

**Springwood Development Nominees – Supermarket**

Construction of a shopping centre comprising supermarket and six (6) specialty shops, two (2) standalone shops, associated car parking and signage.

**Calton Road, Gawler East Lot 9010 CT6205/146 (Proposed lot 2046 DA 490/D026/19)**

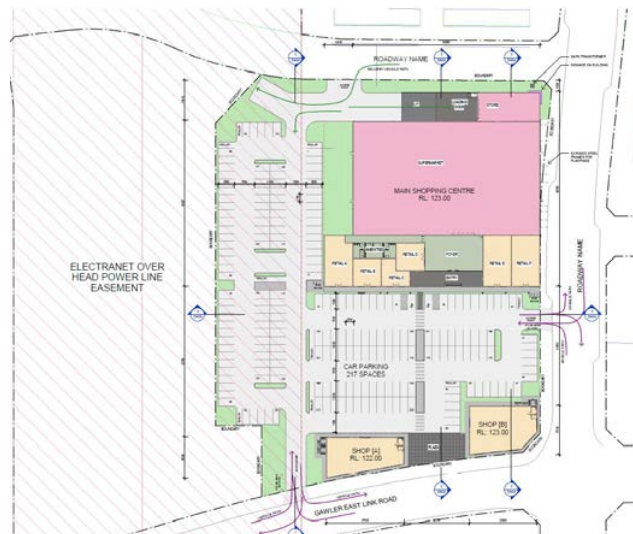
DA 490/E006/19

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3D VIEW STREET VIEW TO NORTH WEST





## OVERVIEW

<b>Application No</b>	490/E006/19
<b>Unique ID/KNET ID</b>	4285
<b>Applicant</b>	Springwood Development Nominees
<b>Proposal</b>	Construction of a shopping centre comprising supermarket and six (6) specialty shops, two (2) standalone shops, associated car parking and signage
<b>Subject Land</b>	Calton Road GAWLER EAST Lot 9010 CT 6205/146
<b>Zone/Policy Area</b>	Residential (Gawler East) Zone/Mixed Use Centre Policy Zone 3
<b>Relevant Authority</b>	State Commission Assessment Panel – Schedule 10, 20(1)
<b>Lodgement Date</b>	18/7/19
<b>Council</b>	Town of Gawler
<b>Development Plan</b>	Gawler (CT) Development Plan (consolidated 18 July 2019)
<b>Type of Development</b>	Merit
<b>Public Notification</b>	Category 2
<b>Representations</b>	Five (5) representations – four (4) to be heard
<b>Referral Agencies</b>	Nil
<b>Report Author</b>	Karen Ferguson, Senior Planning Officer
<b>RECOMMENDATION</b>	Development Plan Consent

## EXECUTIVE SUMMARY

The proposal seeks Development Plan Consent to develop a new 2,600sqm (as yet unnamed Supermarket with 6 additional tenancies and 2 additional freestanding shops, stockroom and amenities with car parking, landscaping, and signs (including two pylon sign).

The internal format or type of supermarket is to be confirmed as the applicant has advised that tenancies have not been contracted at the time of lodgement or during the assessment process. The proposed development is a merit form of development and was subject to Category 2 public notification from which five (5) representations were received including one (1) from the Town of Gawler.

The development of the site is subject to the Land Division DA 490/D026/19 approved by SCAP on 13/2/20. The land division created approximately 1200 allotments facilitating the 'village centre' allotments for the supermarket, child care centre, petrol filling station/car wash and sales centre. The Land Division is subject to Section 51 clearance.

Town of Gawler are supportive of the proposed development and resulting employment generated by the proposal, however have raised some concerns which are summarised in section 4 of this report.

On balance, it is considered that the proposed shopping centre comprising supermarket and six (6) specialty shops, two (2) standalone shops, associated car parking and signage is not seriously at variance with the Gawler (CT) Council Development Plan, such that it is an envisaged use in the Mixed Use Centre Policy Area 3.

Accordingly, it is recommended that the SCAP grants Development Plan Consent subject to conditions.

## **ASSESSMENT REPORT**

### **1. BACKGROUND**

#### **1.1 Strategic Context**

The site area comprises an area of 1.144ha in size

The site is located with the Residential (Gawler East) Zone– Mixed Use Centre Policy Area 3 of the Gawler (CT) Development Plan. The relevant provisions in this Zone (and Policy Area) envisage a functional and diverse zone accommodating a mix of commercial, retail, recreation, community, residential, office, consulting rooms and educational uses.

From a strategic perspective, the nature of the proposal is consistent with the Zoning listed within the Development Plan.

### **2. DESCRIPTION OF PROPOSAL**

Application details are contained in the ATTACHMENTS.

The site forms part of the village centre as part of the Springwood development. The village centre includes proposals for a petrol filling station, car wash, child care centre and sales centre.

Details of the proposal are provided below:

<b>Land Use Description</b>	Construction of a Shopping Centre and six (6) specialty shops, two standalone shops and associated car parking and landscaping. Three internally illuminated signs and two pylon signs, (not staged).
<b>Building Height</b>	4 storey (4.4 – 6.2m)
<b>Site Access</b>	From Gawler East Link Road (under construction) and a yet to be constructed public road.
<b>Car and Bicycle Parking</b>	217 car parks (7 below the required 224) 20 bicycle parks (there are no specific Bicycle parking rates in the development plan; 20 spaces is considered reasonable)
<b>Encroachments</b>	An Electranet overhead powerline easement affects the western part of the site occupied by car parking.
<b>Staging</b>	No Staging is proposed

### **3. SITE AND LOCALITY**

#### **3.1 Site Description**

*The site consistent of one allotment, described as follows:*

<b>Lot No</b>	<b>Plan no</b>	<b>Street</b>	<b>Suburb</b>	<b>Hundred</b>	<b>Title</b>
9010	DP 114845	Calton Road	Gawler East	Barossa	CT 6205/146

The subject site is located on the northern side of the Gawler East Link Road (under construction) in the Springwood development in Gawler East which is also partially developed. A village centre is proposed to support the surrounding residential area including this modestly sized supermarket. The supermarket is sized not to compete with the retail centre that is the main street of Gawler located several kilometres to the north-west of the site.

The site was previously used as a quarry and may contain areas of site contamination. In having regard to the potential contamination on the site, a condition of approval was placed on the land division consent (DA DA 490/D026/19, requiring the applicant to undertake a site contamination audit by an accredited auditor before the issue of titles for the site. A condition of approval relating to contamination is therefore not required.

The site is undulating due to the previous land use disturbance and also includes uncontrolled fill. It also includes some vegetation including two (2) regulated trees which will be removed as part of the land division application.

### **3.2 Locality**

To the north of the site is established residential areas of Gawler. To the east is the newly developed area of residential development for Springwood.

The subject site forms part of an old sand quarry and further to the south of the subject land is Spring Creek with heavily sloping land. Spring Creek drains to the west joining the South Para River and thence to the Gawler River which flows to the coast at Buckland Park. Land to the immediate south is currently vacant and will be developed for residential use as part of the Springwood development.

## **4. COUNCIL COMMENTS or TECHNICAL ADVICE**

### **4.1 Town of Gawler**

The Town of Gawler has indicated that it is supportive of the proposed development in –principle, subject to provision of appropriate allotments, local road access and associated site levels have been established. Further information is contained within the ATTACHMENTS.

The Council has indicated that the proposal is supported by the intent of the Mixed Use Centre Policy Area and that the scale of the retail facility is in keeping with the 10,000 sqm capacity (the proposed Supermarket is 2600sqm plus 780sqm for specialty shops and 680sqm for standalone shops) provided for within the Desired Character Statement of the Policy Area.

Council has requested further information in regards to site levels and cut/fill. The applicant has provided an adequate response, which is contained in the ATTACHMENTS. Furthermore, a condition of approval is attached to the consent requiring the applicant to prepare a 'Site Works Plan' to the satisfaction of the relevant authority.

In terms of design, Council is generally supportive although considers that improvements to the east elevation could assist in softening the bulk of the supermarket building. A pedestrian link from the eastern roadway to the shopping centre would assist in pedestrian movements and complement the design of the development.

The loading dock location is supported by the council, however, this area could be improved by better landscaping screening. In addition, Council has requested that consideration be given to including improved pedestrian linkage on site. A condition of approval is attached, requiring the applicant to prepare a detailed landscape plan to the satisfaction of the relevant authority.

The design and connectivity proposed with the two standalone shops is supported. The car parking is generally well screened.

Whilst the applicant has noted that a site specific stormwater management plan will be prepared for the subject development, the Council has advised that a concept plan showing a high level method of stormwater management, (including provision for WSUD principles) should be provided by the applicant

Council has advised that the anticipated traffic generation from the proposed development can be accommodated via the proposed road network. Internal movements within the site are considered to be appropriate. Should there be any requirement to modify the proposed road network (Gawler East Link Road / local roads) to accommodate access/egress into the subject site, a condition of approval is attached requiring the applicant enter into an agreement with the Town of Gawler, detailing works required to be undertaken.

Car parking proposed is in a manner consistent with Australian Standards. The Development Plan identifies a car parking rate of 5.5 spaces per 100sqm, requiring 224 spaces in the case of this proposal. 217 spaces are to be provided giving a shortfall of 7 car spaces. The Council considers the shortfall can be accommodated within the adjoining road network and the minor shortfall is considered to be acceptable.

Council is supportive of the indicative landscape plan and the proposed species selection. A condition of approval relating to landscaping is included to ensure a final landscaping plan is provided.

The Council has requested further information on the proposed signage on the supermarket although it is generally supportive of the flat wall signage. Council indicated that of the 2 pylon signs (6m and 8m high) the 8m high sign does not suit the advised 6m limit expressed in Table Ga/4 of the Development Plan. The applicant has subsequently reduced the height of one of the pylon signs to 6m and no further action is required.

The Council also raised the issue of the proximity of the site to both the Electranet transmission line corridor and the proximity of the SEAGas pipeline. The proposed car park for the Shopping Centre includes part of the Electranet easement, however no structures are to be built within the easement. This is considered satisfactory.

The location of the SEAGas pipeline is well to the south of the proposed supermarket and was considered extensively during the assessment process for the land division application (now with Development consent). The Supermarket site is separated from the SEAGas pipeline by potential residential development and the proposed sales centre development.

The referral comments from Council are contained in the ATTACHMENTS.

#### **4.2 Response from the Applicant to Council comments**

The Town of Gawler has provided a comprehensive assessment of the proposed development. While the Council is generally supportive of the proposed supermarket and speciality shops, it has suggested a number of potential amendments which have been summarised and addressed below:

- *Earthworks and Retaining* –Council has noted that a site civil plan has not been provided with the application and has further noted that there may be a need for additional earthworks and retaining walls on the site associated with the land division and GELR. In response, the applicant advised that the associated land division has now been approved which means that the site levels for the supermarket will be determined by the works required to implement the land division and associated infrastructure. This includes the vehicular access arrangements.

A condition of approval is attached, requiring the applicant to prepare a 'Site Works Plan' to the satisfaction of the State Commission Assessment Panel.

- *Feature steel frames* –Council has requested additional clarification on the detail of the steel feature frames on the eastern side of the supermarket. In response, that the applicant has advised that the Site Plan has been updated to show the steel feature frames. On this basis, we consider that sufficient information has been provided.

No further action is required.

- *Additional pedestrian link* –Council has suggested that an additional pedestrian link could be provided to the eastern roadway. However, we note that the plans already show a pedestrian linkage from the eastern boundary (Gawler East Link Road) which runs along the front of the shops. For this reason, another pedestrian linkage is not considered warranted.

No further action is required.

- *Treatment of the loading dock* –Council is supportive of the location of the loading dock but has suggested that additional landscaping be provided along with greater pedestrian linkage. In response we note that a Landscaping Plan was provided which shows that the area to the north of the loading dock will be screened with dense plantings. For this reason, additional screening is not warranted. Also, we do not consider that an additional pedestrian linkage through this area would be appropriate given the heavy vehicle movements that will occur on this portion of the site;

No further action is required.

- *Stormwater* – in response to the Council's queries about stormwater, WGA has now prepared a Stormwater Management Plan which has been submitted to SCAP. This Plan shows how stormwater would be managed in accordance with the arrangements agreed with DPTI;

No further action is required. Further, the applicant is obligated to obtain authorisation from the Council under section 221 of the *Local Government Act 1999*, in relation to works in the road reserve and connection to Council infrastructure.

- *Traffic* –Council has requested that additional details be provided in relation to the site levels of adjoining roads. Given that the associated land division has now been approved, the site levels will be governed by the works required to implement the land division and associated infrastructure. This includes the vehicular access arrangements.

A condition of approval is attached, requiring the applicant to prepare a 'Site Works Plan' to the satisfaction of the State Commission Assessment Panel.

- *Car parking* –Council has suggested that additional parking for those with a disability should be provided near the two standalone speciality shops. We note that this is a minor matter which can be dealt with during the detailed design stage and the Building Rules Consent process.

No further action is required.

- *Bicycle parking* – as suggested by the Council, bike parking has been identified on the amended plans.

No further action is required.

- *Landscaping* –Council has suggested that additional landscaping could be provided to further screen the loading dock and also within the car park. In response, we note that



a 4m wide landscaped strip will be provided to the north of the loading dock. In addition, substantial landscaping is provided throughout the car park in the form of trees, shrubs, grasses and groundcovers.

A condition of approval is attached, requiring the applicant to prepare a detailed landscaped plan to the satisfaction of the relevant authority.

- *Signage* – in response to the Council's concerns regarding the height of pylon sign at the southern entrance to the car park, its height has been reduced down to 6m; and
- *Infrastructure provision* – the infrastructure noted by the Council will generally be established via the recently approved land division.

No further action is required.

## 5. STATUTORY REFERRAL BODY COMMENTS

No mandatory referrals were required pursuant to Schedule 8 of the *Development Regulations 2008*.

## 6. PUBLIC NOTIFICATION

Comprising a Category 2 form of development, the application underwent public consultation as prescribed by Section 38(4) of the *Development Act 1993*.

Principle of Development Control (PDC) 38 of the Residential (Gawler East) Zone provides the following guidance in relation to public notification:

*PDC 38 Categories of public notification are prescribed in Schedule 9 of the Development Regulations 2008. Further, all development listed within Principle of Development Control 1 of the Residential (Gawler East) Zone, Mixed Use Policy Area 3 and/or Local Centre Policy Area 19 are designated category 2 (except where the development is classified as Category 1 or non-complying).*

Schedule 9 of the *Development Regulations* does not specifically assign a category of notification for a supermarket and speciality shops in the Residential (Gawler East) Zone. Therefore, it is necessary to refer to the lists of envisaged forms of development contained within PDC 1 of the Zone and PDC 1 of the Policy Area to determine the correct category of notification. Both of these PDCs list a 'shop' as an envisaged form of development and PDC 1 of the Policy Area lists a 'supermarket' as an envisaged form of development.

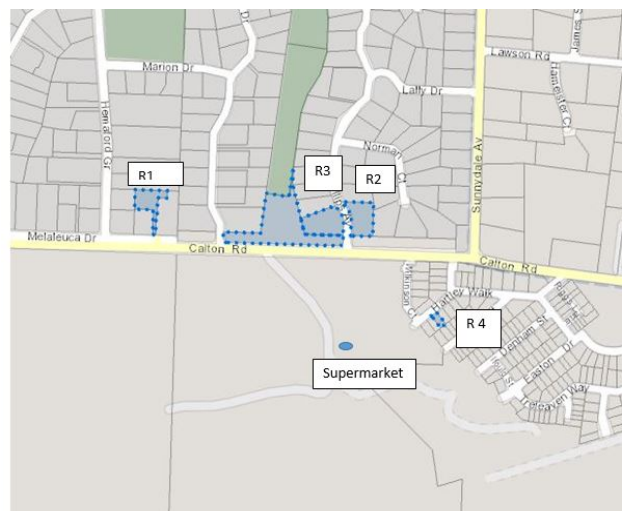
Therefore, the proposal constitutes a 'Category 2' form of development. The Development Plan further stipulates that any development not listed as Category 1 for public notification purposes is Category 2.

Public notification was undertaken (by directly contacting adjoining owners and occupiers of the land) and four (4) representations were received.

Representor ID	Issue	Applicant's Response
R1	<i>Gawler already has a significant number of shopping centres – many individual shops are vacant and we do not need more competition for the existing retail spaces, which are in decline due to the rise of online</i>	<i>The proposal is intended to serve the retail needs of the existing and future population of Springwood (potentially 6,000 people). Policy in the Development Plan includes the provision of a retail facility up to 10,000sqm with a ranges of shops, cafes, restaurants etc. The proposal</i>

<b>Representor ID</b>	<b>Issue</b>	<b>Applicant's Response</b>
	<i>shopping. This is just more development for the sake of it.</i>	<i>is for retail development less than 10,000 sqm</i>
<i>R2</i>	<i>Increased traffic, noise pollution, visual impact, lighting, proposed road changes. Traffic issues re vehicles, pedestrians, odour, increased crime, signs landscaping</i>	<i>The Gawler East Link Road is currently under construction will have the capacity of up to 12,000 vehicles per day. CIRQA has concluded forecast movements from the Supermarket/shops can be accommodated. It is considered that any noise generating activities are located away from sensitive receivers and also buffered by other land uses such as the petrol station and car wash. In terms of visual impact it is considered that the design of the supermarket creates a 'village centre' atmosphere which is supported by the policies in the Mixed Use Centre Zone. Landscaping will also assist in screening the supermarket.</i>
<i>R3</i>	<i>Road access unsafe, Noise, vehicle and pedestrian traffic, signage, visual impact, lighting</i>	<i>See responses for R2 above Signage is considered to complement the design of the building and is pertinent to the land uses on the site. One of the pylon signs was reduced in height from 8m to 6m</i>
<i>R4</i>	<i>Don't know impact of traffic on my street</i>	<i>See Response for R2 above for traffic.</i>

Figure 2 – Representation Map

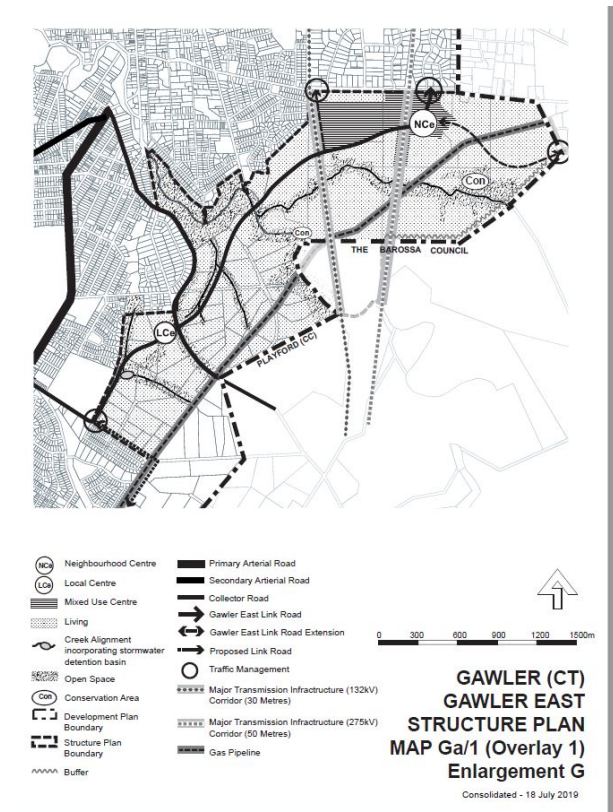


A copy of the representations and the applicant's response is contained in the ATTACHMENTS

## 7. POLICY OVERVIEW

The subject site is within the Residential (Gawler East) Zone, Mixed Use Centre Policy Area 3 as described within the Gawler (CT) Development Plan Consolidated 18 July 2019.

Relevant planning policies are contained in the Attachments summarised below.



### 7.1 Residential (Gawler East) Zone.

The Zone objectives seek to deliver a predominantly residential area comprising a range of low and medium-density dwellings with associated integrated infrastructure, retail, commercial, recreational, educational and community development in accordance with Structure Plan Map Ga/1 (Overlay 1) Enlargement G.

### 7.2 Policy Area

#### *Mixed Use Centre Policy Area 3*

The Policy area seeks to encourage a diverse zone accommodating a mix of commercial, retail, recreation, community, residential, office, consulting rooms and educational uses.

The Zone objectives also seek to ensure that development that minimises any adverse impacts upon the amenity of the locality within the policy area and contributes to the desired character.

#### *Desired Character*

The character of a 'main street' is encouraged through the desired character statement where buildings address the street and car parks are primarily located to

the rear. In order to minimise the overall extent of off street parking shared car parking is encouraged, and the convenience, availability and function of on street parking in mixed use environments will be recognised.

Commercial and mixed use buildings will be established close to the street frontage and incorporate verandas and other protruding elements in order to create a pedestrian friendly environment and outdoor dining opportunities. These areas will be enhanced by large street trees, high quality paving, lighting and street furniture. Building facades will be designed in a manner to create diversity of interest through the appearance of an aggregation of smaller buildings.

### **7.3 Council Wide**

The Council Wide provisions provide guidance as to noise, movement and parking of vehicles, parking, signage and effective design outcomes.

The relevant planning policies are contained in the ATTACHMENTS.

## **8. PLANNING ASSESSMENT**

The application has been assessed against the relevant provisions of the Gawler (CT) Development Plan, which are contained in the ATTACHMENTS.

### **8.1 Quantitative Provisions**

	<b>Development Plan Guideline</b>	<b>Proposed</b>	<b>Guideline Achieved</b>	<b>Comment</b>
Building Height (PDC 5)	Maximum of 5 Storeys in height	4 storeys proposed	YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> PARTIAL <input type="checkbox"/>	Complies with building height
Land Use (PDC 1) Desired Character Statement	Shop and supermarket A retail facility no exceeding 10,000 sqm for a supermarket and a range of specialty shops/restaurants/cafes/hotel	GLA of 2600m <sup>2</sup> for supermarket 780m <sup>2</sup> for specialty shops and 680m <sup>2</sup> for standalone shops. No residential development is proposed.	YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> PARTIAL <input type="checkbox"/>	Shop is envisaged land use within the zone and policy area, GLA is under what is envisaged for a single building
Car Parking (Table Pr/5 Rates)	5.5 spaces per 100m <sup>2</sup> = 224	217 (shortfall of 7 parks)	YES <input type="checkbox"/> NO <input type="checkbox"/> PARTIAL <input checked="" type="checkbox"/>	Shortfall of parking rates by 7 parks
Bicycle Parking	Not defined in the Development Plan	20 general use 0 employee (applicant advice that location of employee bike parking will be defined when tenant is contracted)	YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> PARTIAL <input type="checkbox"/>	Satisfies 'typical' Development Plan bike parking provisions

## 8.2 Land Use

The Residential (Gawler East) Zone seeks to secure retail, commercial, recreational, educational and community development in master-planned locations in accordance with Structure Plan Map Ga/1(overlay 1) Enlargement G.

The Mixed Use Centre Policy Area 3 incorporates a wide range of land uses that serve the local community. A Supermarket and specialty shops including the two standalone shops are considered to be in keeping with the desired character and intent of the Mixed Use Centre Policy Area 3 and complies with the following objectives:

Objective 1: A functional and diverse zone accommodating a mix of commercial, retail, recreation, community, residential, office, consulting rooms and educational uses.

Objective 3: Development that contributes to the desired character of the policy area.

The proposed use is therefore considered to be consistent with the Zone and Policy area and meets the locational requirements for commercial development as identified by the Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G.](#)

## 8.3 Design and Appearance

The proposed supermarket and shops will be sited to the eastern edge of the site on the boundary of the local road to the east with a maximum height of 6.2m. The two standalone shops face the Gawler East Link Road to the south of the site. The built form would be complementary to anticipated development within the locality and provides for a 'main street' appearance. The pedestrian links proposed are considered to complement the site.

The building's exterior would be finished in painted precast concrete panels, cream and black brick work and timber cladding. The overall design approach is positive and incorporates interesting facades with a double height entry space.

The proposed signage on the building and the pylon signs is considered to be appropriate to the size and scale of the development.

On balance, the proposed supermarket and associated shops are considered to result in a high quality design that complements the desired character of the Mixed Use Centre Policy Area 3.

## 8.4 Interface Between Land Uses

*Objective 42: Development located and designed to minimise adverse impact and conflict between land uses.*

*Objective 43: Protect community health and amenity from adverse impacts of development.*

*Objective 44: Protect desired land uses from the encroachment of incompatible development.*

The proposed development is not considered to result in adverse amenity impacts upon neighbouring residents. The site is separated from nearby sensitive uses via public roads (two unnamed roads and the Gawler East Link Road- currently under construction). The proposed supermarket would be separated from potential nearby residential development by the eastern unnamed road reserve.

In terms of operation hours, the applicant requested that no hours of operation be imposed on the operation of the supermarket either for operation or waste collection.



The licensing requirements of other legislation would control the hours of operation whilst waste collection is from the rear of the development well away from sensitive receivers. It is recommended that there is still a need to impose hours of operation for waste collection via a condition in this proposed Development Consent.

The applicant has submitted technical reports to address the traffic and noise impacts associated with the proposal. These are summarised in following sections of the report and are considered to be appropriate.

On balance it is considered that the proposed supermarket and shops results in an acceptable form of development in the Mixed Use Policy Area 3. The proposal has been designed to minimise adverse impacts to the surrounding locality.

### **9. Traffic Impact, Access and Parking (Council Wide)**

Development plan policies seek the safe and efficient movement of vehicular traffic (to and from the development site) and the adequate provision of off-street car parks (to cater for normal operational use).

**PDC 370** states that a Traffic Impact Study should be undertaken to determine the potential impact of developments on the surrounding arterial road network. Works that are required as a direct result of providing safe and efficient access to any proposed development should be provided.

The applicant has satisfied PDC 370 and provided a Traffic and Parking report prepared by CIRQA for the proposed Supermarket and shops. Further information is contained within the ATTACHMENTS.

#### Surrounding Roads

The development site is located on the northern side of Gawler East Link Road. The Gawler East Link road is under construction and is expected to have a volume in the order of 12,500 vehicles per day and a posted 50 km/h speed limit (CIRQA report)

The as yet undeveloped collector roads to the north and east of the site have indicator volumes of 6,500 vehicles per day. These roads would be under the care and control of the Town of Gawler.

#### Traffic Assessment

It is noted that the proposal would increase the number of expected traffic movements to and from the site. This requires a careful consideration of existing traffic flows on the surrounding road network (in terms of capacity constraints) and whether improvements or alternations are required to ensure current service levels are maintained (or not unduly affected).

CIRQA Traffic Engineers have confirmed that the traffic generation resulting from the proposed development is acceptable.

Furthermore, the total traffic generation associated with supermarket and shops was included as part of the previous traffic assessment prepared under Land Division (490/D026/19). This included an analysis of the infrastructure requirements to accommodate traffic movements as depicted within the masterplan into the surrounding road network.

## 10. Access (Council Wide)

**PDC 354** Development should have direct access from an all-weather public road.

**PDC 355** Development should be provided with safe and convenient access which:

- (a) avoids unreasonable interference with the flow of traffic on adjoining roads;
- (b) accommodates the type and volume of traffic likely to be generated by the development or land use and minimises induced traffic through over-provision;
- (c) is sited and designed to minimise any adverse impacts on the occupants of and visitors to neighbouring properties.

The development proposes two, two-way access points to the site. Access to the site would be provided by a local road and allow for safe and convenient access to and from the site. Additionally, access would also be provided from the proposed GELR. The Traffic and Parking Report prepared by CIRQA confirms that all vehicles would be able to enter and exit the site in a forward direction.

It is noted that the Gawler Council states that the access/egress points are of a sufficient width and configuration to facilitate two-way simultaneous turning movements.

On balance, it is considered that the proposed access/egress arrangements would not have a detrimental impact the existing/proposed road network and complies with PDC 355 above.

## 11. Parking

**PDC 362** Development should be consistent with Australian Standard AS 2890 Parking facilities.

The Development Plan identifies a rate of 5.5 spaces per 100m<sup>2</sup> floor area for shop (retail Use) in the Mixed Use Centre Policy Area 3 of the Residential Gawler East Zone). On this basis there is a requirement for 224 car spaces. The development application has indicated a provision of 217 spaces leaving a shortfall of 7 spaces. This is considered a minor shortfall for the proposed development.

The submitted Traffic and Parking Review also confirms that the proposed parking spaces will be designed to Australian Standard AS 2890 in accordance with PDC 362 above.

Overall, the parking requirements are considered to be acceptable for the site.

## 12. Environmental Factors

### 12.1 Crime Prevention

Objective 20: A safe, secure, crime resistant environment where land uses are integrated and designed to facilitate community surveillance.

There are clear sight lines through the site from the adjoining roads and footpaths. Low landscaping or trees with bare trunks will assist in providing safe walking areas around and through the site. Likewise no fencing is proposed on the perimeter of the site.

The site will be illuminated to Australian Standards and would assist in providing casual surveillance of the area.

It is considered that the supermarket/shops have been designed to facilitate a crime resistant environment.

## 12.2 Waste Management

In having regard to waste management, this would be appropriately managed on the site via a 'bin store' proposed in the loading zone area to the western side and rear of the supermarket. The bins will be emptied on an 'as needs basis' by a private contractor. The submitted CIRQA Traffic and Parking Report demonstrates that collection vehicles can be accommodated on the site.

Waste collection and deliveries are proposed to be occurring on a 24 hour basis based on email advice from ekistics (5/3/20).

It is noted that the proposed supermarket and shops are at least 150m from existing residential development with no other sensitive receivers closer than this within the locality. The loading zone is located to the rear of the supermarket. In having regard to the noise generated by waste vehicles/delivery trucks, it should have a negligible impact on current residents. However, the extent and location of future residential development in closer proximity to the supermarket is not currently known and it is suggested that conditions relating to a Waste Management Plan and Waste collection hours of operation be included in this Development consent.

## 12.3 Stormwater

A Stormwater Management Plan (SMP) was provided by the applicant from Wallbridge Gilbert Aztec consultants.

The Stormwater Plan indicates that the proposed detention basin being developed in conjunction with the Gawler East Link Road would need to be enlarged and deepened to accommodate the additional storage volume from the development.

Town of Gawler raised comments regarding the stormwater system (receiving capacity) –given that it is yet to be constructed in its final form. It is noted that the GELR is set to be completed (operation and maintenance) in the 3<sup>rd</sup> quarter of 2020. A condition is recommended to this consent to ensure that a Stormwater Management Plan be prepared for the site.

Further, the applicant is obligated to obtain authorisation from the Council under section 221 of the *Local Government Act 1999*, to appropriately dispose of stormwater from the site and connect to council infrastructure. An advisory note is included to ensure these works are undertaken should they be required.

## 12.4 Signage

*PDC 200 The location, siting, size, shape and materials of construction, of advertisements should be:*

- (a) consistent with the desired character of areas or zones as described by their objectives;*
- (b) consistent with the predominant character of the urban or rural landscape; or*
- (c) in harmony with any building or site of historic significance or heritage value in the locality.*

*PDC 201 Advertisements should not detrimentally affect by way of their siting, size, shape, scale, glare, reflection or colour the amenity of areas, zones, or localities, in which they are situated.*

*PDC 202 Advertisements should not impair the amenity of areas, zones, or localities, in which they are situated by creating, or adding to, clutter, visual disorder and the untidiness of buildings and spaces.*

*PDC 204 The scale of advertisements should be compatible with the buildings on which they are situated and with nearby buildings and spaces.*

*PDC 210 Advertisements should not obscure a driver's view of other road vehicles, of rail vehicles at or approaching level crossings, of pedestrians and of features of the road such as junctions, bends, changes in width, traffic control devices and the like that are potentially hazardous.*

*PDC 211 Advertisements should not be so highly illuminated as to cause discomfort to an approaching driver, or create difficulty in his perception of the road, or of persons or objects on it.*

The proposed development includes the following signage:

- Three flat wall signs one measuring 2.4m by 2.4m to be located on the southern elevations and one each measuring 3m by 3.6m on the east and north elevations;
- Two pylon signs at 6m high at the two road entrances to the site

The signs are likely to be internally illuminated but the content of the signs are unknown currently as tenancies have not been let.

Overall, it is considered that there is an appropriate amount of signage proposed to the Supermarket/shops facility. The signage does not impair the amenity of the locality and will not result in visual clutter on the application site.

Illumination of the signage would be controlled via condition to ensure that light intensity and overspill does not cause nuisance to adjacent occupiers or cause a distraction to drivers on adjacent public roads.

### 13. CONCLUSION

The application seeks to provide a Supermarket, specialty shops and standalone shops with associated car parking and landscaping which is considered to be an appropriate form of development within the Residential (Gawler East) Zone.

The concerns of the representors ranging from, increase in noise, traffic concerns, visual impact and signage have been satisfactorily addressed by the applicant.

After considering the site and its context, relevant technical reports from the applicant, responses from agencies, Council and the community, and the suitability of the proposed use in the Residential Gawler East Zone, the proposal displays sufficient merit to warrant approval.

### 14. RECOMMENDATION

It is recommended that the State Commission Assessment Panel:

- 1) RESOLVE that the proposed development is NOT seriously at variance with the policies in the Development Plan.
- 2) RESOLVE that the State Commission Assessment Panel is satisfied that the proposal generally accords with the related Objectives and Principles of Development Control of the Gawler Council (CT) Development Plan.

- 3) RESOLVE to grant Development Plan Consent to the proposal by Springwood Development Nominees for the construction of a shopping centre comprising supermarket and six (6) specialty shops, two (2) standalone shops, associated car parking and signage at CT 6205/146 subject to the following conditions of consent.

#### PLANNING CONDITIONS

1. That except where minor amendments may be required by other relevant Acts, or by conditions imposed by this application, the development shall be established in strict accordance with the details and plans submitted in Development Application No 490/E006/19.

*Reason for condition: to ensure the development is constructed in accordance with endorsed plans and application details.*

2. Prior to Development Approval being granted, an Engineered Site Works Plan shall be prepared with site levels and access gradients required on site, including the amount and type of fill, to the reasonable satisfaction of the State Commission Assessment Panel.

*Reason for condition: to ensure that the site can be adequately developed for the proposed land use.*

#### Landscaping

3. Prior to development approval, a detailed landscaping plan shall be prepared and submitted to the satisfaction of the relevant authority for the designated landscaped areas and include the following:
  - Landscaping improvements to on the perimeter of the site;
  - Landscaping and tree species associated with the car park;
  - The location and species to be planted and irrigation methods; and
  - Pedestrian access and egress arrangements

*Reason for condition: to ensure appropriate landscaping is provided for the subject land and maintained and nurtured at all times.*

#### Car Park / Access

4. All vehicle car parks, driveways and vehicle entry and manoeuvring areas shall be designed and constructed in accordance with Australian Standards (AS/NZS 2890.1:2004 and AS/NZS 2890.6:2009) and be constructed, drained and paved with bitumen, concrete or paving bricks in accordance with sound engineering practice and appropriately line marked to the reasonable satisfaction of the State Commission Assessment Panel prior to the occupation or use of the development.
5. All bicycle parks shall be designed and constructed in accordance with Australian Standard 2890.3-2015.
6. All car parking areas, driveways and vehicle manoeuvring areas shall be maintained at all times to the reasonable satisfaction of the State Commission Assessment Panel.
7. All materials and goods shall be loaded and unloaded within the boundaries of the subject land.



8. All loading and unloading, parking and manoeuvring areas shall be designed and constructed to ensure that all vehicles can safely enter and exit the subject land in a forward direction.
9. All access points, car parking and vehicle manoeuvring areas shall be of an all-weather surface and must be maintained in a good condition at all times.
10. The largest vehicle permitted on-site shall be restricted to a 19 metres articulated vehicle (AS 2890.2-2002).

*Reason for conditions: to ensure relevant Australian standards are met.*

11. Prior to occupation of the development, the applicant shall enter into an agreement with the Town of Gawler, detailing works required to be undertaken to the Gawler East Link Road (GELR) and any adjoining local roads.

*Reason for condition: to ensure that safe and achievable site levels can connect the development to the GELR and local roads.*

## Waste

12. Waste storage areas shall be:
  - (a) Where possible undercover or contained within the building
  - (b) Constructed or bunded to prevent the entry of external stormwater
  - (c) Constructed to drain to a stormwater treatment system/device capable of removing pollutants
13. Prior to occupation of the development, a final details of the waste collection arrangements which identify how waste would be stored, transported and disposed of, shall be submitted to the satisfaction of the State Commission Assessment Panel.
14. The hours for waste collection vehicles to enter and exit the site shall be restricted to Monday to Saturday: 7am to 7pm; and Sunday or a public holiday shall occur between 9am and 7pm.

*Reason for conditions: To ensure waste collection from the development is appropriately managed, so as to minimise impacts to on surrounding properties.*

## Lighting

15. All external lighting on the site shall be designed and constructed to conform to Australian Standard (AS 4282-1997).

*Reason for condition: to ensure external lighting does not introduce undue potential for hazards to users of the adjacent road network or adjacent properties in accordance with the necessary standard.*

## Stormwater

16. All stormwater design and construction shall be in accordance with Australian Standard AS/NZS 3500.3:2015 (Part 3) to ensure that stormwater does not adversely affect any adjoining property or public road.
17. A final detailed Stormwater Management Plan shall be submitted, in consultation with (Town of Gawler) to the satisfaction of the State Commission Assessment Panel. The details of the plan shall be incorporated within the Building Rules Consent

documentation, submitted for Development Approval, and be implemented prior to occupation or use of the development.

*Reason for conditions: to ensure stormwater infrastructure is designed and constructed to minimise potential for flood risk to adjoining property or public roads associated with stormwater runoff in accordance with the necessary standard.*

### Advertising Signage

18. The applicant shall submit, final signage design details including dimensions and specified graphics including colours and illumination levels (if necessary) to the reasonable satisfaction of the State Commission Assessment Panel.

*Reason for condition: To ensure an orderly form of development*

19. Illuminated signs shall not be of a light intensity to cause light overspill nuisance to adjacent occupiers, or cause a distraction to drivers on adjacent public roads.

*Reason for condition: to ensure illuminated signs do not cause nuisance to sensitive receivers.*

20. The advertisement(s) and support structures shall be prepared and erected in a professional and workmanlike manner and maintained in good repair at all times, to the reasonable satisfaction of the State Commission Assessment Panel.

*Reason for condition: to ensure the safety of signs and their support structures.*

21. Signage on this site visible from the adjacent roads shall not contain any element that flashes, scrolls, moves or changes, or imitates a traffic control device.

*Reason for condition: to ensure that signs do not cause a hazard for nearby traffic.*

### ADVISORY NOTES

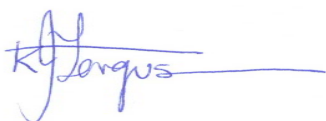
- a. This Development Plan Consent will expire after 12 months from the date of this Notification, unless final Development Approval from Council has been received within that period or this Consent has been extended by the State Commission Assessment Panel.
- b. The applicant is also advised that any act or work authorised or required by this Notification must be substantially commenced within 1 year of the final Development Approval issued by Council and substantially completed within 3 years of the date of final Development Approval issued by Council, unless that Development Approval is extended by the Council.
- c. The applicant has a right of appeal against the conditions which have been imposed on this Development Plan Consent. Such an appeal must be lodged at the Environment, Resources and Development Court within two months from the day of receiving this notice or such longer time as the Court may allow. The applicant is asked to contact the Court if wishing to appeal. The Court is located in the Sir Samuel Way Building, Victoria Square, Adelaide, (telephone number 8204 0289).
- d. The applicant is reminded of its general environmental duty, as required by Section 25 of the *Environment Protection Act 1993*, to take all reasonable and practical measures to ensure that the activities on the whole site, including during construction, do not pollute the environment in a way which causes or may cause environmental harm. Environment Protection Authority information sheets,

guidelines documents, codes of practice technical bulletins etc. can be accessed on the following web site: <http://www.epa.sa.gov.au>

- e. The applicant is reminded the emission of noise from the premises is subject to control under the *Environment Protection Act and Regulations 1993*, and the applicant (or person with the benefit of this consent) should comply with those requirements.
- f. A Construction Environment Management Plan (CEMP) shall be prepared (in consultation with the Gawler Council) for the construction phase of the redevelopment and any additional works relating to the identified contamination area will inform this CEMP. The CEMP shall be implemented in accordance with current industry standards – including the *Local Nuisance and Litter Control Act 2016*, the EPA publications “Handbook for Pollution Avoidance on Commercial and Residential Building Sites – Second Edition” to minimise environmental harm and disturbance during construction.

The management plan should incorporate, without being limited to, the following matters:

- Timing, staging and methodology of the construction process and working hours;
  - Traffic management strategies;
  - Control and management of construction noise, vibration, dust and mud;
  - Management of infrastructure services during construction and re-establishment of local amenity and landscaping;
  - Stormwater and groundwater management during construction;
  - Site security, fencing and safety and management of impact on local amenity for residents, traffic and pedestrians;
  - Disposal of construction waste, any hazardous waste and refuse in an appropriate manner according to the nature of the waste;
  - Protection and cleaning of roads and pathways;
  - Overall site clean-up;
  - Work in the public realm;
  - Hoardings; and
  - Tradesperson vehicle parking
- g. The applicant is reminded of their obligations under the *Local Nuisance and Litter Control Act 2016* and the *Environment Protection Act 1993*, with regard to the appropriate management of environmental impacts and matters of local nuisance. For further information about appropriate management of construction sites, please contact Town of Gawler.
  - h. All stormwater and wastewater disposal from the site shall be connected to Council infrastructure and be engineered to the satisfaction of the Town of Gawler.
  - i. In regards to condition 11, the applicant is to bear all costs associated with works advised within the agreement.



