CAR PARK (SHADE)  
COMMON NAME  
QUERCUS PALUSTRIS (FREEFALL)  
HEIGHT  
3-5m  
ID.  
QPA.

### IMAGE



SUMMER



AUTUMN

GROUND COVER SPECIES SUITABLE FOR PLANTING IN  
CARPARK AND TO FRONT OF, AND AROUND BUILDINGS.  
REFER BELOW FOR SPECIES.

ID.	COMMON NAME	IMAGE
A-1.	MULCHED IRRIGATED PLANTING BED WIDTH 0.375m	
LP	PROPRIETARY 8M HIGH LIGHT POSTS AS SELECTED.	
A-1	IRRIGATED GRASS	
A-1	MULCHED IRRIGATED PLANTING BED	
P-1	INSTU CONCRETE PAVING	
P-2	HOTMIX PATH WITH TOMBER EDGE	
RB	ROOT BARRIER	

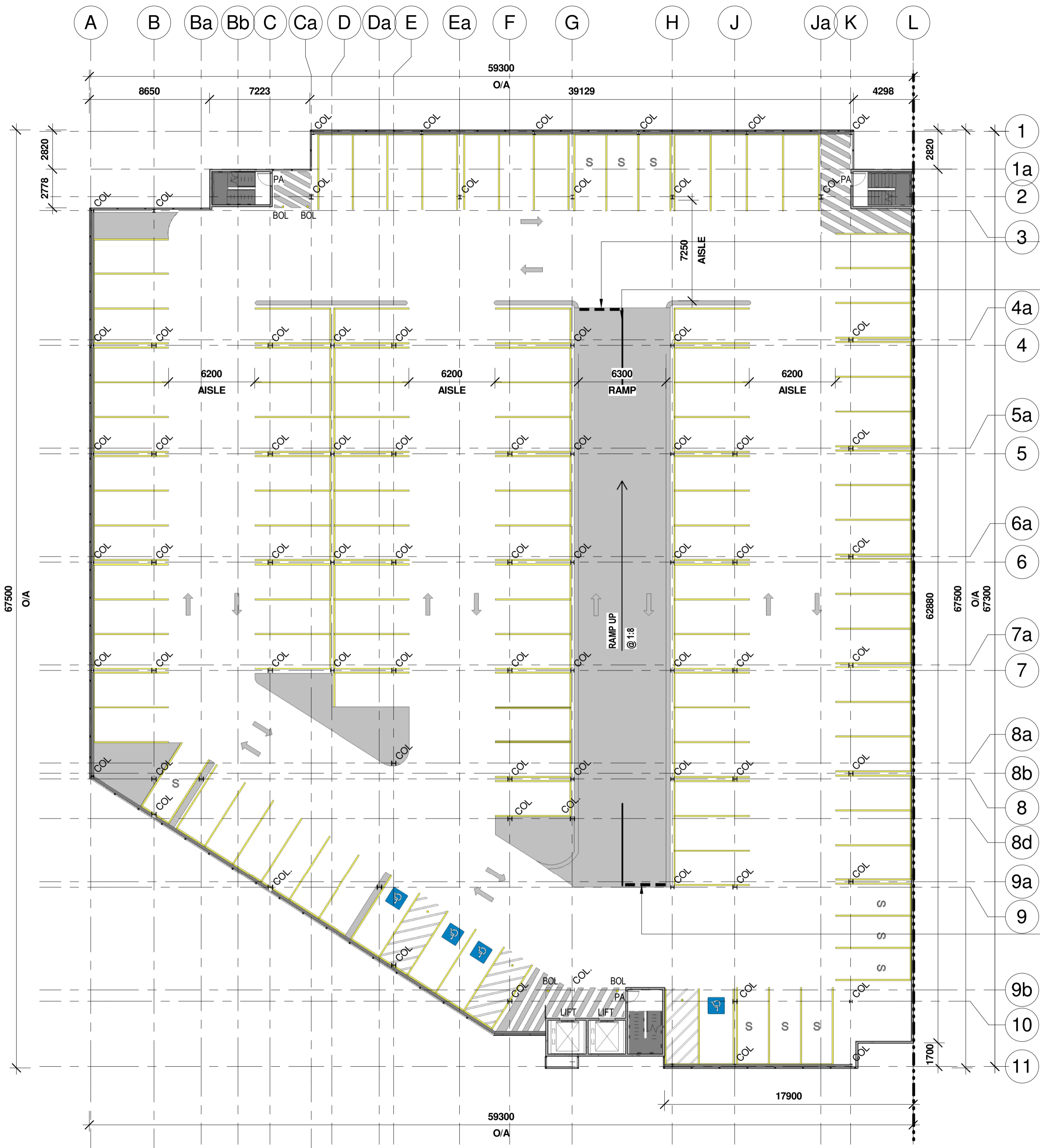
### WATER-WISE NOTES:

- SUB SOIL IRRIGATION SHALL BE USED FOR PROPOSED  
LANDSCAPED AREAS. PROVIDE 75mm MIN. MULCH TO ENTIRE  
LANDSCAPED AREAS.
- LANDSCAPE IRRIGATION SHALL BE CONNECTED TO  
AUTOMATIC TIMERS WITH MOISTURE SENSOR OVERTIME.

6.0m LONG AREAS AT A GRADE  
OF NO MORE THAN 1 IN 20 (5%)  
REQUIRED AT EACH END OF THE  
RAMP DUE TO T-INTERSECTION  
ARRANGEMENT.

807.92 m<sup>2</sup> ADDITIONAL  
LAND REQUIRED.





GIVE WAY LINE MARKING AT ENDS OF THE RAMP TO DEFINE PRIORITIES.

6.0m LONG AREAS AT A GRADE OF NO MORE THAN 1 IN 20 (5%) REQUIRED AT EACH END OF THE RAMP DUE TO T-INTERSECTION ARRANGEMENT.

INSTALL GIVE WAY LINE MARKING AND SIGNAGE

### TYPICAL FLOOR PLAN LEVEL 3,4,5 (STAGE 2)

1 : 200 @ A1

0 2m 4m 6m 8m 10m 20m

SCALE 1:200

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SCALE/S 1 : 200  
DATE 21/08/2017  
DRAWN NKH  
CHECKED NK

TITLE TYPICAL FLOOR PLAN LEVEL 3, 4, 5  
ADDRESS TONSLEY CARPARK, CLOVELLY PARK SA 5042

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CLIENT **TECHNEBUILD**  
PROJECT MULTI STOREY CARPARK

ISSUED TO CLIENT	18/12/2018	ST	H.
ISSUED TO CLIENT	13/11/2018	ST	G.
ISSUED TO CLIENT	13/11/2018	ST	F.
ISSUED TO CLIENT	02/07/2018	NKH	E.
ISSUED TO CLIENT	22/06/2018	NKH	D.
ISSUED TO CLIENT	03/05/2018	NKH	C.
ISSUED TO CLIENT	30/04/2018	NKH	B.
ISSUED TO CLIENT	26/04/2018	NKH	A.

DESCRIPTION	DATE	BY	REV.
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T+618 8362 8860 I			
F+618 8362 8890 I			
E enquiries@tesseract.com.au			
W www.tesseract.com.au			
JOB #	17-205		
SHEET #	PL05	REV.	H.

### PLAN LEGEND

FL LEVEL RELATIVE TO AHD (AUST. HEIGHT DATUM)  
WINDOW  
W0.01 LEVEL WINDOW NUMBER  
DOOR  
D0.01 LEVEL DOOR NUMBER  
I STEEL COLUMN. AS PER STRUCTURAL ENGINEER'S DESIGN

### FLOOR FINISHES

CONC CONCRETE SLAB. BURNISHED FLOOR FINISH.