**SENIOR PLANNER  
PLANNING AND LAND SERVICES  
DEPARTMENT OF PLANNING, TRANSPORT and INFRASTRUCTURE**

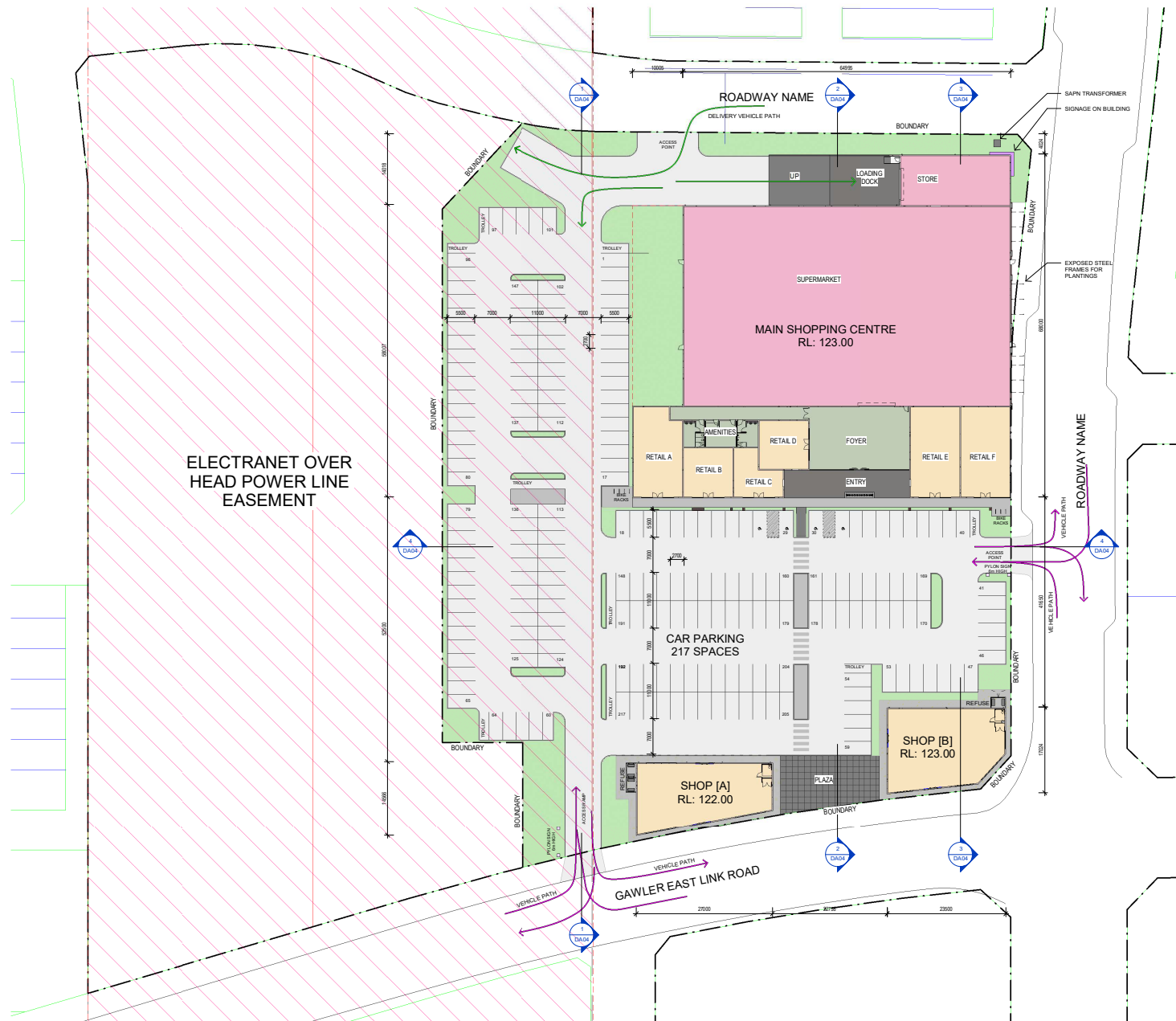


## LODGEMENT ISSUE 14 JUNE 2019

DA01	COVER SHEET & LOCATION PLAN	1
DA02	SITE PLAN	1
DA03	ELEVATIONS	1
DA04	SITE SECTIONS	1
DA06	3D IMAGES	1
DA05	LANDSCAPING PLAN	1



SITE PLAN  
1:400



# DA ISSUE

ISSUED FOR DEVELOPMENT APPROVAL  
09/03/20 09:18 PM

Rev	Description	Date
1	LODGEMENT ISSUE	14/01/19
2	MINOR AMENDMENTS	05/03/20

## SITE SUMMARY

SITE AREA	27,980m <sup>2</sup>
SUPERMARKET	2,600m <sup>2</sup>
RETAIL (A-F)	780m <sup>2</sup>
SHOP A	325m <sup>2</sup>
SHOP B	355m <sup>2</sup>
TOTAL BUILDING AREA	4,060m <sup>2</sup>
CARPARKING SPACES	217

**SPRINGWOOD**  
GAWLER EAST

**BROOK FALCONER**  
28 Chesser Street, Adelaide, South Australia 5000  
Telephone: 08 8223 5800 Facsimile: 08 8223 2440  
ABN 65 007 846 586 brookfalconer.com.au

ARCADIAN PROPERTY

SPRINGWOOD TOWN CENTRE

SITE PLAN

Scale As indicated

Drawn JSM

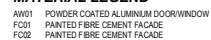
Date 16/05/2019

Job No. 20160-40

Dwg No. 2569 DA02 Rev. 2

AT SHEET



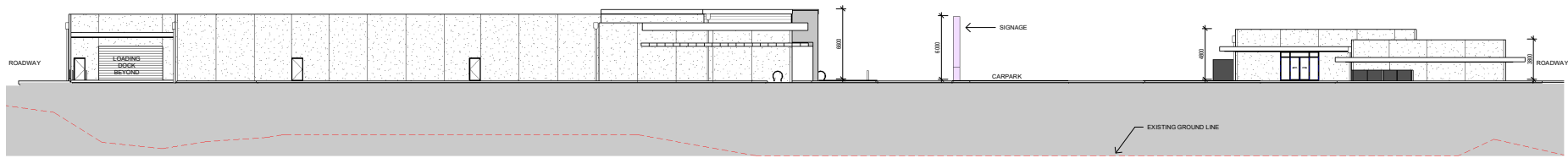


Drawn: JSM  
 Date: 16/05/2019  
 Job No.: 2016040  
 Dag No.: **2569 DA03**    Rev: **2**    A1 SHEET

# DA ISSUE

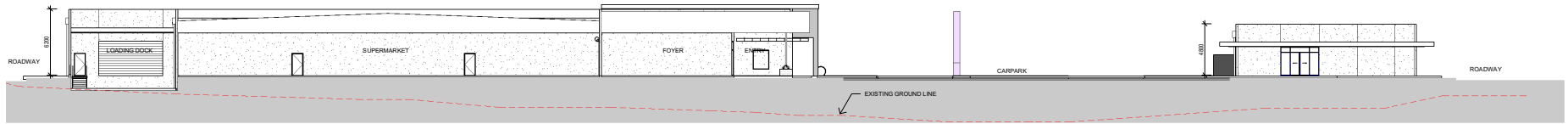
ISSUED FOR DEVELOPMENT APPROVAL  
09/09/2019 09:34 PM

Rev	Description	Date
1	LODGEMENT ISSUE	14/07/19
2	MINOR AMENDMENTS	09/03/20



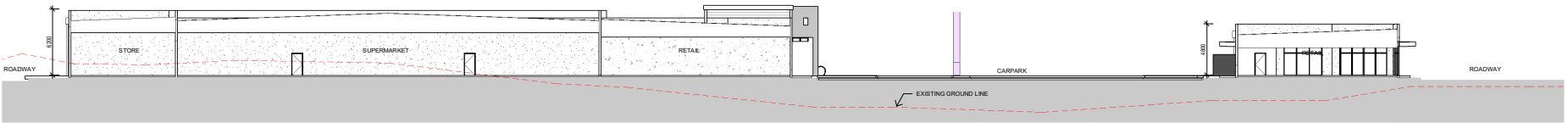
DA - Section 1

1 : 200



DA - Section 2

1 : 200



DA - Section 3

1 : 200



DA - Section 4

1 : 200

**SPRINGWOOD**  
CAWIFR FAST

**BROOK FALCONER**  
28 Chesser Street, Adelaide, South Australia 5000  
Telephone: 08 8223 5800 Facsimile: 08 8223 2440  
ABN 65 007 846 586 brookfalconer.com.au

ARCADIAN PROPERTY

SPRINGWOOD TOWN CENTRE

SITE SECTIONS

Scale 1 : 200  
Drawn JSM  
Date 16/05/2019  
Job No. 2016040  
Dwg No. 2569 DA04 Rev. 2 A1 SHEET

# DA ISSUE

ISSUED FOR DEVELOPMENT APPROVAL  
\$59,232.00 0.00 38 PM

No.	Amendment	Date
1	LODGE/MENT ISSUE	14/07/19
2	MINOR AMENDMENTS	05/03/20

## LANDSCAPE LEGEND

NEW LANDSCAPING  
WITHIN RELIEVED  
BEDS  
AREAS WITHIN  
PROPOSED SITE ONLY

NOTES  
➤ ALL LANDSCAPING TO BE IRRIGATED  
➤ MULCH TO BE INSTALLED TO ALL  
GARDEN BEDS

## TREE SCHEDULE

T1 CALLERY PEAR  
T2 JAPANESE ELM



## PLANT SCHEDULE

ALL TUBE STOCK

## BEDGES / GRASSES

AGAPANTHUS SMALL CULTIVARS	ASC	2 per m²
DISTICHLIS DISTICHOPHYLLA	DD	2 per m²
LOMANDRA CULTIVARS	LC	2 per m²
NANDINA SPP	NS	2 per m²
POA SPP	PS	2 per m²

## GROUND COVERS

ASPERULA CONFERTA	AC	2 per m²
LEPTOSPERMUM LAMBERGII	LL	2 per m²



**SPRINGWOOD**  
GAWLER EAST

**BROOK FALCONER**  
28 Chesser Street, Adelaide, South Australia 5000  
Telephone: 08 8223 5800 Facsimile: 08 8223 2440  
ABN 55 007 846 588 brookfalconer.com.au

ARCADIAN PROPERTY

SPRINGWOOD TOWN CENTRE

LANDSCAPING PLAN

Scale As indicated  
Drawn Author  
Date 06/07/19  
Job No. 20160-43  
Dwg No. 2569 DA05 Rev. 2 AT SHEET



ELECTRANET OVER  
HEAD POWER LINE  
EASEMENT

ROADWAY NAME

ROADWAY NAME

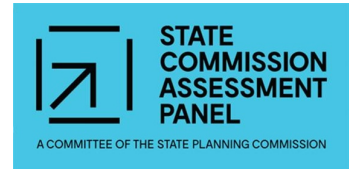
GAWLER EAST LINK ROAD

LANDSCAPING PLAN  
1:400



14/06/2019 11:36:49 AM C:\Users\wkampes\Documents\2016040 DA\_wkamps.rvt





Our Ref: 2019/09325/01  
Your Ref: 490/E006/19

Level 5, 50 Flinders Street  
Adelaide SA 5000

GPO Box 1815  
Adelaide SA 5001

Telephone: 08 7109 7060  
ABN 92 366 288 135

<http://www.saplanningcommission.sa.gov.au/scap>

"Enter today's Date ie 9 April 2018"

Salutation: Christian Name: Surname:  
Address Line 1:  
Address Line 2:  
Suburb: State: Post Code:

Dear Salutation: Surname:

**Applicant:** Springwood Development Nominees  
**Application Number:** 490/E006/19  
**Proposed Development:** Construction of a shopping centre comprising supermarket and six (6) specialty shops, two (2) standalone shops, associated car parking and signage.  
**Subject Land:** Calton Road GAWLER EAST (Proposed lot 2046 DA 490/D026/19)

As an adjoining owner/person potentially affected by the above development application, you are invited to view details of the application and make comment.

The application may be examined during normal business hours at the office of the State Commission Assessment Panel (SCAP), Level 5, 50 Flinders Street, Adelaide and at the office of Gawler Council: The application documentation is also available on the SCAP website [https://www.saplanningportal.sa.gov.au/public\\_notices](https://www.saplanningportal.sa.gov.au/public_notices).

If you wish to comment on the application please complete the attached form. This must reach the Secretary, State Commission Assessment Panel, GPO BOX 1815, Adelaide SA 5001 by no later than **Close of Business Thursday 19<sup>th</sup> December 2019**.

You may be given an opportunity to appear before the SCAP to further explain your views. You will be contacted should a hearing be arranged.

If you have any questions relating to this matter, please contact Hannah Connell of this office by telephone on 7109 7828 or email [Hannah.connell@sa.gov.au](mailto:Hannah.connell@sa.gov.au).

Yours sincerely

**Hannah Connell**  
**Project Planner**  
*as delegate of the*  
**STATE COMMISSION ASSESSMENT PANEL**

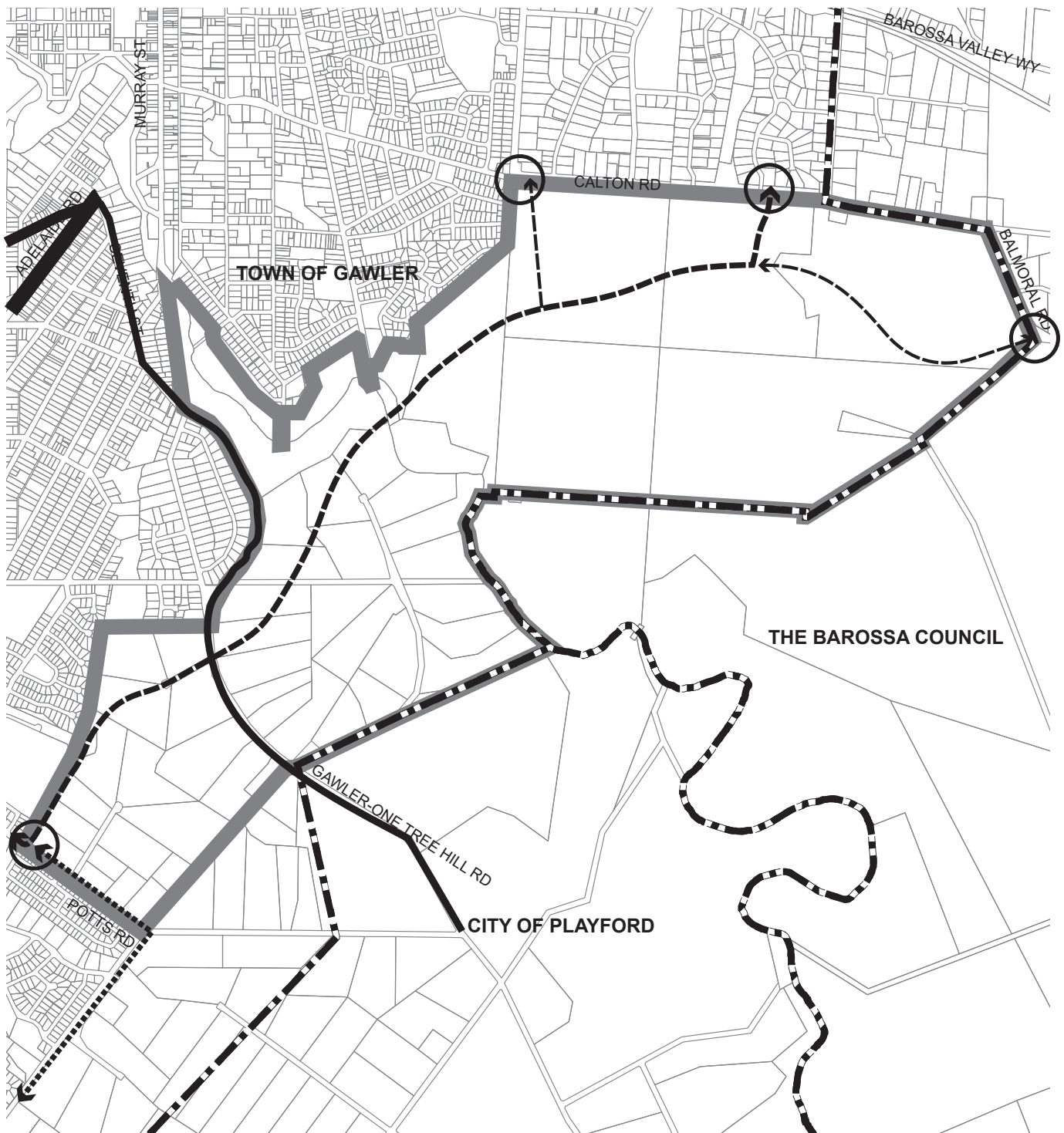
Assessment No.	Property_Address	Property_Suburb	Owner Name 1
123079	Calton RD	GAWLER EAST	H D AMES & L H BRUGGEMANN & B R
133031	Calton RD	GAWLER EAST	L H BRUGGEMANN & H D & B R AMES
144467	Calton RD	GAWLER EAST	L H BRUGGEMANN & B R & H D AMES
40551	Calton RD	GAWLER EAST	H D AMES & L H BRUGGEMANN & B R
140744	Calton RD	GAWLER EAST	
140744	Calton RD	GAWLER EAST	
40868	Calton RD	GAWLER EAST	
137532	21 Poole ST	GAWLER EAST	
140582	8 Wilkinson CT	GAWLER EAST	
140614	2 Wilkinson CT	GAWLER EAST	
140614	2 Wilkinson CT	GAWLER EAST	
140558	35 Hartley WALK	GAWLER EAST	
137549	16 Poole ST	GAWLER EAST	
40576	Calton RD	GAWLER EAST	
40584	Stithians DR	GAWLER EAST	
117594	10 Melaleuca DR	GAWLER EAST	
40592	Stithians DR	GAWLER EAST	
140574	10 Wilkinson CT	GAWLER EAST	
80737	Calton RD	GAWLER EAST	
140606	4 Wilkinson CT	GAWLER EAST	
140599	6 Wilkinson CT	GAWLER EAST	
140566	12 Wilkinson CT	GAWLER EAST	













Owner Name 2	Owner Address 1	Owner Address 2
& K D AMES	C/- William Buck	GPO Box 11050
& K D PRIESTLEY	PO Box 109	GAWLER SA 5118
& K D PRIESTLEY	C/- William Buck	GPO Box 11050
& K D AMES	63 Gawler Terrace	GAWLER SOUTH SA 5118
FIVE AMES FARMING PTY LTD	PO Box 7126	Hutt Street
FIVE AMES FARMING PTY LTD	PO Box 7126	Hutt Street
R L AMES	C/- William Buck	GPO Box 11050
L J BARASSI	C/- 70 Kent Road	PASCOW VALE VIC 3044
C D & G R WILSON	8 Wilkinson Court	GAWLER EAST SA 5118
J M LEE & T O HALL	2 Wilkinson Court	GAWLER EAST SA 5118
J M LEE & T O HALL	2 Wilkinson Court	GAWLER EAST SA 5118
G A WAGENFELLER & D E CURNOW	35 Hartley Walk	GAWLER EAST SA 5118
R C DAHLER & C A MCCUSKER	16 Poole Street	GAWLER EAST SA 5118
TOWN OF GAWLER	PO Box 130	GAWLER SA 5118
TOWN OF GAWLER	PO Box 130	GAWLER SA 5118
KONETSCHKA NOMINEES PTY LTD	10 Melaleuca Drive	GAWLER EAST SA 5118
TOWN OF GAWLER	PO Box 130	GAWLER SA 5118
J M & C V VORKAPICH	10 Wilkinson Court	GAWLER EAST SA 5118
TOWN OF GAWLER	PO Box 130	GAWLER SA 5118
B J & B J SCHMIDT	C/- Barker Real Estate Gawler	30 Krieg Road
B W BLOFFWITCH	6 Wilkinson Court	GAWLER EAST SA 5118
M W JONES & M J BOTCHEN	12 Wilkinson Court	GAWLER EAST SA 5118

Owner Address 3	Lot Number	Parcel Type	Plan	Plan Type	Title Id	Valuation No.
ADELAIDE SA 5001	4	A	28814	D	CT6162/334	4926915026
	4	A	28814	D	CT6162/334	4926915771
ADELAIDE SA 5001	4	A	28814	D	CT6162/334	4926915720
	4	A	28814	D	CT6162/334	4926915050
ADELAIDE SA 5000	9010	A	114845	D	CT6205/146	4926904407
ADELAIDE SA 5000	9011	A	114845	D	CT6186/896	4926904407
ADELAIDE SA 5001	2	A	7765	F	CT6118/249	492693901*
	446	A	111875	D	CT6170/559	4926937794
	482	A	114845	D	CT6186/880	4926905389
	485	A	114845	D	CT6186/883	4926905477
	485	A	114845	D	CT6186/883	4926905477
	462	A	114845	D	CT6186/877	492690604*
	449	A	111875	D	CT6170/560	4926938295
	805	A	36048	D	CT5111/209	4926724342
	797	A	36048	D	CT5111/207	4926739309
	19	A	81664	D	CT6051/431	4926890017
	803	A	36048	D	CT5111/208	492673935*
	481	A	114845	D	CT6186/879	4926905354
	9004	A	114845	D	CT6186/898	4939999003
	484	A	114845	D	CT6186/882	4926905442
	483	A	114845	D	CT6186/881	4926905418
	480	A	114845	D	CT6186/878	492690532*
EVANSTON PARK SA 5116						



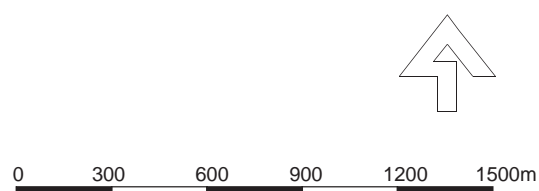
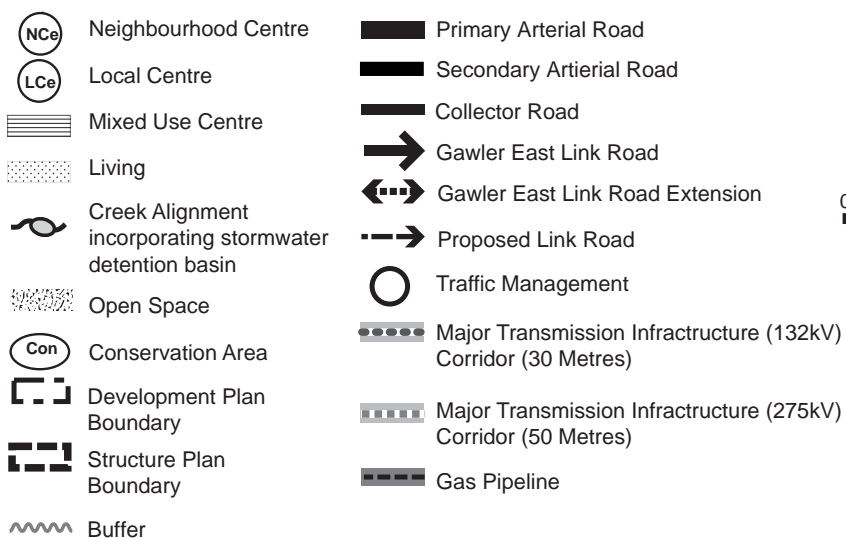
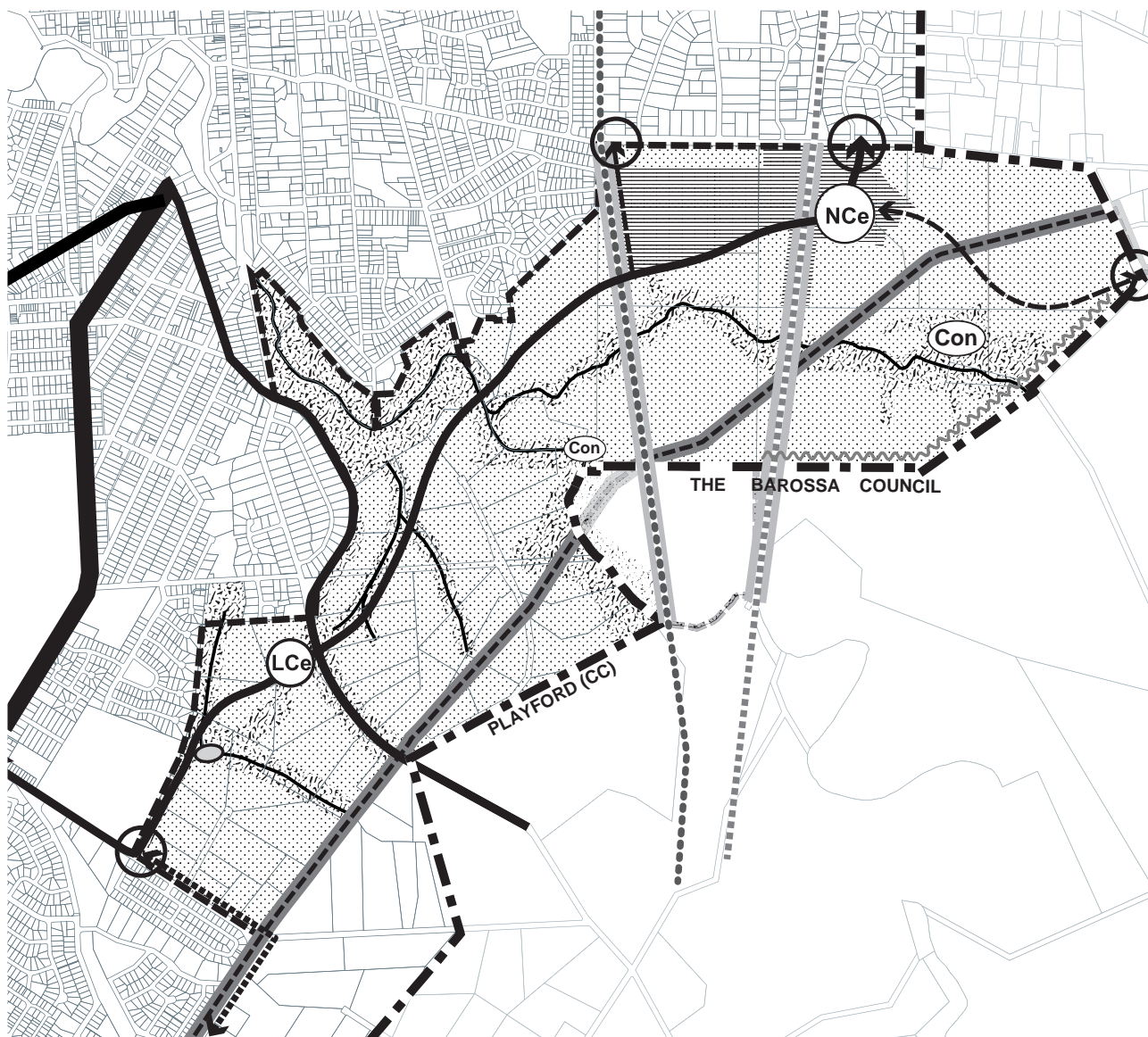


-  Concept Plan Boundary
-  Primary Arterial Road
-  Secondary Arterial Road
-  Collector Road
-  Road Link
-  Traffic Management
-  Proposed Vehicle access
-  Development Plan Boundary
-  Gawler East Link Road
-  Gawler East Link Road Extension



1:20,000  
0 250 500 750 1000 1250 metres

## GAWLER (CT) GAWLER EAST DEVELOPMENT CONSTRAINTS CONCEPT PLAN FIGURE CoP/5



# **GAWLER (CT)** **GAWLER EAST** **STRUCTURE PLAN** **MAP Ga/1 (Overlay 1)** **Enlargement G**

Consolidated - 18 July 2019



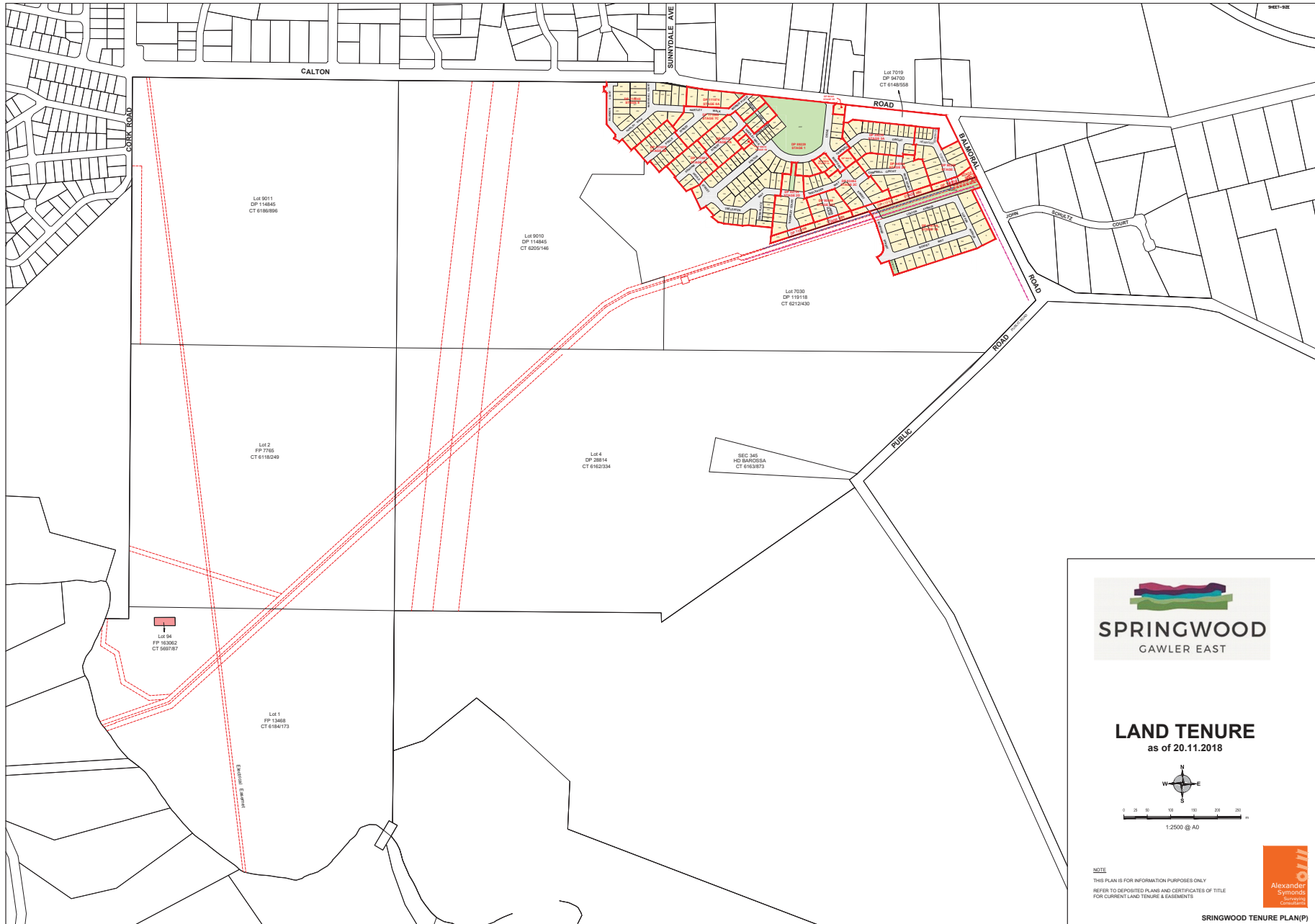
# **GAWLER (CT)** **POLICY AREAS** **MAP Ga/15**

Consolidated - 18 July 2019



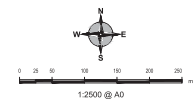


## Appendix 1. Land Tenure Plan and Certificate of Title



# LAND TENURE

as of 20.11.2018



**NOTE**  
THIS PLAN IS FOR INFORMATION PURPOSES ONLY  
REFER TO DEPOSITED PLANS AND CERTIFICATES OF TITLE  
FOR CURRENT LAND TENURE & EASEMENTS



SPRINGWOOD TENURE PLAN(P)

REAL PROPERTY ACT, 1886



The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



## Certificate of Title - Volume 6205 Folio 146

Parent Title(s)	CT 6186/895		
Creating Dealing(s)	VE 12885392		
Title Issued	26/03/2018	Edition 2	Edition Issued 06/07/2018

### Estate Type

FEE SIMPLE

### Registered Proprietor

FIVE AMES FARMING PTY. LTD. (ACN: 609 760 536)  
OF 63 GAWLER TERRACE GAWLER SOUTH SA 5118

### Description of Land

ALLOTMENT 9010 DEPOSITED PLAN 114845  
IN THE AREA NAMED GAWLER EAST  
HUNDRED OF BAROSSA

### Easements

SUBJECT TO EASEMENT(S) OVER THE LAND MARKED A ON D114845 TO THE MINISTER FOR INFRASTRUCTURE (T 1374106)

SUBJECT TO EASEMENT(S) OVER THE LAND MARKED C ON D114845 TO TRANSMISSION LESSOR CORPORATION OF 1 UNDIVIDED 2ND PART (SUBJECT TO LEASE 9061500) AND ELECTRANET PTY. LTD. OF 1 UNDIVIDED 2ND PART (T 2370109)

SUBJECT TO EASEMENT(S) OVER THE LAND MARKED E ON D114845 TO ELECTRANET PTY. LTD. (TG 12371822)

SUBJECT TO EASEMENT(S) OVER THE LAND MARKED T ON D114845 (TG 9662213)

SUBJECT TO EASEMENT(S) OVER THE LAND MARKED U ON D114845 (TG 10297076)

SUBJECT TO SERVICE EASEMENT(S) OVER THE LAND MARKED H ON D114845 FOR DRAINAGE PURPOSES TO THE COUNCIL FOR THE AREA (223LG RPA)

SUBJECT TO SERVICE EASEMENT(S) OVER THE LAND MARKED J ON F252234 FOR ELECTRICITY SUPPLY PURPOSES TO DISTRIBUTION LESSOR CORPORATION (SUBJECT TO LEASE 8890000) (223LG RPA)

SUBJECT TO SERVICE EASEMENT(S) OVER THE LAND MARKED K(T/F) ON F252234 FOR ELECTRICITY SUPPLY PURPOSES TO DISTRIBUTION LESSOR CORPORATION (SUBJECT TO LEASE 8890000) (223LG RPA)

### Schedule of Dealings

Dealing Number	Description
11764743	AGREEMENT UNDER DEVELOPMENT ACT 1993 PURSUANT TO SECTION 57A
12459905	AGREEMENT UNDER DEVELOPMENT ACT, 1993 PURSUANT TO SECTION 57(2)
12915311	MORTGAGE TO NATIONAL AUSTRALIA BANK LTD. (ACN: 004 044 937)



Product	Register Search (CT 6205/146)
Date/Time	22/08/2018 09:41AM
Customer Reference	a010816
Order ID	20180822001699
Cost	\$28.75

---

## Notations

Dealings Affecting Title	NIL
Priority Notices	NIL
Notations on Plan	NIL
Registrar-General's Notes	NIL
Administrative Interests	NIL

**DEVELOPMENT REGULATIONS 1993**  
**Form of Declaration**  
**(Schedule 5 clause 2A)**

**To:** State Commission Assessment Panel

**From:** Springwood Development Nominees

**Date of Application:** 14/06/2019

**Location of Proposed Development:** Proposed Lot 2046 in DA 490/D026/19 .....

House No: ..... Lot No: .... Street: ..... Town/Suburb: Gawler East .....

Section No (full/part): ..... Hundred: .....

Volume: ..... Folio: .....

**Nature of Proposed Development:**

Supermarket and speciality shops with associated car parking, landscaping and signage

I Warwick Mittiga .....being the applicant/  
a person acting on behalf of the applicant (delete the inapplicable statement) for  
the development described above declare that the proposed development will  
involve the construction of a building which would, if constructed in accordance  
with the plans submitted, not be contrary to the regulations prescribed for the  
purposes of section 86 of the *Electricity Act 1996*. I make this declaration under  
clause 2A(1) of Schedule 5 of the *Development Regulations 1993*.

**Date:** 14 / 6 / 19

**Signed:** .....  


**Note 1**

This declaration is only relevant to those development applications seeking  
authorisation for a form of development that involves the construction of a building  
(there is a definition of 'building' contained in section 4(1) of the *Development Act*  
*1993*), other than where the development is limited to –

- a) an internal alteration of a building; or
- b) an alteration to the walls of a building but not so as to alter the shape of the building.

Note 2

The requirements of section 86 of the *Electricity Act 1996* do not apply in relation to:

- a) a fence that is less than 2.0 m in height; or
- b) a service line installed specifically to supply electricity to the building or structure by the operator of the transmission or distribution network from which the electricity is being supplied.

Note 3

Section 86 of the *Electricity Act 1996* refers to the erection of buildings in proximity to powerlines. The regulations under this Act prescribe minimum safe clearance distances that must be complied with.

Note 4

The majority of applications will not have any powerline issues, as normal residential setbacks often cause the building to comply with the prescribed powerline clearance distances. Buildings/renovations located far away from powerlines, for example towards the back of properties, will usually also comply.

Particular care needs to be taken where high voltage powerlines exist; where the development:

- is on a major road;
- commercial/industrial in nature; or
- built to the property boundary.

Note 5

Information brochures 'Powerline Clearance Declaration Guide' and 'Building Safely Near Powerlines' have been prepared by the Technical Regulator to assist applicants and other interested persons. Copies of these brochures are available from council and the Office of the Technical Regulator. The brochures and other relevant information can also be found at [www.technicalregulator.sa.gov.au](http://www.technicalregulator.sa.gov.au)

Note 6

In cases where applicants have obtained a written approval from the Technical Regulator to build the development specified above in its current form within the prescribed clearance distances, the applicant is able to sign the form.

PLN/06/0024



# DEVELOPMENT APPLICATION FORM

PLEASE USE BLOCK LETTERS

**COUNCIL:** Town of Gawler

**APPLICANT:** Springwood Development Nominees

Postal Address: 7 Easton Drive, Gawler East  
SA 5118

**Owner:** Five Ames Farming Pty Ltd

Postal Address: 63 Gawler Terrace, Gawler South  
SA 5118

**BUILDER:** N/A

Postal Address: \_\_\_\_\_

\_\_\_\_\_  
Licence No: \_\_\_\_\_

## CONTACT PERSON FOR FURTHER INFORMATION

Name: Kieron Barnes - Ekistics Planning & Design

Telephone: 7231 0286 [work] \_\_\_\_\_ [Ah]

Fax: \_\_\_\_\_ [work] \_\_\_\_\_ [Ah]

**EXISTING USE:** Vacant

## FOR OFFICE USE

Development No: \_\_\_\_\_

Previous Development No: \_\_\_\_\_

Assessment No: \_\_\_\_\_

☐ Complying

☐ Non Complying

☐ Notification Cat 2

☐ Notification Cat 3

☐ Referrals/Concurrences

☐ DA Commission

Application forwarded to DA

Commission/Council on

/ /

Decision: \_\_\_\_\_

Type: \_\_\_\_\_

Date: / /

	Decision required	Fees	Receipt No	Date
Planning:	_____	_____	_____	_____
Building:	_____	_____	_____	_____
Land Division:	_____	_____	_____	_____
Additional:	_____	_____	_____	_____
Development Approval	_____	_____	_____	_____

**DESCRIPTION OF PROPOSED DEVELOPMENT:** Supermarket and speciality shops with associated car park, landscaping and signage

**LOCATION OF PROPOSED DEVELOPMENT:** Proposed Lot 2046 in DA 490/D026/19

House No: \_\_\_\_\_ Lot No: \_\_\_\_\_ Street: \_\_\_\_\_ Town/Suburb: Gawler East

Section No [full/part] \_\_\_\_\_ Hundred: \_\_\_\_\_ Volume: \_\_\_\_\_ Folio: \_\_\_\_\_

Section No [full/part] \_\_\_\_\_ Hundred: \_\_\_\_\_ Volume: \_\_\_\_\_ Folio: \_\_\_\_\_

## LAND DIVISION:

Site Area [m<sup>2</sup>] \_\_\_\_\_ Reserve Area [m<sup>2</sup>] \_\_\_\_\_ No of existing allotments \_\_\_\_\_

Number of additional allotments [excluding road and reserve]: \_\_\_\_\_ Lease: YES ☐ NO ☐

**BUILDING RULES CLASSIFICATION SOUGHT:** \_\_\_\_\_ Present classification: \_\_\_\_\_

If Class 5,6,7,8 or 9 classification is sought, state the proposed number of employees: Male: \_\_\_\_\_ Female: \_\_\_\_\_

If Class 9a classification is sought, state the number of persons for whom accommodation is provided: \_\_\_\_\_

If Class 9b classification is sought, state the proposed number of occupants of the various spaces at the premises: \_\_\_\_\_

**DOES EITHER SCHEDULE 21 OR 22 OF THE DEVELOPMENT REGULATIONS 2008 APPLY?** YES ☐ NO ☒

**HAS THE CONSTRUCTION INDUSTRY TRAINING FUND ACT 2008 LEVY BEEN PAID?** YES ☐ NO ☒

**DEVELOPMENT COST** [do not include any fit-out costs]: \$ 6,000,000

I acknowledge that copies of this application and supporting documentation may be provided to interested persons in accordance with the Development Regulations 2008.

**SIGNATURE:** 

Dated: 14 / 6 / 19.

**DEVELOPMENT REGULATIONS 1993**  
**Form of Declaration**  
**(Schedule 5 clause 2A)**

**To:** State Commission Assessment Panel

**From:** Springwood Development Nominees

**Date of Application:** 14/06/2019

**Location of Proposed Development:** Proposed Lot 2046 in DA 490/D026/19 .....

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**Nature of Proposed Development:**

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PLN/06/0024

# ekistics

**SPRINGWOOD  
SUPERMARKET & SPECIALTY  
SHOPS**  
Planning Statement

Prepared for:  
**Springwood Communities**

Date:  
**June 2019**

ekistics

## Proprietary Information Statement

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## Document Control

Revision	Description	Author	Date
V1	Draft Planning Statement	KB	13 June 2019
V2	Final Planning Statement	KB	14 June 2019

Approved by:



Date: 14 June 2019

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## 1. Executive Summary

Category	Details
PROJECT	Springwood Supermarket and Specialty Shops
ADDRESS OF SITE	Proposed Lot 2046 in DA 490/D026/19 – Calton Road, Gawler East
CERTIFICATE OF TITLE	Volume 6205 Folio 146 (Parent title)
SITE AREA	1.144 Hectares
FRONTAGE	50.82m to the Gawler East Link Road (under construction)
LOCAL GOVERNMENT	Town of Gawler
RELEVANT AUTHORITY	State Commission Assessment Panel
PRE-LODGE MENT PANEL MEETINGS	29 March 2019
DEVELOPMENT PLAN	Gawler (CT) (consolidated 20 February 2018)
ZONING	Residential (Gawler East) Zone
POLICY AREA/PRECINCT	Mixed Use Centre Policy Area 3
EXISTING USE	Vacant
PROPOSAL DESCRIPTION	Springwood supermarket and specialty shops with associated car parking, landscaping and signage
AGENCY REFERRALS	N/A
PUBLIC NOTIFICATION	Category 2
APPLICANT	Springwood Nominees Pty Ltd
CONTACT PERSON	Kieron Barnes – Ekistics Planning and Design – (08) 7231 0286
OUR REFERENCE	00739

## 2. Introduction/Background

Springwood is a 217.5 ha master planned community in Gawler East and Kalbeeba which, once completed, will feature approximately 1,750 residential allotments. Approximately 350 of these allotments have already been created (or are in the process of being created), while an additional 1,404 residential allotments will be created via separate land division applications that have been lodged with the State Commission Assessment Panel. In addition to the proposed residential allotments, eleven allotments will be created for commercial or educational purposes. This includes proposed Lot 2046 in DA 490/D026/19 on which a supermarket and a number of speciality shops to serve the retail needs of the Springwood development will be located.

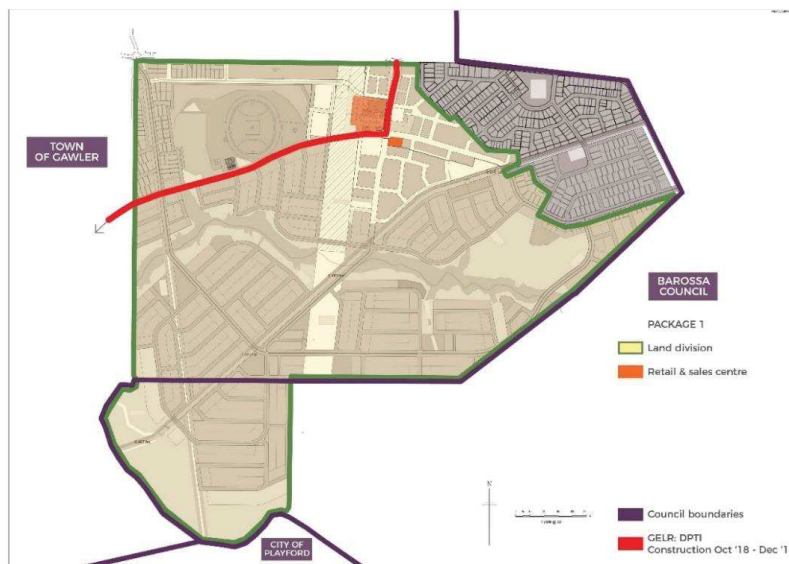
By letter dated 15 November 2018, the State Coordinator General advised that the State Commission Assessment Panel would be the relevant authority to assess all of the remaining portions of the Springwood development. This 'call-in' by the State Coordinator General was conditional on the development proceeding via a series of 'Development Packages' as follows.

- **Development Package 1** comprising the following elements:
  - » Land division for all of the remaining portions of the Springwood development (including residential areas, retail areas, community areas and school precinct);
  - » Retail area (supermarket, speciality shops and associated car parks); and

- » Sales Centre with retail component.
- **Development Package 2** comprising the following elements:
  - » Retirement village;
  - » Childcare Centre; and
  - » Additional retail.
- **Development Package 3** comprising the following elements:
  - » School precinct.

Figure 2.1 below broadly illustrates the area and scope of Development Package 1. In addition to the application for a supermarket and speciality shops, four land division applications and a development application for a Sales Centre have been lodged concurrently with SCAP.

Figure 2.1 Development Package 1



The Springwood development is proceeding in accordance with a Masterplan developed by Tract Consultants and will include a 'village centre' together with a supermarket and other speciality retail stores, various community uses and a school with large sporting grounds (see **Figures 2.2, 2.3 and 2.4**).

As a large master planned community, Springwood represents a significant economic investment and development for the Gawler Region and the State. Importantly, Springwood will result in significant employment generation and employment multipliers. It will also capitalise on significant embedded Government investment in infrastructure including the new 'Gawler East Link Road' (GELR) which will pass through the northern precinct of Springwood to service the proposed new Springwood Village Centre within which the supermarket and speciality shops will be located.

Figure 2.2 Springwood Master Plan (Tract)

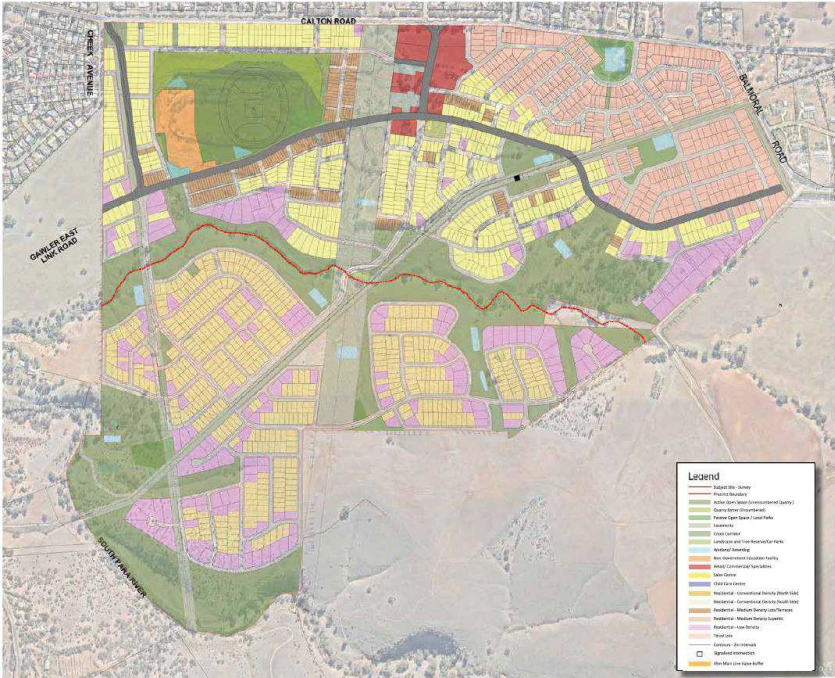


Figure 2.3 Springwood Place and Community Masterplan (Tract)

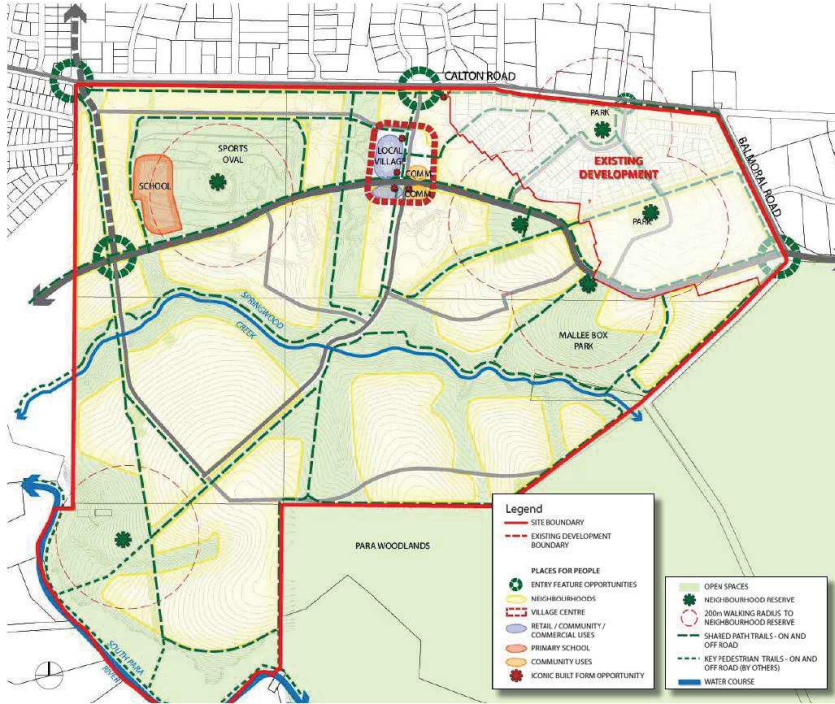




Figure 2.4 Springwood Village Centre Masterplan (Tract)



This planning statement provides information about the subject site and the proposed development while also addressing the merits of the proposal against the relevant provisions of the Gawler (CT) Development Plan – particularly the ‘Mixed Use Centre Policy Area 3’ of the ‘Residential (Gawler East) Zone’.

This planning statement has been informed by specialist advice which forms part of the application and is appended as follows:

- **Appendix 1:** Land Tenure Plan and Certificate of Title
- **Appendix 2:** Proposed Plan of Division (DA No. 490/D026/19)
- **Appendix 3:** Proposed plans and elevations Brown Falconer
- **Appendix 4:** Traffic Impact Assessment CIRQA

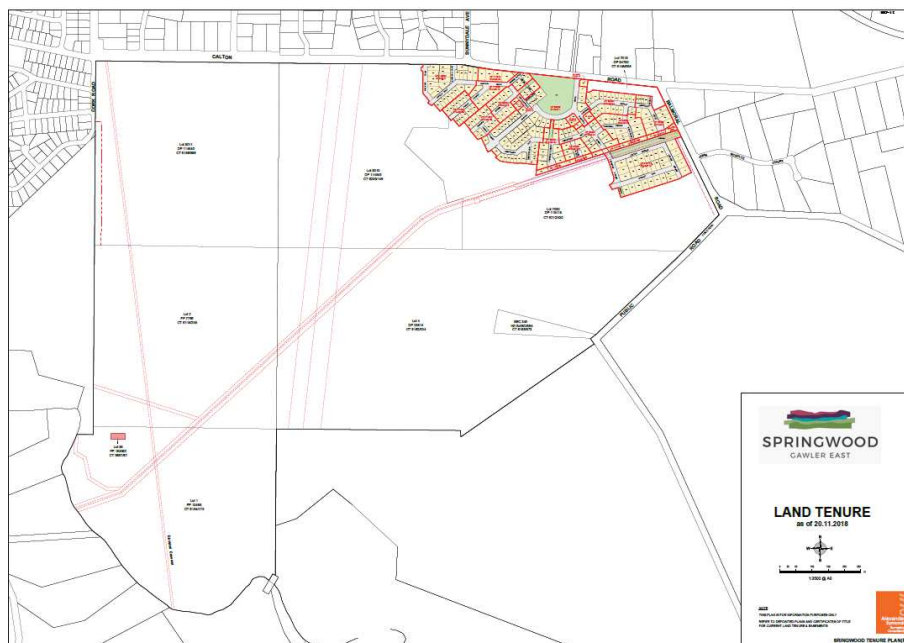
A site specific Stormwater Management Plan for the subject site will be provided by Wallbridge Gilbert Aztec (WGA). This plan will complement and build upon the overall Stormwater Management Plan for the entire Springwood development which has been submitted with the concurrent land division applications.

### 3. The Subject Site and Locality

#### 3.1 The Subject Land

Currently, the subject land forms part of Allotment 9010, Deposited Plan 114845 in Certificate of Title Volume 6205 Folio 146 (refer to **Appendix 1**). Allotment 9010 forms part of a number of titles which are being developed for the broader Springwood development (see **Figure 3.1** below and **Appendix 1**).

**Figure 3.1** Land Tenure Plan



There are a number of easements affecting the subject land including (but not limited to):

- A 275kV transmission line within a 100m wide ElectraNet easement that runs in a north-south direction;
- A DN750 MSCL potable water main, known as the Barossa Trunk Main, is located within a 10.06m wide easement running through the south-eastern corner of the land; and
- A 450mm diameter high pressure gas transmission pipeline, owned and operated by SEA Gas, is located within a 15m wide easement that runs parallel to the Barossa Trunk Main.

It is noted that a portion of the ElectraNet easement affects the western edge of the subject site. However, it is further noted that this portion of the site will only be used for car parking, vehicular access and landscaping. All the proposed buildings will be located to the east of the easement and will not compromise ElectraNet's access to its assets. This design is consistent with ElectraNet's easement requirements and the Plan of Division has previously been shared with and endorsed as a concept by ElectraNet.



### 3.2 The Subject Site

While the subject land currently forms part of Allotment 9010 in Deposited Plan 114845, the subject site comprises proposed Allotment 2046 in Development Application 490/D026/19 see **Figures 3.2** and **3.3**.

**Figure 3.2** Associated land division – subject site is Lot 2046



**Figure 3.3** Associated land division (enlargement) – subject site is Lot 2046



Proposed Lot 2046 has an area of 1.144ha and is rectangular in shape. It has a maximum depth of 142.8m and a maximum width of 85.06m.

The subject site, which previously formed part of the nearby quarry, is currently vacant. A significant amount of fill has been placed in mounds around the site as a consequence of development of earlier stages of Springwood. Some vegetation in the form of small trees, shrubs and groundcovers is present on the site. In addition, the Plan of Division prepared by Alexander Symonds identifies two Regulated Trees (shown in blue) on the site (see **Figures 3.2** and **3.3**). It is understood that these two Regulated Trees are located on uncontrolled fill and will need to be removed as part of the associated land division application to enable the construction of the supermarket to proceed.

The subject site will sit on a prominent corner at the junction of the Gawler East Link Road and the Springwood north-south connector road (see **Figure 3.4**). Given the site's prominence, the supermarket and speciality shops will be a key focal point for the 'village centre' along with the proposed Springwood Sales Centre which will be located on the opposite side of the intersection.

**Figure 3.4** Subject site





Figure 3.5 Site of the proposed supermarket and specialty shops (looking west)



Figure 3.6 Site of the proposed supermarket and specialty shops (looking north)



Figure 3.7 Site of the proposed supermarket and specialty shops (looking north)



### 3.3 The Locality and Surrounding Development

The locality features a range of land uses including the former quarry and associated activities to the immediate west and south of the site, residential development to the north and east and farmland further to the south. Generally speaking, the locality comprises land south of Calton Road which forms part of the master planned Springwood development. For this reason, the nature and character of the locality will be transformed over the coming years to create a vibrant residential community supported by a range of facilities and services within a proposed 'village centre'.

The former quarry, which forms a major element in the existing locality, was historically associated with the excavation of sand. More specifically, excavated sand was processed on-site through a washing plant where silt and clay were separated from the sand and deposited into a series of sedimentation and drying ponds. The material was periodically excavated from the pond and stockpiled on-site or sold as a product. It is understood that sand mining activities ceased operations in 2000.

Beyond the former quarry, the natural topography of the site comprises undulating hills which increase in gradient towards the South Para River which runs roughly in an east-west direction through the central portion of the Springwood development.



Major infrastructure within the locality includes:

- The Gawler East Link Road (under construction);
- An above-ground 750mm diameter Barossa Trunk Main;
- SEA Gas pipeline and associated 'Mail Line' valve; and
- 275kV overhead electricity power lines within a 100m wide easement (which partly affects the site).

Beyond the Springwood development, low-density residential development is located on the northern side of Calton Road and on the western side of Cheek Avenue. Further west is the Gawler Town Centre which features a traditional 'high street' and contains the main shopping, administrative and cultural facilities for the Council area. **Figure 3.8** identifies the key features of the locality.

**Figure 3.8** Subject Site and Surrounds

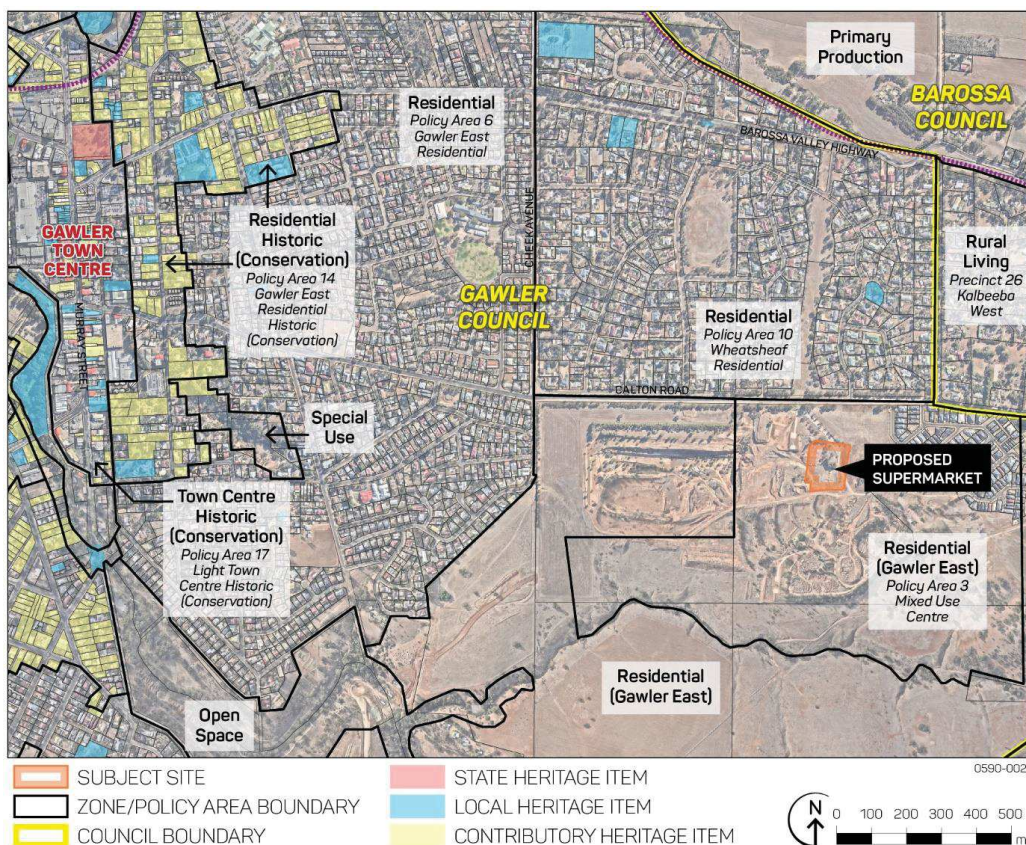


Figure 3.9 Future residential areas of the Springwood development (looking south-west)



## 4. Proposed Development

### 4.1 Land Use, Built Form, Access and Signage

The development proposes the construction of a large floorplate building which will accommodate a supermarket and six speciality shops to be located in the northern half of the subject site. Two additional smaller speciality shops will be located on the southern portion of the site fronting the Gawler East Link Road (refer to architectural plans prepared by Brown Falconer in **Appendix 3**).

The proposed development will comprise the following key elements:

- A main 'shopping centre' comprising a supermarket with a gross leasable floor area of 2,600m<sup>2</sup> and six speciality shops with a combined gross leasable floor area of 780m<sup>2</sup> which will be arranged the main entrance to the supermarket fronting the car park;
- A loading dock located at the north of the supermarket; and
- Two standalone speciality shops with a combined gross leasable floor area of 680m<sup>2</sup> which will be located along the southern boundary of the subject site.

A car park providing 217 parking spaces, including four spaces for those with a disability, will be located to the south and west of the main shopping centre. A pedestrian link will be provided through the car park between the entrance to the shopping centre and the Gawler East Link Road to the south.



Access to the car park will be provided via the Gawler East Link Road (under construction) and the proposed north-south collector road.

**Appendix 4** contains a detailed assessment undertaken by CIRQA Traffic Consultants of the access and parking arrangements for the supermarket and speciality shops. CIRQA's assessment confirms that the access points will be provided in accordance with the relevant Australian Standards and there will be sufficient car parking spaces to accommodate the various elements of the proposed development.

The proposed development also includes substantial landscaping both within the car park and around the edges of the site.

A variety of building materials will be used including painted concrete panels, face brickwork, feature stone facing and light weight timber cladding. A number of architectural features have been included in the design to create visual interest including feature steel frames and canopies.

Proposed signage includes a 6m high pylon sign at the eastern entrance to the site and an 8m high pylon sign at the southern entrance to the site from the Gawler East Link Road. Further signage identifying various tenancies will be located on the facades and walls of the building. The positioning and dimensions of the proposed signage reflects the retail use of the buildings and has been carefully designed to complement the overall architectural theme of the development.

**Figure 4.1** 3D street view to the north west (Brown Falconer Architects)





## 4.2 Stormwater Management

A site specific Stormwater Management Plan for the subject site will be provided by Wallbridge Gilbert Aztec (WGA). This plan will complement and build upon the overall Stormwater Management Plan for the entire Springwood development which has been submitted with the concurrent land division applications.

## 5. Procedural Requirements

### 5.1 Relevant Authority

By letter dated 15 November 2018 and pursuant to Schedule 10(20) of the *Development Regulations 2009*, the State Coordinator General has assigned the State Commission Assessment Panel (SCAP) as the relevant Planning Authority.

### 5.2 Nature of Development

The proposed development is perhaps best described as a 'Supermarket and specialty shops with associated car parking, landscaping and signage'.

Pursuant to the Procedural Matters section of the Residential (Gawler East Zone), the application is neither a 'Complying' nor 'Non-complying' form of development. Therefore, this Development Application is to be assessed as a '**Consent use**' on its merits against the relevant provisions of the Gawler (CT) Development Plan.

### 5.3 Public Notification

Principle of Development Control (PDC) 38 of the Residential (Gawler East) Zone provides the following guidance in relation to public notification:

**PDC 38** *Categories of public notification are prescribed in Schedule 9 of the Development Regulations 2008. Further, all development listed within Principle of Development Control 1 of the Residential (Gawler East) Zone, Mixed Use Policy Area 3 and/or Local Centre Policy Area 19 are designated category 2 (except where the development is classified as Category 1 or non-complying).*

We note that Schedule 9 of the *Development Regulations* does not specifically assign a category of notification for a supermarket and speciality shops in the Residential (Gawler East) Zone. Therefore, it is necessary to refer to the lists of envisaged forms of development contained within PDC 1 of the Zone and PDC 1 of the Policy Area to determine the correct category of notification. Both of these PDCs list a 'shop' as an envisaged form of development and PDC 1 of the Policy Area lists a 'supermarket' as an envisaged form of development. Therefore, the proposal constitutes a '**Category 2**' form of development.

### 5.4 Agency Referrals

Pursuant to Schedule 8 of the *Development Regulations*, the proposed development does not trigger any referrals to Government Agencies. However, given that SCAP is the relevant Authority, the proposal will be referred to the Town of Gawler for comments.

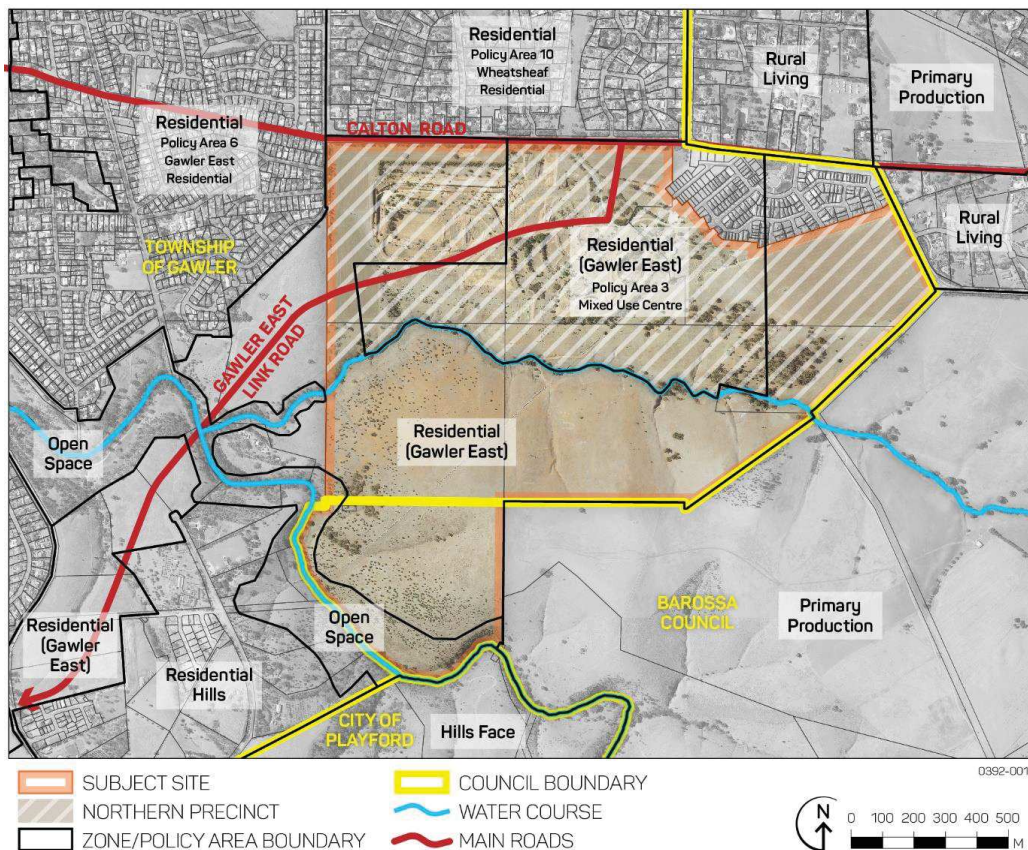
## 6. Development Plan Assessment

### 6.1 Overview

The subject site is located within the Town of Gawler and, accordingly, the relevant Development Plan is the Gawler (CT) Development Plan – consolidated 20 February 2018. More specifically, the subject site is located within the ‘Residential (Gawler East) Zone’, and the ‘Mixed Use Centre Policy Area 3’ as identified in Figure 6.1 below.

The following section provides an assessment of the proposal against the Gawler (CT) Development Plan. For convenience, this assessment has been grouped under a series of headings which reflect the key relevant planning ‘themes’ from the Development Plan.

Figure 6.1 Zoning and Policy Areas



### 6.2 Land Use

The Residential (Gawler East) Zone contemplates a wide range of land uses within broadly defined areas that are illustrated at a 'high-level' on the *Gawler East Structure Plan Map Ga/1 (Overlay 1) Enlargement G*. The

Structure Plan identifies areas that will be developed for residential uses as well as other areas that are intended to be developed as a 'Mixed Use Centre' and 'Neighbourhood Centres'. This desired mix of land uses is expressed within Objective (Obj) 1 and the Desired Character of the Zone as well as Obj 1 and the Desired Character of the Policy Area:

#### **Zone**

**Obj 1** *A predominately residential area comprising a range of low and medium-density dwellings, with associated infrastructure, retail, commercial, recreational, educational and community development in master-planned locations in accordance with Structure Plan Map Ga/1 (Overlay 1) Enlargement G.*

#### **Desired Character (extract)**

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*The zone will accommodate a diversity of housing forms. The Mixed Use Centre Policy Area 3 will comprise a mix of residential development and a range of commercial, retail, educational, recreational and community facilities. ---*

#### **Policy Area**

**Obj 1:** *A functional and diverse zone accommodating a mix of commercial, retail, recreation, community, residential, office, consulting rooms and educational uses.*

#### **Desired Character (extract)**

*Mixed Use Centre Policy Area 3 will accommodate retail, commercial, community, education and formal recreation facilities and clubrooms to service the local community.*

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*Low impact, commercial business activities that provide employment opportunities for the local population are envisaged. Such development will need to have particular regard to ensuring that minimal off-site impacts occur with respect to noise, air, water and waste emissions, commercial traffic generation and movement.*

*A retail facility, not exceeding 10 000 square metres in floor area, will comprise a supermarket and a range of specialty shops to serve the weekly shopping needs of the community. Restaurants, cafes, hotels (incorporating dining) and take away outlets are envisaged and will develop the centre as a destination point and provide uses that extend the hours of the centres operation to promote surveillance and safety of the adjacent recreation and education facilities.*

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In addition, specific guidance in relation to envisaged land uses is provided in PDC 1 of the Zone and PDC 1 of the Policy Area. Both a 'shop' and a 'supermarket' are listed as envisaged uses:

#### Zone

**PDC 1** *The following forms of development are envisaged in the zone:*

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*Non-residential use that serves the local community, for example:*

- *child care facility*
- *health and welfare service*
- *open space*
- *primary and secondary school*
- *recreation area*
- *shop, office or consulting room*

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#### Policy Area

**PDC 1** *The following forms of development are envisaged in the policy area:*

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*Shop*

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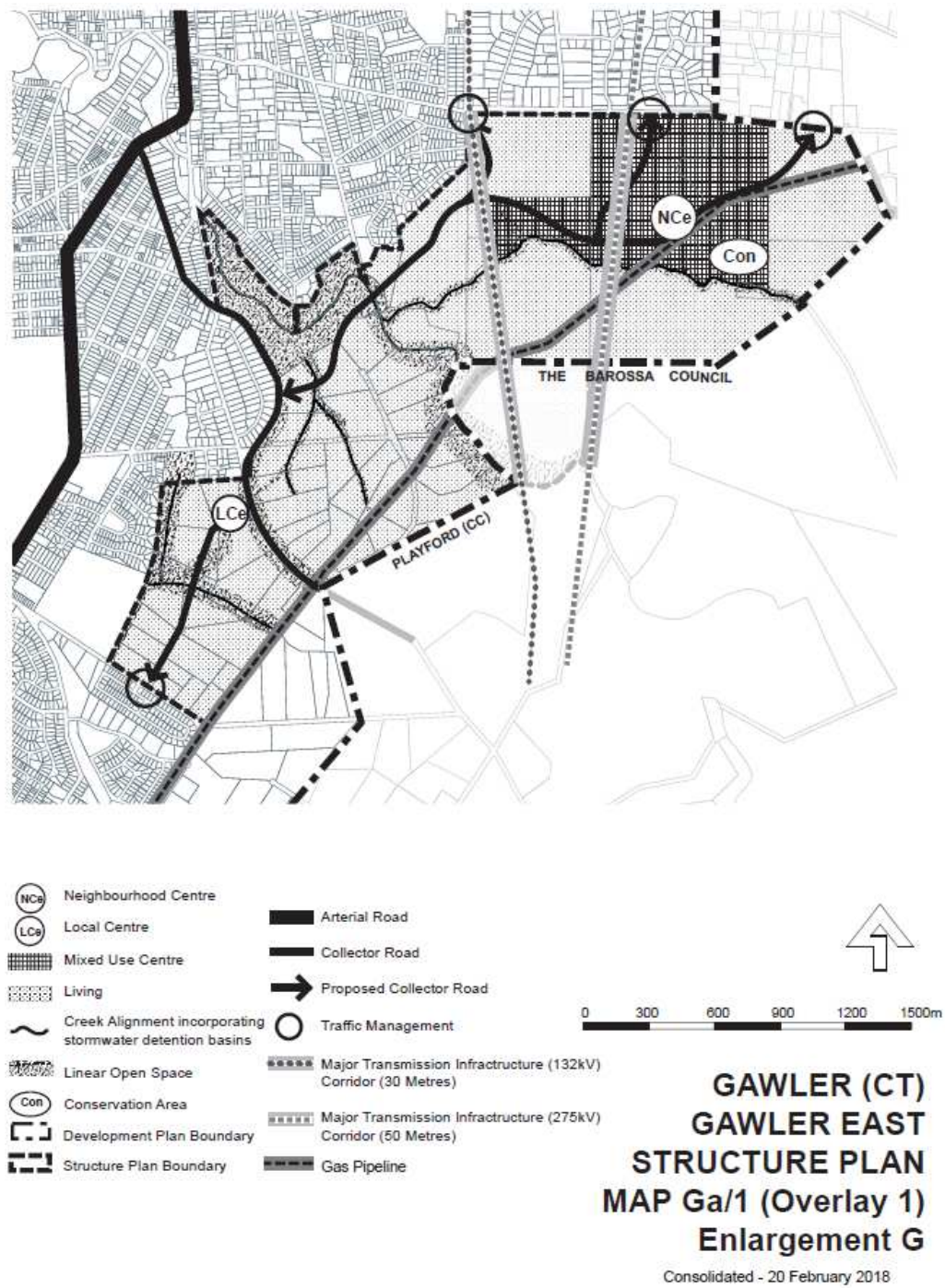
*Supermarket*

For the above reasons, the proposed supermarket and speciality shops are an appropriate land use on the subject site. In terms of the strategic location of the proposed supermarket, *Structure Plan Map Ga/1 (Overlay 1) Enlargement G* (see Figure 6.2 on the following page) provides indicative, high-level direction for development within the Springwood development. In particular, it provides indicative locations for a potential Neighbourhood Centre (see **Figure 6.2**).

The proposed supermarket and speciality shops and other proposed development in the Springwood 'village centre' are located reasonably close to the Neighbourhood Centre nominated on the Structure Plan and reflect the final alignment of the Gawler East Link Road. For this reason and noting that the Springwood 'village centre' will still be located in the 'heart' of the Mixed Use Policy Area 3, the location of the supermarket and speciality shops are generally consistent with the strategic direction provided by the Structure Plan.



Figure 6.2 Gawler East Structure Plan



### 6.3 Design and Appearance

The Development Plan contains a variety of provisions which seek to ensure that development is of a high architectural standard, complements the character of the locality and minimises any visual impact on nearby existing development. For example, the Desired Character of the Policy Area provides the following guidelines in relation to the design and appearance of development.

***Desired Character*** (extract).

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*In terms of urban design and built form, a 'main street' environment will be created, where buildings address the street and car parks are primarily located to the rear. In order to minimise the overall extent of off street parking shared car parking is encouraged, and the convenience, availability and function of on street parking in mixed use environments will be recognised.*

*Gathering points for formal and informal community events will be established, either by means of a central pedestrian plaza, a village green or series of nodes.*

*Commercial and mixed use buildings will be established close to the street frontage and incorporate verandas and other protruding elements in order to create a pedestrian-friendly environment and outdoor dining opportunities. These areas will be enhanced by large street trees, high quality paving, lighting and street furniture. Building facades will be designed in a manner to create diversity of interest through the appearance of an aggregation of smaller buildings.*

The Desired Character of the Policy Area is reinforced by the following PDCs which seek to provide more specific guidance in terms of the design and appearance of new buildings:

**PDC 3** Development should be designed to ensure that:

- (a) buildings are designed to address the street frontage with servicing areas located internal to the centre and appropriately screened from public view;
- (b) the establishment of shared car parking areas to the rear of buildings and on-street.

**PDC 4** Public space established should be activated by uses around its edges.

**PDC 5** Buildings should have a maximum of 5 storeys in height.

**PDC 6** Development should not be undertaken unless it is consistent with the desired character for the policy area.

The proposed supermarket and speciality shops directly responds to the design and appearance provisions in the following ways:

- It will assist to create a 'main street' environment through the siting of buildings close to the Gawler East Link Road and the proposed north-south collector road;



- The associated car park will generally be located to the rear of the buildings apart from a relatively small portion of car park which will be visible from the proposed north-south collector road;
- The architecturally designed buildings will address the street frontages and will incorporate "... *protruding elements in order to create a pedestrian-friendly environment and outdoor dining opportunities*";
- The façade of the building and the materials used have been designed to reflect the high-profile location of the subject site in the 'village centre' with additional visual interest created by architectural features such as feature steel frames and canopies; and
- The building will assist to activate the street frontages by providing a plaza along the Gawler East Link Road and through the inclusion of high quality paving, street furniture and landscaping.

Additional guidance for the design, appearance and function of shopping centres if provided in PDC 25 of the Council Wide section of the Development Plan:

**PDC 25** *Development or redevelopment within business, centre, shopping or mixed use zone or associated policy areas, should:*

- (a) *conform with the objectives and desired character of the zone or policy area;*
- (b) *preserve buildings of historical or architectural significance;*
- (c) *provide for the integration of existing and future facilities so as to promote ease of pedestrian movement and sharing of facilities as well as to retain the opportunity for future expansion within the zone.*
- (d) *stage development taking into consideration any future expansion of the zone, or policy area, as a whole.*
- (e) *make multiple use of facilities and share utility spaces.*
- (f) *unified design of buildings to produce a close relationship between uses in a lively setting.*
- (g) *use materials compatible with the natural features of the site and adjacent buildings.*
- (h) *be designed and laid out to avoid nuisance or hazard arising from:*
  - (i) *microclimatic conditions;*
  - (ii) *excessive noise;*
  - (iii) *odours;*
  - (iv) *overlooking;*
  - (v) *overshadowing; or*
  - (vi) *visual intrusion.*

- (i) *be designed to be compatible with existing development through:*
  - (i) *a buffer between development in the zone and adjacent areas;*
  - (ii) *landscaping which complements adjacent development and enhances the visual appearance and character of the zone;*
  - (iii) *pedestrian paths and spaces which are shaded, defined and protected from the wind; and*
  - (iv) *service yards, loading areas and outdoor storage areas which are screened;*
  - (v) *foster human scale, define spaces, reinforce paths and edges, screen utility areas, and generally enhance the visual amenity of the area.*
- (j) *ensure outdoor signs, both free-standing and attached to buildings, are located and designed in such a way as to:*
  - (i) *be in scale with the development as a whole, the building therein, and the desired character of the zone or policy area, or otherwise be compatible with the character of the locality;*
  - (ii) *not impair the view of or from nearby developments;*
  - (iii) *not distract attention from traffic control information; and*
  - (iv) *illumination from signs or floodlights should not spill over to adjacent areas.*
- (k) *provide access and car parking for residential areas located within centres separate from the access and car parking areas serving the other centre facilities.*
- (l) *integrate of public transport requirements and sheltered waiting areas for passengers.*
- (m) *provide retail showrooms for the trading of bulky goods on the periphery of centres, or in designated precincts within zones or policy areas.*
- (n) *provide:*
  - (i) *off-street loading, service areas and service vehicle manoeuvring areas;*
  - (ii) *lighting for buildings and ancillary areas, with no light-spill causing nuisance or hazard; and*
  - (iii) *unobtrusive facilities for storage and removal of waste materials;*
  - (iv) *parking, access and facilities for the disabled;*
  - (v) *public spaces such as malls, plazas and courtyards;*
  - (vi) *public facilities including toilets, infant changing facilities for parents, seating, telephones and community information boards; and*
  - (vii) *design of buildings and infrastructure to minimise energy consumption for lighting, heating, cooling and ventilation and reuse of stormwater.*

The proposed development appropriately responds to PDC 25 in the following ways:

- Through the integration of facilities across the site and the broader Springwood 'village centre' to ensure pedestrians can move between areas easily and conveniently;
- By establishing a unified design theme for the supermarket and speciality shops which has taken into consideration other proposed development in the 'village centre' and has been 'tied' together by the overall Urban Design Masterplan for Springwood;
- Through the careful design and placement of the loading dock, vehicular access and parking areas in order to minimise the potential for conflict and ensure the free movement of traffic and pedestrians through the centre; and
- Through the provision of public spaces such as the proposed plaza along the Gawler East Link Road to provide a focal point for the shopping centre and to activate the streetscape.

#### 6.4 Access and Parking

In order to address the relevant transportation and access provisions of the Development Plan, CIRQA Traffic Consultants has been engaged to review the proposed development and provide a traffic impact assessment (*Appendix 4*).

In terms of parking supply, the proposed development will provide a total of 217 onsite parking spaces which will wrap around the supermarket. This represents a slight shortfall of 7 spaces when compared against PDC 32 of the Residential (Gawler East) Zone which indicates that a shop should provide 5.5 spaces per 100m<sup>2</sup> (i.e. a gross leasable floor area of 4,060m<sup>2</sup> would theoretically require 224 spaces). However, CIRQA has referred to recent surveys of shopping centres which indicate peak parking demands are more commonly in the range of 3.0 to 5.0 spaces per 100m<sup>2</sup> which equate to a peak demand in the order of 122 to 203 spaces. On this basis, CIRQA considers that the proposal will provide sufficient parking to meet the peak demands.

In terms of the design of the car parks, the CIRQA report demonstrates that all spaces have been designed to comply with *AS/NZS 2890.1:2004* as summarised below:

- Regular parking spaces will be 2.7 m wide and 5.4 m long (or 4.8 m long with 0.6 m overhang);
- Disabled parking spaces will be at least 2.4 m wide and 5.4 m long (with an adjacent shared space of the same dimension);
- The parking aisles will exceed 6.6 m in width;
- 0.3 m clearance will be provided to all objects greater than 0.15 m in height; and
- Pedestrian sightlines will be provided at the site's property boundary.