### GENERAL ABBREVIATIONS

NCC DENOTES, NATIONAL CONSTRUCTION CODES SERIES OF THE BUILDING CODE OF AUSTRALIA.  
AS\_# DENOTES, AUSTRALIAN STANDARD.  
AS/NZS\_# DENOTES, AUSTRALIAN / NEW ZEALAND STANDARD.  
PROVIDE DOOR AS SHOWN IN DRAWINGS  
A DENOTES DOOR OF 850 MIN. CLEAR OPENING IN ACCORDANCE WITH AS1428.1 CLAUSE 7.2  
B DENOTES STANDARD 820 DOOR  
C DENOTES STANDARD 720 DOOR  
EXIT SIGN. INSTALLED IN ACCORDANCE WITH AS. 2293.1.  
PORTABLE FIRE EXTINGUISHER. INSTALLED IN ACCORDANCE WITH AS. 2444.  
COL STRUCTURAL STEEL COLUMN. AS PER STRUCTURAL ENGINEERS DESIGN.  
BOL DENOTES NEW BOLLARDS. CONCRETE FILLED. TO CIVIL ENGINEER'S DESIGN.  
PA COLORBOND CLAD PERSONAL ACCESS DOOR. PAINTED FINISH

### PARKING SCHEDULE

#### NUMBER OF STANDARD CARPARKS PER LEVEL

GROUND FLOOR	105 SPACES
LEVEL 1	100 SPACES
LEVEL 2	100 SPACES
LEVEL 3	100 SPACES
LEVEL 4	100 SPACES
LEVEL 5	100 SPACES
LEVEL 6	116 SPACES
TOTAL CARPARK SPACES	721 SPACES

#### NUMBER OF SMALL CARPARKS PER LEVEL

GROUND FLOOR	3 SPACES
LEVEL 1	10 SPACES
LEVEL 2	10 SPACES
LEVEL 3	10 SPACES
LEVEL 4	10 SPACES
LEVEL 5	10 SPACES
LEVEL 6	- SPACES
TOTAL SMALL CARPARK SPACES	53 SPACES

#### NUMBER OF DISABLED CARPARKS PER LEVEL

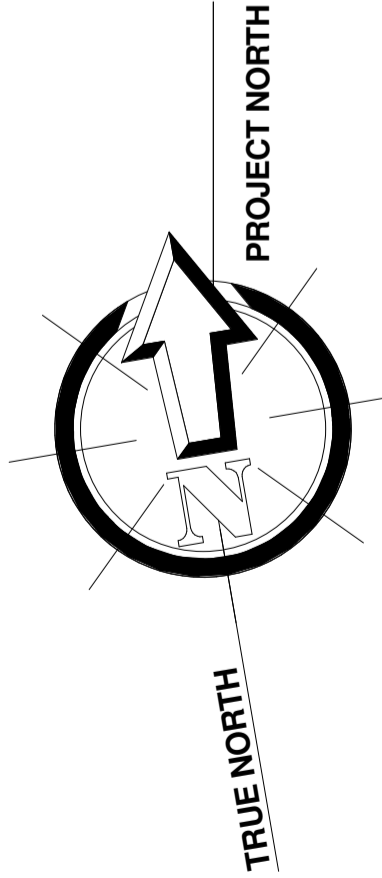
GROUND FLOOR	2 SPACES
LEVEL 1	4 SPACES
LEVEL 2	4 SPACES
LEVEL 3	4 SPACES
LEVEL 4	4 SPACES
LEVEL 5	4 SPACES
LEVEL 6	4 SPACES
TOTAL DIS CARPARK SPACES	26 SPACES

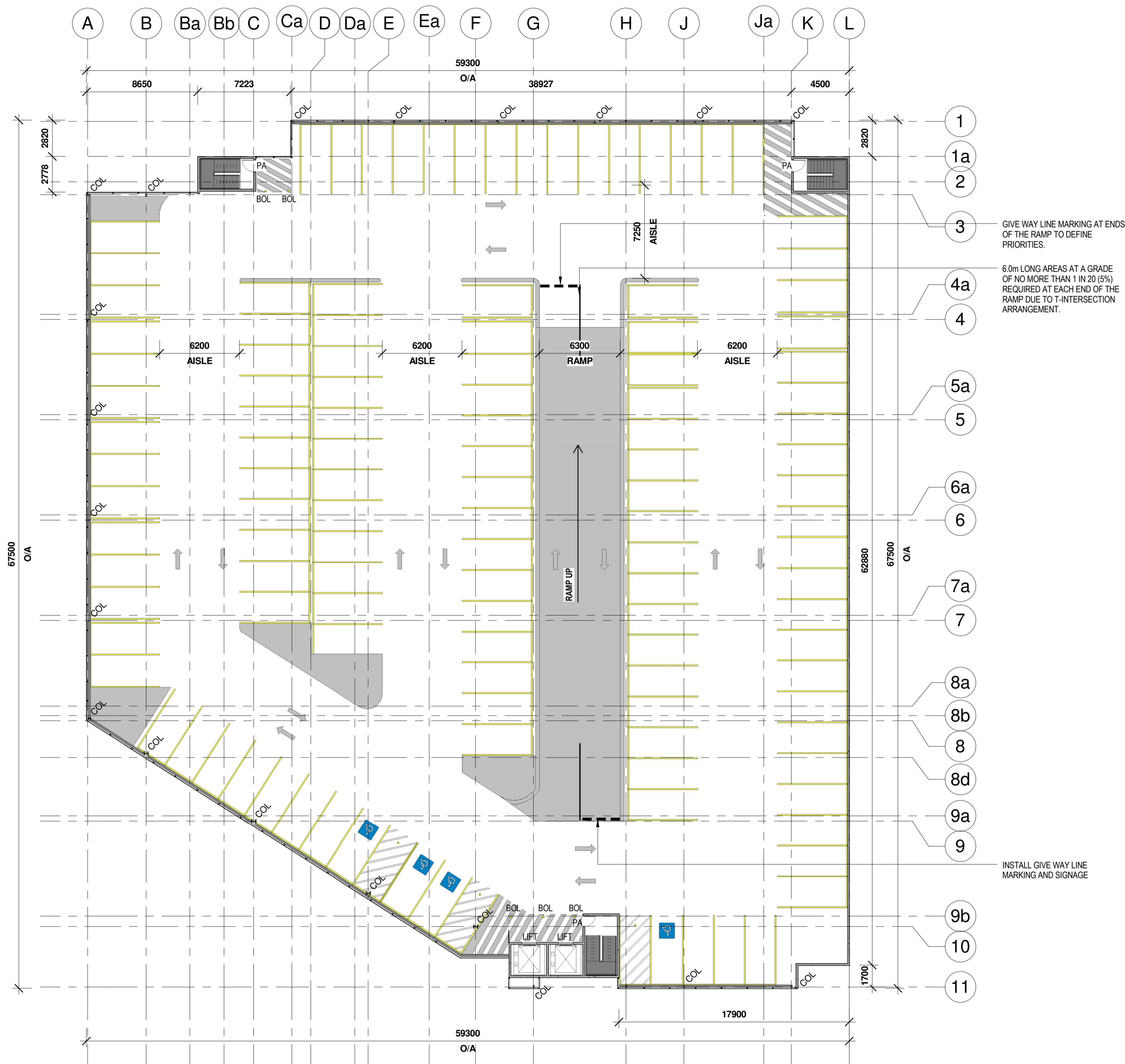
#### TOTAL CARPARK SPACES

TOTAL CARPARK SPACES	800 SPACES
GROUND FLOOR EXTERNAL CARPARKS	204 SPACES
GROUND FLOOR EXTERNAL DIS CARPARKS	1 SPACES
BIKE RACKS	28 SPACES

### DEVELOPMENT AREA SCHEDULE

AREA	SQM
EXISTING SITE TOTAL	10058.15m <sup>2</sup>
ALLOTMENT 1 AREA	6264.80m <sup>2</sup>
ALLOTMENT 2 AREA	3793.36m <sup>2</sup>
<b>BUILDING</b>	
GROUND FLOOR	3416.00m <sup>2</sup>
LEVEL 1	3564.40m <sup>2</sup>
LEVEL 2	3564.40m <sup>2</sup>
LEVEL 3	3564.40m <sup>2</sup>
LEVEL 4	3564.40m <sup>2</sup>
LEVEL 5	3564.40m <sup>2</sup>
LEVEL 6	3564.40m <sup>2</sup>
TOTAL BUILDING	24802.4m <sup>2</sup>
<b>LANDSCAPING</b>	
PROPOSED LANDSCAPING	773.4m <sup>2</sup>
<b>PAVEMENT</b>	
CONCRETE PAVEMENT	770.300m <sup>2</sup>
BITUMEN PAVEMENT	5981.8m <sup>2</sup>





PLAN LEGEND

- FL LEVEL RELATIVE TO AHD (AUST. HEIGHT DATUM)
- W0.01 WINDOW
- LEVEL WINDOW NUMBER
- DOOR
- D0.01 LEVEL DOOR NUMBER
- STEEL COLUMN, AS PER STRUCTURAL ENGINEER'S DESIGN

FLOOR FINISHES

CONC CONCRETE SLAB, BURNISHED FLOOR FINISH.