In terms of access, the following arrangements are proposed:

- A two-way access point on the Gawler East Link Road (which will accommodate left-in, left-out, right-in and right-out movements);

- A two-way access point on the north-south collector road (which will accommodate left-in, left-out, right-in and right-out movements); and
- A two-way access point on the local road to the north which will primarily provide access for commercial vehicle movements, albeit customers will also be able to utilise the access point.

CIRQA's assessment concludes as follows:

*The proposal comprises the development of a shopping centre within the Springwood land development. Specifically, the shopping centre will include a supermarket as well as specialty tenancies.*

*The site will be well serviced by active (walking and cycling) facilities and public transport services which will minimise reliance on private motor vehicles. Access to the parking areas will be provided via the Gawler East Link Road, a new north-south collector road and a local road (adjacent the site's northern boundary).*

*The development will be serviced by parking areas containing 217 vehicle parking spaces (in total). A total of 20 bicycle spaces will also be provided. The proposed level of parking will result in a small shortfall against the requirements of Council's Development Plan. However, based on more recent surveys for shopping centres, it is considered that more than adequate parking will be provided as part of the development.*

*Traffic impacts associated with the proposal have been assessed as part of the broader land division application and planning for the Gawler East Link Road. The proposed access arrangements and associated traffic impacts will be in line with the previous assessments. Additional SIDRA modelling of the primary access points indicates that the forecast movements will be adequately accommodated.*

Based on CIRQA's assessment, the proposed development appropriately addresses the following key provisions of the Development Plan in relation to transportation and access.

**PDC 26** *Provision for the movement of people and goods within business, centre, shopping or mixed use zones or associated policy areas, should:*

- not cause inconvenient and unsafe traffic and pedestrian movements;*
- provide safe and convenient access for private cars, cyclists, pedestrians, service vehicles, emergency vehicles and public utility vehicles;*
- concentrate development for pedestrian convenience, and not extend unnecessarily along road frontages; (increasing the depth of development is a more desirable alternative);*
- separate pedestrian and vehicle movements within zones or areas, for pedestrian safety and convenience.*
- provide access points onto the arterial roads as shown on the relevant Structure Plans to: minimize traffic hazards; queuing on the roads; right turn movements; and interference with the function of intersections, junctions and traffic control devices.*

- (f) *not cause congestion or detract from the safety of traffic on abutting roads, and minimize: traffic hazards; queuing on public roads; and intrusion into adjacent residential areas.*
- (g) *provide for service vehicles and the storage and removal of waste goods and materials.*
- (h) *provide sufficient off-street parking to accommodate customer, employee and service vehicles if there is insufficient public car parking in the locality.*
- (i) *consolidate and co-ordinate parking areas into convenient groups, rather than located individually, and the access points minimized, and so located and designed that:*
  - (i) *vehicular movement between them does not require the use of public roads;*
  - (ii) *the number of access points is minimized; and*
  - (iii) *it is not necessary for vehicles to back onto public roads.*
- (j) *locate, design and orientate car parks in such a way as to facilitate safe, direct and convenient access of pedestrians between them and the facilities they serve, safe and convenient traffic circulation, minimal conflict between customer and service vehicles, and should include adequate provision for manoeuvring into and out of parking bays.*
- (k) *provide on-site parking determined by:*
  - (i) *the amount, type and timing of movement generated by the use;*
  - (ii) *the design, location and configuration of parking spaces;*
  - (iii) *the ability of the site to accommodate the parking spaces;*
  - (iv) *the potential for shared use of parking spaces; and*
  - (v) *the effect on surrounding activities and uses.*
- (l) *share use of car parking between developments to reduce the total extent of car parking areas.*
- (m) *provide for landscaping in order to screen, shade and enhance the appearance of car parking areas.*
- (n) *ensure that all sources of noise, including refrigeration and air conditioning equipment, garbage collection and car parking, do not cause excessive or disturbing noise at neighbouring properties.*

**PDC 340** *Development should provide safe and convenient access for all anticipated modes of transport including cycling, walking, public and community transport, and motor vehicles.*

**PDC 341** *Development at intersections, pedestrian and cycle crossings, and crossovers to allotments should maintain or enhance sightlines for motorists, cyclists and pedestrians to ensure safety for all road users and pedestrians.*

**PDC 355** *Development should be provided with safe and convenient access which:*

- (a) avoids unreasonable interference with the flow of traffic on adjoining roads;*
- (b) accommodates the type and volume of traffic likely to be generated by the development or land use and minimises induced traffic through over-provision;*
- (c) is sited and designed to minimise any adverse impacts on the occupants of and visitors to neighbouring properties.*

**PDC 362** *Development should be consistent with Australian Standard AS 2890 Parking facilities.*

**PDC 363** *Vehicle parking areas should be sited and designed in a manner that will:*

- (a) facilitate safe and convenient pedestrian linkages to the development and areas of significant activity or interest in the vicinity of the development;*
- (b) include safe pedestrian and bicycle linkages that complement the overall pedestrian and cycling network;*
- (c) not inhibit safe and convenient traffic circulation;*
- (d) result in minimal conflict between customer and service vehicles;*
- (e) avoid the necessity to use public roads when moving from one part of a parking area to another;*
- (f) minimise the number of vehicle access points to public roads;*
- (g) avoid the necessity for backing onto public roads;*
- (h) provide the opportunity for shared use of car parking and integration of car parking areas with adjoining development to reduce the total extent of vehicle parking areas and the requirement for access points;*
- (i) not dominate the character and appearance of a centre when viewed from public roads and spaces;*
- (j) provide landscaping that will shade and enhance the appearance of the vehicle parking areas.*

**PDC 367** *Parking areas should be sealed or paved in order to minimise dust and mud nuisance.*

**PDC 368** *To assist with stormwater detention and reduce heat loads in summer, vehicle parking areas should include soft (living) landscaping.*

**PDC 369** *Parking areas should be line-marked to indicate parking bays, movement aisles and direction of traffic flow.*



## 6.5 Crime Prevention

The Crime Prevention provisions in the Development Plan seek to ensure that new development prioritises safety and security by incorporating design features which discourage crime and facilitates community surveillance. The proposed development has responded to these provisions in the following ways:

- Through the siting of two of speciality shops, at the intersection of two collector roads where clear views into the site are available to traffic travelling along the Gawler East Link Road and the proposed north-south collector road;
- By providing large windows which will overlook the adjoining streets and car park to maximise opportunities for casual surveillance;
- By ensuring that landscaping selected for the development will be limited to low level plants to provide unobstructed sightlines;
- By avoiding the construction of boundary fencing or other structures which may obstruct sightlines or be used for concealment or entrapment spots; and
- By ensuring that the site will be illuminated in accordance with the relevant Australian Standards to maximise opportunities for casual surveillance.

For the reasons outlined above, the development is closely aligned with the following relevant Crime Prevention provisions of the Development Plan.

- Obj 20:** *A safe, secure, crime resistant environment where land uses are integrated and designed to facilitate community surveillance.*
- PDC 42** *Development should be designed to maximise surveillance of public spaces through the incorporation of clear lines of sight, appropriate lighting and the use of visible permeable barriers wherever practicable.*
- PDC 43** *Buildings should be designed to overlook public and communal streets and public open space to allow casual surveillance.*
- PDC 44** *Development should provide a robust environment that is resistant to vandalism and graffiti.*
- PDC 45** *Development should provide lighting in frequently used public spaces including those:*
- (a) *along dedicated cyclist and pedestrian pathways, laneways and access routes*
  - (b) *around public facilities such as toilets, telephones, bus stops, seating, litter bins, automatic teller machines, taxi ranks and car parks.*
- PDC 46** *Development, including car park facilities should incorporate signage and lighting that indicate the entrances and pathways to, from and within sites.*
- PDC 47** *Landscaping should be used to assist in discouraging crime by:*
- (a) *screen planting areas susceptible to vandalism*

- (b) *planting trees or ground covers, rather than shrubs, alongside footpaths*
- (c) *planting vegetation other than ground covers a minimum distance of 2 metres from footpaths to reduce concealment opportunities.*

**PDC 48** *Site planning, buildings, fences, landscaping and other features should clearly differentiate public, communal and private areas.*

**PDC 49** *Buildings should be designed to minimise and discourage access between roofs, balconies and windows of adjoining dwellings.*

**PDC 51** *Development should avoid pedestrian entrapment spots and movement predictors (eg routes or paths that are predictable or unchangeable and offer no choice to pedestrians).*

**PDC 364** *Vehicle parking areas should be designed to reduce opportunities for crime by:*

- (a) *maximising the potential for passive surveillance by ensuring they can be overlooked from nearby buildings and roads;*
- (b) *incorporating walls and landscaping that do not obscure vehicles or provide potential hiding places;*
- (c) *being appropriately lit;*
- (d) *having clearly visible walkways.*

## 6.6 Stormwater and Flooding Considerations

A site specific Stormwater Management Plan for the subject site will be provided by Wallbridge Gilbert Aztec (WGA). This plan will complement and build upon the overall Stormwater Management Plan for the entire Springwood development which has been submitted with the concurrent land division applications. In particular, the Stormwater Management Plan will address the relevant provisions of the Development Plan.





## 6.7 Signage

The architectural plans show indicative locations and proportions for signage at the main entrances to the site and on the facades and walls of the buildings. The signage reflects the retail use of the buildings and is designed to easily identify the future tenants while also responding to the overall architectural theme of the 'village centre'. The design, extent and size of the proposed signage is relatively modest and does not visually dominate the buildings on which they will be attached. Further, the proposed signage will:

- Complement the design of the building;
- Be contained within the boundaries of the site;
- Not affect existing street trees or obscure views to attractive landscaping;
- Not cause a distraction for drivers or obstruct their views of other vehicles; and
- Be limited to information relating to the proposed use of the land.

For the reasons outlined above, the nature, quantity and design of the proposed signage on the current plans is closely aligned with the following key 'Advertisements' provisions of the Development Plan including *Table Ga/4 – Advertising and Advertising Displays Definitions and Design Criteria*:

Figure 6.3 Extract from Table Ga/4

Sign Type	Criteria	
FLAT WALL SIGN	Maximum number: 1 per side wall 1 per rear wall	
	Placement and size of signs in scale and integrated with the architectural features and elements of the building gable wall.	
	Positioned not to conceal architectural features or detailing.	
		

**Obj 69:** An urban environment and rural landscape not disfigured by advertisements.

**Obj 70:** Advertisements in retail, commercial and industrial urban areas, and centre zones, designed to enhance the appearance of those areas.

**Obj 71:** Advertisements not hazardous to any person.

**PDC 199** Advertising and advertising displays should, in addition to general provisions, conform with the performance criteria in Table Ga/4.

**PDC 200** The location, siting, size, shape and materials of construction, of advertisements should be:

- (a) consistent with the desired character of areas or zones as described by their objectives;
- (b) consistent with the predominant character of the urban or rural landscape; or
- (c) in harmony with any building or site of historic significance or heritage value in the locality.

**PDC 201** Advertisements should not detrimentally affect by way of their siting, size, shape, scale, glare, reflection or colour the amenity of areas, zones, or localities, in which they are situated.

**PDC 202** Advertisements should not impair the amenity of areas, zones, or localities, in which they are situated by creating, or adding to, clutter, visual disorder and the untidiness of buildings and spaces.

- PDC 203** *Advertisements should not obscure views of attractive landscapes or particular trees or groups of trees.*
- PDC 204** *The scale of advertisements should be compatible with the buildings on which they are situated and with nearby buildings and spaces.*
- PDC 207** *Advertisements on buildings that have a single architectural theme but which contain a number of tenancies, should be attached and displayed so as to be co-coordinated with that theme.*
- PDC 209** *Advertisements should not create a hazard to persons travelling by any means.*
- PDC 210** *Advertisements should not obscure a driver's view of other road vehicles, of rail vehicles at or approaching level crossings, of pedestrians and of features of the road such as junctions, bends, changes in width, traffic control devices and the like that are potentially hazardous.*
- PDC 211** *Advertisements should not be so highly illuminated as to cause discomfort to an approaching driver, or create difficulty in his perception of the road, or of persons or objects on it.*
- PDC 212** *Advertisements should not be liable to interpretation by drivers as an official traffic sign, or convey to drivers information that might be confused with instructions given by traffic signals or other control devices, or impair the conspicuous nature of traffic signs or signals.*
- PDC 213** *Advertisements should not detract drivers from the primary driving task at a location.*
- PDC 214** *Advertisements or advertising displays should:*
- (a) convey in clear and concise language, symbols, print style, layout, and in a limited number of colours, the owner/occupier and/or generic type of business, merchandise or services;*
  - (b) be minimised in number to avoid proliferation of advertisements or advertising displays;*
  - (c) be of a form, style, scale and size and in locations appropriate to the desired character and future use of land or character of the locality or zone and to preserve reasonable exposure to the public for all adjacent sites;*
  - (d) not have an adverse impact on the amenity of adjacent land uses and areas from which they are visible and without limiting the foregoing, no bunting, whirligigs or other like displays should be displayed;*
  - (e) conceal the supporting structure from view wherever possible;*
  - (f) be completely contained within the boundaries of the subject property, and if road widening is applicable, be within the proposed property boundary realignment;*
  - (g) be unified in colour, style, placement and proportion to present a coordinated theme and design for a site, centre or building; and*
  - (h) be designed and located to avoid damage to, pruning or lopping of on-site landscaping or street trees.*

**PDC 221** *Advertisements or advertising displays should only identify premises or the nature of business, goods or services conducted on the land or building.*

## 6.8 Landscaping

A conceptual landscaping plan has been prepared for the development and is included with the architectural plans contained in **Appendix 3**.

The proposal includes landscaping around the perimeter of the site as well as within the car parking area. These landscaped areas will be planted with a range of species of trees, bushes and groundcovers which will complement the overall landscape theme of the Springwood development. More specifically, the proposed landscaping will assist to create a sense of place and a high-quality, coordinated development outcome.

Accordingly, the development satisfies PDC 25 of the Development Plan which seeks:

**PDC 25** *Development or redevelopment within business, centre, shopping or mixed use zone or associated policy areas, should:*

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- (i) *be designed to be compatible with existing development through:*
  - (i) *a buffer between development in the zone and adjacent areas;*
  - (ii) *landscaping which complements adjacent development and enhances the visual appearance and character of the zone;*
  - (iii) *pedestrian paths and spaces which are shaded, defined and protected from the wind;*  
*and*
  - (iv) *service yards, loading areas and outdoor storage areas which are screened;*
  - (v) *foster human scale, define spaces, reinforce paths and edges, screen utility areas, and generally enhance the visual amenity of the area.*

## 6.9 Interface Considerations

The subject site is located within the Mixed Use Centre Policy Area 3 which clearly anticipates a neighbourhood level centre (including a supermarket) in this area. Currently, the site and nearby land is vacant but will be developed over time to create a master-planned community comprising residential development supported by a range of non-residential land uses. On this basis, potential interface issues have been carefully considered during the preparation of the Springwood masterplan and as part of the urban design of the whole project.

Further to the above, the loading dock will be located on the northern side of the supermarket – a considerable distance away from any existing or proposed residential development. Similarly, the car park and associated access points will not face any existing or proposed residential areas. For these reasons, potential issues relating to noise, light spillage, overshadowing and overlooking will be managed appropriately. Therefore, the proposed

development appropriately responds to the following relevant interface provisions within the Development Plan:

#### Zone

**PDC 28** *Development should be designed and sited to minimise negative impact on existing and potential future land uses considered appropriate in the locality.*

#### Council Wide

**Obj 42:** *Development located and designed to minimise adverse impact and conflict between land uses.*

**Obj 43:** *Protect community health and amenity from adverse impacts of development.*

**PDC 107** *Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:*

- (a) the emission of effluent, odour, smoke, fumes, dust or other airborne pollutants;*
- (b) noise;*
- (c) vibration;*
- (d) electrical interference;*
- (e) light spill;*
- (f) glare;*
- (g) hours of operation;*
- (h) traffic impacts.*

**PDC 108** *Development should be sited and designed to minimise negative impacts on existing and potential future land uses desired in the locality.*

## 7. Conclusion

This development application seeks approval to establish a supermarket and specialty shops with associated car parking, signage and landscaping.

Having undertaken a comprehensive assessment of the application against the relevant provisions of the Gawler (CT) Development Plan, we believe that the proposal represents orderly development which accords with the relevant provisions of the Development Plan for the reasons summarised below:

- A 'supermarket' and 'shop' are envisaged forms of development in the Mixed Use Policy Area 3 of the Residential (Gawler East) Zone;
- The general area of the subject site is indicatively identified as a Neighbourhood Centre within the *Gawler East Structure Plan Map Ga/1 (Overlay 1) Enlargement G* as well as the Springwood Masterplan;



- The location and design of the proposed development will complement other proposed development in the Springwood 'village centre' such as the Sales Centre on the opposite side of the Gawler East Link Road;
- The proposed development has been designed to address both road frontages, and the building exhibits a high degree of architectural merit achieved by using a variety of complementary colours, building materials and articulation;
- A coordinated signage theme is proposed, with the indicative signage shown on the plans designed in accordance with the relevant provisions of the Development Plan;
- Large windows along the elevations will maximise opportunities for casual surveillance across the site;
- The proposed development has been designed to accommodate safe and convenient movements for vehicles, and the CIRQA report concludes that sufficient onsite parking will be provided to cater for the anticipated parking demand;
- The additional traffic volumes to be generated by the development will not have a significant impact on the road network;
- Stormwater will be managed appropriately in accordance with the overall stormwater management plan for the Springwood development which has been submitted with the land division applications; and
- The proposed landscaping will assist to create a sense of place and enhance the amenity of the locality.

Based on our assessment, the application is closely aligned within the relevant provisions of the Development Plan and therefore warrants Development Plan Consent subject to reasonable and relevant conditions.

# ekistics

**SPRINGWOOD  
SUPERMARKET & SPECIALTY  
SHOPS**  
Planning Statement

Prepared for:  
**Springwood Communities**

Date:  
**June 2019**

ekistics

## Proprietary Information Statement

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## Document Control

Revision	Description	Author	Date
V1	Draft Planning Statement	KB	13 June 2019
V2	Final Planning Statement	KB	14 June 2019

Approved by:



Date: 14 June 2019

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## 1. Executive Summary

Category	Details
PROJECT	Springwood Supermarket and Specialty Shops
ADDRESS OF SITE	Proposed Lot 2046 in DA 490/D026/19 – Calton Road, Gawler East
CERTIFICATE OF TITLE	Volume 6205 Folio 146 (Parent title)
SITE AREA	1.144 Hectares
FRONTAGE	50.82m to the Gawler East Link Road (under construction)
LOCAL GOVERNMENT	Town of Gawler
RELEVANT AUTHORITY	State Commission Assessment Panel
PRE-LODGE MENT PANEL MEETINGS	29 March 2019
DEVELOPMENT PLAN	Gawler (CT) (consolidated 20 February 2018)
ZONING	Residential (Gawler East) Zone
POLICY AREA/PRECINCT	Mixed Use Centre Policy Area 3
EXISTING USE	Vacant
PROPOSAL DESCRIPTION	Springwood supermarket and specialty shops with associated car parking, landscaping and signage
AGENCY REFERRALS	N/A
PUBLIC NOTIFICATION	Category 2
APPLICANT	Springwood Nominees Pty Ltd
CONTACT PERSON	Kieron Barnes – Ekistics Planning and Design – (08) 7231 0286
OUR REFERENCE	00739

## 2. Introduction/Background

Springwood is a 217.5 ha master planned community in Gawler East and Kalbeeba which, once completed, will feature approximately 1,750 residential allotments. Approximately 350 of these allotments have already been created (or are in the process of being created), while an additional 1,404 residential allotments will be created via separate land division applications that have been lodged with the State Commission Assessment Panel. In addition to the proposed residential allotments, eleven allotments will be created for commercial or educational purposes. This includes proposed Lot 2046 in DA 490/D026/19 on which a supermarket and a number of speciality shops to serve the retail needs of the Springwood development will be located.

By letter dated 15 November 2018, the State Coordinator General advised that the State Commission Assessment Panel would be the relevant authority to assess all of the remaining portions of the Springwood development. This 'call-in' by the State Coordinator General was conditional on the development proceeding via a series of 'Development Packages' as follows.

- **Development Package 1** comprising the following elements:
  - » Land division for all of the remaining portions of the Springwood development (including residential areas, retail areas, community areas and school precinct);
  - » Retail area (supermarket, speciality shops and associated car parks); and



- » Sales Centre with retail component.
- **Development Package 2** comprising the following elements:
  - » Retirement village;
  - » Childcare Centre; and
  - » Additional retail.
- **Development Package 3** comprising the following elements:
  - » School precinct.

Figure 2.1 below broadly illustrates the area and scope of Development Package 1. In addition to the application for a supermarket and speciality shops, four land division applications and a development application for a Sales Centre have been lodged concurrently with SCAP.

Figure 2.1 Development Package 1



The Springwood development is proceeding in accordance with a Masterplan developed by Tract Consultants and will include a 'village centre' together with a supermarket and other speciality retail stores, various community uses and a school with large sporting grounds (see **Figures 2.2, 2.3 and 2.4**).

As a large master planned community, Springwood represents a significant economic investment and development for the Gawler Region and the State. Importantly, Springwood will result in significant employment generation and employment multipliers. It will also capitalise on significant embedded Government investment in infrastructure including the new 'Gawler East Link Road' (GELR) which will pass through the northern precinct of Springwood to service the proposed new Springwood Village Centre within which the supermarket and speciality shops will be located.



Figure 2.4 Springwood Village Centre Masterplan (Tract)



This planning statement provides information about the subject site and the proposed development while also addressing the merits of the proposal against the relevant provisions of the Gawler (CT) Development Plan – particularly the ‘Mixed Use Centre Policy Area 3’ of the ‘Residential (Gawler East) Zone’.

This planning statement has been informed by specialist advice which forms part of the application and is appended as follows:

- **Appendix 1:** Land Tenure Plan and Certificate of Title
- **Appendix 2:** Proposed Plan of Division (DA No. 490/D026/19)
- **Appendix 3:** Proposed plans and elevations Brown Falconer
- **Appendix 4:** Traffic Impact Assessment CIRQA

A site specific Stormwater Management Plan for the subject site will be provided by Wallbridge Gilbert Aztec (WGA). This plan will complement and build upon the overall Stormwater Management Plan for the entire Springwood development which has been submitted with the concurrent land division applications.

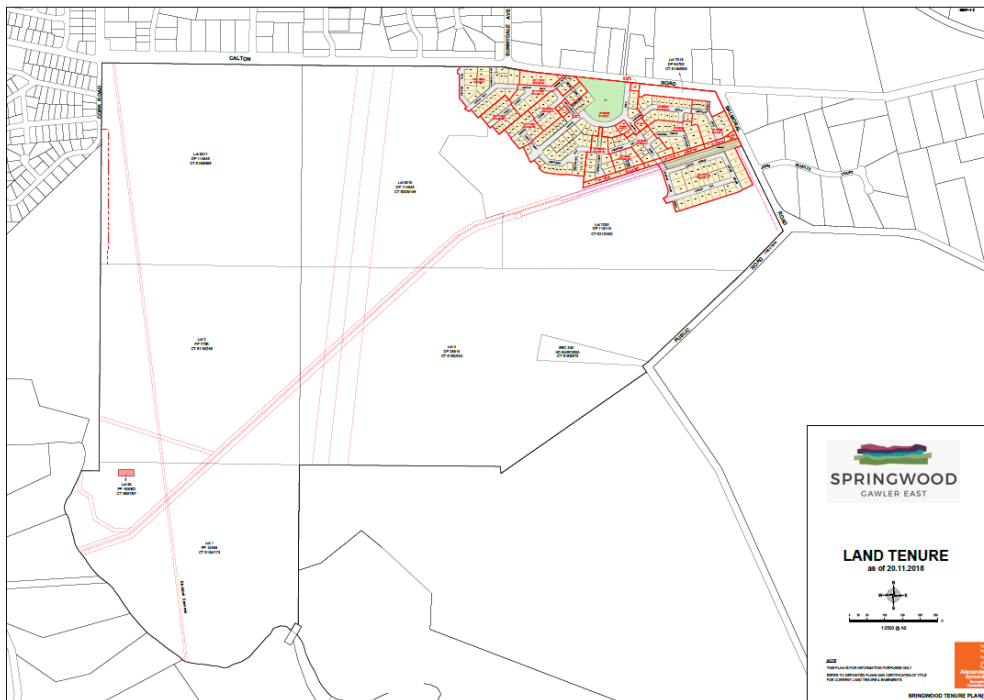


### 3. The Subject Site and Locality

#### 3.1 The Subject Land

Currently, the subject land forms part of Allotment 9010, Deposited Plan 114845 in Certificate of Title Volume 6205 Folio 146 (refer to **Appendix 1**). Allotment 9010 forms part of a number of titles which are being developed for the broader Springwood development (see **Figure 3.1** below and **Appendix 1**).

**Figure 3.1** Land Tenure Plan



There are a number of easements affecting the subject land including (but not limited to):

- A 275kV transmission line within a 100m wide ElectraNet easement that runs in a north-south direction;
- A DN750 MSCL potable water main, known as the Barossa Trunk Main, is located within a 10.06m wide easement running through the south-eastern corner of the land; and
- A 450mm diameter high pressure gas transmission pipeline, owned and operated by SEA Gas, is located within a 15m wide easement that runs parallel to the Barossa Trunk Main.

It is noted that a portion of the ElectraNet easement affects the western edge of the subject site. However, it is further noted that this portion of the site will only be used for car parking, vehicular access and landscaping. All the proposed buildings will be located to the east of the easement and will not compromise ElectraNet's access to its assets. This design is consistent with ElectraNet's easement requirements and the Plan of Division has previously been shared with and endorsed as a concept by ElectraNet.

### 3.2 The Subject Site

While the subject land currently forms part of Allotment 9010 in Deposited Plan 114845, the subject site comprises proposed Allotment 2046 in Development Application 490/D026/19 see **Figures 3.2** and **3.3**.

**Figure 3.2** Associated land division – subject site is Lot 2046



**Figure 3.3** Associated land division (enlargement) – subject site is Lot 2046



Proposed Lot 2046 has an area of 1.144ha and is rectangular in shape. It has a maximum depth of 142.8m and a maximum width of 85.06m.

The subject site, which previously formed part of the nearby quarry, is currently vacant. A significant amount of fill has been placed in mounds around the site as a consequence of development of earlier stages of Springwood. Some vegetation in the form of small trees, shrubs and groundcovers is present on the site. In addition, the Plan of Division prepared by Alexander Symonds identifies two Regulated Trees (shown in blue) on the site (see **Figures 3.2** and **3.3**). It is understood that these two Regulated Trees are located on uncontrolled fill and will need to be removed as part of the associated land division application to enable the construction of the supermarket to proceed.

The subject site will sit on a prominent corner at the junction of the Gawler East Link Road and the Springwood north-south connector road (see **Figure 3.4**). Given the site's prominence, the supermarket and speciality shops will be a key focal point for the 'village centre' along with the proposed Springwood Sales Centre which will be located on the opposite side of the intersection.

**Figure 3.4** Subject site





**Figure 3.5** Site of the proposed supermarket and specialty shops (looking west)



**Figure 3.6** Site of the proposed supermarket and specialty shops (looking north)



Figure 3.7 Site of the proposed supermarket and specialty shops (looking north)



### 3.3 The Locality and Surrounding Development

The locality features a range of land uses including the former quarry and associated activities to the immediate west and south of the site, residential development to the north and east and farmland further to the south. Generally speaking, the locality comprises land south of Calton Road which forms part of the master planned Springwood development. For this reason, the nature and character of the locality will be transformed over the coming years to create a vibrant residential community supported by a range of facilities and services within a proposed 'village centre'.

The former quarry, which forms a major element in the existing locality, was historically associated with the excavation of sand. More specifically, excavated sand was processed on-site through a washing plant where silt and clay were separated from the sand and deposited into a series of sedimentation and drying ponds. The material was periodically excavated from the pond and stockpiled on-site or sold as a product. It is understood that sand mining activities ceased operations in 2000.

Beyond the former quarry, the natural topography of the site comprises undulating hills which increase in gradient towards the South Para River which runs roughly in an east-west direction through the central portion of the Springwood development.



Major infrastructure within the locality includes:

- The Gawler East Link Road (under construction);
- An above-ground 750mm diameter Barossa Trunk Main;
- SEA Gas pipeline and associated 'Mail Line' valve; and
- 275kV overhead electricity power lines within a 100m wide easement (which partly affects the site).

Beyond the Springwood development, low-density residential development is located on the northern side of Calton Road and on the western side of Cheek Avenue. Further west is the Gawler Town Centre which features a traditional 'high street' and contains the main shopping, administrative and cultural facilities for the Council area. **Figure 3.8** identifies the key features of the locality.

**Figure 3.8** Subject Site and Surrounds

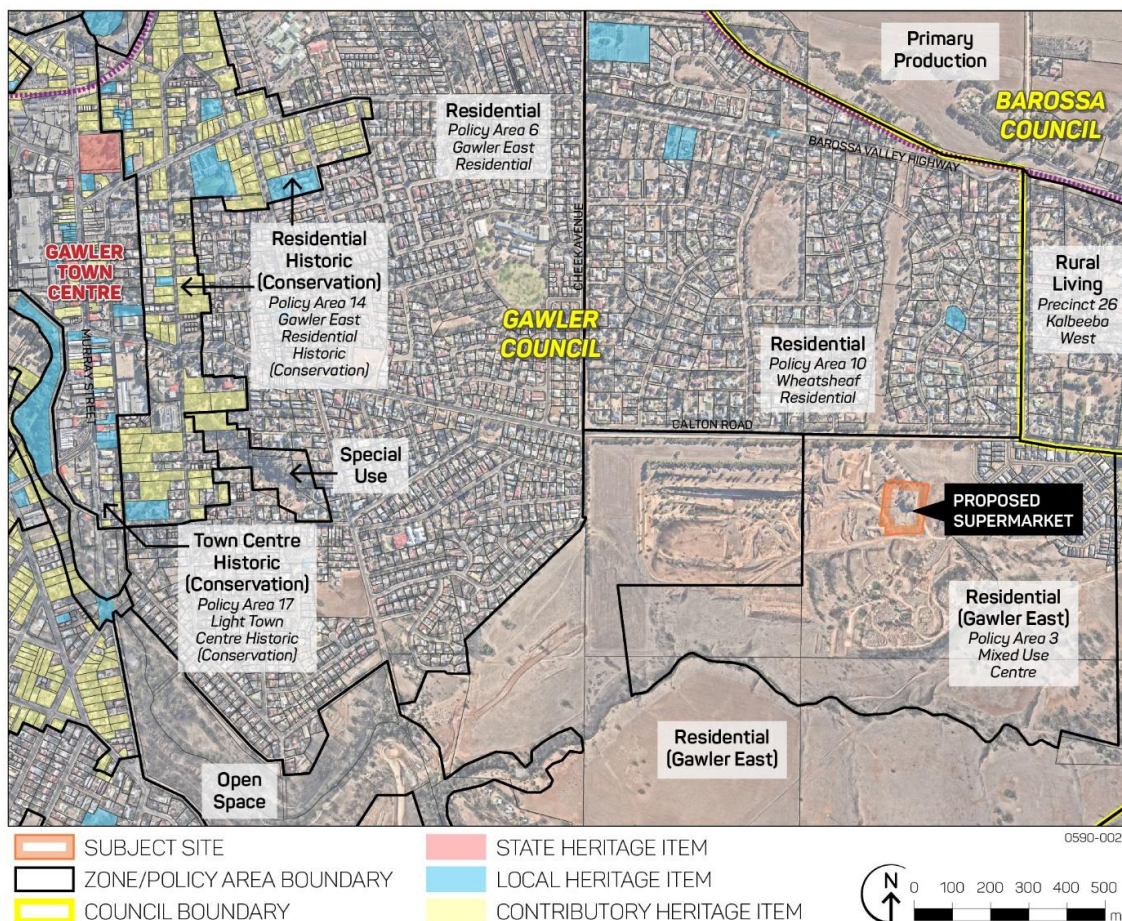


Figure 3.9 Future residential areas of the Springwood development (looking south-west)



## 4. Proposed Development

### 4.1 Land Use, Built Form, Access and Signage

The development proposes the construction of a large floorplate building which will accommodate a supermarket and six speciality shops to be located in the northern half of the subject site. Two additional smaller speciality shops will be located on the southern portion of the site fronting the Gawler East Link Road (refer to architectural plans prepared by Brown Falconer in **Appendix 3**).

The proposed development will comprise the following key elements:

- A main 'shopping centre' comprising a supermarket with a gross leasable floor area of 2,600m<sup>2</sup> and six speciality shops with a combined gross leasable floor area of 780m<sup>2</sup> which will be arranged the main entrance to the supermarket fronting the car park;
- A loading dock located at the north of the supermarket; and
- Two standalone speciality shops with a combined gross leasable floor area of 680m<sup>2</sup> which will be located along the southern boundary of the subject site.

A car park providing 217 parking spaces, including four spaces for those with a disability, will be located to the south and west of the main shopping centre. A pedestrian link will be provided through the car park between the entrance to the shopping centre and the Gawler East Link Road to the south.



Access to the car park will be provided via the Gawler East Link Road (under construction) and the proposed north-south collector road.

**Appendix 4** contains a detailed assessment undertaken by CIRQA Traffic Consultants of the access and parking arrangements for the supermarket and speciality shops. CIRQA's assessment confirms that the access points will be provided in accordance with the relevant Australian Standards and there will be sufficient car parking spaces to accommodate the various elements of the proposed development.

The proposed development also includes substantial landscaping both within the car park and around the edges of the site.

A variety of building materials will be used including painted concrete panels, face brickwork, feature stone facing and light weight timber cladding. A number of architectural features have been included in the design to create visual interest including feature steel frames and canopies.

Proposed signage includes a 6m high pylon sign at the eastern entrance to the site and an 8m high pylon sign at the southern entrance to the site from the Gawler East Link Road. Further signage identifying various tenancies will be located on the facades and walls of the building. The positioning and dimensions of the proposed signage reflects the retail use of the buildings and has been carefully designed to complement the overall architectural them of the development.

**Figure 4.1** 3D street view to the north west (Brown Falconer Architects)



## 4.2 Stormwater Management

A site specific Stormwater Management Plan for the subject site will be provided by Wallbridge Gilbert Aztec (WGA). This plan will complement and build upon the overall Stormwater Management Plan for the entire Springwood development which has been submitted with the concurrent land division applications.

## 5. Procedural Requirements

### 5.1 Relevant Authority

By letter dated 15 November 2018 and pursuant to Schedule 10(20) of the *Development Regulations 2009*, the State Coordinator General has assigned the State Commission Assessment Panel (SCAP) as the relevant Planning Authority.

### 5.2 Nature of Development

The proposed development is perhaps best described as a 'Supermarket and specialty shops with associated car parking, landscaping and signage'.

Pursuant to the Procedural Matters section of the Residential (Gawler East Zone), the application is neither a 'Complying' nor 'Non-complying' form of development. Therefore, this Development Application is to be assessed as a '**Consent use**' on its merits against the relevant provisions of the Gawler (CT) Development Plan.

### 5.3 Public Notification

Principle of Development Control (PDC) 38 of the Residential (Gawler East) Zone provides the following guidance in relation to public notification:

**PDC 38** *Categories of public notification are prescribed in Schedule 9 of the Development Regulations 2008. Further, all development listed within Principle of Development Control 1 of the Residential (Gawler East) Zone, Mixed Use Policy Area 3 and/or Local Centre Policy Area 19 are designated category 2 (except where the development is classified as Category 1 or non-complying).*

We note that Schedule 9 of the *Development Regulations* does not specifically assign a category of notification for a supermarket and speciality shops in the Residential (Gawler East) Zone. Therefore, it is necessary to refer to the lists of envisaged forms of development contained within PDC 1 of the Zone and PDC 1 of the Policy Area to determine the correct category of notification. Both of these PDCs list a 'shop' as an envisaged form of development and PDC 1 of the Policy Area lists a 'supermarket' as an envisaged form of development. Therefore, the proposal constitutes a '**Category 2**' form of development.

### 5.4 Agency Referrals

Pursuant to Schedule 8 of the *Development Regulations*, the proposed development does not trigger any referrals to Government Agencies. However, given that SCAP is the relevant Authority, the proposal will be referred to the Town of Gawler for comments.



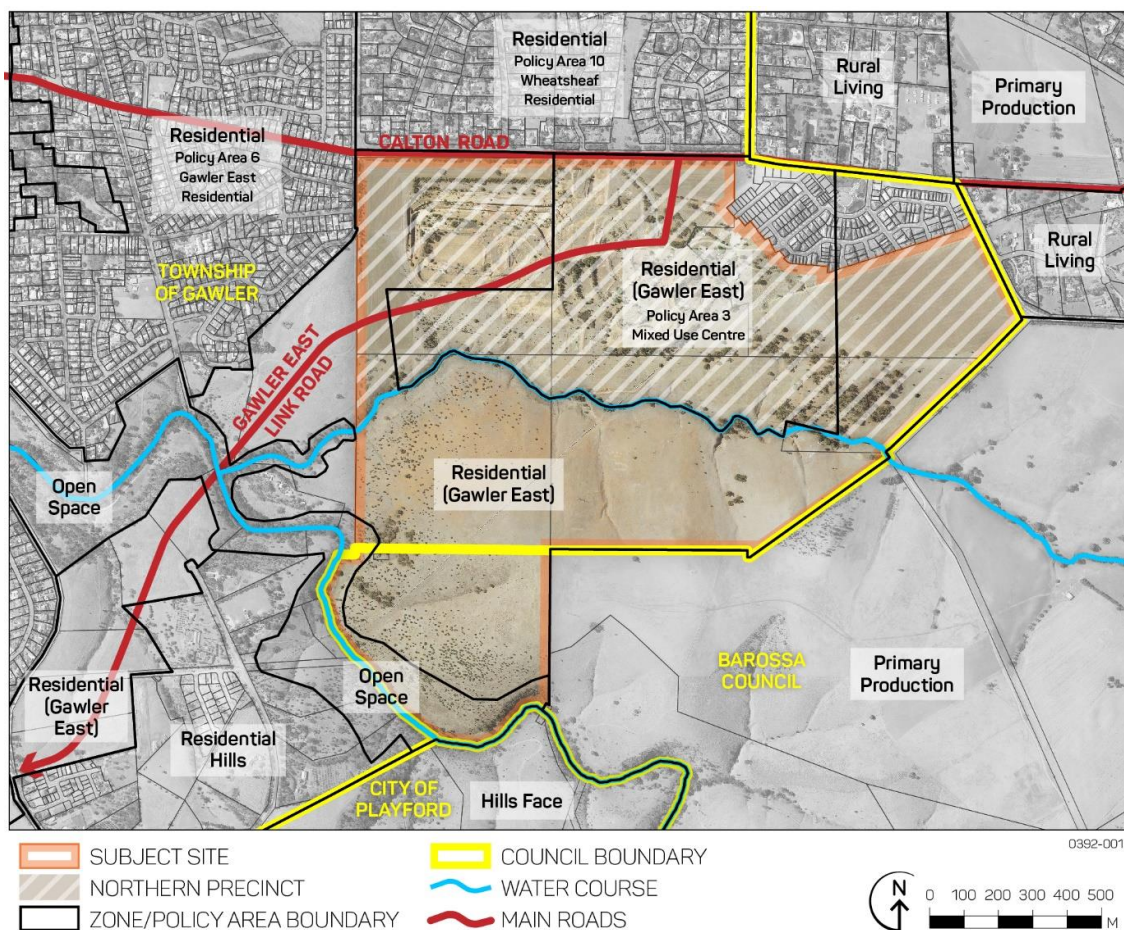
## 6. Development Plan Assessment

### 6.1 Overview

The subject site is located within the Town of Gawler and, accordingly, the relevant Development Plan is the Gawler (CT) Development Plan – consolidated 20 February 2018. More specifically, the subject site is located within the ‘Residential (Gawler East) Zone’, and the ‘Mixed Use Centre Policy Area 3’ as identified in *Figure 6.1* below.

The following section provides an assessment of the proposal against the Gawler (CT) Development Plan. For convenience, this assessment has been grouped under a series of headings which reflect the key relevant planning ‘themes’ from the Development Plan.

**Figure 6.1** Zoning and Policy Areas



### 6.2 Land Use

The Residential (Gawler East) Zone contemplates a wide range of land uses within broadly defined areas that are illustrated at a 'high-level' on the *Gawler East Structure Plan Map Ga/1 (Overlay 1) Enlargement G*. The

Structure Plan identifies areas that will be developed for residential uses as well as other areas that are intended to be developed as a 'Mixed Use Centre' and 'Neighbourhood Centres'. This desired mix of land uses is expressed within Objective (Obj) 1 and the Desired Character of the Zone as well as Obj 1 and the Desired Character of the Policy Area:

#### **Zone**

***Obj 1** A predominately residential area comprising a range of low and medium-density dwellings, with associated infrastructure, retail, commercial, recreational, educational and community development in master-planned locations in accordance with Structure Plan Map Ga/1 (Overlay 1) Enlargement G.*

#### **Desired Character (extract)**

---

*The zone will accommodate a diversity of housing forms. The Mixed Use Centre Policy Area 3 will comprise a mix of residential development and a range of commercial, retail, educational, recreational and community facilities. ---*

#### **Policy Area**

***Obj 1:** A functional and diverse zone accommodating a mix of commercial, retail, recreation, community, residential, office, consulting rooms and educational uses.*

#### **Desired Character (extract)**

*Mixed Use Centre Policy Area 3 will accommodate retail, commercial, community, education and formal recreation facilities and clubrooms to service the local community.*

---

*Low impact, commercial business activities that provide employment opportunities for the local population are envisaged. Such development will need to have particular regard to ensuring that minimal off-site impacts occur with respect to noise, air, water and waste emissions, commercial traffic generation and movement.*

*A retail facility, not exceeding 10 000 square metres in floor area, will comprise a supermarket and a range of specialty shops to serve the weekly shopping needs of the community. Restaurants, cafes, hotels (incorporating dining) and take away outlets are envisaged and will develop the centre as a destination point and provide uses that extend the hours of the centres operation to promote surveillance and safety of the adjacent recreation and education facilities.*

---

In addition, specific guidance in relation to envisaged land uses is provided in PDC 1 of the Zone and PDC 1 of the Policy Area. Both a 'shop' and a 'supermarket' are listed as envisaged uses:

#### Zone

**PDC 1** *The following forms of development are envisaged in the zone:*

---

*Non-residential use that serves the local community, for example:*

- *child care facility*
- *health and welfare service*
- *open space*
- *primary and secondary school*
- *recreation area*
- *shop, office or consulting room*

---

#### Policy Area

**PDC 1** *The following forms of development are envisaged in the policy area:*

---

*Shop*

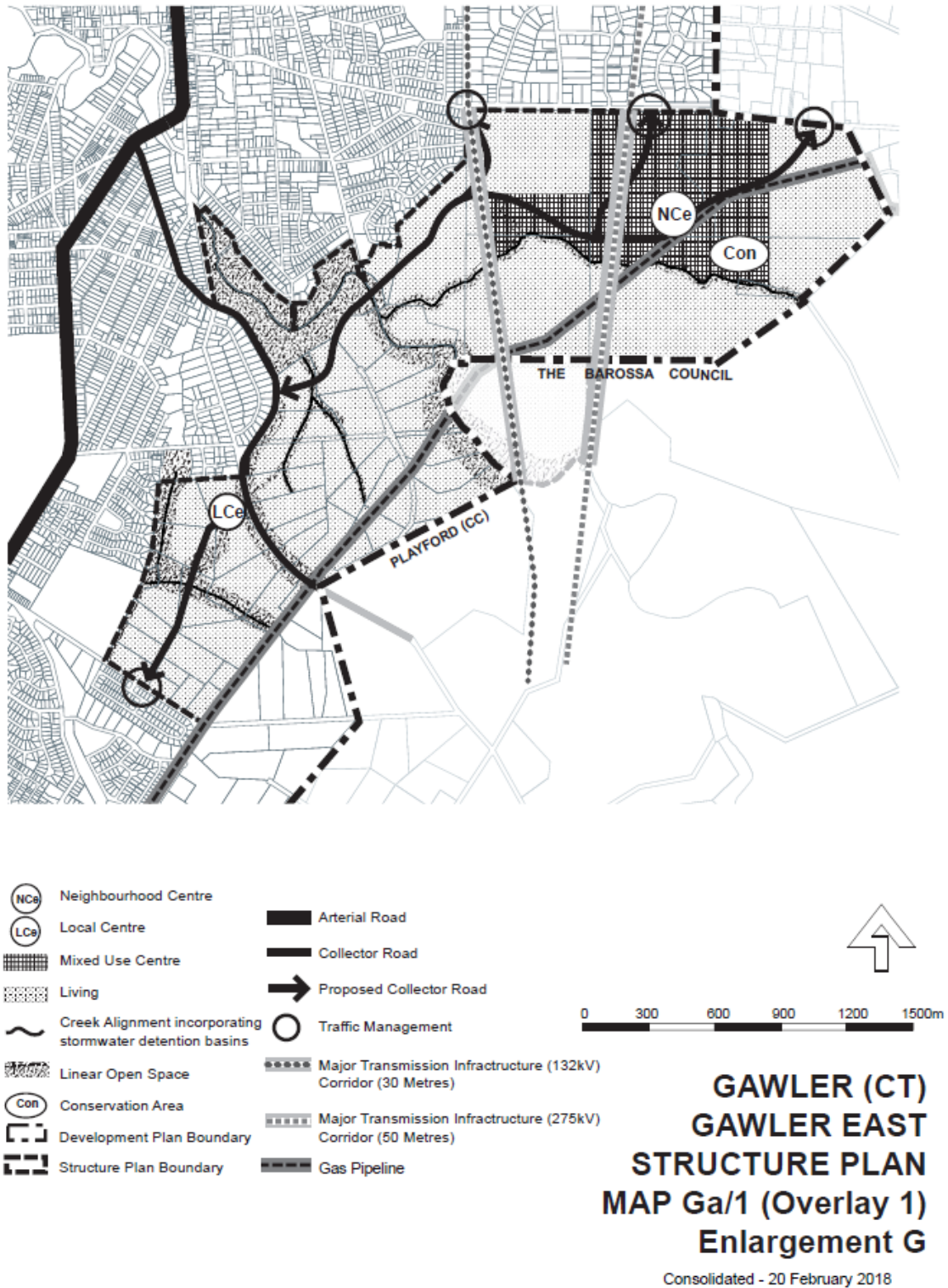
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*Supermarket*

For the above reasons, the proposed supermarket and speciality shops are an appropriate land use on the subject site. In terms of the strategic location of the proposed supermarket, *Structure Plan Map Ga/1 (Overlay 1) Enlargement G* (see Figure 6.2 on the following page) provides indicative, high-level direction for development within the Springwood development. In particular, it provides indicative locations for a potential Neighbourhood Centre (see **Figure 6.2**).

The proposed supermarket and speciality shops and other proposed development in the Springwood 'village centre' are located reasonably close to the Neighbourhood Centre nominated on the Structure Plan and reflect the final alignment of the Gawler East Link Road. For this reason and noting that the Springwood 'village centre' will still be located in the 'heart' of the Mixed Use Policy Area 3, the location of the supermarket and speciality shops are generally consistent with the strategic direction provided by the Structure Plan.

Figure 6.2 Gawler East Structure Plan





### 6.3 Design and Appearance

The Development Plan contains a variety of provisions which seek to ensure that development is of a high architectural standard, complements the character of the locality and minimises any visual impact on nearby existing development. For example, the Desired Character of the Policy Area provides the following guidelines in relation to the design and appearance of development.

***Desired Character (extract)***

---

*In terms of urban design and built form, a 'main street' environment will be created, where buildings address the street and car parks are primarily located to the rear. In order to minimise the overall extent of off street parking shared car parking is encouraged, and the convenience, availability and function of on street parking in mixed use environments will be recognised.*

*Gathering points for formal and informal community events will be established, either by means of a central pedestrian plaza, a village green or series of nodes.*

*Commercial and mixed use buildings will be established close to the street frontage and incorporate verandas and other protruding elements in order to create a pedestrian-friendly environment and outdoor dining opportunities. These areas will be enhanced by large street trees, high quality paving, lighting and street furniture. Building facades will be designed in a manner to create diversity of interest through the appearance of an aggregation of smaller buildings.*

The Desired Character of the Policy Area is reinforced by the following PDCs which seek to provide more specific guidance in terms of the design and appearance of new buildings:

***PDC 3*** Development should be designed to ensure that:

- (a) buildings are designed to address the street frontage with servicing areas located internal to the centre and appropriately screened from public view;*
- (b) the establishment of shared car parking areas to the rear of buildings and on-street.*

***PDC 4*** Public space established should be activated by uses around its edges.

***PDC 5*** Buildings should have a maximum of 5 storeys in height.

***PDC 6*** Development should not be undertaken unless it is consistent with the desired character for the policy area.

The proposed supermarket and speciality shops directly responds to the design and appearance provisions in the following ways:

- It will assist to create a 'main street' environment through the siting of buildings close to the Gawler East Link Road and the proposed north-south collector road;



- The associated car park will generally be located to the rear of the buildings apart from a relatively small portion of car park which will be visible from the proposed north-south collector road;
- The architecturally designed buildings will address the street frontages and will incorporate "... *protruding elements in order to create a pedestrian-friendly environment and outdoor dining opportunities*";
- The façade of the building and the materials used have been designed to reflect the high-profile location of the subject site in the 'village centre' with additional visual interest created by architectural features such as feature steel frames and canopies; and
- The building will assist to activate the street frontages by providing a plaza along the Gawler East Link Road and through the inclusion of high quality paving, street furniture and landscaping.

Additional guidance for the design, appearance and function of shopping centres is provided in PDC 25 of the Council Wide section of the Development Plan:

**PDC 25** *Development or redevelopment within business, centre, shopping or mixed use zone or associated policy areas, should:*

- (a) *conform with the objectives and desired character of the zone or policy area;*
- (b) *preserve buildings of historical or architectural significance;*
- (c) *provide for the integration of existing and future facilities so as to promote ease of pedestrian movement and sharing of facilities as well as to retain the opportunity for future expansion within the zone.*
- (d) *stage development taking into consideration any future expansion of the zone, or policy area, as a whole.*
- (e) *make multiple use of facilities and share utility spaces.*
- (f) *unified design of buildings to produce a close relationship between uses in a lively setting.*
- (g) *use materials compatible with the natural features of the site and adjacent buildings.*
- (h) *be designed and laid out to avoid nuisance or hazard arising from:*
  - (i) *microclimatic conditions;*
  - (ii) *excessive noise;*
  - (iii) *odours;*
  - (iv) *overlooking;*
  - (v) *overshadowing; or*
  - (vi) *visual intrusion.*

- (i) *be designed to be compatible with existing development through:*
  - (i) *a buffer between development in the zone and adjacent areas;*
  - (ii) *landscaping which complements adjacent development and enhances the visual appearance and character of the zone;*
  - (iii) *pedestrian paths and spaces which are shaded, defined and protected from the wind; and*
  - (iv) *service yards, loading areas and outdoor storage areas which are screened;*
  - (v) *foster human scale, define spaces, reinforce paths and edges, screen utility areas, and generally enhance the visual amenity of the area.*
- (j) *ensure outdoor signs, both free-standing and attached to buildings, are located and designed in such a way as to:*
  - (i) *be in scale with the development as a whole, the building therein, and the desired character of the zone or policy area, or otherwise be compatible with the character of the locality;*
  - (ii) *not impair the view of or from nearby developments;*
  - (iii) *not distract attention from traffic control information; and*
  - (iv) *illumination from signs or floodlights should not spill over to adjacent areas.*
- (k) *provide access and car parking for residential areas located within centres separate from the access and car parking areas serving the other centre facilities.*
- (l) *integrate of public transport requirements and sheltered waiting areas for passengers.*
- (m) *provide retail showrooms for the trading of bulky goods on the periphery of centres, or in designated precincts within zones or policy areas.*
- (n) *provide:*
  - (i) *off-street loading, service areas and service vehicle manoeuvring areas;*
  - (ii) *lighting for buildings and ancillary areas, with no light-spill causing nuisance or hazard; and*
  - (iii) *unobtrusive facilities for storage and removal of waste materials;*
  - (iv) *parking, access and facilities for the disabled;*
  - (v) *public spaces such as malls, plazas and courtyards;*
  - (vi) *public facilities including toilets, infant changing facilities for parents, seating, telephones and community information boards; and*
  - (vii) *design of buildings and infrastructure to minimise energy consumption for lighting, heating, cooling and ventilation and reuse of stormwater.*

The proposed development appropriately responds to PDC 25 in the following ways:

- Through the integration of facilities across the site and the broader Springwood 'village centre' to ensure pedestrians can move between areas easily and conveniently;
- By establishing a unified design theme for the supermarket and speciality shops which has taken into consideration other proposed development in the 'village centre' and has been 'tied' together by the overall Urban Design Masterplan for Springwood;
- Through the careful design and placement of the loading dock, vehicular access and parking areas in order to minimise the potential for conflict and ensure the free movement of traffic and pedestrians through the centre; and
- Through the provision of public spaces such as the proposed plaza along the Gawler East Link Road to provide a focal point for the shopping centre and to activate the streetscape.

## 6.4 Access and Parking

In order to address the relevant transportation and access provisions of the Development Plan, CIRQA Traffic Consultants has been engaged to review the proposed development and provide a traffic impact assessment (**Appendix 4**).

In terms of parking supply, the proposed development will provide a total of 217 onsite parking spaces which will wrap around the supermarket. This represents a slight shortfall of 7 spaces when compared against PDC 32 of the Residential (Gawler East) Zone which indicates that a shop should provide 5.5 spaces per 100m<sup>2</sup> (i.e. a gross leasable floor area of 4,060m<sup>2</sup> would theoretically require 224 spaces). However, CIRQA has referred to recent surveys of shopping centres which indicate peak parking demands are more commonly in the range of 3.0 to 5.0 spaces per 100m<sup>2</sup> which equate to a peak demand in the order of 122 to 203 spaces. On this basis, CIRQA considers that the proposal will provide sufficient parking to meet the peak demands.

In terms of the design of the car parks, the CIRQA report demonstrates that all spaces have been designed to comply with *AS/NZS 2890.1:2004* as summarised below:

- Regular parking spaces will be 2.7 m wide and 5.4 m long (or 4.8 m long with 0.6 m overhang);
- Disabled parking spaces will be at least 2.4 m wide and 5.4 m long (with an adjacent shared space of the same dimension);
- The parking aisles will exceed 6.6 m in width;
- 0.3 m clearance will be provided to all objects greater than 0.15 m in height; and
- Pedestrian sightlines will be provided at the site's property boundary.

In terms of access, the following arrangements are proposed:

- A two-way access point on the Gawler East Link Road (which will accommodate left-in, left-out, right-in and right-out movements);

- A two-way access point on the north-south collector road (which will accommodate left-in, left-out, right-in and right-out movements); and
- A two-way access point on the local road to the north which will primarily provide access for commercial vehicle movements, albeit customers will also be able to utilise the access point.

CIRQA's assessment concludes as follows:

*The proposal comprises the development of a shopping centre within the Springwood land development. Specifically, the shopping centre will include a supermarket as well as specialty tenancies.*

*The site will be well serviced by active (walking and cycling) facilities and public transport services which will minimise reliance on private motor vehicles. Access to the parking areas will be provided via the Gawler East Link Road, a new north-south collector road and a local road (adjacent the site's northern boundary).*

*The development will be serviced by parking areas containing 217 vehicle parking spaces (in total). A total of 20 bicycle spaces will also be provided. The proposed level of parking will result in a small shortfall against the requirements of Council's Development Plan. However, based on more recent surveys for shopping centres, it is considered that more than adequate parking will be provided as part of the development.*

*Traffic impacts associated with the proposal have been assessed as part of the broader land division application and planning for the Gawler East Link Road. The proposed access arrangements and associated traffic impacts will be in line with the previous assessments. Additional SIDRA modelling of the primary access points indicates that the forecast movements will be adequately accommodated.*

Based on CIRQA's assessment, the proposed development appropriately addresses the following key provisions of the Development Plan in relation to transportation and access.

**PDC 26** *Provision for the movement of people and goods within business, centre, shopping or mixed use zones or associated policy areas, should:*

- not cause inconvenient and unsafe traffic and pedestrian movements;*
- provide safe and convenient access for private cars, cyclists, pedestrians, service vehicles, emergency vehicles and public utility vehicles;*
- concentrate development for pedestrian convenience, and not extend unnecessarily along road frontages; (increasing the depth of development is a more desirable alternative);*
- separate pedestrian and vehicle movements within zones or areas, for pedestrian safety and convenience.*
- provide access points onto the arterial roads as shown on the relevant Structure Plans to: minimize traffic hazards; queuing on the roads; right turn movements; and interference with the function of intersections, junctions and traffic control devices.*

- (f) *not cause congestion or detract from the safety of traffic on abutting roads, and minimize: traffic hazards; queuing on public roads; and intrusion into adjacent residential areas.*
- (g) *provide for service vehicles and the storage and removal of waste goods and materials.*
- (h) *provide sufficient off-street parking to accommodate customer, employee and service vehicles if there is insufficient public car parking in the locality.*
- (i) *consolidate and co-ordinate parking areas into convenient groups, rather than located individually, and the access points minimized, and so located and designed that:*
  - (i) *vehicular movement between them does not require the use of public roads;*
  - (ii) *the number of access points is minimized; and*
  - (iii) *it is not necessary for vehicles to back onto public roads.*
- (j) *locate, design and orientate car parks in such a way as to facilitate safe, direct and convenient access of pedestrians between them and the facilities they serve, safe and convenient traffic circulation, minimal conflict between customer and service vehicles, and should include adequate provision for manoeuvring into and out of parking bays.*
- (k) *provide on-site parking determined by:*
  - (i) *the amount, type and timing of movement generated by the use;*
  - (ii) *the design, location and configuration of parking spaces;*
  - (iii) *the ability of the site to accommodate the parking spaces;*
  - (iv) *the potential for shared use of parking spaces; and*
  - (v) *the effect on surrounding activities and uses.*
- (l) *share use of car parking between developments to reduce the total extent of car parking areas.*
- (m) *provide for landscaping in order to screen, shade and enhance the appearance of car parking areas.*
- (n) *ensure that all sources of noise, including refrigeration and air conditioning equipment, garbage collection and car parking, do not cause excessive or disturbing noise at neighbouring properties.*

**PDC 340** *Development should provide safe and convenient access for all anticipated modes of transport including cycling, walking, public and community transport, and motor vehicles.*

**PDC 341** *Development at intersections, pedestrian and cycle crossings, and crossovers to allotments should maintain or enhance sightlines for motorists, cyclists and pedestrians to ensure safety for all road users and pedestrians.*



**PDC 355** *Development should be provided with safe and convenient access which:*

- (a) avoids unreasonable interference with the flow of traffic on adjoining roads;*
- (b) accommodates the type and volume of traffic likely to be generated by the development or land use and minimises induced traffic through over-provision;*
- (c) is sited and designed to minimise any adverse impacts on the occupants of and visitors to neighbouring properties.*

**PDC 362** *Development should be consistent with Australian Standard AS 2890 Parking facilities.*

**PDC 363** *Vehicle parking areas should be sited and designed in a manner that will:*

- (a) facilitate safe and convenient pedestrian linkages to the development and areas of significant activity or interest in the vicinity of the development;*
- (b) include safe pedestrian and bicycle linkages that complement the overall pedestrian and cycling network;*
- (c) not inhibit safe and convenient traffic circulation;*
- (d) result in minimal conflict between customer and service vehicles;*
- (e) avoid the necessity to use public roads when moving from one part of a parking area to another;*
- (f) minimise the number of vehicle access points to public roads;*
- (g) avoid the necessity for backing onto public roads;*
- (h) provide the opportunity for shared use of car parking and integration of car parking areas with adjoining development to reduce the total extent of vehicle parking areas and the requirement for access points;*
- (i) not dominate the character and appearance of a centre when viewed from public roads and spaces;*
- (j) provide landscaping that will shade and enhance the appearance of the vehicle parking areas.*

**PDC 367** *Parking areas should be sealed or paved in order to minimise dust and mud nuisance.*

**PDC 368** *To assist with stormwater detention and reduce heat loads in summer, vehicle parking areas should include soft (living) landscaping.*

**PDC 369** *Parking areas should be line-marked to indicate parking bays, movement aisles and direction of traffic flow.*

## 6.5 Crime Prevention

The Crime Prevention provisions in the Development Plan seek to ensure that new development prioritises safety and security by incorporating design features which discourage crime and facilitates community surveillance. The proposed development has responded to these provisions in the following ways:

- Through the siting of two of speciality shops, at the intersection of two collector roads where clear views into the site are available to traffic travelling along the Gawler East Link Road and the proposed north-south collector road;
- By providing large windows which will overlook the adjoining streets and car park to maximise opportunities for casual surveillance;
- By ensuring that landscaping selected for the development will be limited to low level plants to provide unobstructed sightlines;
- By avoiding the construction of boundary fencing or other structures which may obstruct sightlines or be used for concealment or entrapment spots; and
- By ensuring that the site will be illuminated in accordance with the relevant Australian Standards to maximise opportunities for casual surveillance.

For the reasons outlined above, the development is closely aligned with the following relevant Crime Prevention provisions of the Development Plan.

**Obj 20:** *A safe, secure, crime resistant environment where land uses are integrated and designed to facilitate community surveillance.*

**PDC 42** *Development should be designed to maximise surveillance of public spaces through the incorporation of clear lines of sight, appropriate lighting and the use of visible permeable barriers wherever practicable.*

**PDC 43** *Buildings should be designed to overlook public and communal streets and public open space to allow casual surveillance.*

**PDC 44** *Development should provide a robust environment that is resistant to vandalism and graffiti.*

**PDC 45** *Development should provide lighting in frequently used public spaces including those:*

- (a) *along dedicated cyclist and pedestrian pathways, laneways and access routes*
- (b) *around public facilities such as toilets, telephones, bus stops, seating, litter bins, automatic teller machines, taxi ranks and car parks.*

**PDC 46** *Development, including car park facilities should incorporate signage and lighting that indicate the entrances and pathways to, from and within sites.*

**PDC 47** *Landscaping should be used to assist in discouraging crime by:*

- (a) *screen planting areas susceptible to vandalism*

- (b) *planting trees or ground covers, rather than shrubs, alongside footpaths*
- (c) *planting vegetation other than ground covers a minimum distance of 2 metres from footpaths to reduce concealment opportunities.*

**PDC 48** *Site planning, buildings, fences, landscaping and other features should clearly differentiate public, communal and private areas.*

**PDC 49** *Buildings should be designed to minimise and discourage access between roofs, balconies and windows of adjoining dwellings.*

**PDC 51** *Development should avoid pedestrian entrapment spots and movement predictors (eg routes or paths that are predictable or unchangeable and offer no choice to pedestrians).*

**PDC 364** *Vehicle parking areas should be designed to reduce opportunities for crime by:*

- (a) *maximising the potential for passive surveillance by ensuring they can be overlooked from nearby buildings and roads;*
- (b) *incorporating walls and landscaping that do not obscure vehicles or provide potential hiding places;*
- (c) *being appropriately lit;*
- (d) *having clearly visible walkways.*

## 6.6 Stormwater and Flooding Considerations

A site specific Stormwater Management Plan for the subject site will be provided by Wallbridge Gilbert Aztec (WGA). This plan will complement and build upon the overall Stormwater Management Plan for the entire Springwood development which has been submitted with the concurrent land division applications. In particular, the Stormwater Management Plan will address the relevant provisions of the Development Plan.





## 6.7 Signage

The architectural plans show indicative locations and proportions for signage at the main entrances to the site and on the facades and walls of the buildings. The signage reflects the retail use of the buildings and is designed to easily identify the future tenants while also responding to the overall architectural theme of the 'village centre'. The design, extent and size of the proposed signage is relatively modest and does not visually dominate the buildings on which they will be attached. Further, the proposed signage will:

- Complement the design of the building;
- Be contained within the boundaries of the site;
- Not affect existing street trees or obscure views to attractive landscaping;
- Not cause a distraction for drivers or obstruct their views of other vehicles; and
- Be limited to information relating to the proposed use of the land.

For the reasons outlined above, the nature, quantity and design of the proposed signage on the current plans is closely aligned with the following key 'Advertisements' provisions of the Development Plan including *Table Ga/4 – Advertising and Advertising Displays Definitions and Design Criteria*:

**Figure 6.3** Extract from *Table Ga/4*

Sign Type	Criteria	
FLAT WALL SIGN	Maximum number: 1 per side wall 1 per rear wall	
	Placement and size of signs in scale and integrated with the architectural features and elements of the building gable wall.	
	Positioned not to conceal architectural features or detailing.	
		

**Obj 69:** An urban environment and rural landscape not disfigured by advertisements.

**Obj 70:** Advertisements in retail, commercial and industrial urban areas, and centre zones, designed to enhance the appearance of those areas.

**Obj 71:** Advertisements not hazardous to any person.

**PDC 199** Advertising and advertising displays should, in addition to general provisions, conform with the performance criteria in *Table Ga/4*.

**PDC 200** The location, siting, size, shape and materials of construction, of advertisements should be:

- (a) consistent with the desired character of areas or zones as described by their objectives;
- (b) consistent with the predominant character of the urban or rural landscape; or
- (c) in harmony with any building or site of historic significance or heritage value in the locality.

**PDC 201** Advertisements should not detrimentally affect by way of their siting, size, shape, scale, glare, reflection or colour the amenity of areas, zones, or localities, in which they are situated.

**PDC 202** Advertisements should not impair the amenity of areas, zones, or localities, in which they are situated by creating, or adding to, clutter, visual disorder and the untidiness of buildings and spaces.

- PDC 203** *Advertisements should not obscure views of attractive landscapes or particular trees or groups of trees.*
- PDC 204** *The scale of advertisements should be compatible with the buildings on which they are situated and with nearby buildings and spaces.*
- PDC 207** *Advertisements on buildings that have a single architectural theme but which contain a number of tenancies, should be attached and displayed so as to be co-coordinated with that theme.*
- PDC 209** *Advertisements should not create a hazard to persons travelling by any means.*
- PDC 210** *Advertisements should not obscure a driver's view of other road vehicles, of rail vehicles at or approaching level crossings, of pedestrians and of features of the road such as junctions, bends, changes in width, traffic control devices and the like that are potentially hazardous.*
- PDC 211** *Advertisements should not be so highly illuminated as to cause discomfort to an approaching driver, or create difficulty in his perception of the road, or of persons or objects on it.*
- PDC 212** *Advertisements should not be liable to interpretation by drivers as an official traffic sign, or convey to drivers information that might be confused with instructions given by traffic signals or other control devices, or impair the conspicuous nature of traffic signs or signals.*
- PDC 213** *Advertisements should not detract drivers from the primary driving task at a location.*
- PDC 214** *Advertisements or advertising displays should:*
- (a) convey in clear and concise language, symbols, print style, layout, and in a limited number of colours, the owner/occupier and/or generic type of business, merchandise or services;*
  - (b) be minimised in number to avoid proliferation of advertisements or advertising displays;*
  - (c) be of a form, style, scale and size and in locations appropriate to the desired character and future use of land or character of the locality or zone and to preserve reasonable exposure to the public for all adjacent sites;*
  - (d) not have an adverse impact on the amenity of adjacent land uses and areas from which they are visible and without limiting the foregoing, no bunting, whirligigs or other like displays should be displayed;*
  - (e) conceal the supporting structure from view wherever possible;*
  - (f) be completely contained within the boundaries of the subject property, and if road widening is applicable, be within the proposed property boundary realignment;*
  - (g) be unified in colour, style, placement and proportion to present a coordinated theme and design for a site, centre or building; and*
  - (h) be designed and located to avoid damage to, pruning or lopping of on-site landscaping or street trees.*



**PDC 221** *Advertisements or advertising displays should only identify premises or the nature of business, goods or services conducted on the land or building.*

## 6.8 Landscaping

A conceptual landscaping plan has been prepared for the development and is included with the architectural plans contained in **Appendix 3**.

The proposal includes landscaping around the perimeter of the site as well as within the car parking area. These landscaped areas will be planted with a range of species of trees, bushes and groundcovers which will complement the overall landscape theme of the Springwood development. More specifically, the proposed landscaping will assist to create a sense of place and a high-quality, coordinated development outcome.

Accordingly, the development satisfies PDC 25 of the Development Plan which seeks:

**PDC 25** *Development or redevelopment within business, centre, shopping or mixed use zone or associated policy areas, should:*

---

- (i) *be designed to be compatible with existing development through:*
  - (i) *a buffer between development in the zone and adjacent areas;*
  - (ii) *landscaping which complements adjacent development and enhances the visual appearance and character of the zone;*
  - (iii) *pedestrian paths and spaces which are shaded, defined and protected from the wind;*  
*and*
  - (iv) *service yards, loading areas and outdoor storage areas which are screened;*
  - (v) *foster human scale, define spaces, reinforce paths and edges, screen utility areas, and generally enhance the visual amenity of the area.*

## 6.9 Interface Considerations

The subject site is located within the Mixed Use Centre Policy Area 3 which clearly anticipates a neighbourhood level centre (including a supermarket) in this area. Currently, the site and nearby land is vacant but will be developed over time to create a master-planned community comprising residential development supported by a range of non-residential land uses. On this basis, potential interface issues have been carefully considered during the preparation of the Springwood masterplan and as part of the urban design of the whole project.

Further to the above, the loading dock will be located on the northern side of the supermarket – a considerable distance away from any existing or proposed residential development. Similarly, the car park and associated access points will not face any existing or proposed residential areas. For these reasons, potential issues relating to noise, light spillage, overshadowing and overlooking will be managed appropriately. Therefore, the proposed

development appropriately responds to the following relevant interface provisions within the Development Plan:

#### Zone

**PDC 28** *Development should be designed and sited to minimise negative impact on existing and potential future land uses considered appropriate in the locality.*

#### Council Wide

**Obj 42:** *Development located and designed to minimise adverse impact and conflict between land uses.*

**Obj 43:** *Protect community health and amenity from adverse impacts of development.*

**PDC 107** *Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:*

- (a) the emission of effluent, odour, smoke, fumes, dust or other airborne pollutants;*
- (b) noise;*
- (c) vibration;*
- (d) electrical interference;*
- (e) light spill;*
- (f) glare;*
- (g) hours of operation;*
- (h) traffic impacts.*

**PDC 108** *Development should be sited and designed to minimise negative impacts on existing and potential future land uses desired in the locality.*

## 7. Conclusion

This development application seeks approval to establish a supermarket and specialty shops with associated car parking, signage and landscaping.

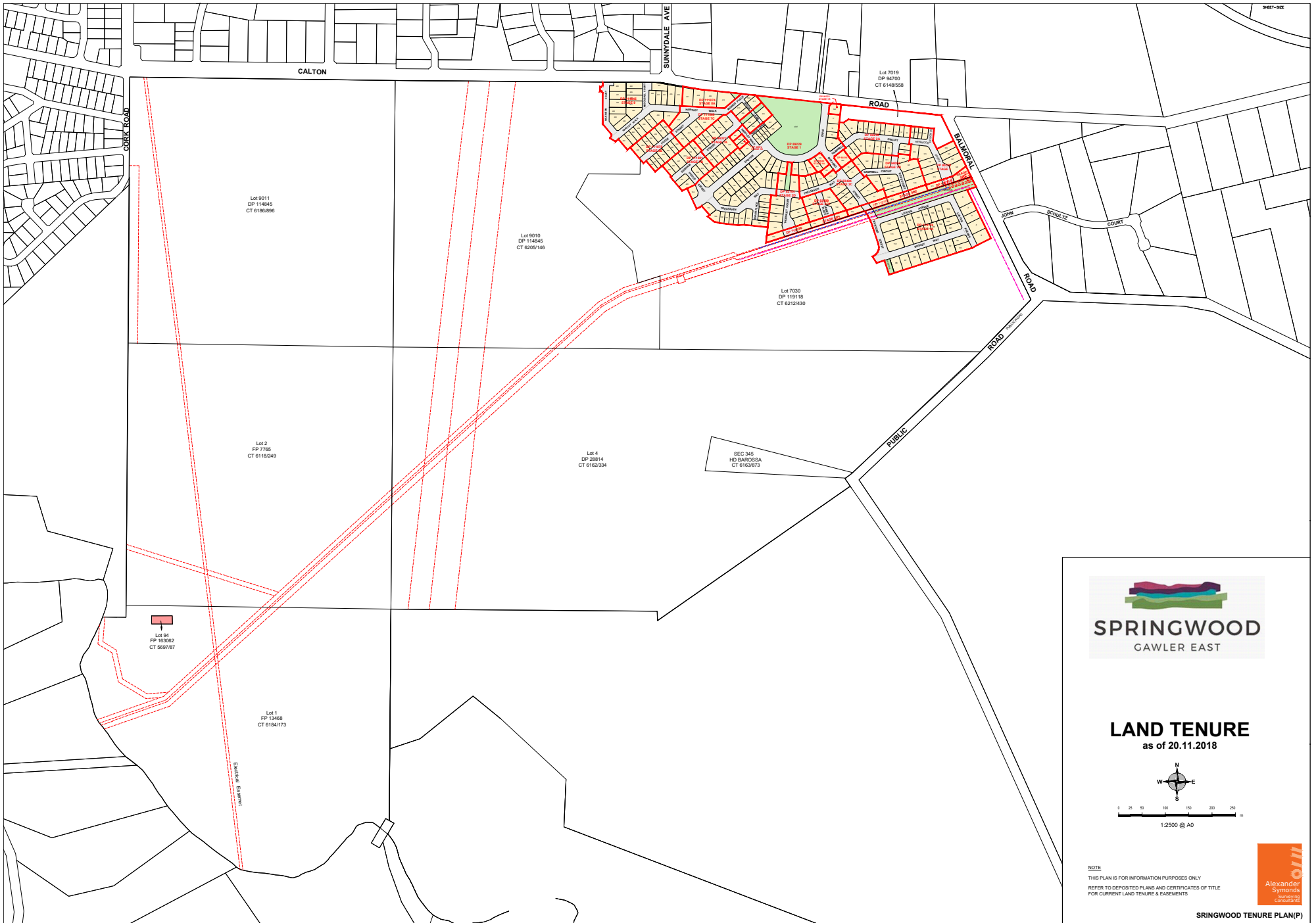
Having undertaken a comprehensive assessment of the application against the relevant provisions of the Gawler (CT) Development Plan, we believe that the proposal represents orderly development which accords with the relevant provisions of the Development Plan for the reasons summarised below:

- A 'supermarket' and 'shop' are envisaged forms of development in the Mixed Use Policy Area 3 of the Residential (Gawler East) Zone;
- The general area of the subject site is indicatively identified as a Neighbourhood Centre within the *Gawler East Structure Plan Map Ga/1 (Overlay 1) Enlargement G* as well as the Springwood Masterplan;

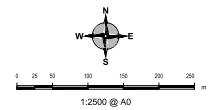
- The location and design of the proposed development will complement other proposed development in the Springwood 'village centre' such as the Sales Centre on the opposite side of the Gawler East Link Road;
- The proposed development has been designed to address both road frontages, and the building exhibits a high degree of architectural merit achieved by using a variety of complementary colours, building materials and articulation;
- A coordinated signage theme is proposed, with the indicative signage shown on the plans designed in accordance with the relevant provisions of the Development Plan;
- Large windows along the elevations will maximise opportunities for casual surveillance across the site;
- The proposed development has been designed to accommodate safe and convenient movements for vehicles, and the CIRQA report concludes that sufficient onsite parking will be provided to cater for the anticipated parking demand;
- The additional traffic volumes to be generated by the development will not have a significant impact on the road network;
- Stormwater will be managed appropriately in accordance with the overall stormwater management plan for the Springwood development which has been submitted with the land division applications; and
- The proposed landscaping will assist to create a sense of place and enhance the amenity of the locality.

Based on our assessment, the application is closely aligned within the relevant provisions of the Development Plan and therefore warrants Development Plan Consent subject to reasonable and relevant conditions.

## Appendix 1. Land Tenure Plan and Certificate of Title



## LAND TENURE as of 20.11.2018



**NOTE**  
THIS PLAN IS FOR INFORMATION PURPOSES ONLY  
REFER TO DEPOSITED PLANS AND CERTIFICATES OF TITLE  
FOR CURRENT LAND TENURE & EASEMENTS



SRINGWOOD TENURE PLAN(P)



REAL PROPERTY ACT, 1886



The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



## Certificate of Title - Volume 6205 Folio 146

Parent Title(s)	CT 6186/895			
Creating Dealing(s)	VE 12885392			
Title Issued	26/03/2018	Edition 2	Edition Issued	06/07/2018

### Estate Type

FEE SIMPLE

### Registered Proprietor

FIVE AMES FARMING PTY. LTD. (ACN: 609 760 536)  
OF 63 GAWLER TERRACE GAWLER SOUTH SA 5118

### Description of Land

ALLOTMENT 9010 DEPOSITED PLAN 114845  
IN THE AREA NAMED GAWLER EAST  
HUNDRED OF BAROSSA

### Easements

SUBJECT TO EASEMENT(S) OVER THE LAND MARKED A ON D114845 TO THE MINISTER FOR INFRASTRUCTURE (T 1374106)

SUBJECT TO EASEMENT(S) OVER THE LAND MARKED C ON D114845 TO TRANSMISSION LESSOR CORPORATION OF 1 UNDIVIDED 2ND PART (SUBJECT TO LEASE 9061500) AND ELECTRANET PTY. LTD. OF 1 UNDIVIDED 2ND PART (T 2370109)

SUBJECT TO EASEMENT(S) OVER THE LAND MARKED E ON D114845 TO ELECTRANET PTY. LTD. (TG 12371822)

SUBJECT TO EASEMENT(S) OVER THE LAND MARKED T ON D114845 (TG 9662213)

SUBJECT TO EASEMENT(S) OVER THE LAND MARKED U ON D114845 (TG 10297076)

SUBJECT TO SERVICE EASEMENT(S) OVER THE LAND MARKED H ON D114845 FOR DRAINAGE PURPOSES TO THE COUNCIL FOR THE AREA (223LG RPA)

SUBJECT TO SERVICE EASEMENT(S) OVER THE LAND MARKED J ON F252234 FOR ELECTRICITY SUPPLY PURPOSES TO DISTRIBUTION LESSOR CORPORATION (SUBJECT TO LEASE 8890000) (223LG RPA)

SUBJECT TO SERVICE EASEMENT(S) OVER THE LAND MARKED K(T/F) ON F252234 FOR ELECTRICITY SUPPLY PURPOSES TO DISTRIBUTION LESSOR CORPORATION (SUBJECT TO LEASE 8890000) (223LG RPA)

### Schedule of Dealings

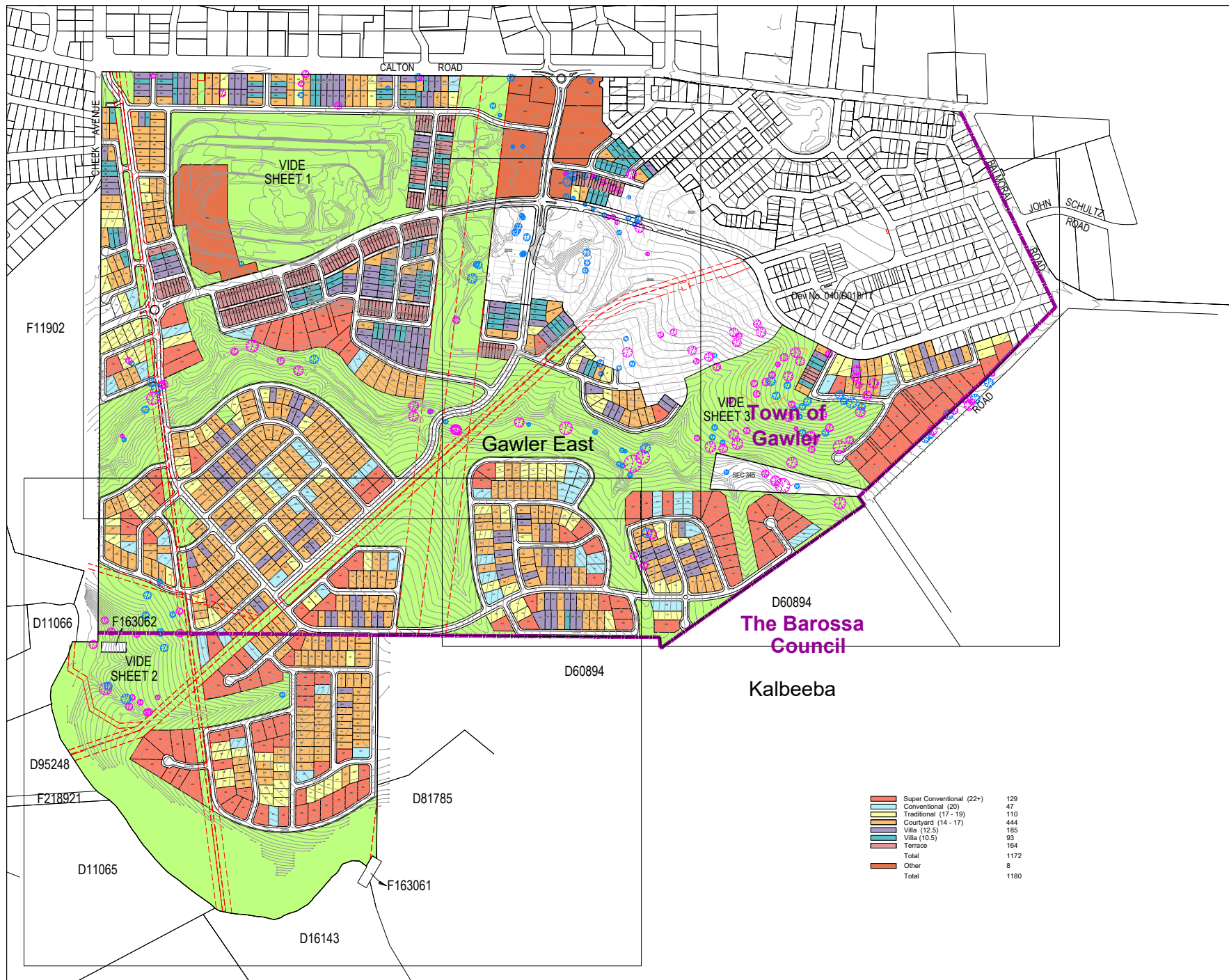
Dealing Number	Description
11764743	AGREEMENT UNDER DEVELOPMENT ACT 1993 PURSUANT TO SECTION 57A
12459905	AGREEMENT UNDER DEVELOPMENT ACT, 1993 PURSUANT TO SECTION 57(2)
12915311	MORTGAGE TO NATIONAL AUSTRALIA BANK LTD. (ACN: 004 044 937)



## Notations

Dealings Affecting Title	NIL
Priority Notices	NIL
Notations on Plan	NIL
Registrar-General's Notes	NIL
Administrative Interests	NIL

## Appendix 2. Plan of Division



Development No. 490/D026/19  
Town of Gawler  
Development No. 960/D025/19  
The Barossa Council

Proposed Plan of Division  
Allotment 2 in F7765  
Allotment 1 in F13465  
Allotment 9010 & 9011 in D114845  
Allotment 9201 in Dev No. 490/D010/17  
Allotment 4 in D26814  
Hundred of Barossa

In the area named  
**GAWLER EAST  
& KALBEEBA**  
CT 6168094 CT 6205146  
CT 6168094 CT 6162334  
CT 6164173 PT CT 6212430

1:2500

**SHEET 1 OF 4**

Significant tree  
Regulated tree

No. of proposed allotments	1190
Total area	185.8ha
Area of Development	168.5ha
Reserve area	71.29ha
Length of new roads	19.8k

Contour interval 2m.  
Datum AHD.