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- COL STRUCTURAL STEEL COLUMN, AS PER STRUCTURAL ENGINEERS DESIGN.
- BOL DENOTES NEW BOLLARDS. CONCRETE FILLED. TO CIVIL ENGINEER'S DESIGN.
- PA COLORBOND CLAD PERSONAL ACCESS DOOR. PAINTED FINISH

PARKING SCHEDULE

NUMBER OF STANDARD CARPARKS PER LEVEL

GROUND FLOOR	105 SPACES
LEVEL 1	100 SPACES
LEVEL 2	100 SPACES
LEVEL 3	100 SPACES
LEVEL 4	100 SPACES
LEVEL 5	100 SPACES
LEVEL 6	116 SPACES
TOTAL CARPARK SPACES	721 SPACES

NUMBER OF SMALL CARPARKS PER LEVEL

GROUND FLOOR	3 SPACES
LEVEL 1	10 SPACES
LEVEL 2	10 SPACES
LEVEL 3	10 SPACES
LEVEL 4	10 SPACES
LEVEL 5	10 SPACES
LEVEL 6	- SPACES
TOTAL SMALL CARPARK SPACES	53 SPACES

NUMBER OF DISABLED CARPARKS PER LEVEL

GROUND FLOOR	2 SPACES
LEVEL 1	4 SPACES
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LEVEL 3	4 SPACES
LEVEL 4	4 SPACES
LEVEL 5	4 SPACES
LEVEL 6	4 SPACES
TOTAL DIS CARPARK SPACES	26 SPACES

TOTAL CARPARK SPACES

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GROUND FLOOR EXTERNAL DIS CARPARKS	1 SPACES
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AREA	SQM
EXISTING SITE TOTAL	10058.15m <sup>2</sup>
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ALLOTMENT 2 AREA	3793.36m <sup>2</sup>
BUILDING	
GROUND FLOOR	3416.00m <sup>2</sup>
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LEVEL 3	3564.40m <sup>2</sup>
LEVEL 4	3564.40m <sup>2</sup>
LEVEL 5	3564.40m <sup>2</sup>
LEVEL 6	3564.40m <sup>2</sup>
TOTAL BUILDING	24802.4m <sup>2</sup>
LANDSCAPING	
PROPOSED LANDSCAPING	773.4m <sup>2</sup>
PAVEMENT	
CONCRETE PAVEMENT	770.300m <sup>2</sup>
BITUMEN PAVEMENT	5981.8m <sup>2</sup>

PARKING LEVEL 6 (STAGE 2)

1 : 200 @ A1

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CLIENT TECHNEBUILD

TITLE PARKING LEVEL 6  
ADDRESS TONSLEY CARPARK, CLOVELLY PARK SA 5042

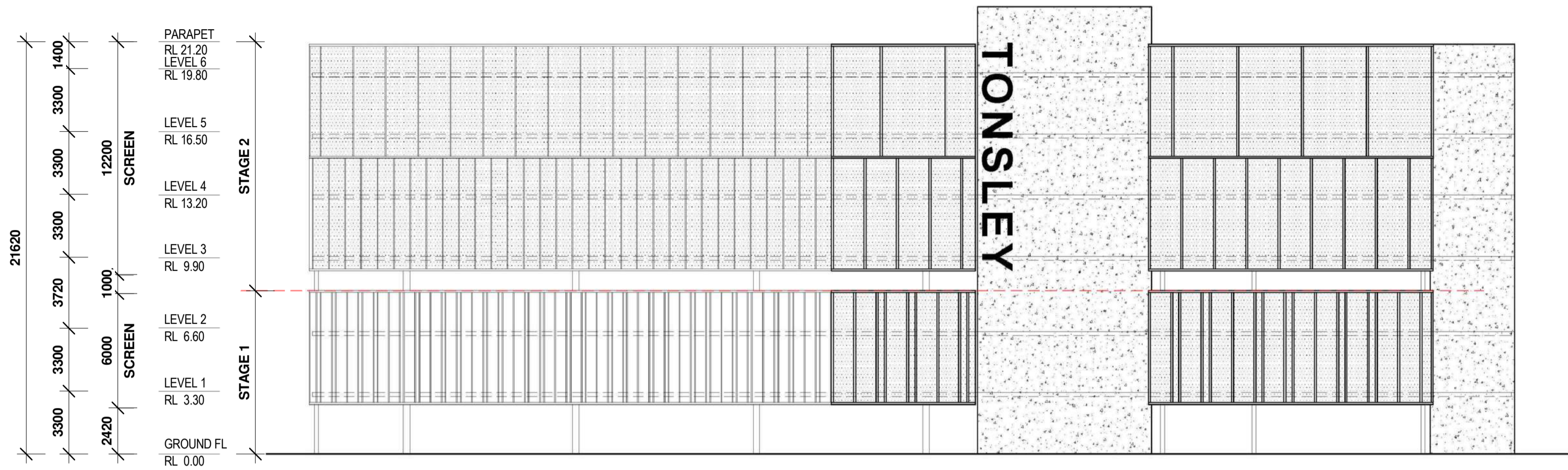
PROJECT MULTI STOREY CARPARK

DESCRIPTION	DATE	BY	REV.
ISSUED TO CLIENT	18/12/2018	ST	H.
ISSUED TO CLIENT	13/11/2018	ST	G.
ISSUED TO CLIENT	13/11/2018	ST	F.
ISSUED TO CLIENT	02/07/2018	NKH	E.
ISSUED TO CLIENT	22/06/2018	NKH	D.
ISSUED TO CLIENT	03/05/2018	NKH	C.
ISSUED TO CLIENT	30/04/2018	NKH	B.
ISSUED TO CLIENT	26/04/2018	NKH	A.

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E enquiries@tesseract.com.au  
W www.tesseract.com.au

JOB # 17-205

SHEET # PL06 REV.H.

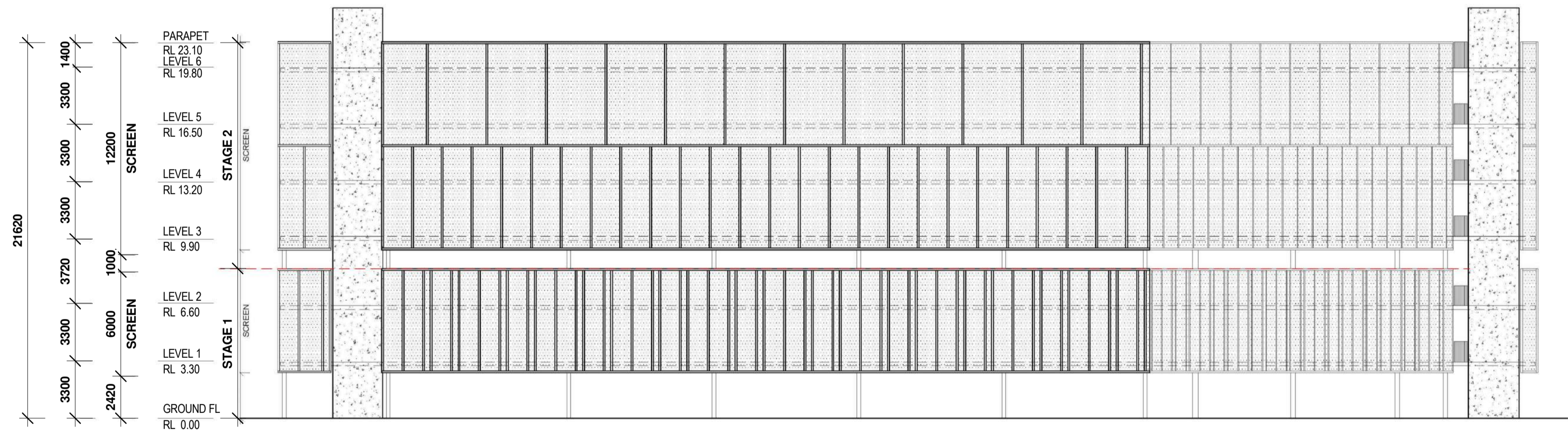


### ELEVATION LEGEND

- ###.##(AHD) LEVEL RELATIVE TO AHD (AUST. HEIGHT DATUM)
- RL.###.## LEVEL RELATIVE TO FLOOR LEVEL
- RL REDUCED LEVEL  
CL CEILING LEVEL  
FL FLOOR LEVEL
- W0.01 WINDOW  
LEVEL. WINDOW NUMBER
- D0.01 DOOR  
LEVEL. DOOR NUMBER
- DP PVC STORMWATER DOWNPIPE, PAINTED FINISH.  
RAINHEAD / OVERFLOW SPOUT IF SHOWN. COLORBOND FINISH.  
REFER TO ARCHITECTURAL ROOF PLAN.
- BOL NEW BOLLARDS. CONCRETE FILLED. REFER TO ARCHITECTURAL  
SITE PLAN FOR LOCATIONS AND CIVIL DRAWINGS FOR DETAILS.