VIDE Titles for disposition of easements.  
Road pavements shown are indicative only.  
\*\*Not to be used for detailed engineering design.\*\*  
Dimensions and areas are subject to survey.

© ALEXANDER & SYMONDS PTY LTD. Original Sheet Size A3  
**Glenn Ian Hordacre**  
LICENSED SURVEYOR

REF.	ADVISOR
DWG NO.	ADVISOR'S PROPERTY DEV ID
REVISION ID	
DATE	15.08.2019

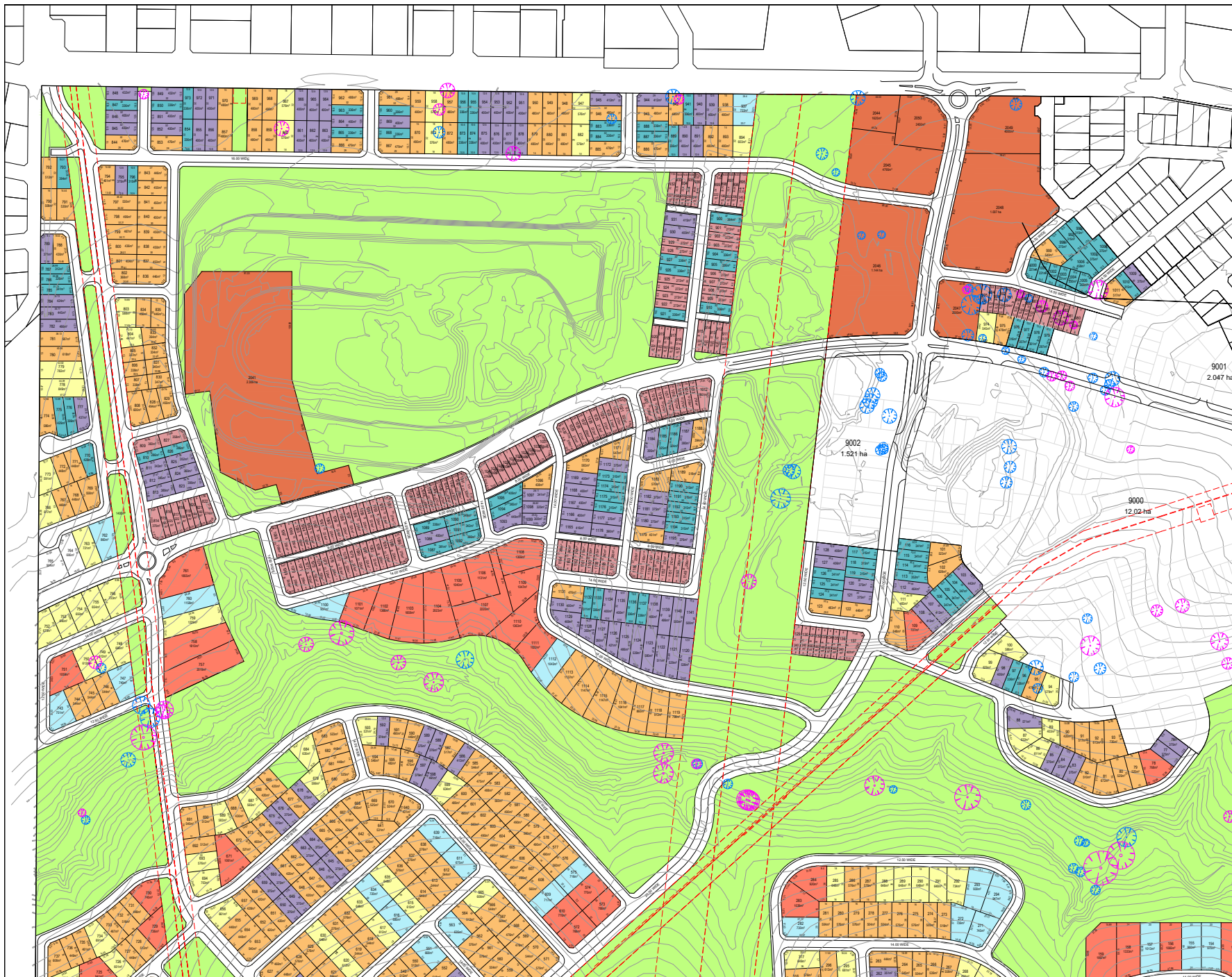
Alexander & Symonds Pty Ltd  
12 Kings Highway, Gawler East Town,  
South Australia 5052  
PO Box 1000 Gawler East Town, SA 5051  
080 8500 715 1000

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Symonds**  
CONCRETE



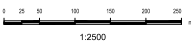


Development No. 490/D026/19  
Town of Gawler  
Development No. 960/D025/19  
The Barossa Council

Proposed Plan of Division  
Allotment 2 in F7765  
Allotment 1 in F13465  
Allotment 9010 & 9011 in D114345  
Allotment 4 in D26814  
Hundred of Barossa

In the area named  
**GAWLER EAST  
& KALBEEBA**

CT 6168089 CT 6205146  
CT 6168094 CT 6162334  
CT 6164173 PT CT 6212430



## SHEET 2 OF 4

Significant tree  
Regulated tree

No. of proposed allotments 1190  
Total area 185.8ha  
Area of Development 168.5ha  
Reserve area 71.29ha  
Length of new roads 19.8k

Contour interval 2m.  
Datum AHD.

Vide Titles for disposition of easements.  
Road pavements shown are indicative only.

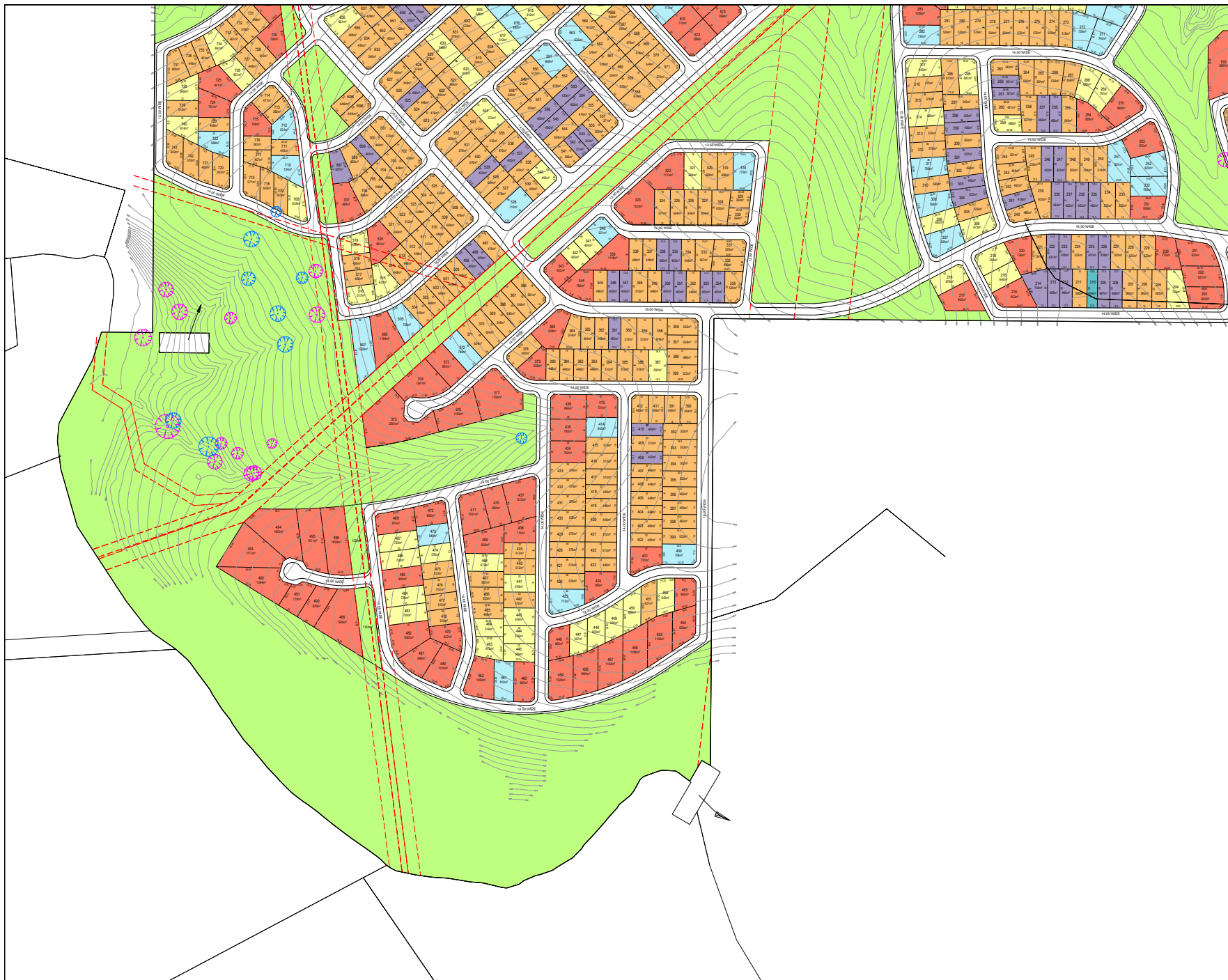
\*\*Not to be used for detailed engineering design.\*\*  
Dimensions and areas are subject to survey.

D. ALEXANDER & SYMONDS PTY LTD  
**Glenn Ian Hordacre**  
LICENSED SURVEYOR

REV	DATE	DESCRIPTION
1	13.08.2019	ISSUED FOR TENDERS
2	13.08.2019	ISSUED FOR TENDERS
3	13.08.2019	ISSUED FOR TENDERS
4	13.08.2019	ISSUED FOR TENDERS
5	13.08.2019	ISSUED FOR TENDERS
6	13.08.2019	ISSUED FOR TENDERS
7	13.08.2019	ISSUED FOR TENDERS
8	13.08.2019	ISSUED FOR TENDERS
9	13.08.2019	ISSUED FOR TENDERS
10	13.08.2019	ISSUED FOR TENDERS
11	13.08.2019	ISSUED FOR TENDERS
12	13.08.2019	ISSUED FOR TENDERS
13	13.08.2019	ISSUED FOR TENDERS
14	13.08.2019	ISSUED FOR TENDERS
15	13.08.2019	ISSUED FOR TENDERS
16	13.08.2019	ISSUED FOR TENDERS
17	13.08.2019	ISSUED FOR TENDERS
18	13.08.2019	ISSUED FOR TENDERS
19	13.08.2019	ISSUED FOR TENDERS
20	13.08.2019	ISSUED FOR TENDERS
21	13.08.2019	ISSUED FOR TENDERS
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Development No. 490/D02619  
Town of Gawler  
Development No. 960/D02519  
The Barossa Council

Proposed Plan of Division  
Allotment 2 in F7765  
Allotment 1 in F13465  
Allotment 9010 & 9011 in D114845  
Allotment 9201 in Dev No. 490/D010117  
Allotment 4 in D26814  
Hundred of Barossa

In the area named  
**GAWLER EAST  
& KALBEEBA**  
CT 6168096 CT 6205146  
CT 6168094 CT 6162334  
CT 6164173 PT CT 6212430

0 25 50 100 150 200 250  
1:2500

**SHEET 3 OF 4**

- Significant tree
- Regulated tree

No. of proposed allotments 1190  
Total area 185.8ha  
Area of Development 168.5ha  
Reserve area 71.29ha  
Length of new roads 19.8k

Contour interval 2m.  
Datum AHD.

Vide Titles for disposition of easements.

Road pavements shown are indicative only.

**\*\*Not to be used for detailed engineering design.\*\***

**Dimensions and areas are subject to survey.**

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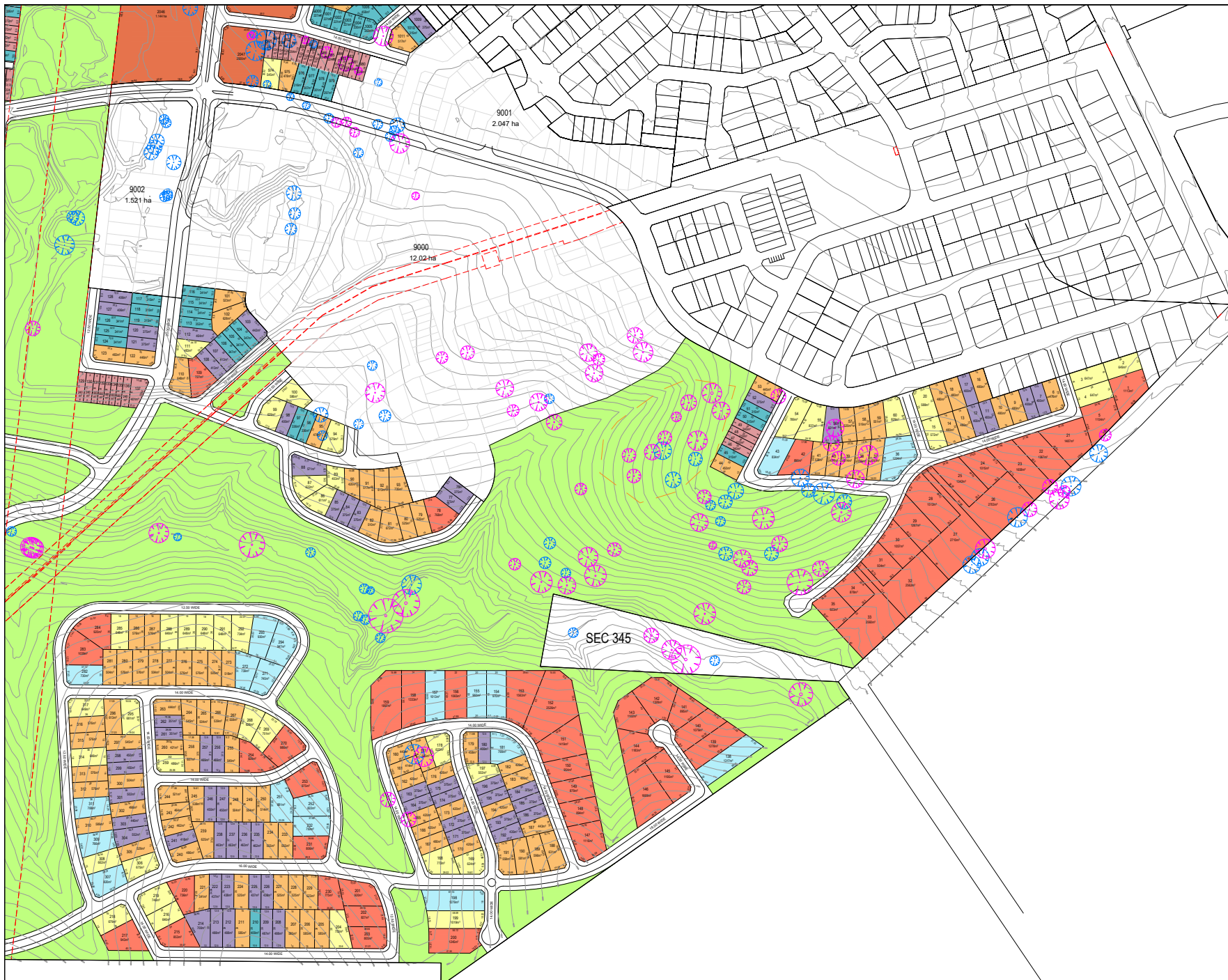
**Glenn Ian Hordacre**  
LICENSED SURVEYOR

REF.	DATE	DESCRIPTION
AD101616-001	12.08.2019	AD101616-001

Alexander & Symonds Pty Ltd  
12 Park Street, Glenelg, SA 5015  
PO Box 1000, Glenelg, SA 5015  
080 850 715 1000

**Alexander  
Symonds**  
CONSULTANTS

- Property • Land Development •
- Construction • Mining •
- Spatial Information Management •



Development No. 490/D026/19  
Town of Gawler  
Development No. 960/D025/19  
The Barossa Council

Proposed Plan of Division  
Allotment 2 in F7765  
Allotment 1 in F13465  
Allotment 9010 & 9011 in D114345  
Allotment 9201 in Dev No. 490/D010/17  
Allotment 4 in D26814  
Hundred of Barossa

In the area named  
**GAWLER EAST  
& KALBEEBA**  
CT 61680896 CT 6205146  
CT 61680894 CT 61623334  
CT 61684173 PT CT 6212430



**SHEET 4 OF 4**

Significant tree  
Regulated tree

No. of proposed allotments 1190  
Total area 185.8ha  
Area of Development 168.5ha  
Reserve area 71.29ha  
Length of new roads 19.8k

Contour interval 2m.  
Datum AHD.

Vide Titles for disposition of easements.  
Road pavements shown are indicative only.  
\*\*Not to be used for detailed engineering design.\*\*

Dimensions and areas are subject to survey.

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**Glenn Ian Hordacre**  
LICENSED SURVEYOR

REV	DATE	DESCRIPTION
001	12-05-2019	ISSUED FOR PROPOSAL

Alexander & Symonds Pty Ltd  
12 Kings Highway, Gawler East, SA 5153  
PO Box 1000, Gawler East, SA 5153  
080 8507 715 1000

**Alexander  
Symonds**  
CONSTRUCTION

• Property • Land Development •  
• Construction • Mining •  
• Spatial Information Management •

## Appendix 3. Proposed plans and elevations

Brown Falconer



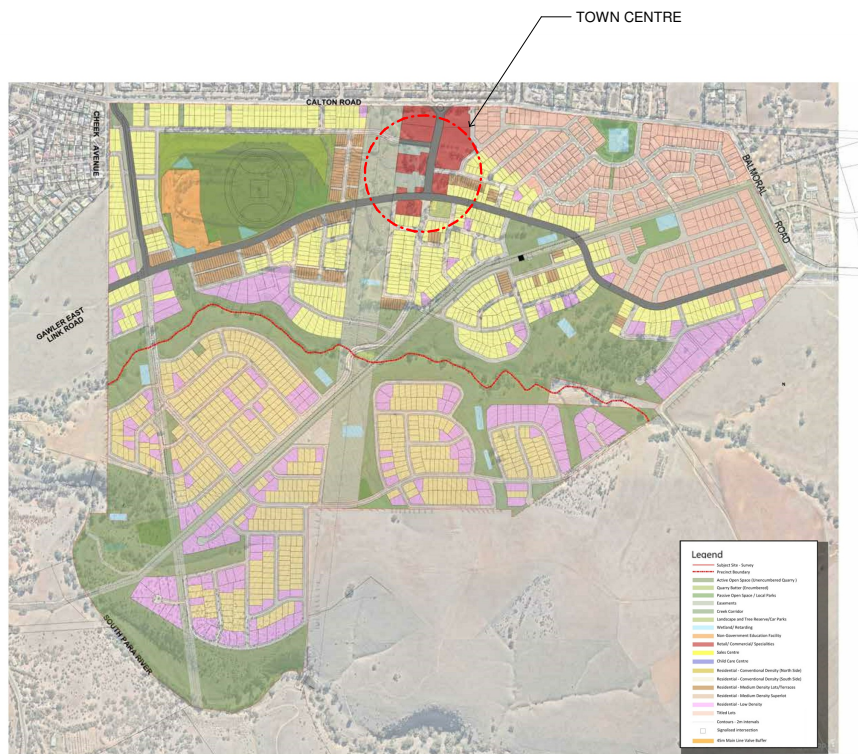


Figure 1. Springwood Masterplan

## LOCALITY PLAN

# SPRINGWOOD TOWN CENTRE

## PLANNING APPLICATION PACKAGE

LODGEMENT ISSUE 14 JUNE 2019

DA01	COVER SHEET & LOCATION PLAN	1
DA02	SITE PLAN	1
DA03	ELEVATIONS	1
DA04	SITE SECTIONS	1
DA06	3D IMAGES	1
DA05	LANDSCAPING PLAN	1



3D VIEW STREET VIEW TO NORTH WEST



ARCADIAN PROPERTY

SPRINGWOOD TOWN CENTRE

COVER SHEET &amp; LOCATION PLAN

Scale  
Drawn JSM  
Date 16/05/2019  
Job No. 2016040  
Dwg No. **2569 DA01** Rev: **1** A1 SHEET

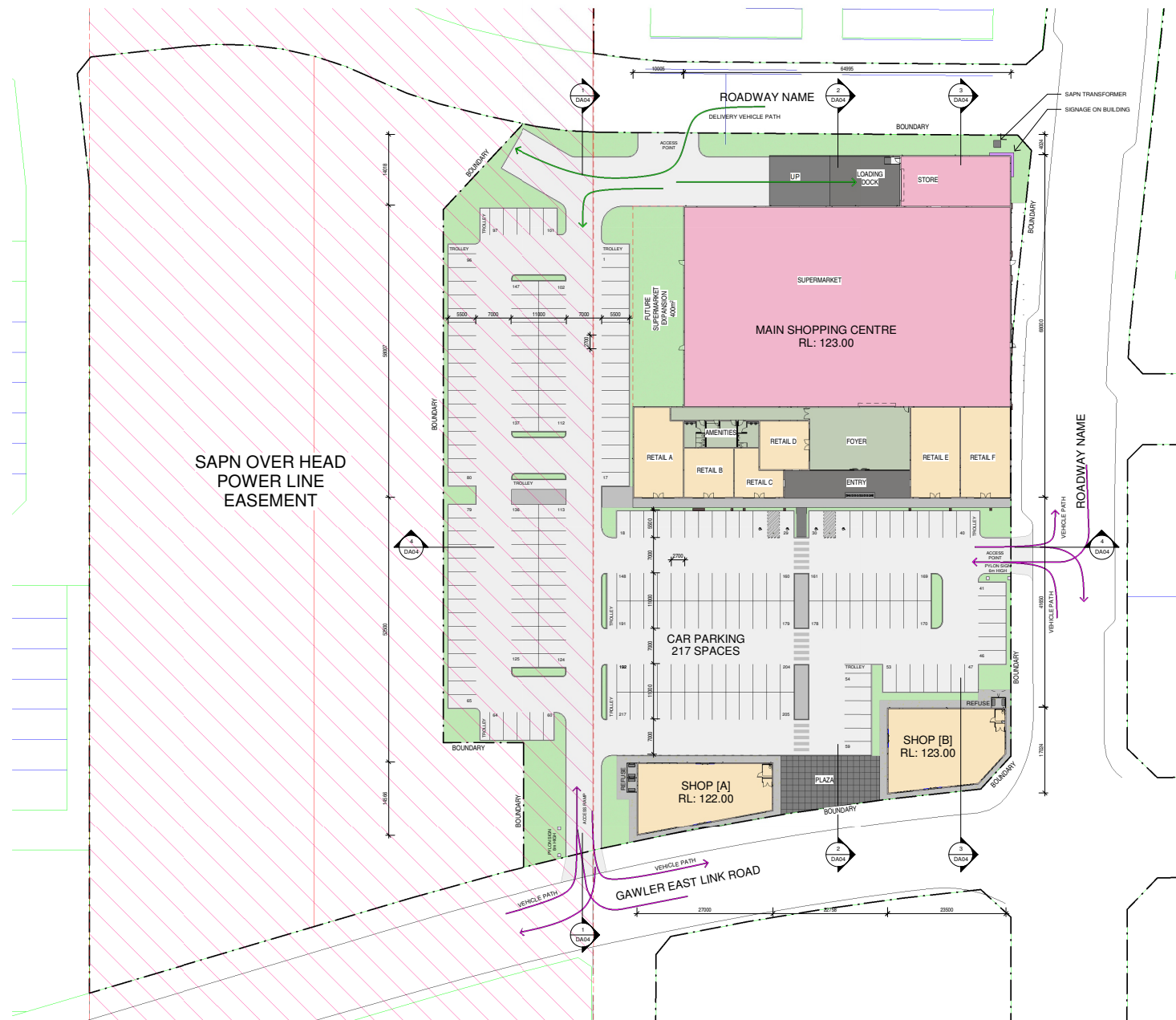
# DA ISSUE

ISSUED FOR DEVELOPMENT APPROVAL  
14/05/2019 11:36:44 AM

Rev: 1 Amendment: 1 Date: 14/05/2019  
Location: ISSUE

## SITE SUMMARY

SITE AREA	27,980m <sup>2</sup>
SUPERMARKET	2,600m <sup>2</sup>
RETAIL (A-F)	780m <sup>2</sup>
SHOP A	325m <sup>2</sup>
SHOP B	355m <sup>2</sup>
TOTAL BUILDING AREA	4,060m <sup>2</sup>
CARPARKING SPACES	217



SITE PLAN

1:400



**BROWN FALCONER**

38 Chesser Street, Adelaide, South Australia 5000  
Telephone: 08 8223 5800 Facsimile: 08 8223 2440  
ABN 65 007 846 588 brownfalconer.com.au

ARCADIAN PROPERTY

SPRINGWOOD TOWN CENTRE

SITE PLAN

Scale As Indicated

Drawn JSM

Date 16/05/2019

Job No. 2016040

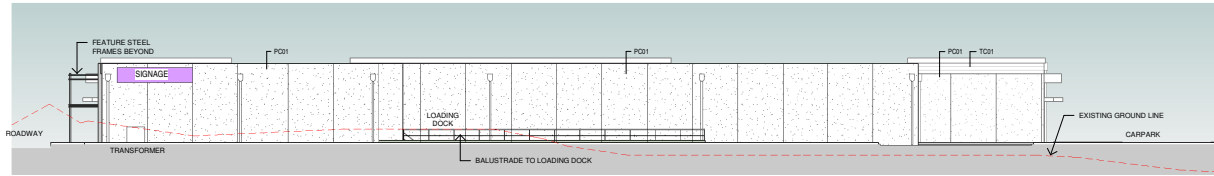
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Rev: 1

A1 SHEET

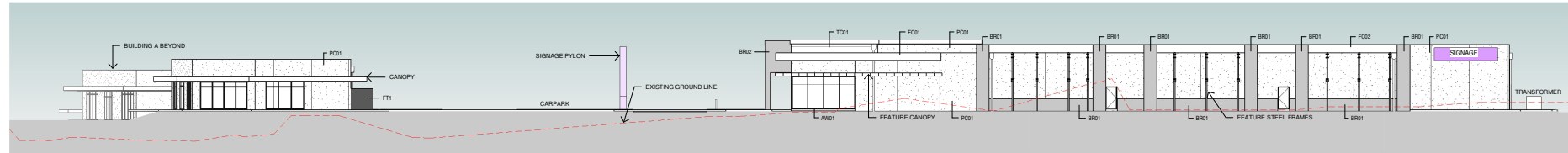


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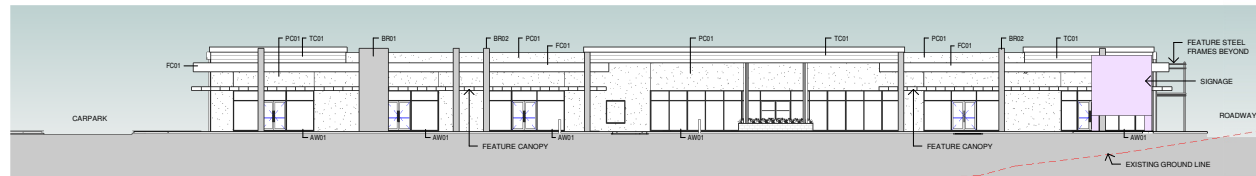
North - Supermarket

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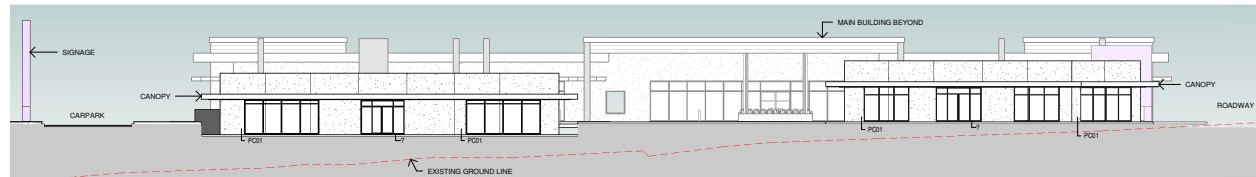
East - Street Elevation

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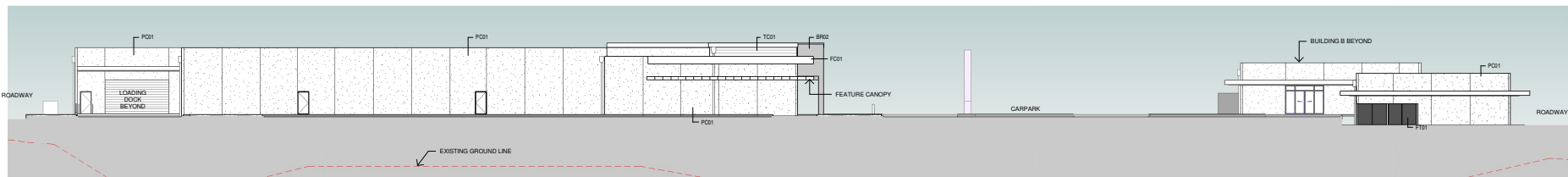
South - Supermarket

1:200



South - Street Elevation

1:200



West

1:200



**MATERIAL LEGEND**

AW01 POWDER COATED ALUMINUM DOOR/WINDOW  
PC01 PAINTED FIBRE CEMENT FACADE  
PC02 PAINTED FIBRE CEMENT FACADE



**SPRINGWOOD**  
CAWIFR FAST

**BROOK FALCONER**  
28 Chesser Street, Adelaide, South Australia 5000  
Telephone: 08 8223 5800 Facsimile: 08 8223 2440  
ABN 65 007 846 588  
brownfalconer.com.au

ARCADIAN PROPERTY

SPRINGWOOD TOWN CENTRE

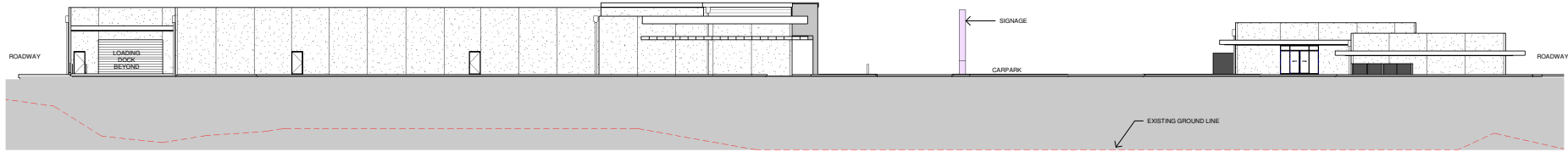
ELEVATIONS

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Date 16/05/2019  
Job No. 2016049  
Dwg No. 2569 DA03 Rev. 1 A1 SHEET

# DA ISSUE

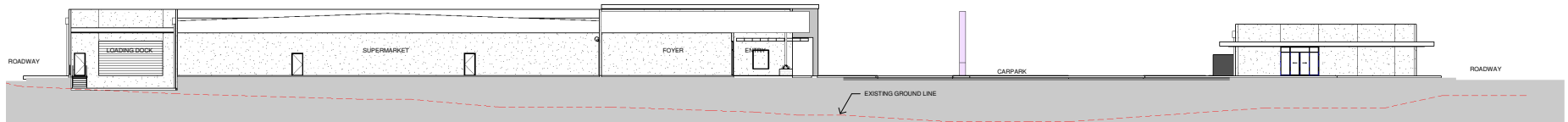
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Rev	Description	Date
1	LODGEMENT ISSUE	14/06/19



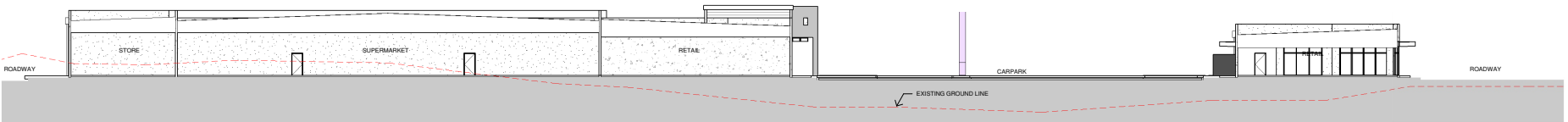
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DA - Section 2

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DA - Section 3

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DA - Section 4

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**SPRINGWOOD**  
CRAWFORD FAST

**BROWN FALCONER**  
28 Chesser Street, Adelaide, South Australia 5000  
Telephone: 08 8223 5800 Facsimile: 08 8223 2440  
ABN 65 007 846 588 brownfalconer.com.au

ARCADIAN PROPERTY

SPRINGWOOD TOWN CENTRE

SITE SECTIONS

Scale 1:200  
Drawn JSM  
Date 16/05/2019  
Job No. 2016049  
Dwg No. 2569 DA04 Rev: 1 A1 SHEET

# DA ISSUE

ISSUED FOR DEVELOPMENT APPROVAL  
14/09/2019 11:30:49 AM

Rev. 1 14/09/19

## LANDSCAPE LEGEND

NEW LANDSCAPING  
WITHIN MULCHED  
BEDS  
AREAS WITHIN  
PROPOSED SITE ONLY

## NOTES

> ALL LANDSCAPING TO BE IRRIGATED  
> MULCH TO BE INSTALLED TO ALL  
GARDEN BEDS

## TREE SCHEDULE

T1 CALLERY PEAR  
T2 JAPANESE ELM

## PLANT SCHEDULE

ALL TUBE STOCK

## SEDGES / GRASSES

AGAPANTHUS SMALL CULTIVARS ASC 2 per m<sup>2</sup>  
DISTICHIS DISTICHOPHYLLA DD 2 per m<sup>2</sup>  
LOMANDRA CULTIVARS LC 2 per m<sup>2</sup>  
NANDINA SPP NS 2 per m<sup>2</sup>  
POA SPP PS 2 per m<sup>2</sup>

## GROUND COVERS

ASPERULA CONFERTA AC 2 per m<sup>2</sup>  
LEPTOSPERMUM LANGIERUM LL 2 per m<sup>2</sup>



**BROOK FALCONER**

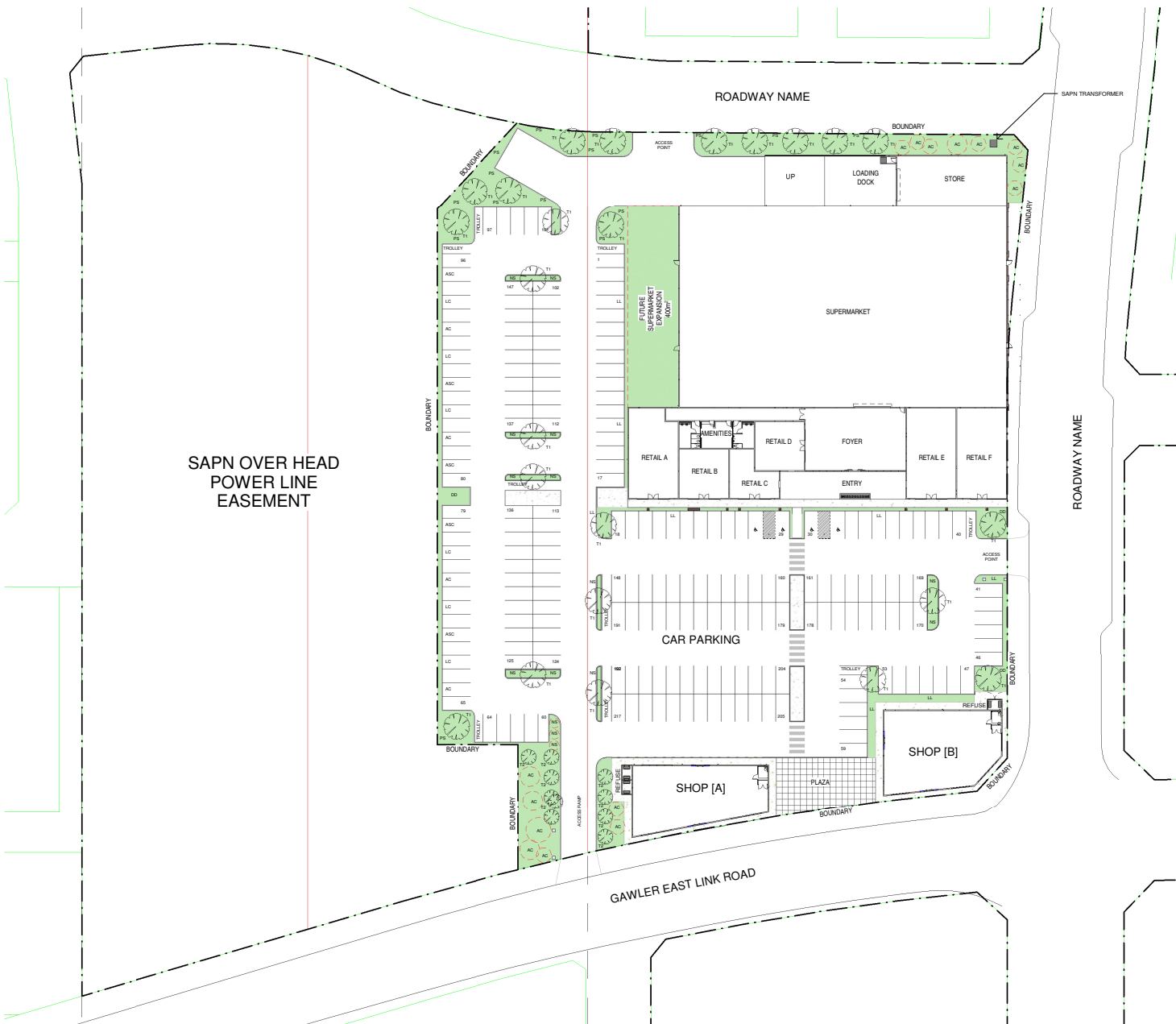
38 Chesser Street, Adelaide, South Australia 5000  
Telephone: 08 8223 5800 Facsimile: 08 8223 2440  
ABN 65 007 846 588 brookfalconer.com.au

ARCADIAN PROPERTY

SPRINGWOOD TOWN CENTRE

LANDSCAPING PLAN

Scale As indicated  
Drawn Author  
Date 06/07/19  
Job No. 2016040  
Dwg No. 2569 DA05 Rev. 1 A1 SHEET



LANDSCAPING PLAN  
1:400



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## Appendix 4. Traffic and Parking Review

CIRQA





**SPRINGWOOD SHOPPING CENTRE  
GAWLER EAST**

**TRAFFIC AND PARKING REPORT**



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## DOCUMENT CONTROL

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Client:	Arcadian Property			
Client contact:	Warwick Mittiga			
Version	Date	Details/status	Prepared by	Approved by
Draft	5 Jun 19	For review	BNW	BNW
V1	12 Jun 19	For submission	BNW	BNW

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## **1. INTRODUCTION**

CIRQA has been engaged to provide design and assessment advice for the Springwood Shopping Centre at Gawler East. Specifically, CIRQA has been engaged to provide advice in respect to traffic and parking aspects of the proposal.

This report provides a review of the subject site, the proposed development (and its associated operation), its access and parking provisions and the associated traffic impact on the adjacent road network. The traffic and parking assessments have been based upon plans prepared by Brown Falconer (drawing no. 2569-DA01 to DA05, dated 16 May 2019, refer Appendix A).

## **2. BACKGROUND**

### **2.1 SUBJECT SITE**

The subject site is located adjacent the north-western corner of the Gawler East Link Road and a new north-south collector road at Gawler East. The site (Lot 2046) forms part of the Village Centre of the broader Springwood development (approximately 2 km east of the township of Gawler). The site will be bound by the Gawler East Link Road to the south, a new north-south collector road to the east, a local road to the north and a reserve/easement to the west.

The City of Gawler's Development Plan identifies that the site is located within the Residential Zone (Gawler East) Zone and the Mixed Use Centre Policy Area.