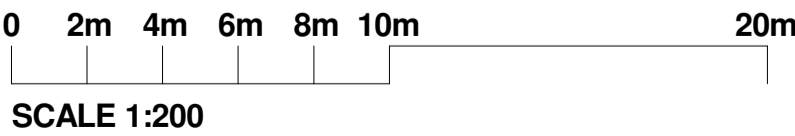
### SOUTH ELEVATION

1 : 200 @ A1



### WEST ELEVATION

1 : 200 @ A1



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TITLE ELEVATIONS - SHEET 1 OF 2  
ADDRESS TONSLEY CARPARK, CLOVELLY PARK SA 5042



CLIENT **TECHNEBUILD**  
PROJECT **MULTI STOREY CARPARK**

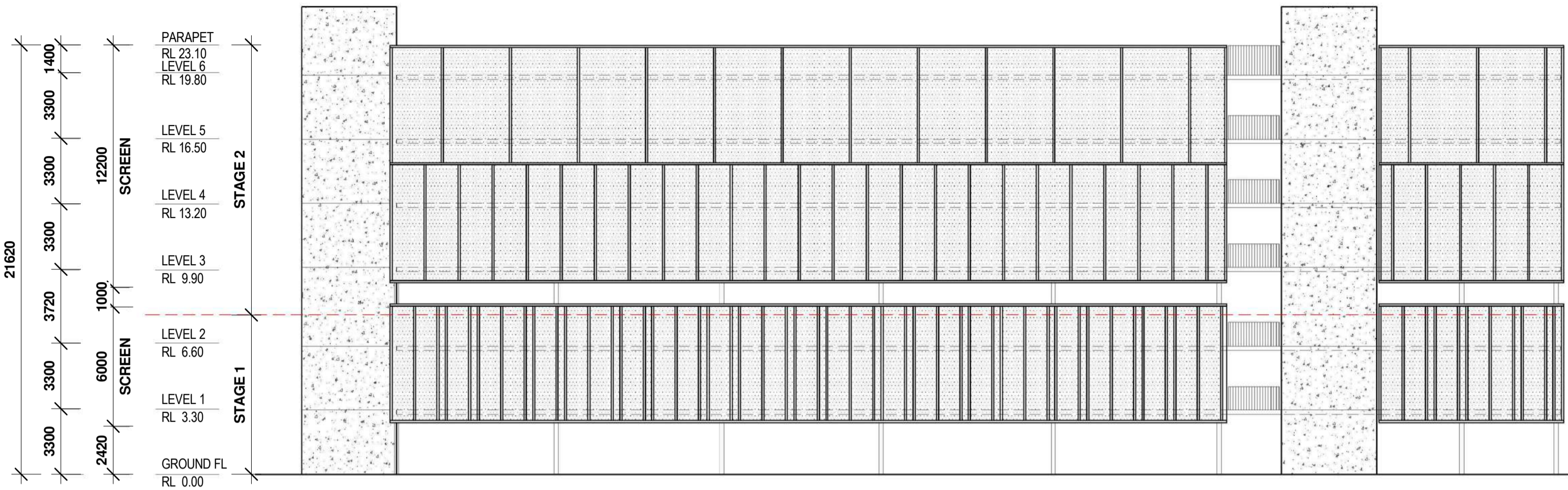
JOB # **17-205**  
SHEET # **PL07** REV. H.

DESCRIPTION	DATE	BY	REV.
ISSUED TO CLIENT	18/12/2018	ST	H.
ISSUED TO CLIENT	02/07/2018	NKH	G.
ISSUED TO CLIENT	02/07/2018	NKH	F.
ISSUED TO CLIENT	22/06/2018	NKH	E.
ISSUED TO CLIENT	25/05/2018	NKH	D.
ISSUED TO CLIENT	03/05/2018	NKH	C.
ISSUED TO CLIENT	30/04/2018	NKH	B.
ISSUED TO CLIENT	26/04/2018	NKH	A.

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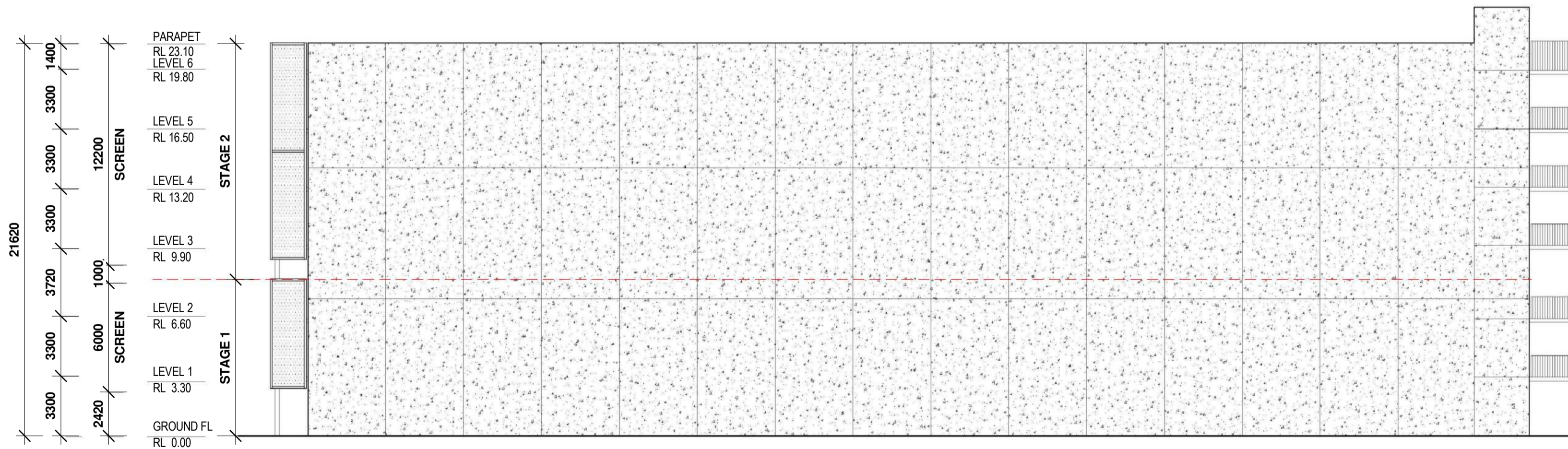
ELEVATION LEGEND

- ###(AHD)
- LEVEL RELATIVE TO AHD  
(AUST. HEIGHT DATUM)
- RL####
- LEVEL RELATIVE TO FLOOR LEVEL
- RL
- REDUCED LEVEL
- CL
- CEILING LEVEL
- FL
- FLOOR LEVEL
- W0.01
- WINDOW
- LEVEL. WINDOW NUMBER
- D0.01
- DOOR
- LEVEL. DOOR NUMBER
- DP
- PVC STORMWATER DOWNPIPE, PAINTED FINISH.  
RAINHEAD / OVERFLOW SPOUT IF SHOWN. COLORBOND FINISH.  
REFER TO ARCHITECTURAL ROOF PLAN.
- BOL
- NEW BOLLARDS. CONCRETE FILLED. REFER TO ARCHITECTURAL  
SITE PLAN FOR LOCATIONS AND CIVIL DRAWINGS FOR DETAILS.



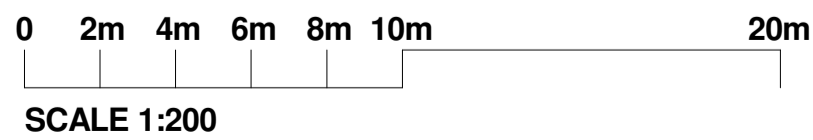
NORTH ELEVATION

1 : 200 @ A1



EAST ELEVATION

1 : 200 @ A1



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DATE 21/08/2017  
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TITLE ELEVATIONS - SHEET 2 OF 2  
ADDRESS TONSLEY CARPARK, CLOVELLY PARK SA 5042

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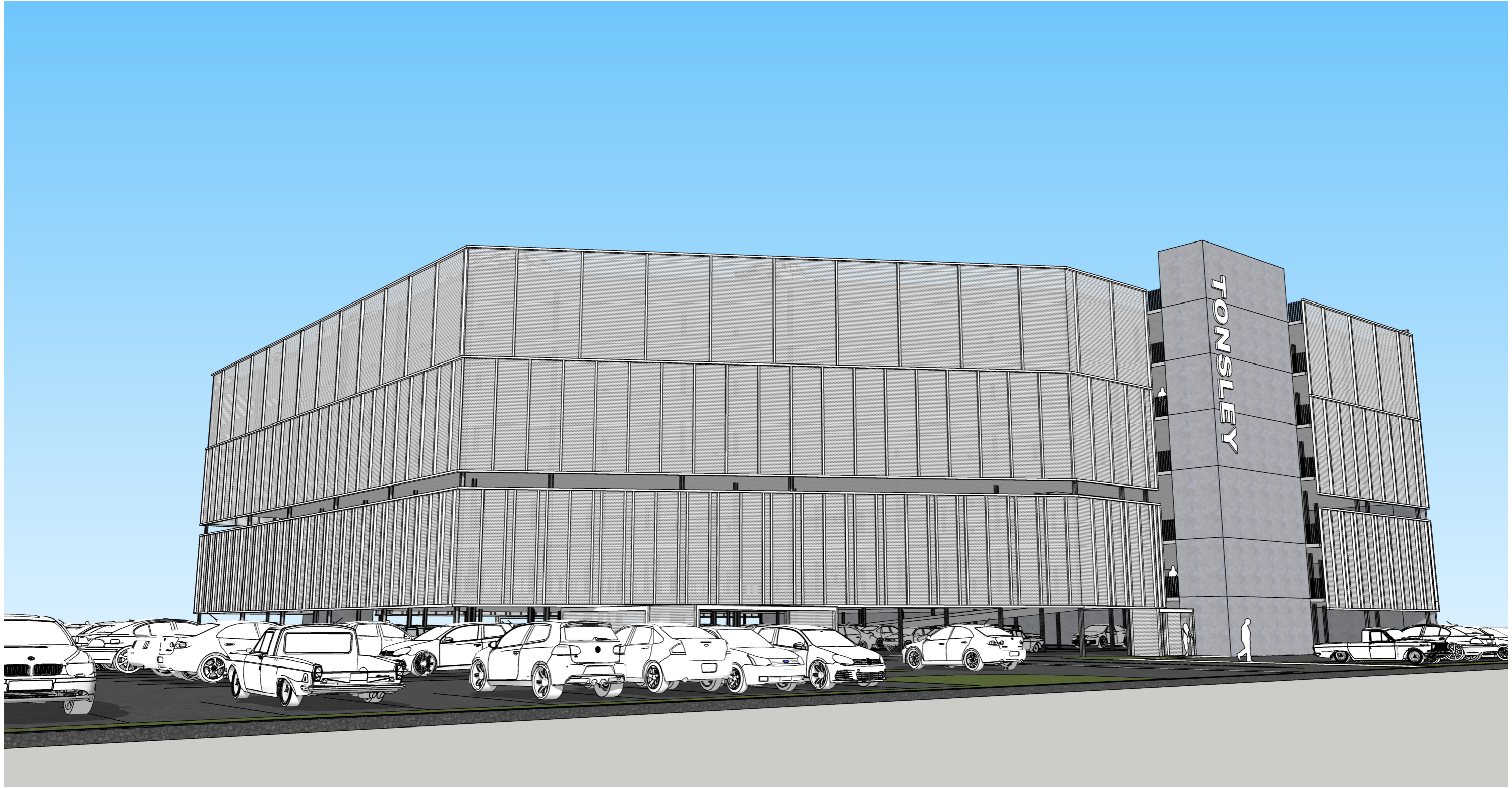


CLIENT **TECHNEBUILD**  
PROJECT **MULTI STOREY CARPARK**  
JOB # **17-205**  
SHEET # **PL08** REV. H.

DESCRIPTION	DATE	BY	REV.
ISSUED TO CLIENT	18/12/2018	ST	H.
ISSUED TO CLIENT	02/07/2018	NKH	G.
ISSUED TO CLIENT	02/07/2018	NKH	F.
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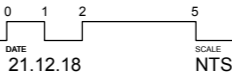




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0 1 2 5  
DATE 21.12.18 SCALE NTS

PROJECT NO. 18007 DRAWING NO. A5.01[A2]

CLIENT  
TECHNEBUILD

PROJECT  
MULTI STORY CARPARK

DRAWING  
PERSPECTIVE 02



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0 1 2 5  
DATE 21.12.18 SCALE NTS

PROJECT NO. 18007 DRAWING NO. A5.02[A2]



A1 PLANNING APPLICATION 28.06.18

**ENZO CAROSCIO ARCHITECTURE**

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PROJECT NO. 18007 DRAWING NO. A5.03[A2]

CLIENT  
TECHNEBUILD

PROJECT  
MULTI STORY CARPARK

DRAWING  
PERSPECTIVE 04



# INTRO

## APPENDIX 02 – SITE LAYOUT ASSESSMENT

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REF: S163660

DATE: 18 December 2018

Technebuild  
 Level 5, 33 Pirie Street  
 ADELAIDE SA 5000

Attention: Mr. Emanuel Palyaris

Dear Emanuel,

**RE: TONSLEY MULTI-DECK CAR PARK – MAB CIRCUIT, TONSLEY**

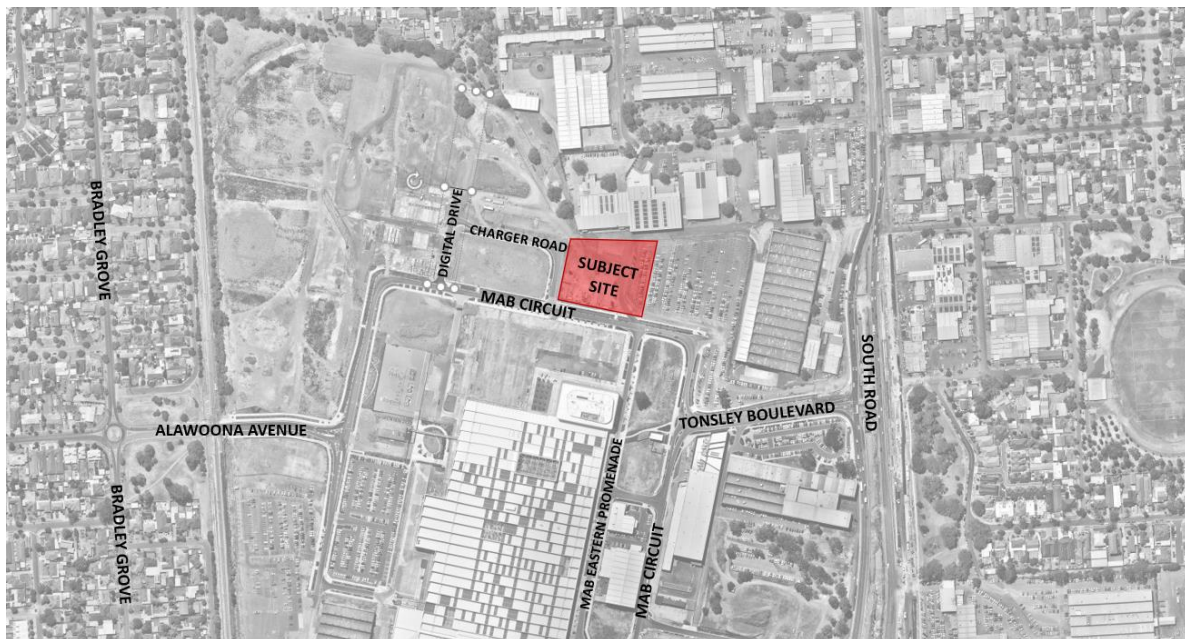
### **SITE LAYOUT ASSESSMENT**

I refer to our recent discussions and correspondence pursuant to the proposed Multideck Car Park development to be located on MAB Circuit in Tonsley. As requested, we have undertaken a site layout review in accordance with Australian Standards of the proposed site layout. The assessment has been undertaken based on the plans prepared by Tersseract International (dated December 2018).

### **Subject Site and Surrounds**

The subject site is located on MAB Circuit in Tonsley and is located within a Regional Activity zone within the City of Marion. The site of approximately 10,050sq.m has a frontage of approximately 100 metres to MAB Circuit and 67m to Charger Road. The properties surrounding the subject site are a mix of commercial and industrial land uses within the Tonsley Innovation District, with residential allotments located outside of the district. Figure 1 shows the subject site in relation to its surrounds.