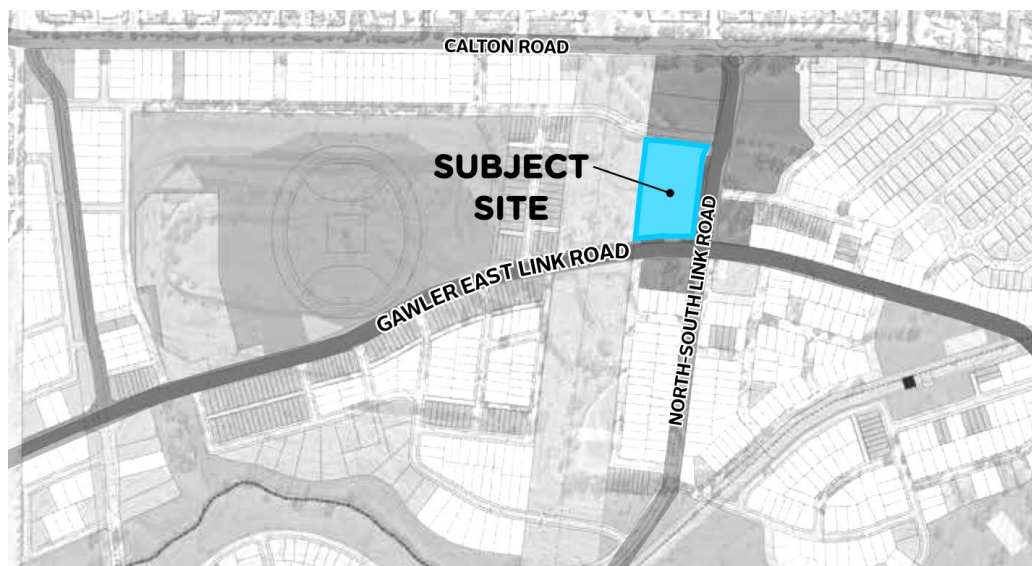
### **2.2 ADJACENT ROAD NETWORK**

The Gawler East Link Road is a proposed major link road which will provide access to/from Main North Road without the need to travel through the centre of the Gawler township. In the vicinity of the subject site, the road will generally comprise a single traffic lane and bicycle lane in each direction separated by a raised central median (with opportunities for sheltered right turn lanes where required). Footpaths will be provided on both sides of the Gawler East Link Road. Previous traffic forecasts (by others) indicate volumes on the Gawler East Link Road will ultimately be in the order of 12,500 vehicles per day (vpd).

Adjacent the site, the Gawler East Link Road will intersect with the new north-south collector road. The intersection will be controlled with traffic signals (including pedestrian crossing facilities on all legs). Additional right turn lanes will be provided on all approaches. The design of the intersection has been based on detailed analysis prepared as part of the Gawler East Link Road planning and design inputs (by others). It is understood that a 50 km/h speed limit will apply on Gawler East Link Road.

The north-south collector road will also comprise a single traffic lane and a bicycle lane in each direction separated by a raised central median (or right turn lanes where required). Footpaths will be provided on both sides of the north-south road. A 50 km/h speed limit will apply on the north-south collector road. Previous forecasting for the broader development has indicated ultimate volumes in the order of 6,500 vpd will be experienced on the north-south collector road.

Figure 1 illustrates the location of the subject site and adjacent road network.



*Figure 1 – Location of the subject site and adjacent road network*

*(Source: after Tract, 2019)*

## **2.3 WALKING AND CYCLING**

The site will be highly accessible for residents of the surrounding area by walking and cycling. Footpaths will be provided on both sides of all bounding roads and on-road cycling facilities will be provided on the Gawler East Link Road and north-south collector road (including 'bicycle boxes' at the signalised intersection approaches).

## **2.4 PUBLIC TRANSPORT**

It is understood that public transport (bus) services will operate along the Gawler East Link Road with bus stops in close proximity to the subject site.

### **3. PROPOSED DEVELOPMENT**

#### **3.1 LAND USE AND YIELD**

The proposed development comprises the construction of a new shopping centre on the subject site. Specifically, the shopping centre will comprise a 2,600 m<sup>2</sup> floor area supermarket and a total of 1,460 m<sup>2</sup> floor area of specialty tenancies (totalling 4,060 m<sup>2</sup> of floor area).

#### **3.2 ACCESS AND PARKING DESIGN**

The site will be serviced by a 217-space parking area (inclusive of 4 spaces reserved exclusively for use by people with disabilities). A further 20 bicycle parking spaces are also proposed. The parking area will comply with the requirements of Australian/New Zealand Standard, *Parking Facilities Part 1: Off-street car parking* (AS/NZS 2890.1:2004) and Australian/New Zealand Standard, *Parking Facilities Part 6: Off-street parking for people with disabilities* (AS/NZS 2890.6:2009) in that:

- regular parking spaces will be 2.7 m wide and 5.4 m long (or 4.8 m long with 0.6 m overhang);
- disabled parking spaces will be at least 2.4 m wide and 5.4 m long (with an adjacent shared space of the same dimension);
- the parking aisles will exceed 6.6 m in width;
- 0.3 m clearance will be provided to all objects greater than 0.15 m in height; and
- pedestrian sightlines will be provided at the site's property boundary.

Vehicle access to the site will be provided via the following locations:

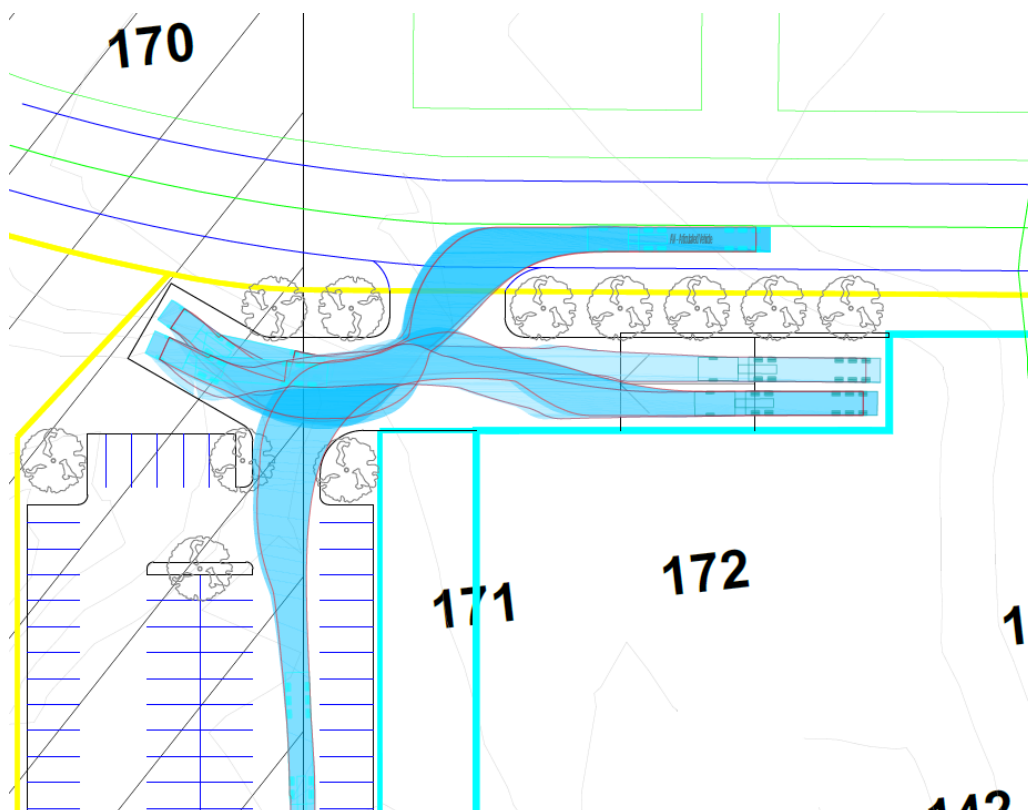
- a two-way access point on the Gawler East Link Road (which will accommodate left-in, left-out, right-in and right-out movements);
- a two-way access point on the north-south collector road (which will accommodate left-in, left-out, right-in and right-out movements); and
- a two-way access point on the local road to the north which will primarily provide access for commercial vehicle movements, albeit customers will also be able to utilise the access point.

All vehicles will be able to enter and exit the site in a forward direction.



### 3.3 SERVICING AND DELIVERIES

Commercial vehicle access will primarily be accommodated via the loading area located at the rear (north) of the shopping centre. This will include a recessed dock for the supermarket tenant. Figure 2 illustrates the turn path of a Semi-Trailer entering and exiting the dock.



*Figure 2 - Semi-Trailer access movements at the loading dock*

In addition, other minor service and delivery movements will be undertaken via the car park (particularly for the individual tenancies fronting the Gawler East Link Road. Such movements would be undertaken via medium rigid vehicles or smaller and would typically be undertaken outside of peak trading periods (particularly refuse collection which would occur outside of trading hours).

## 4. PARKING ASSESSMENT

### 4.1 CAR PARKING

The Development Plan identifies a rate of 5.5 spaces per 100 m<sup>2</sup> floor area for shop (retail use) within the Mixed Use Centre Policy Area 3 of the Residential (Gawler East) Zone). On this basis, there is a theoretical requirement for 224 Parking spaces associated with the proposal. The provision of 217 spaces therefore results in a theoretical shortfall of 7 spaces.

However, recent surveys of shopping centres indicate peak parking demands are more commonly in the range of 3.0 to 5.0 spaces per 100 m<sup>2</sup> floor area. These rates equate to peak demands in the order of 122 to 203 spaces. Such demands would be accommodated by the proposed level of parking provisions.

In comparison, the Aurecon "Parking Spaces for Urban Places" (parking provision) study recommended a rate of 6 spaces per 100 m<sup>2</sup> for shops within a retail centre with a potential discount of up to 25% (depending on a variety of factors). A discount of 15% is considered appropriate for application to the proposal. On this basis, there would be a requirement for 207 parking spaces. The proposed provision would meet such a requirement.

On the basis of the above, it is considered that the proposed level of parking will more than adequately accommodate peak demands.

## **4.2 BICYCLE PARKING**

The Development Plan does not identify rates for the provision of bicycle parking. However, the following rates are typically applied for bicycle parking provision for shops:

- one bicycle parking space for every 300 m<sup>2</sup> of gross leasable floor area for employees, plus
- one visitor space for every 600 m<sup>2</sup> of gross leasable floor area for shoppers.

On the basis of the above, the provision of 20 bicycle spaces (10 bicycle rails) is considered sufficient.

## **5. TRAFFIC ASSESSMENT**

### **5.1 TRAFFIC GENERATION AND DISTRIBUTION**

Previous traffic impact assessments have been undertaken as part of the broader Springwood land division project. This has included consideration of the traffic generation of development within the town centre including the shopping centre site.

Previous planning and design inputs (by others) have also considered the appropriateness of the proposed access arrangements for the shopping centre. Adequate design provisions have been made to accommodate the proposed turning movements at all access points.

The broader land division layout (including adjacent signalised intersection) has been designed to accommodate future traffic flows, including traffic generation

associated with the subject site. As such, the traffic movements associated with the proposed use will be easily accommodated in the surrounding road network.

Notwithstanding the above, a high level assessment of conditions at the two primary access points has been undertaken utilising SIDRA intersection analysis. The assessment has been based on the traffic generation and distribution detailed in the GTA "Springwood Land Division – Transport Impact Assessment" report (2019). The worst-case pm weekday peak has been modelled based on the traffic generation rates for retail uses (as identified in the GTA report) and forecast future movements on the adjacent road network.

Detailed output of the SIDRA modelling is provided in Appendix B. The SIDRA analysis confirms that the proposed access arrangements will have sufficient capacity to accommodate the forecast traffic movements associated with the proposed development (as well as other movements on the adjacent roads).

## **6. SUMMARY**

The proposal comprises the development of a shopping centre within the Springwood land development. Specifically, the shopping centre will include a supermarket as well as specialty tenancies.

The site will be well serviced by active (walking and cycling) facilities and public transport services which will minimise reliance on private motor vehicles. Access to the parking areas will be provided via the Gawler East Link Road, a new north-south collector road and a local road (adjacent the site's northern boundary).

The development will be serviced by parking areas containing 217 vehicle parking spaces (in total). A total of 20 bicycle spaces will also be provided. The proposed level of parking will result in a small shortfall against the requirements of Council's Development Plan. However, based on more recent surveys for shopping centres, it is considered that more than adequate parking will be provided as part of the development.

Traffic impacts associated with the proposal have been assessed as part of the broader land division application and planning for the Gawler East Link Road. The proposed access arrangements and associated traffic impacts will be in line with the previous assessments. Additional SIDRA modelling of the primary access points indicates that the forecast movements will be adequately accommodated.

# **APPENDIX A**

## **BROWN FALCONER'S PLANS DATED MAY 2019**

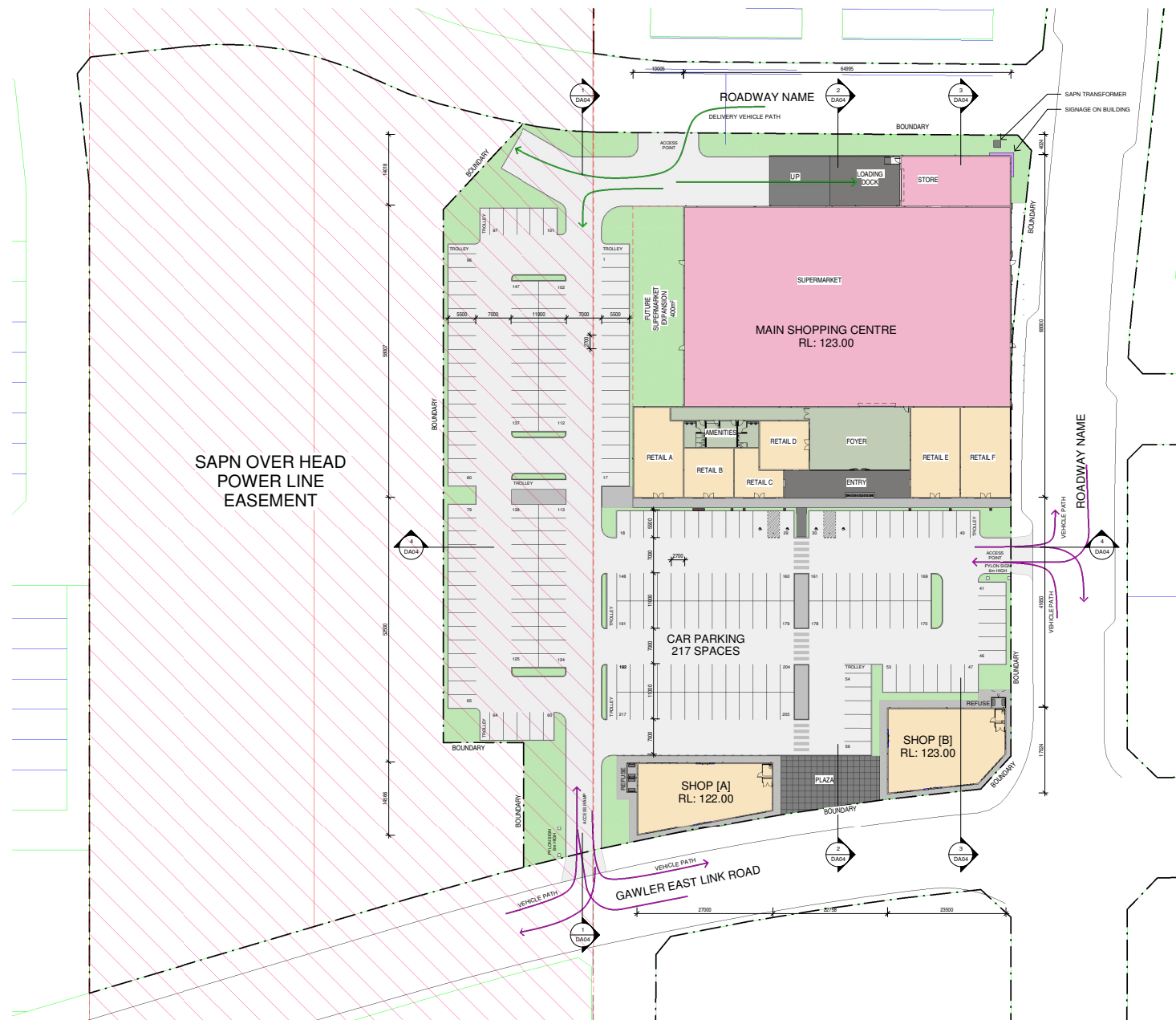
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14/05/2019 11:36:44 AM

Rev: 1 Amendment: 1 Date: 14/05/2019  
Location: ISSUE

## SITE SUMMARY

SITE AREA	27,980m <sup>2</sup>
SUPERMARKET	2,600m <sup>2</sup>
RETAIL (A-F)	780m <sup>2</sup>
SHOP A	325m <sup>2</sup>
SHOP B	355m <sup>2</sup>
TOTAL BUILDING AREA	4,060m <sup>2</sup>
CARPARKING SPACES	217



SITE PLAN

1:400

**SPRINGWOOD**  
GAWLER EAST

**BROWN FALCONER**  
38 Chesser Street, Adelaide, South Australia 5000  
Telephone: 08 8223 5800 Facsimile: 08 8223 2440  
ABN 65 007 846 588 brownfalconer.com.au

ARCADIAN PROPERTY

SPRINGWOOD TOWN CENTRE

SITE PLAN

Scale As Indicated

Drawn JSM

Date 16/05/2019

Job No. 2016040

Dwg No. 2569 DA02 Rev: 1 A1 SHEET



## **APPENDIX B**

### **SIDRA OUTPUT**

## MOVEMENT SUMMARY

 **Site: 101 [North-South Link Rd - Future PM Peak ]**

New Site  
 Giveway / Yield (Two-Way)

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Total veh/h	Flows HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: North-South Link Rd (S)											
1	L2	84	5.0	0.153	5.6	LOS A	0.0	0.0	0.00	0.18	56.6
2	T1	200	5.0	0.153	0.0	LOS A	0.0	0.0	0.00	0.18	58.4
Approach		284	5.0	0.153	1.7	NA	0.0	0.0	0.00	0.18	57.8
North: North-South Link Rd (N)											
8	T1	379	5.0	0.201	0.0	LOS A	0.0	0.0	0.00	0.00	60.0
9	R2	53	5.0	0.040	6.5	LOS A	0.2	1.3	0.38	0.60	51.9
Approach		432	5.0	0.201	0.8	NA	0.2	1.3	0.05	0.07	58.8
West: Shopping Centre Access											
10	L2	53	5.0	0.084	6.3	LOS A	0.3	2.3	0.37	0.62	51.7
12	R2	21	5.0	0.084	11.6	LOS B	0.3	2.3	0.37	0.62	51.2
Approach		74	5.0	0.084	7.8	LOS A	0.3	2.3	0.37	0.62	51.6
All Vehicles		789	5.0	0.201	1.8	NA	0.3	2.3	0.06	0.16	57.7

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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Project: P:\2019\19140 Springwood Shopping Centre\SIDRA\19140\_12JUN19.sip7

## MOVEMENT SUMMARY

 **Site: 101 [Gawler East Link Rd - Future PM Peak]**

New Site  
Giveway / Yield (Two-Way)

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Total veh/h	Flows HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
East: Gawler East Link Rd (E)											
8	T1	426	5.0	0.226	0.0	LOS A	0.0	0.0	0.00	0.00	60.0
9	R2	63	5.0	0.145	12.9	LOS B	0.5	3.7	0.72	0.89	47.8
Approach		489	5.0	0.226	1.7	NA	0.5	3.7	0.09	0.11	58.0
North: Shopping Centre Access											
10	L2	84	5.0	0.399	15.6	LOS C	1.5	11.2	0.83	1.00	43.0
12	R2	21	5.0	0.399	48.9	LOS E	1.5	11.2	0.83	1.00	42.8
Approach		105	5.0	0.399	22.3	LOS C	1.5	11.2	0.83	1.00	42.9
West: Gawler East Link Rd (W)											
1	L2	84	5.0	0.047	5.6	LOS A	0.0	0.0	0.00	0.58	53.4
2	T1	784	5.0	0.415	0.1	LOS A	0.0	0.0	0.00	0.00	59.9
Approach		868	5.0	0.415	0.6	NA	0.0	0.0	0.00	0.06	59.2
All Vehicles		1463	5.0	0.415	2.5	NA	1.5	11.2	0.09	0.14	57.2

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).

Vehicle movement LOS values are based on average delay per movement.

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Project: P:\2019\19140 Springwood Shopping Centre\SIDRA\19140\_12JUN19.sip7

## INTERSECTION SUMMARY

▽ Site: 101 [North-South Link Rd - Future PM Peak ]

New Site  
Giveway / Yield (Two-Way)

Intersection Performance - Hourly Values		
Performance Measure	Vehicles	Persons
Travel Speed (Average)	57.7 km/h	57.7 km/h
Travel Distance (Total)	798.2 veh-km/h	957.8 pers-km/h
Travel Time (Total)	13.8 veh-h/h	16.6 pers-h/h
Demand Flows (Total)	789 veh/h	947 pers/h
Percent Heavy Vehicles (Demand)	5.0 %	
Degree of Saturation	0.201	
Practical Spare Capacity	388.4 %	
Effective Intersection Capacity	3935 veh/h	
Control Delay (Total)	0.39 veh-h/h	0.47 pers-h/h
Control Delay (Average)	1.8 sec	1.8 sec
Control Delay (Worst Lane)	7.8 sec	
Control Delay (Worst Movement)	11.6 sec	11.6 sec
Geometric Delay (Average)	1.5 sec	
Stop-Line Delay (Average)	0.3 sec	
Idling Time (Average)	0.1 sec	
Intersection Level of Service (LOS)	NA	
95% Back of Queue - Vehicles (Worst Lane)	0.3 veh	
95% Back of Queue - Distance (Worst Lane)	2.3 m	
Queue Storage Ratio (Worst Lane)	0.00	
Total Effective Stops	127 veh/h	153 pers/h
Effective Stop Rate	0.16 per veh	0.16 per pers
Proportion Queued	0.06	0.06
Performance Index	15.0	15.0
Cost (Total)	324.08 \$/h	324.08 \$/h
Fuel Consumption (Total)	61.4 L/h	
Carbon Dioxide (Total)	146.5 kg/h	
Hydrocarbons (Total)	0.011 kg/h	
Carbon Monoxide (Total)	0.168 kg/h	
NOx (Total)	0.264 kg/h	

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).

NA: Intersection LOS for Vehicles is Not Applicable for two-way sign control since the average intersection delay is not a good LOS measure due to zero delays associated with major road movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Intersection Performance - Annual Values		
Performance Measure	Vehicles	Persons
Demand Flows (Total)	378,947 veh/y	454,737 pers/y
Delay	187 veh-h/y	224 pers-h/y
Effective Stops	61,010 veh/y	73,212 pers/y
Travel Distance	383,125 veh-km/y	459,750 pers-km/y
Travel Time	6,637 veh-h/y	7,964 pers-h/y
Cost	155,559 \$/y	155,559 \$/y
Fuel Consumption	29,495 L/y	
Carbon Dioxide	70,322 kg/y	
Hydrocarbons	5 kg/y	
Carbon Monoxide	81 kg/y	
NOx	127 kg/y	

## INTERSECTION SUMMARY

 Site: 101 [Gawler East Link Rd - Future PM Peak]

New Site  
Giveway / Yield (Two-Way)

Intersection Performance - Hourly Values		
Performance Measure	Vehicles	Persons
Travel Speed (Average)	57.2 km/h	57.2 km/h
Travel Distance (Total)	1478.8 veh-km/h	1774.6 pers-km/h
Travel Time (Total)	25.8 veh-h/h	31.0 pers-h/h
Demand Flows (Total)	1463 veh/h	1756 pers/h
Percent Heavy Vehicles (Demand)	5.0 %	
Degree of Saturation	0.415	
Practical Spare Capacity	100.5 %	
Effective Intersection Capacity	3524 veh/h	
Control Delay (Total)	1.02 veh-h/h	1.23 pers-h/h
Control Delay (Average)	2.5 sec	2.5 sec
Control Delay (Worst Lane)	22.3 sec	
Control Delay (Worst Movement)	48.9 sec	48.9 sec
Geometric Delay (Average)	1.0 sec	
Stop-Line Delay (Average)	1.6 sec	
Idling Time (Average)	1.1 sec	
Intersection Level of Service (LOS)	NA	
95% Back of Queue - Vehicles (Worst Lane)	1.5 veh	
95% Back of Queue - Distance (Worst Lane)	11.2 m	
Queue Storage Ratio (Worst Lane)	0.01	
Total Effective Stops	210 veh/h	252 pers/h
Effective Stop Rate	0.14 per veh	0.14 per pers
Proportion Queued	0.09	0.09
Performance Index	29.7	29.7
Cost (Total)	592.22 \$/h	592.22 \$/h
Fuel Consumption (Total)	110.2 L/h	
Carbon Dioxide (Total)	262.8 kg/h	
Hydrocarbons (Total)	0.019 kg/h	
Carbon Monoxide (Total)	0.303 kg/h	
NOx (Total)	0.457 kg/h	

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).

NA: Intersection LOS for Vehicles is Not Applicable for two-way sign control since the average intersection delay is not a good LOS measure due to zero delays associated with major road movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Intersection Performance - Annual Values		
Performance Measure	Vehicles	Persons
Demand Flows (Total)	702,316 veh/y	842,779 pers/y
Delay	492 veh-h/y	590 pers-h/y
Effective Stops	100,849 veh/y	121,019 pers/y
Travel Distance	709,844 veh-km/y	851,813 pers-km/y
Travel Time	12,401 veh-h/y	14,882 pers-h/y
Cost	284,266 \$/y	284,266 \$/y
Fuel Consumption	52,886 L/y	
Carbon Dioxide	126,125 kg/y	
Hydrocarbons	9 kg/y	
Carbon Monoxide	146 kg/y	
NOx	219 kg/y	





## Appendix 4. Traffic and Parking Review

CIRQA



**SPRINGWOOD SHOPPING CENTRE  
GAWLER EAST**  
TRAFFIC AND PARKING REPORT



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## DOCUMENT CONTROL

Report title:	Springwood Shopping Centre, Gawler East – Traffic and Parking report			
Project number:	19140			
Client:	Arcadian Property			
Client contact:	Warwick Mittiga			
Version	Date	Details/status	Prepared by	Approved by
Draft	5 Jun 19	For review	BNW	BNW
V1	12 Jun 19	For submission	BNW	BNW

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## **1. INTRODUCTION**

CIRQA has been engaged to provide design and assessment advice for the Springwood Shopping Centre at Gawler East. Specifically, CIRQA has been engaged to provide advice in respect to traffic and parking aspects of the proposal.

This report provides a review of the subject site, the proposed development (and its associated operation), its access and parking provisions and the associated traffic impact on the adjacent road network. The traffic and parking assessments have been based upon plans prepared by Brown Falconer (drawing no. 2569-DA01 to DA05, dated 16 May 2019, refer Appendix A).

## **2. BACKGROUND**

### **2.1 SUBJECT SITE**

The subject site is located adjacent the north-western corner of the Gawler East Link Road and a new north-south collector road at Gawler East. The site (Lot 2046) forms part of the Village Centre of the broader Springwood development (approximately 2 km east of the township of Gawler). The site will be bound by the Gawler East Link Road to the south, a new north-south collector road to the east, a local road to the north and a reserve/easement to the west.

The City of Gawler's Development Plan identifies that the site is located within the Residential Zone (Gawler East) Zone and the Mixed Use Centre Policy Area.

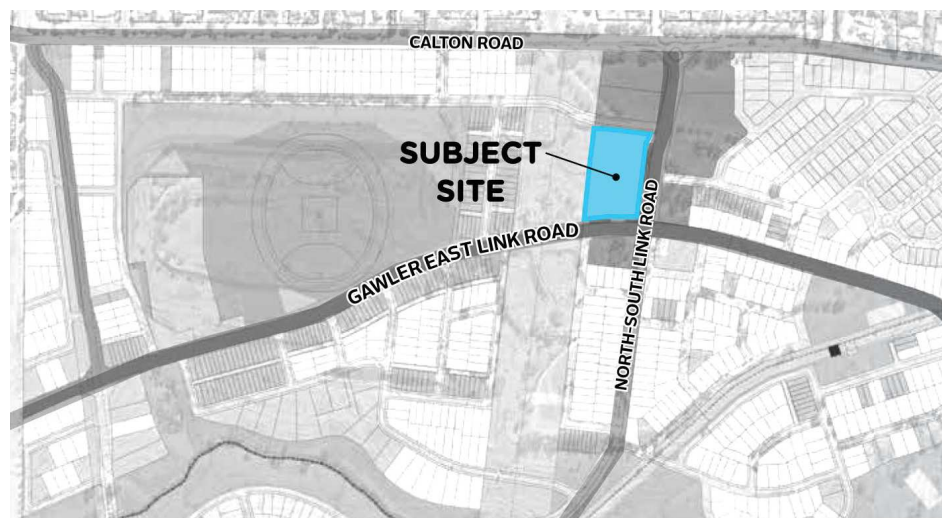
### **2.2 ADJACENT ROAD NETWORK**

The Gawler East Link Road is a proposed major link road which will provide access to/from Main North Road without the need to travel through the centre of the Gawler township. In the vicinity of the subject site, the road will generally comprise a single traffic lane and bicycle lane in each direction separated by a raised central median (with opportunities for sheltered right turn lanes where required). Footpaths will be provided on both sides of the Gawler East Link Road. Previous traffic forecasts (by others) indicate volumes on the Gawler East Link Road will ultimately be in the order of 12,500 vehicles per day (vpd).

Adjacent the site, the Gawler East Link Road will intersect with the new north-south collector road. The intersection will be controlled with traffic signals (including pedestrian crossing facilities on all legs). Additional right turn lanes will be provided on all approaches. The design of the intersection has been based on detailed analysis prepared as part of the Gawler East Link Road planning and design inputs (by others). It is understood that a 50 km/h speed limit will apply on Gawler East Link Road.

The north-south collector road will also comprise a single traffic lane and a bicycle lane in each direction separated by a raised central median (or right turn lanes where required). Footpaths will be provided on both sides of the north-south road. A 50 km/h speed limit will apply on the north-south collector road. Previous forecasting for the broader development has indicated ultimate volumes in the order of 6,500 vpd will be experienced on the north-south collector road.

Figure 1 illustrates the location of the subject site and adjacent road network.



*Figure 1 – Location of the subject site and adjacent road network*

*(Source: after Tract, 2019)*

### **2.3 WALKING AND CYCLING**

The site will be highly accessible for residents of the surrounding area by walking and cycling. Footpaths will be provided on both sides of all bounding roads and on-road cycling facilities will be provided on the Gawler East Link Road and north-south collector road (including 'bicycle boxes' at the signalised intersection approaches).

### **2.4 PUBLIC TRANSPORT**

It is understood that public transport (bus) services will operate along the Gawler East Link Road with bus stops in close proximity to the subject site.



### **3. PROPOSED DEVELOPMENT**

#### **3.1 LAND USE AND YIELD**

The proposed development comprises the construction of a new shopping centre on the subject site. Specifically, the shopping centre will comprise a 2,600 m<sup>2</sup> floor area supermarket and a total of 1,460 m<sup>2</sup> floor area of specialty tenancies (totalling 4,060 m<sup>2</sup> of floor area).

#### **3.2 ACCESS AND PARKING DESIGN**

The site will be serviced by a 217-space parking area (inclusive of 4 spaces reserved exclusively for use by people with disabilities). A further 20 bicycle parking spaces are also proposed. The parking area will comply with the requirements of Australian/New Zealand Standard, *Parking Facilities Part 1: Off-street car parking* (AS/NZS 2890.1:2004) and Australian/New Zealand Standard, *Parking Facilities Part 6: Off-street parking for people with disabilities* (AS/NZS 2890.6:2009) in that:

- regular parking spaces will be 2.7 m wide and 5.4 m long (or 4.8 m long with 0.6 m overhang);
- disabled parking spaces will be at least 2.4 m wide and 5.4 m long (with an adjacent shared space of the same dimension);
- the parking aisles will exceed 6.6 m in width;
- 0.3 m clearance will be provided to all objects greater than 0.15 m in height; and
- pedestrian sightlines will be provided at the site's property boundary.

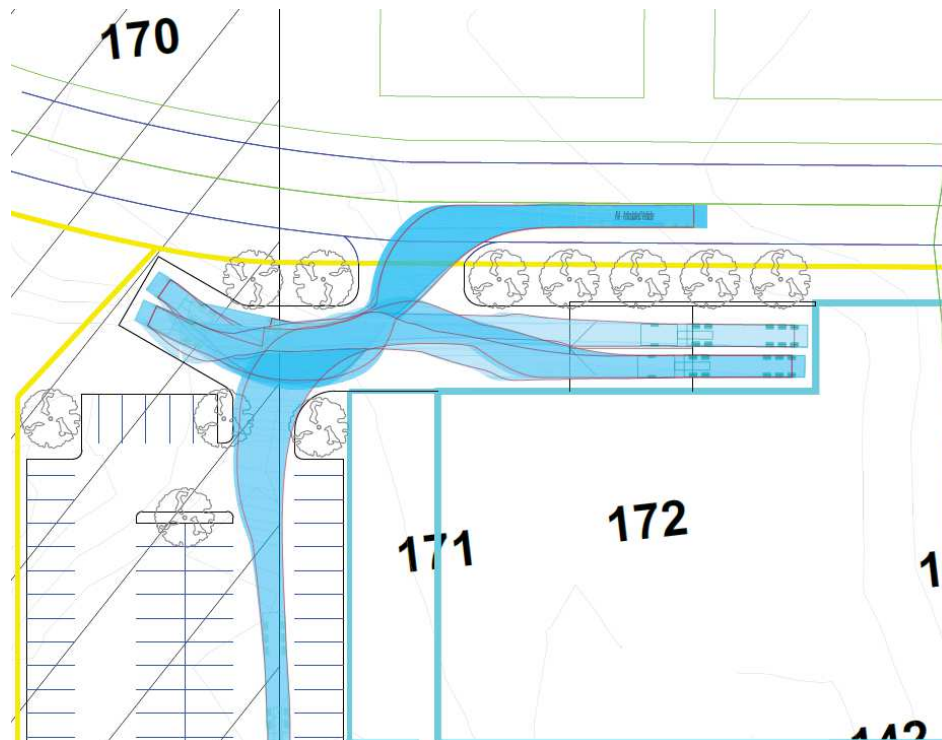
Vehicle access to the site will be provided via the following locations:

- a two-way access point on the Gawler East Link Road (which will accommodate left-in, left-out, right-in and right-out movements);
- a two-way access point on the north-south collector road (which will accommodate left-in, left-out, right-in and right-out movements); and
- a two-way access point on the local road to the north which will primarily provide access for commercial vehicle movements, albeit customers will also be able to utilise the access point.

All vehicles will be able to enter and exit the site in a forward direction.

### 3.3 SERVICING AND DELIVERIES

Commercial vehicle access will primarily be accommodated via the loading area located at the rear (north) of the shopping centre. This will include a recessed dock for the supermarket tenant. Figure 2 illustrates the turn path of a Semi-Trailer entering and exiting the dock.



*Figure 2 - Semi-Trailer access movements at the loading dock*

In addition, other minor service and delivery movements will be undertaken via the car park (particularly for the individual tenancies fronting the Gawler East Link Road. Such movements would be undertaken via medium rigid vehicles or smaller and would typically be undertaken outside of peak trading periods (particularly refuse collection which would occur outside of trading hours).

## 4. PARKING ASSESSMENT

### 4.1 CAR PARKING

The Development Plan identifies a rate of 5.5 spaces per 100 m<sup>2</sup> floor area for shop (retail use) within the Mixed Use Centre Policy Area 3 of the Residential (Gawler East) Zone). On this basis, there is a theoretical requirement for 224 Parking spaces associated with the proposal. The provision of 217 spaces therefore results in a theoretical shortfall of 7 spaces.

However, recent surveys of shopping centres indicate peak parking demands are more commonly in the range of 3.0 to 5.0 spaces per 100 m<sup>2</sup> floor area. These rates equate to peak demands in the order of 122 to 203 spaces. Such demands would be accommodated by the proposed level of parking provisions.

In comparison, the Aurecon "Parking Spaces for Urban Places" (parking provision) study recommended a rate of 6 spaces per 100 m<sup>2</sup> for shops within a retail centre with a potential discount of up to 25% (depending on a variety of factors). A discount of 15% is considered appropriate for application to the proposal. On this basis, there would be a requirement for 207 parking spaces. The proposed provision would meet such a requirement.

On the basis of the above, it is considered that the proposed level of parking will more than adequately accommodate peak demands.

#### **4.2 BICYCLE PARKING**

The Development Plan does not identify rates for the provision of bicycle parking. However, the following rates are typically applied for bicycle parking provision for shops:

- one bicycle parking space for every 300 m<sup>2</sup> of gross leasable floor area for employees, plus
- one visitor space for every 600 m<sup>2</sup> of gross leasable floor area for shoppers.

On the basis of the above, the provision of 20 bicycle spaces (10 bicycle rails) is considered sufficient.

### **5. TRAFFIC ASSESSMENT**

#### **5.1 TRAFFIC GENERATION AND DISTRIBUTION**

Previous traffic impact assessments have been undertaken as part of the broader Springwood land division project. This has included consideration of the traffic generation of development within the town centre including the shopping centre site.

Previous planning and design inputs (by others) have also considered the appropriateness of the proposed access arrangements for the shopping centre. Adequate design provisions have been made to accommodate the proposed turning movements at all access points.

The broader land division layout (including adjacent signalised intersection) has been designed to accommodate future traffic flows, including traffic generation

associated with the subject site. As such, the traffic movements associated with the proposed use will be easily accommodated in the surrounding road network.

Notwithstanding the above, a high level assessment of conditions at the two primary access points has been undertaken utilising SIDRA intersection analysis. The assessment has been based on the traffic generation and distribution detailed in the GTA "Springwood Land Division – Transport Impact Assessment" report (2019). The worst-case pm weekday peak has been modelled based on the traffic generation rates for retail uses (as identified in the GTA report) and forecast future movements on the adjacent road network.

Detailed output of the SIDRA modelling is provided in Appendix B. The SIDRA analysis confirms that the proposed access arrangements will have sufficient capacity to accommodate the forecast traffic movements associated with the proposed development (as well as other movements on the adjacent roads).

## **6. SUMMARY**

The proposal comprises the development of a shopping centre within the Springwood land development. Specifically, the shopping centre will include a supermarket as well as specialty tenancies.

The site will be well serviced by active (walking and cycling) facilities and public transport services which will minimise reliance on private motor vehicles. Access to the parking areas will be provided via the Gawler East Link Road, a new north-south collector road and a local road (adjacent the site's northern boundary).

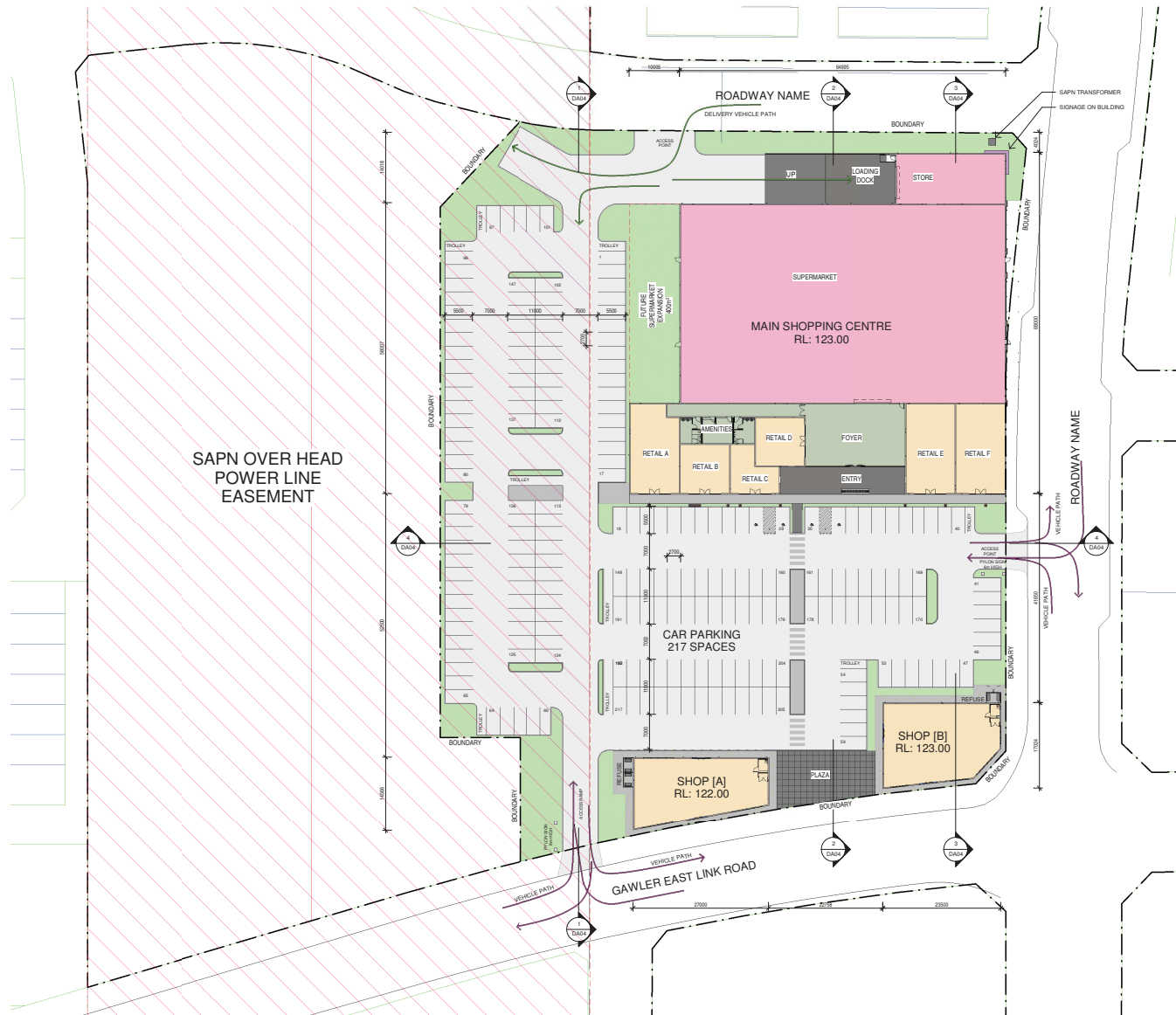
The development will be serviced by parking areas containing 217 vehicle parking spaces (in total). A total of 20 bicycle spaces will also be provided. The proposed level of parking will result in a small shortfall against the requirements of Council's Development Plan. However, based on more recent surveys for shopping centres, it is considered that more than adequate parking will be provided as part of the development.