**Figure 1: Subject Site and Surrounds**



MAB Circuit is a two-way road aligned in an approximate east/west direction adjacent the subject site. It is configured with 1 lane in each direction, with a 6.6 metre wide carriageway set within a 21.4 metre wide road reserve (approx.). Indented parking bays are located on the southern side of MAB Circuit directly adjacent the subject site.

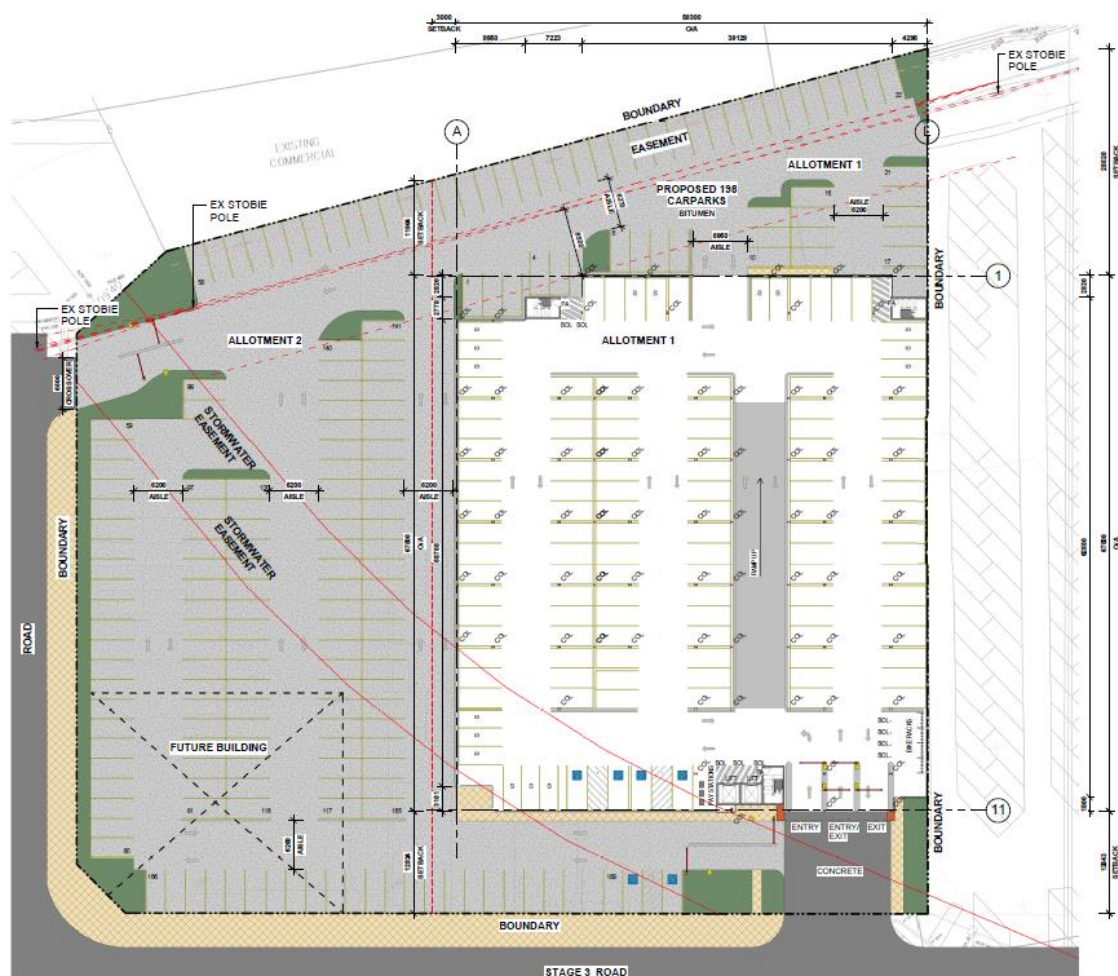
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Based on peak hour traffic surveys undertaken by GTA in February 2018, MAB Circuit carries in the order of 3,000 vehicles per day (based on a peak-to-daily ratio of 10%). MAB Circuit is subject to the default urban speed limit of 50km/h.

## Previous Application Layout

The previous application layout is outlined within Figure 2. The main access of the previous layout was located in close proximity to the MAB Eastern Promenade/MAB Circuit intersection and was located within the prohibited zone. The access provided three lanes, with one tidal lane, allowing for a 3 vehicle queue within the property boundary. The updated layout relocates the access to be located outside of the prohibited zone, and also provides 4 access lanes with a entry queue capacity of 4 vehicles per lane.

**Figure 2: Previous Application**



## Proposed Development

The proposed development includes the construction of a 6-level multi-deck car park with at-grade parking, providing an overall parking provision of 1020 car parking spaces. 204 spaces are proposed within the at-grade parking (excluding multi-deck ground level) and 816 spaces are proposed within the multi-deck parking structure. The proposed ground floor/site layout is shown in Figure 3. Within the ground level of the multi-deck car park, a total of 28 bicycle parking spaces are proposed.

Figure 3: Ground Floor/Site Layout



Vehicle access to the multi-deck car park is proposed via two crossovers, one located on MAB Circuit and one located on Charger Road. The MAB Circuit access is proposed to comprise of two entry lanes for access to the multi-deck and the at grade-parking and two exit lanes from the multi-deck car park. The access located on Charger Road will allow for an additional vehicular entry point to the at-grade car parking, and provide exit movements for all vehicles accessing the at-grade parking.

Preliminary discussions with the proposed operator, Wilson Parking, outlined that the planned operation of the car park is to involve the use of smart parking technologies, with there being no need for customers to interface with car park equipment whilst entering or exiting the site (email correspondence enclosed). Notwithstanding the above, the site has been designed to allow for typical boom gated systems to be installed for the multi-deck car park, with smart parking technologies within the at-grade car park.

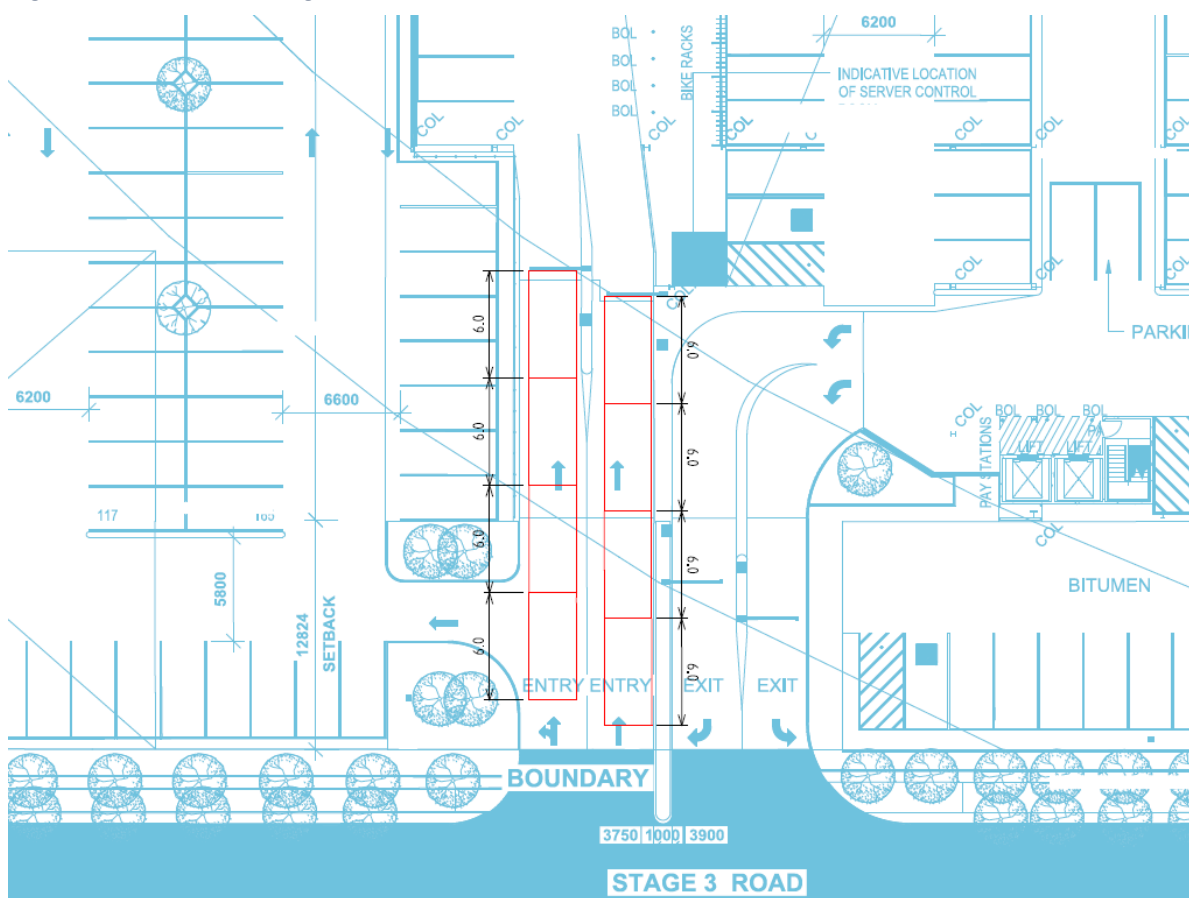
### Car Parking Layout Review

The car parking layout has been assessed in accordance with the Australian Standard for Off-Street Car Parking (AS2890.1:2004) and the Australian Standard for Off-street Parking for People with Disabilities (AS2890.6:2009). GTA notes the following features:

- The land uses that are anticipated to utilise the car park are primarily longer term parking, associated with the current and future office/commercial land uses within the Tonsley Innovation District. Therefore, the proposed development meets the User Class 2 parking facility requirements.
- Parking spaces will be 2.5m wide by 5.4m long set within a 6.2m wide aisle, exceeding the requirement for a User Class 2 parking facility.

- Small car parking spaces will be a 2.3m wide by 5.0m long set within a 6.2m wide aisle, meeting the small car parking space requirements set out within the Australian Standard. The small car provision accounts for approximately 6% of the overall parking provision, which is considered to be appropriate.
- Parking spaces for people with disabilities and their associated shared spaces will be 2.4m wide by 5.4m long with a bollard located in the centre of the shared space, meeting the requirements of AS2890.6:2009.
- The proposed access will be located a minimum of 6.0m from the projection of the property boundary, to be positioned outside of the design envelope.
- It is understood that the blind aisle located within the ground level is to be reserved parking. It is recommended that the aisle be appropriately signed as reserved parking. As the aisle is for reserved parking, no turn around provision is required.
- One metre blind aisle extensions will be provided within the development where required.
- Walls, columns and obstructions greater than 150mm in height are located outside of the design envelope.
- The proposed access is anticipated to operate with the use of smart parking facilities without the need for a typical boom gate system. Notwithstanding, the Multi-Deck car park has been designed to allow for the use of boom gates if required. Information provided by Wilson Parking indicate that most of their CBD car parks of a similar size in parking provision have queuing space for approximately 3-4 vehicles per lane, including car parks with single entries that facilitate upwards of 900 vehicle movements per day. As a result, the proposed car park provides a queuing capacity of 4 spaces per entry lane, as outlined in Figure 4.

**Figure 4: Available Queuing at Multi-Deck Entrance**



- The parking aisle upon entry to the multi-deck car park is shown to be one way as a result of the two entry lanes. No-entry signage is also proposed to help define the extent of the one-way flow.
- At the end of the ramps at each level, give way line marking and signage is proposed be installed to define vehicle priorities at these locations.
- Figure 5 and Figure 6 show the ability for a B99 and B85 to circulate through the ramps and the entry and exit to the site. To accommodate simultaneous movements at the southern ramp access within the upper levels, it is recommended that the ramp be flared as per Figure 6 to accommodate vehicular entry. **This can be reviewed in Detailed Design.**

Figure 5: Ground Floor Circulation

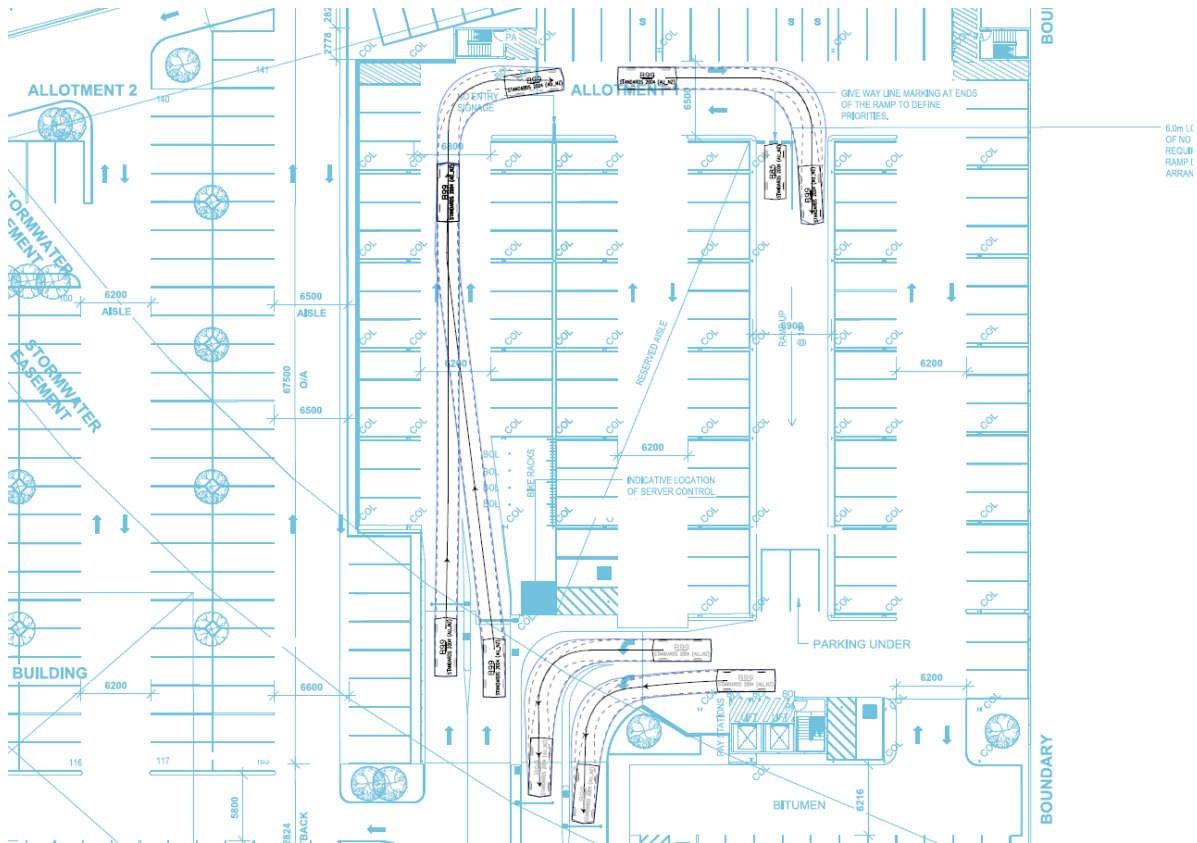


Figure 6: Upper Level Circulation



- The floor to floor height is shown to be 3.3m in height difference. This available height will be more than adequate to allow for a minimum 2.2m head height clearance, with 2.5m head height clearance above the disability parking space and shared space.
- Vertical clearances associated with the parking spaces under the ground floor ramp will provide a minimum 2.2m head height clearance.
- The maximum ramp grades shall be 1 in 8 (12.5%) to allow for a single grade ramp without the need for transitions as per the Standards. An area of 6.0m in length at a grade not exceeding 1 in 20 (5%) shall be provided at each end of the ramp to allow for a vehicle to be positioned at the give way locations. There will be adequate available ramp length to accommodate a maximum 1 in 8 (12.5%) grade with the 6.0m areas at each end of the ramp.

## Traffic Generation

Given the nature of the proposed development as a car parking facility, the movements associated with the subject site will be trips related to other uses within the Tonsley Innovation District. These trips will have been considered as part of the overall precinct masterplan and as part of individual Development Applications for traffic these generating sites. Therefore, the proposed development (excluding future building) is not anticipated to generate any additional traffic movements to the precinct that would not have been forecast as part of the broader site planning and masterplan.

It is anticipated that during operation, the proposed car park will operate at 85% capacity at any one time, resulting in an anticipated demand of 867 car parking spaces. It is estimated that during the peak hour, the car park will have a turnover/car park occupancy of 70%. Based on the 70% turnover of the anticipated demand, it is estimated that in the order of 610 trips could be associated with the subject site in the peak hour.

## Distribution and Alignment

The directional distribution and assignment of traffic generated by the proposed development will be influenced by a number of factors, including the:

1. configuration of the road network in the immediate vicinity of the site
2. existing operation of intersections providing access between the local and arterial road network
3. distribution of households in the vicinity of the site
4. likely distribution of employee's residences in relation to the site
5. configuration of access points to the site.

Having consideration to the above, for the purposes of estimating vehicle movements, the following directional distributions have been assumed:

### Inbound

- MAB Circuit (East) 65%
- MAB Circuit (West) 30%
- Charger Road 5%

### Outbound

- MAB Circuit (East) 50%
- MAB Circuit (West) 30%
- Charger Road 20%

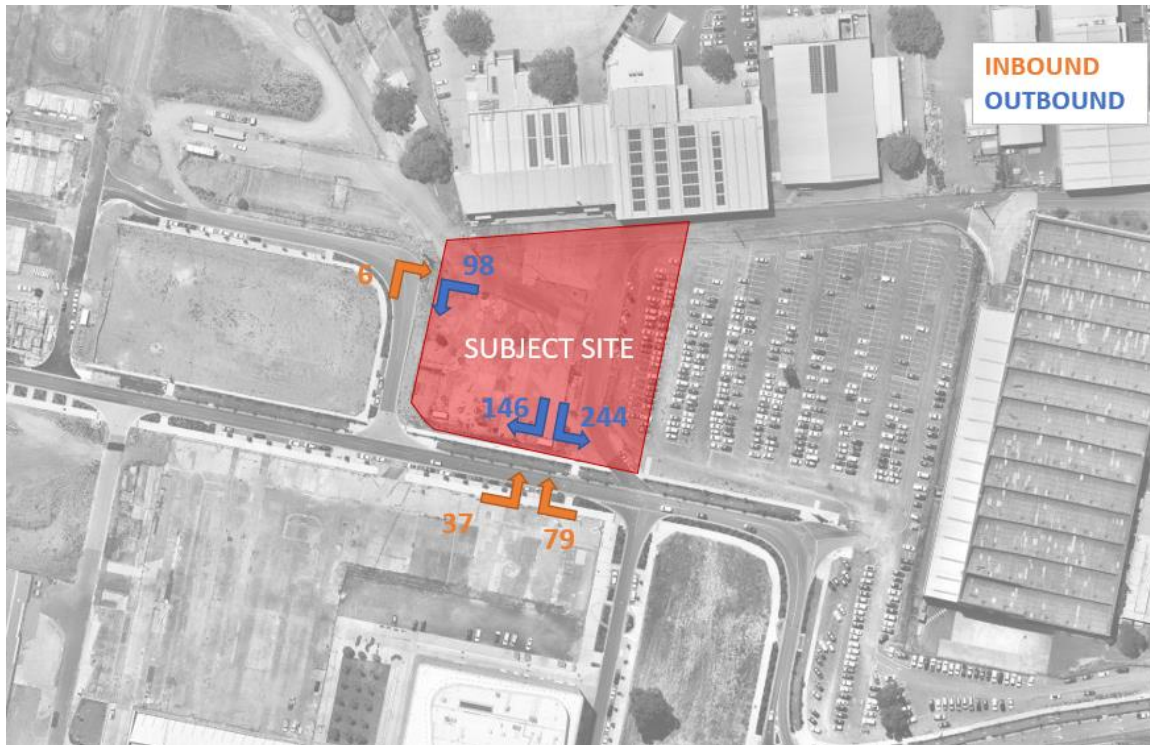
In addition, the directional split of traffic (i.e. the ratio between the inbound and outbound traffic movements) is assumed to be 80% inbound and 20% outbound for the AM period, and 20% inbound and 80% outbound for the PM period.

Based on the above, Figure 7 and Figure 8 have been prepared to show the estimated increase in turning movements in the vicinity of the subject property following full site development.

**Figure 7: AM Peak Period**



Figure 8: PM Peak Period



Given that the majority of the traffic associated with the multi-deck will be present on the road network regardless of the subject site (i.e. generated by other developments in the precinct), and the low numbers experienced on MAB Circuit, it is anticipated that the site and access points will operate effectively.

## Conclusion

Based on the analysis and discussions presented within this report, the following conclusions are made:

1. The proposed development provides a total of 1020 car parking spaces, with 816 spaces associated with the multi-deck and 204 spaces within the at-grade parking area.
2. The proposed parking layout is generally consistent with the dimensional requirements as set out in the Australian/New Zealand Standards for Off Street Car Parking (AS/NZS2890.1:2004 and AS/NZS2890.6:2009).  
**The required changes can be reviewed within Detailed Design.**
3. The site is expected to experience up to 610 vehicle movements in the peak hour. Given the nature of the proposed development as a car parking facility, the proposed development (excluding future building) is not anticipated to generate any additional traffic movements to the precinct that would not have been forecast as part of the broader site planning and masterplan.
4. Based on the predicted volumes associated with the vehicles parking at the proposed development, it is anticipated that the site and access points will operate effectively.

Should you have any questions or require any further information, please do not hesitate to contact me on (08) 8334 3600.

Yours sincerely

**GTA CONSULTANTS**

**David Kwong**  
Associate Director

## David Kwong

---

**From:** Jeremy Dyson <jeremy.dyson@wilsonparking.com.au>  
**Sent:** Monday, 26 November 2018 4:10 PM  
**To:** David Kwong  
**Subject:** Tonsley Car park

Hi David,

Thank you again for your time last Friday.

As discussed I have gathered some specifications for the site requirements for any future car park equipment at Tonsley.

- Entry and exit plinths. Minimum specification is 600mm wide by 200mm deep by 4000mm long. Under our proposal we would lay conduit and cap the ends prior to the concrete being laid. This would save us having to cut the concrete at a later date.
- Server room: we would need a space to install a computer and server rack that will operate the equipment. This can be located anywhere in the car park provided we could get cabling to the entry and exit gates. Ideally a space of 2.5x2.5m (or bigger). This also allows for storage of parts and signs etc used in the sites operation. Smallest we could go would be 1.5x2m (this would also require a custom server rack).

As also discussed I understand that the Marion Council is seeking some feedback regarding queuing of cars entering the site. Below are some dot points that you may find useful in your response:

- Typically with most gated access control systems Customer interface with the equipment is between 3-6 seconds.
- Most of our CBD car parks have queuing space for approximately 3-4 vehicles per lane. At peak times it is rare for customer vehicles to spill onto roads. This includes car parks with single entries that facilitate upwards of 900 vehicle movements per day.
- The demographic of the Tonsley site means that usage will be more evenly spread across the day. (i.e. 9-5 works in addition to students who will rotate throughout the day). This will reduce the peak entry and exit pressures as demand is spread across a greater period.
- The planned operation of the Car park involves a smart technology. At this stage there will be no need for customers to interface with car park equipment whilst egressing in or out of the site. This will further reduce queuing (particularly on entry) to the site.

Please let me know if there is anything I can do further to assist.

Kind regards,

Jeremy

**Jeremy Dyson**  
Development Manager SA/NT



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# INTRO

## APPENDIX 03 – LETTER OF ENDORSMENT FROM RENEWAL SA

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Ref:

Att: Sara Zuidland  
Department of Planning, Transport and  
Infrastructure (Development Division)  
PO Box 1815  
Adelaide SA 5001

20/11/18

T 08 8207 1300  
F 08 8207 1301  
E [renewalsa.info@sa.gov.au](mailto:renewalsa.info@sa.gov.au)  
W [www.renewalsa.sa.gov.au](http://www.renewalsa.sa.gov.au)

Dear Sara,

**DEVELOPMENT APPLICATION – TONSLEY MULTI-DECK CAR PARK**

Renewal SA, as the master developer of Tonsley Innovation District, support the proposed development of the Tonsley Multi-Deck Car Park for Lot C39 at Tonsley, lodged by MAB Park Pty Ltd.

A Land Sale Contract has been signed between Renewal SA and MAB Park Pty Ltd with the proposed use of the site as a multi-deck car park. The Flinders Tonsley Precinct Integrating Transport Strategy identifies the requirement for a multi deck car park within the District, we believe a multi-deck car park is required on site as soon as possible in order to reduce the car parking difficulties on site and reduce the amount of at-grade car parking on site.

This allotment is a suitable location within the precinct for a multi-deck car park, the immediately surrounding allotments will be commercial/industrial use, plus it is in close proximity to Flinders University and the Main Assembly Building.

Generally, the proposed design of the car park is supported by Renewal SA. We will however request that additional landscaping/vegetation is included in the development.

If you have any queries, please do not hesitate to contact me on 8207 1300.

Yours sincerely



**Vincent Rigter**  
**Project Director**

cc: Emanuel Palyaris, technebuild (MAB Park Pty Ltd)