Traffic impacts associated with the proposal have been assessed as part of the broader land division application and planning for the Gawler East Link Road. The proposed access arrangements and associated traffic impacts will be in line with the previous assessments. Additional SIDRA modelling of the primary access points indicates that the forecast movements will be adequately accommodated.

# **APPENDIX A**

## **BROWN FALCONER'S PLANS DATED MAY 2019**

SITE PLAN  
1 - 400



# DA ISSUE

BOARD OF DEVELOPMENT APPROVAL

14/06/2019 11:26:44 AM

Rev	Amendment	Date
1	LOGEMENT ISSUE	14/6/19

## SITE SUMMARY

SITE AREA	27,980m <sup>2</sup>
SUPERMARKET	2,600m <sup>2</sup>
RETAIL (A-F)	780m <sup>2</sup>
SHOP A	325m <sup>2</sup>
SHOP B	355m <sup>2</sup>
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CARPARKING SPACES	217



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ARCADIAN PROPERTY

SPRINGWOOD TOWN CENTRE

SITE PLAN

Scale As indicated  
Drawn JSM  
Date 16/05/2019  
Job No. 2016040

Dwg No. 2569 DA02 Rev: 1 A1 SHEET



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HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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Project: P:\2019\19140 Springwood Shopping Centre\SIDRA\19140\_12JUN19.sip7

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Giveway / Yield (Two-Way)

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Total veh/h	Flows HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Vehicles veh	Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
East: Gawler East Link Rd (E)											
8	T1	426	5.0	0.226	0.0	LOS A	0.0	0.0	0.00	0.00	60.0
9	R2	63	5.0	0.145	12.9	LOS B	0.5	3.7	0.72	0.89	47.8
Approach		489	5.0	0.226	1.7	NA	0.5	3.7	0.09	0.11	58.0
North: Shopping Centre Access											
10	L2	84	5.0	0.399	15.6	LOS C	1.5	11.2	0.83	1.00	43.0
12	R2	21	5.0	0.399	48.9	LOS E	1.5	11.2	0.83	1.00	42.8
Approach		105	5.0	0.399	22.3	LOS C	1.5	11.2	0.83	1.00	42.9
West: Gawler East Link Rd (W)											
1	L2	84	5.0	0.047	5.6	LOS A	0.0	0.0	0.00	0.58	53.4
2	T1	784	5.0	0.415	0.1	LOS A	0.0	0.0	0.00	0.00	59.9
Approach		868	5.0	0.415	0.6	NA	0.0	0.0	0.00	0.06	59.2
All Vehicles		1463	5.0	0.415	2.5	NA	1.5	11.2	0.09	0.14	57.2

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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Organisation: CIRQA PTY LTD | Processed: Wednesday, June 12, 2019 1:39:53 PM

Project: P:\2019\19140 Springwood Shopping Centre\SIDRA\19140\_12JUN19.sip7

## INTERSECTION SUMMARY

 **Site: 101 [North-South Link Rd - Future PM Peak ]**

New Site  
Giveway / Yield (Two-Way)

Intersection Performance - Hourly Values		
Performance Measure	Vehicles	Persons
Travel Speed (Average)	57.7 km/h	57.7 km/h
Travel Distance (Total)	798.2 veh-km/h	957.8 pers-km/h
Travel Time (Total)	13.8 veh-h/h	16.6 pers-h/h
Demand Flows (Total)	789 veh/h	947 pers/h
Percent Heavy Vehicles (Demand)	5.0 %	
Degree of Saturation	0.201	
Practical Spare Capacity	388.4 %	
Effective Intersection Capacity	3935 veh/h	
Control Delay (Total)	0.39 veh-h/h	0.47 pers-h/h
Control Delay (Average)	1.8 sec	1.8 sec
Control Delay (Worst Lane)	7.8 sec	
Control Delay (Worst Movement)	11.6 sec	11.6 sec
Geometric Delay (Average)	1.5 sec	
Stop-Line Delay (Average)	0.3 sec	
Idling Time (Average)	0.1 sec	
Intersection Level of Service (LOS)	NA	
95% Back of Queue - Vehicles (Worst Lane)	0.3 veh	
95% Back of Queue - Distance (Worst Lane)	2.3 m	
Queue Storage Ratio (Worst Lane)	0.00	
Total Effective Stops	127 veh/h	153 pers/h
Effective Stop Rate	0.16 per veh	0.16 per pers
Proportion Queued	0.06	0.06
Performance Index	15.0	15.0
Cost (Total)	324.08 \$/h	324.08 \$/h
Fuel Consumption (Total)	61.4 L/h	
Carbon Dioxide (Total)	146.5 kg/h	
Hydrocarbons (Total)	0.011 kg/h	
Carbon Monoxide (Total)	0.168 kg/h	
NOx (Total)	0.264 kg/h	

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).

NA: Intersection LOS for Vehicles is Not Applicable for two-way sign control since the average intersection delay is not a good LOS measure due to zero delays associated with major road movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Intersection Performance - Annual Values		
Performance Measure	Vehicles	Persons
Demand Flows (Total)	378,947 veh/y	454,737 pers/y
Delay	187 veh-h/y	224 pers-h/y
Effective Stops	61,010 veh/y	73,212 pers/y
Travel Distance	383,125 veh-km/y	459,750 pers-km/y
Travel Time	6,637 veh-h/y	7,964 pers-h/y
Cost	155,559 \$/y	155,559 \$/y
Fuel Consumption	29,495 L/y	
Carbon Dioxide	70,322 kg/y	
Hydrocarbons	5 kg/y	
Carbon Monoxide	81 kg/y	
NOx	127 kg/y	

## INTERSECTION SUMMARY

 **Site: 101 [Gawler East Link Rd - Future PM Peak]**

New Site  
Giveway / Yield (Two-Way)

Intersection Performance - Hourly Values		
Performance Measure	Vehicles	Persons
Travel Speed (Average)	57.2 km/h	57.2 km/h
Travel Distance (Total)	1478.8 veh-km/h	1774.6 pers-km/h
Travel Time (Total)	25.8 veh-h/h	31.0 pers-h/h
Demand Flows (Total)	1463 veh/h	1756 pers/h
Percent Heavy Vehicles (Demand)	5.0 %	
Degree of Saturation	0.415	
Practical Spare Capacity	100.5 %	
Effective Intersection Capacity	3524 veh/h	
Control Delay (Total)	1.02 veh-h/h	1.23 pers-h/h
Control Delay (Average)	2.5 sec	2.5 sec
Control Delay (Worst Lane)	22.3 sec	
Control Delay (Worst Movement)	48.9 sec	48.9 sec
Geometric Delay (Average)	1.0 sec	
Stop-Line Delay (Average)	1.6 sec	
Idling Time (Average)	1.1 sec	
Intersection Level of Service (LOS)	NA	
95% Back of Queue - Vehicles (Worst Lane)	1.5 veh	
95% Back of Queue - Distance (Worst Lane)	11.2 m	
Queue Storage Ratio (Worst Lane)	0.01	
Total Effective Stops	210 veh/h	252 pers/h
Effective Stop Rate	0.14 per veh	0.14 per pers
Proportion Queued	0.09	0.09
Performance Index	29.7	29.7
Cost (Total)	592.22 \$/h	592.22 \$/h
Fuel Consumption (Total)	110.2 L/h	
Carbon Dioxide (Total)	262.8 kg/h	
Hydrocarbons (Total)	0.019 kg/h	
Carbon Monoxide (Total)	0.303 kg/h	
NOx (Total)	0.457 kg/h	

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).

NA: Intersection LOS for Vehicles is Not Applicable for two-way sign control since the average intersection delay is not a good LOS measure due to zero delays associated with major road movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Intersection Performance - Annual Values		
Performance Measure	Vehicles	Persons
Demand Flows (Total)	702,316 veh/y	842,779 pers/y
Delay	492 veh-h/y	590 pers-h/y
Effective Stops	100,849 veh/y	121,019 pers/y
Travel Distance	709,844 veh-km/y	851,813 pers-km/y
Travel Time	12,401 veh-h/y	14,882 pers-h/y
Cost	284,266 \$/y	284,266 \$/y
Fuel Consumption	52,886 L/y	
Carbon Dioxide	126,125 kg/y	
Hydrocarbons	9 kg/y	
Carbon Monoxide	146 kg/y	
NOx	219 kg/y	



## Appendix 2. Plan of Division

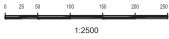




Development No. 490/D026/19  
Town of Gawler  
Development No. 960/D026/19  
The Barossa Council

Proposed Plan of Division  
Allotment 2 in F7765  
Allotment 1 in F13468  
Allotment 9010 & 9011 in D114845  
Allotment 9201 in Dev No. 490/D0101/17  
Allotment 4 in D28514  
Hundred of Barossa

in the name of  
**GAWLER EAST  
& KALBEEBA**  
CT 6188886 CT 6285146  
CT 6188249 CT 6188254  
CT 6188173 PT CT 6324208



**SHEET 1 OF 4**



No. of proposed allotments	1180
Total area	185.6ha
Area of Development	168.5ha
Reserve area	71.28ha
Length of new roads	19.8k

Contour interval 2m.  
Datum AHD.

Vide Titles for disposition of easements.

Road pavements shown are indicative only.

\*\*Not to be used for detailed engineering design.\*\*

Dimensions and areas are subject to survey.

BY AUTHORIZING & ENDORSEMENT IN ACCORDANCE WITH THE SURVEY ACT 1988

**Glenn Ian Hordacre**  
LICENSED SURVEYOR

REF: **AT18016-05**

DATE: 13.06.2019

PROJECT: 0

DATE: 13.06.2019

PROJECT: 0

PROJECT: 0

PROJECT: 0

PROJECT: 0

PROJECT: 0

PROJECT: 0

PROJECT: 0

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PROJECT: 0

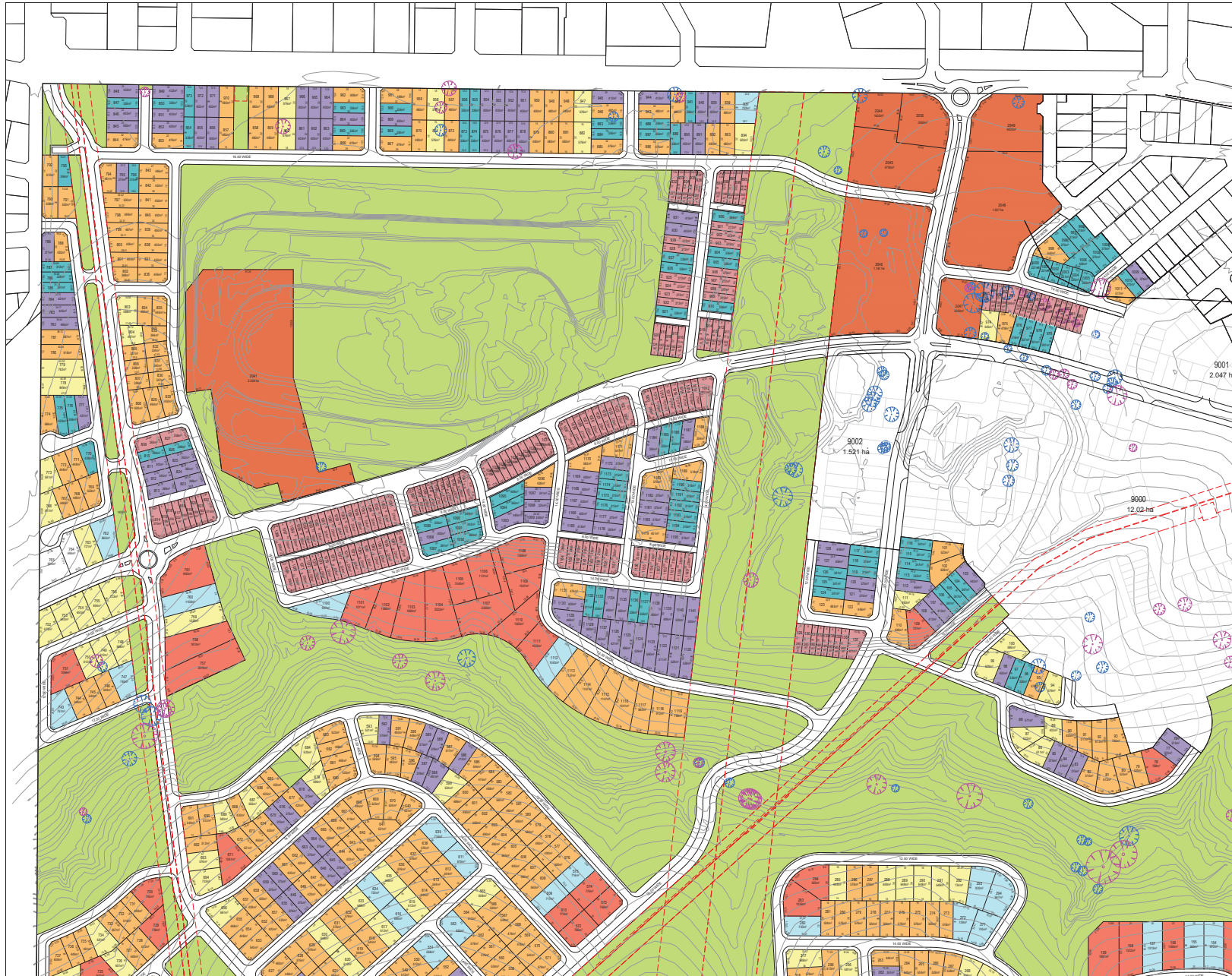
PROJECT: 0

PROJECT: 0

PROJECT: 0

PROJECT: 0





Development No. 490/D026/19  
Town of Gawler

Development No. 960/D025/19  
The Barossa Council

Proposed Plan of Division  
Allotment 2 in F7765  
Allotment 1 in F13468  
Allotment 9010 & 9011 in D114845  
Allotment 9201 in Dev No. 490/D010/17  
Allotment 4 in D28514  
Hundred of Barossa

**GAWLER EAST  
& KALBEEBA**

CT 6188886 CT 6285146  
CT 6188249 CT 6182334  
CT 6186172 PT CT 6202400

0 55 10 15 20 25  
1:2500

**SHEET 2 OF 4**

9001  
2.047 ha

9002  
1.521 ha

9000  
12.02 ha

1180  
Total area 185.6ha  
Area of Development 168.5ha  
Reserve area 71.28ha  
Length of new roads 19.8k

Contour interval 2m.  
Datum AHD.

Vide Tiles for disposition of easements.  
Road pavements shown are indicative only.  
\*\*Not to be used for detailed engineering design.\*\*

Dimensions and areas are subject to survey.

GL 14/2018/001 & 002/2018/001/002/003/004/005/006/007/008/009/010/011/012/013/014/015/016/017/018/019/020/021/022/023/024/025/026/027/028/029/030/031/032/033/034/035/036/037/038/039/040/041/042/043/044/045/046/047/048/049/050/051/052/053/054/055/056/057/058/059/060/061/062/063/064/065/066/067/068/069/070/071/072/073/074/075/076/077/078/079/080/081/082/083/084/085/086/087/088/089/090/091/092/093/094/095/096/097/098/099/100/101/102/103/104/105/106/107/108/109/110/111/112/113/114/115/116/117/118/119/120/121/122/123/124/125/126/127/128/129/130/131/132/133/134/135/136/137/138/139/140/141/142/143/144/145/146/147/148/149/150/151/152/153/154/155/156/157/158/159/160/161/162/163/164/165/166/167/168/169/170/171/172/173/174/175/176/177/178/179/180/181/182/183/184/185/186/187/188/189/190/191/192/193/194/195/196/197/198/199/200/201/202/203/204/205/206/207/208/209/210/211/212/213/214/215/216/217/218/219/220/221/222/223/224/225/226/227/228/229/230/231/232/233/234/235/236/237/238/239/240/241/242/243/244/245/246/247/248/249/250/251/252/253/254/255/256/257/258/259/260/261/262/263/264/265/266/267/268/269/270/271/272/273/274/275/276/277/278/279/280/281/282/283/284/285/286/287/288/289/290/291/292/293/294/295/296/297/298/299/300/301/302/303/304/305/306/307/308/309/310/311/312/313/314/315/316/317/318/319/320/321/322/323/324/325/326/327/328/329/330/331/332/333/334/335/336/337/338/339/340/341/342/343/344/345/346/347/348/349/350/351/352/353/354/355/356/357/358/359/360/361/362/363/364/365/366/367/368/369/370/371/372/373/374/375/376/377/378/379/380/381/382/383/384/385/386/387/388/389/390/391/392/393/394/395/396/397/398/399/400/401/402/403/404/405/406/407/408/409/410/411/412/413/414/415/416/417/418/419/420/421/422/423/424/425/426/427/428/429/430/431/432/433/434/435/436/437/438/439/440/441/442/443/444/445/446/447/448/449/450/451/452/453/454/455/456/457/458/459/460/461/462/463/464/465/466/467/468/469/470/471/472/473/474/475/476/477/478/479/480/481/482/483/484/485/486/487/488/489/490/491/492/493/494/495/496/497/498/499/500/501/502/503/504/505/506/507/508/509/510/511/512/513/514/515/516/517/518/519/520/521/522/523/524/525/526/527/528/529/530/531/532/533/534/535/536/537/538/539/540/541/542/543/544/545/546/547/548/549/550/551/552/553/554/555/556/557/558/559/560/561/562/563/564/565/566/567/568/569/570/571/572/573/574/575/576/577/578/579/580/581/582/583/584/585/586/587/588/589/590/591/592/593/594/595/596/597/598/599/600/601/602/603/604/605/606/607/608/609/610/611/612/613/614/615/616/617/618/619/620/621/622/623/624/625/626/627/628/629/630/631/632/633/634/635/636/637/638/639/640/641/642/643/644/645/646/647/648/649/650/651/652/653/654/655/656/657/658/659/660/661/662/663/664/665/666/667/668/669/670/671/672/673/674/675/676/677/678/679/680/681/682/683/684/685/686/687/688/689/690/691/692/693/694/695/696/697/698/699/700/701/702/703/704/705/706/707/708/709/710/711/712/713/714/715/716/717/718/719/720/721/722/723/724/725/726/727/728/729/730/731/732/733/734/735/736/737/738/739/740/741/742/743/744/745/746/747/748/749/750/751/752/753/754/755/756/757/758/759/760/761/762/763/764/765/766/767/768/769/770/771/772/773/774/775/776/777/778/779/780/781/782/783/784/785/786/787/788/789/790/791/792/793/794/795/796/797/798/799/800/801/802/803/804/805/806/807/808/809/810/811/812/813/814/815/816/817/818/819/820/821/822/823/824/825/826/827/828/829/830/831/832/833/834/835/836/837/838/839/840/841/842/843/844/845/846/847/848/849/850/851/852/853/854/855/856/857/858/859/860/861/862/863/864/865/866/867/868/869/870/871/872/873/874/875/876/877/878/879/880/881/882/883/884/885/886/887/888/889/890/891/892/893/894/895/896/897/898/899/900/901/902/903/904/905/906/907/908/909/910/911/912/913/914/915/916/917/918/919/920/921/922/923/924/925/926/927/928/929/930/931/932/933/934/935/936/937/938/939/940/941/942/943/944/945/946/947/948/949/950/951/952/953/954/955/956/957/958/959/960/961/962/963/964/965/966/967/968/969/970/971/972/973/974/975/976/977/978/979/980/981/982/983/984/985/986/987/988/989/990/991/992/993/994/995/996/997/998/999/1000/1001/1002/1003/1004/1005/1006/1007/1008/1009/1010/1011/1012/1013/1014/1015/1016/1017/1018/1019/1020/1021/1022/1023/1024/1025/1026/1027/1028/1029/1030/1031/1032/1033/1034/1035/1036/1037/1038/1039/1040/1041/1042/1043/1044/1045/1046/1047/1048/1049/1050/1051/1052/1053/1054/1055/1056/1057/1058/1059/1060/1061/1062/1063/1064/1065/1066/1067/1068/1069/1070/1071/1072/1073/1074/1075/1076/1077/1078/1079/1080/1081/1082/1083/1084/1085/1086/1087/1088/1089/1090/1091/1092/1093/1094/1095/1096/1097/1098/1099/1100/1101/1102/1103/1104/1105/1106/1107/1108/1109/1110/1111/1112/1113/1114/1115/1116/1117/1118/1119/1120/1121/1122/1123/1124/1125/1126/1127/1128/1129/1130/1131/1132/1133/1134/1135/1136/1137/1138/1139/1140/1141/1142/1143/1144/1145/1146/1147/1148/1149/1150/1151/1152/1153/1154/1155/1156/1157/1158/1159/1160/1161/1162/1163/1164/1165/1166/1167/1168/1169/1170/1171/1172/1173/1174/1175/1176/1177/1178/1179/1180/1181/1182/1183/1184/1185/1186/1187/1188/1189/1190/1191/1192/1193/1194/1195/1196/1197/1198/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ORIGINAL SHEET 908 A3

Development No. 490/D026/19  
Town of Gawler

Development No. 960/D025/19  
The Barossa Council

Proposed Plan of Division  
Allotment 2 in F7765  
Allotment 1 in F13468  
Allotment 9010 & 9011 in D114845  
Allotment 9201 in Dev No. 490/D01017  
Allotment 4 in D28514  
Hundred of Barossa

**GAWLER EAST  
& KALBEEBA**

CT 618886 CT 6285146  
CT 618249 CT 6182334  
CT 6186172 PT CT 6202400

0 55 100 150 200 250  
1:2500

**SHEET 3 OF 4**



No. of proposed allotments 1180  
Total area 185.6ha  
Area of Development 168.5ha  
Reserve area 71.28ha  
Length of new roads 19.8k

Contour interval 2m.  
Datum AHD.

Vide Titles for disposition of easements.  
Road pavements shown are indicative only.  
\*\*Not to be used for detailed engineering design.\*\*

Dimensions and areas are subject to survey.

BY AUTHORIZER & INSTRUMENTED: A30	
Glenn Ian Hordacre	
LICENSED SURVEYOR	
REF.	ART0816-C6
ENVS REF.	ART0816-C6-PROPT1 REV D
REV. NO.	0
DEL.	12.06.2019
Authorised & Instrumented By: GIL 13 King William Street, Kent Town, South Australia 5091 P.O. Box 1206, Kent Town, SA 5091 M08 7500 752 868	
• (08) 85 50 1444 • 0800 540 0000 • alexander@alexander.com.au • alexander@alexander.com.au	
• Property & Land Management & • Construction & Mining & • Spatial Information Management &	





Development No. 490/D026/19  
Town of Gawler  
Development No. 960/D026/19  
The Barossa Council

Proposed Plan of Division  
Allotment 2 in F7765  
Allotment 1 in F13468  
Allotment 9010 & 9011 in D114845  
Allotment 9201 in Dev No. 490/D01017  
Allotment 4 in D28514  
Hundred of Barossa

**GAWLER EAST  
& KALBEEBA**

CT 6188886 CT 6285146  
CT 6188249 CT 6188234  
CT 6188172 PT CT 6282408

0 25 50 100 150 200 250  
1:2500

**SHEET 4 OF 4**

Significant tree  
Regulated tree

No. of proposed allotments	1180
Total area	185.6ha
Area of Development	168.5ha
Reserve area	71.28ha
Length of new roads	19.8k

Contour interval 2m.  
Datum AHD.

Vide Titles for disposition of easements.  
Road pavements shown are indicative only.  
\*\*Not to be used for detailed engineering design.\*\*

Dimensions and areas are subject to survey.

REGISTERED & RESPONSIBLE SURVEYOR  
Glenn Ian Hordacre  
REGISTERED SURVEYOR

REF.	410816-05
ENVS REF.	410816-05-PROJ1 REV 01
REV.	01
DATE	12.09.2019

Prepared & Issued by: GIL  
13 King William Street, Kent Town,  
South Australia 5091  
P.O. Box 1000, Kent Town, SA 5091  
Phone 08 8377 7333 Fax 08 8377 7333

• 0800 83 30 3444  
• 0800 830 0000  
• alexander@alexander.com.au  
• alexander@alexander.com.au

• Property • Land Management •  
• Construction • Mining •  
• Spatial Information Management •

**Alexander  
Sydney  
Geomatics  
Consultants**



## CATEGORY 2 REPRESENTATION

*Development Act 1993, Section 38*

**Gawler**



**Applicant:** Springwood Development Nominees  
**Development Number:** 490/E006/19  
**Nature of Development:** Construction of a shopping centre comprising supermarket and six (6) specialty shops, two (2) standalone shops, associated car-parking and signage  
**Development Type:** Merit  
**Zone / Policy Area:** Residential (Gawler East) Zone/ Mixed Use Centre Policy Area 3  
**Subject Land:** Calton Road, Gawler East, Lot 9010 CT 6205/146 (Proposed Lot 1006 in DA 490/D025/19)  
**Contact Officer:** Hannah Connell  
**Phone Number:** 7109 7828  
**Close Date:** 5:00pm Thursday 19 December 2019

### CONTACT DETAILS:

**Organisation:** Town of Gawler  
**Primary Contact:** Henry Inat  
**Phone:** 8522 9241  
**Email:** Henry.Inat@gawler.sa.gov.au  
**Postal Address:** PO Box 130 Gawler SA 5118

### REPRESENTATION DETAILS:

**My Interests are:** A representative of a company/other organisation affected by the proposal  
**Address of Property Affected:** Public Reserve – Calton Road (Certificate of Title Volume 5111 Folio 207)

### REPRESENTATION:

The specific aspects of the application to which the Town of Gawler would like to make comment on include:

- Procedural Components being the categorisation
- Land Use
- Design
- Stormwater
- Traffic
- Car and Bicycle Parking
- Landscaping
- Signage
- General Infrastructure Provision
- SEAGas and ElectraNet informal referral

### Categorisation

Council has concerns with the extent of properties notified during the assessment of the application. This concern is premised on the interpretation of the definition of 'adjacent land'. In accordance with the *Development Act 1993*, adjacent land is defined as follows:

***adjacent land in relation to other land, means land—***

- a) that abuts on the other land; or*
- b) that is no more than 60 metres from the other land and is directly separated from the other land only by—*
  - i. a road, street, footpath, railway or thoroughfare; or*
  - ii. a watercourse; or*
  - iii. a reserve or other similar open space*

The following properties are situated within 60 metres of the subject land and are separated by a Council Reserve and Road (Calton Road):

- 34 Stithians Drive;
- 43 Stithians Drive;
- 48 Phillips Avenue;
- 47 Phillips Avenue;
- 10 Norman Court; and
- 9 Norman Court.

In accordance with Section 38 Clause (4)(a), notification must be given to an owner or occupier of each piece of adjacent land. As the abovementioned properties fall within the ambit of 'adjacent land', notification to these properties should have occurred during the consultation period. Given the omission of properties and resulting error in process, the public notification period should be re-undertaken such that all properties entitled to submit a representation are enabled to do so.

#### Other Elements

The other elements of the application raised above are discussed within the attached document which also forms part of the Town of Gawler's representation. These comments have previously been provided to SCAP within Council's referral response.

In summary, Council has some concerns with the proposed development, until such time as the above mentioned elements have been adequately addressed. Notwithstanding, Council is supportive of the proposed economic development and resulting employment generating opportunities this development will provide to the wider community.

Council would also like the opportunity for a representatives of the Council to be heard by the State Commission Assessment Panel in relation to this matter.

Signature:



Date:

19 December 2019



23 October 2019



Ms Simone Fogerty  
Presiding Member  
State Assessment Commission Panel  
C/- Department of Planning Transport and Infrastructure  
PO BOX 1815 Adelaide SA 5001

Town of Gawler Administration Centre  
43 High Street  
PO Box 130  
Gawler East SA 5118  
Phone: (08) 8522 9211  
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[council@gawler.sa.gov.au](mailto:council@gawler.sa.gov.au)  
[gawler.sa.gov.au](http://gawler.sa.gov.au)

Dear Ms Fogerty, 


**Re: Development Applications 490/E005/19 (Construction of a Sales Centre (two storey) comprising Showroom, Office and Café with associated car park and signage), 490/E006/19 (Construction of a shopping centre comprising supermarket and six (6) specialty shops, two (2) standalone shops, associated car parking and signage) and 490/E009/19 (Childcare centre with associated car park, landscaping, fencing and signage).**

Please find enclosed a report of the Town of Gawler Council Assessment Panel on the three (3) Development Applications as above.

The report was adopted unanimously by the Panel at its meeting held on 23 October 2019.

Should you require any further information do not hesitate to contact Ryan Viney, Manager Development, Environment and Regulatory Services on 85229271 or [ryan.viney@gawler.sa.gov.au](mailto:ryan.viney@gawler.sa.gov.au).

Yours sincerely,



Dr Michael Llewellyn-Smith AM KStJ JP  
Presiding Member



TO THE STATE COMMISSION ASSESSMENT PANEL  
FROM THE  
TOWN OF GAWLER  
COUNCIL ASSESSMENT PANEL

Attachment Number	Document	Pages
Attachment 1 -	Procedural Matter Concerns	1-2
Attachment 2 –	Council Comments relating to: 1. 490/E005/19 (490/440/19) 2. 490/E006/19 (490/441/19) 3. 490/E007/19 (490/442/19)	3-32
Attachment 3	Recommended Conditions for: 1. 490/E005/19 (490/440/19) 2. 490/E006/19 (490/441/19) 3. 490/E007/19 (490/442/19)	33-38
Attachment 4	Council Assessment Panel Report	1-45
Attachment 5	Council Assessment Panel Report Minutes	46

## Attachment 1 – Procedural Concerns

Legal advice has been sought with regards to procedural matters in processing the subject three applications without each corresponding allotment existing for each of the proposals (including lack of legal road access) and further the need to consider earthworks.

In summary, the legal advice received in summary included:

1. Each of the respective land divisions cannot and should not be assessed and determined unless and until the divisions which create the relevant public roads from which access will be taken, are approved. Until this occurs, none of the proposed developments in the 3 development applications have a lawful means of access. The 3 development applications cannot be granted a Development Plan consent on this basis. To do so would be contrary to the Gawler (CT) Development Plan and would, be against general practice (see, e.g. *Rohrlach & Anor v City of Unley* [2011] SAERDC 19).
2. Further, fundamental aspects of the 3 development applications, being floor levels, stormwater management and retaining wall heights are uncertain and are dependent on as-yet undetermined development applications. It would be inappropriate and contrary to the Development Plan to grant Development Plan consent to the 3 development applications without particular information in this regard. Specifically, floor levels and retaining wall heights are significant planning merit considerations such that they cannot be dealt with as reserve matters for the reasons expressed in the Supreme Court judgment of *Mar Mina (SA) Pty Ltd v City of Marion & Anor* [2008] SASC 120.
3. The lack of lawful access does not render the 3 development applications hypothetical. The test for a “hypothetical” development is whether there is a reasonable prospect of the development proceeding (see *Hackney Hotel Pty Ltd v Corporation of the Town of St Peters* (1983) 32 SASR 145 and *Rampling v City of Holdfast Bay* [2010] SAERDC 60).
4. Once the relevant land division applications are approved and plans deposited at the Land Titles Office (LTO) such that the proposed public roads are sufficiently certain so as to facilitate future developments relying on them for access, and so that levels and retaining wall heights can be confirmed, the 3 development applications may then be granted Development Plan consent.

In conclusion, none of the 3 development applications can be determined until:

1. The divisions creating the public roads for which each of the 3 development applications are reliant for access are approved and the relevant plans of division deposited at the LTO;

or

2. Each of the 3 development applications is amended to provide access to an existing public road and to remove all uncertainty concerning levels and retaining walls (which are currently specified as being contingent on the outcome of the relevant land division applications).



## Attachment 2 - Assessment Comments

490/440/2019 – 490/E005/19

CONSTRUCTION OF A SALES CENTRE (TWO STOREY) COMPRISING SHOWROOM, OFFICE AND CAFE WITH ASSOCIATED CAR PARK AND SIGNAGE



Element	Discussion
Land Use	<ol style="list-style-type: none"> <li>The proposed land use has been defined by the applicant as a Sales Centre and will consist of a number of associated elements which include: <ol style="list-style-type: none"> <li>A sales Centre (132.55m<sup>2</sup>)</li> <li>A Café (79.4m<sup>2</sup>)</li> <li>Office with Amenities (159.93m<sup>2</sup>)</li> <li>35 Car parks</li> </ol> </li> <li>The Mixed Use Centre Policy area contemplates a wide range of land uses that serve the local community. The proposed use is in keeping with this intent and therefore satisfies Objective 1, 3 and PDC 1 and 6 of the policy area.</li> </ol>
Earthworks and Retaining	<ol style="list-style-type: none"> <li>No site civil plans have been provided as part of the application. The existing and finished levels of the land are dependent on the final design and approval of the</li> </ol>

	<p>associated land division applications and completion of the Gawler East Link Road.</p> <ol style="list-style-type: none"> <li>Depending on the level of earthworks that result in both the associated land division and Gawler East Link Road works, there may be a need for additional earthworks and retaining walls.</li> <li>At this stage of the proposal, there is insufficient information to properly assess the development against the following PDC's: <p><b>PDC 7 (Policy Area)</b></p> <p>Filling of land and retaining walls exceeding 1.0 metre in height are appropriate where they are associated with the remediation and development of the former quarry site.</p> <p><b>PDC 29 (Zone)</b></p> <p>Retaining walls greater than 1.0 metre in height should:</p> <ol style="list-style-type: none"> <li>not be directly visible from a public road or the public realm</li> <li>where located between a building and a road frontage, be stepped in increments of not more than 1.0 metre</li> <li>not cause significant adverse impacts on the amenity of adjacent properties, particularly in relation to overshadowing, cutting off light or views.</li> </ol> <p><b>PDC 31 (Zone)</b></p> <p>Development should be designed and sited to relate to the slope of the land, so that:</p> <ol style="list-style-type: none"> <li>the bulk and scale of the buildings do not dominate the landscape;</li> <li>the amount of cut and fill is minimised.</li> </ol> </li> </ol>
<b>Design</b>	<ol style="list-style-type: none"> <li>The desired character statement of the Policy Area seeks to create a 'main street' environment with buildings addressing the street and car parking located primarily to the rear. The appearance of siting of buildings is encouraged to be within close proximity to</li> </ol>

	<p>street frontages and be of an architecturally high standard to create diversity of interest through the appearance of an aggregation of smaller buildings.</p> <ol style="list-style-type: none"> <li>2. This intent is achieved through the siting of the building being closely aligned with the intersection of the Gawler East Link Road and the intersecting north-south collector road and rear located car parking area. The built form comprises of a high standard façade incorporating protruding elements and features which provide for visual interest and promote a pedestrian-friendly environment.</li> <li>3. The two-storey building addresses both street frontages and incorporates paving, street furniture and landscaping that integrate the adjoining public spaces with the subject development.</li> <li>4. Zone Objective 7, PDC 22(b) and Policy Area Objectives 2, 3 and PDC's 3, 4, 5 and 6 are considered to be satisfied by the proposal.</li> <li>5. The sales centre is considered to achieve a strong design outcome which sets a positive benchmark for future development within the Policy Area.</li> </ol>
<b>Stormwater</b>	<ol style="list-style-type: none"> <li>1. The applicant has noted that a site specific Stormwater Management Plan will be prepared for the subject development. The plan will build upon the overall Stormwater Management Plan for the entire Springwood development which has been submitted with the concurrent land division applications.</li> <li>2. Whilst a Reserved Matter or at the very least a condition would generally be appropriate for the provision of the final design, a concept plan demonstrating the high level method of stormwater management should be provided for the purposes of the planning assessment prior to the issuing of any Development Plan Consent.</li> <li>3. The stormwater system should incorporate adequate</li> </ol>

	<p>detention, water quality control (i.e. Gross Pollutant Trap to separate oils and other pollutants) for the carparking area and encourage the use of Water Sensitive Urban Design (WSUD) principles.</p> <p>4. Applicable WSUD principles that could be adopted within the design are identified with CW PDC 148, 150, 151, 152, 154, 155, 157 and 158.</p>
<b>Traffic</b>	<p>1. The traffic volume associated with the site is considered to be in keeping with the traffic volumes proposed as part of the associated land division application 490/D025/19 and 490/D026/19 for the adjoining road network. Accordingly, the traffic network proposed as part of the associated land division applications is not considered to be exceeded by the proposal.</p> <p>2. Access and egress from the subject land to the adjoining public road network will be obtained via two two-way simultaneous crossovers. These crossovers are situated upon a Local Road and the North-South Collector Road proposed as part of the associated land division 490/D025/19. The car parking area is configured in a manner to accommodate on-site vehicle manoeuvring to enable vehicles to egress the site in a forward direction. As such, the movement of vehicles in association with the site will not conflict with the flow of traffic on adjoining roads.</p> <p>3. The traffic impact has been considered by Cirqa, the applicant's independent traffic consultant and Council's traffic engineers. Both of which, consider this element of the proposal to be acceptable.</p> <p>4. Should waste collection not be achieved via kerb side collection, additional consideration should be given to the manner in which waste is collected from the site.</p> <p>5. The current plans do not provide for the construction site levels (including existing or design levels) of the</p>

	<p>adjoining roads (including the Gawler East Link Road) compared to proposed finished levels of the site and proposed development. Without this level of detail, there is concern relating to access and gradients between the subject site and the external roads (some being partially constructed or not yet approved).</p> <p>6. The proposal also lacks detail with regards to any required works relating to access into the site from any external roads (including the Gawler East Link Road) and the need for alterations to traffic devices such as any median strips, or access arrangements. Whilst it is acknowledged that not all external road infrastructure has been constructed, the associated designs may already exist and may require refinement to accommodate the proposal.</p> <p>7. Any works or refinements to roads that are required to accommodate the proposed development should be completed as part of the associated land division applications and secured via an Infrastructure Agreement (being as part of external infrastructure works) for the subject application to ensure such works (if required) are provided.</p>
<b>Car Parking</b>	<p>1. The proposal seeks to provide for 35 car parking spaces to service the proposed use(s) in a manner that is consistent with AS2890.1:2004 and AS2891.6:2009 (being the Australian Standard for off street parking and off street parking for people with disabilities).</p> <p>2. A car parking assessment has been undertaken for the Sales Centre and associated Office and Café use with consideration to the car parking rates outlined within the Gawler East Residential Zone and Table Ga/1. Based on the carparking requirements outlined within the Development Plan, a total of 31 car parks is required to service the proposed development. Therefore a surplus of four car parks exists.</p>

	<p>3. It is noted that the provided Cirqa Traffic Consultant report further discusses the provision of car parking on site for a Display Village comprising of approximately 33 display homes. This Display Village and associated display homes has not been formally proposed as part of the subject application and as such, is not a consideration at this point in time. When/if a display village is lodged, then the provision of carparking can be further considered in this regard.</p> <p>4. The indicative layout of the car parking area is sufficient to demonstrate that the appropriate amount of car parking has been provided to service the proposed use. Final detail however, is required for the design levels of the car parking area, to confirm gradients comply with both the <i>Disability Discrimination Act 1992</i> (DDA) and AS2890.</p>
<b>Bicycle Parking</b>	<p>1. Bike parking and storage facilities should be considered and provided to the site as an alternative mode of transport.</p>
<b>Landscaping</b>	<p>1. Landscaping is provided adjacent to all four road frontages servicing the subject land. This landscaping is provided in the following forms:</p> <p>1.1 Landscaped perimeter to car parking area;</p> <p>1.2 Internal landscaping to car parking area;</p> <p>1.3 Landscaping to curtilage of Sale Centre; and</p> <p>1.4 Creation of a privately owned landscaped recreation area.</p> <p>2. Whilst a detailed landscaping plan has not been provided, the Site Plan adequately demonstrates that sufficient area has been designated for the provision of landscaping. In addition to the preparation of a detailed landscaping plan, minor improvements could be made to the car parking perimeter adjacent to the Southern Boundary road frontage. Increased tree and shrub</p>



	plantings to this perimeter would be encouraged.
<b>Signage</b>	<ol style="list-style-type: none"> <li>1. The proposed development includes signage at the upper level of the northern and western elevations which reflect the intended use of the site as a Sales Centre. Directional signage will provided around the site.</li> <li>2. The proposed signage does not amount to proliferation and will not conceal any architectural elements and is in keeping with the scale of the building.</li> <li>3. Signage contained within the proposal aligns with Table Ga/4 of the Town of Gawler Development Plan and does not offend CW Objectives 69, 70 nor PDC's 199, 200, 201, 202 and 204.</li> </ol>
<b>General Infrastructure Provision</b>	<ol style="list-style-type: none"> <li>(a) Zone PDC 42 outlines that development should only occur if the required physical and community infrastructure has been provided or secured via a legally binding agreement.</li> <li>(b) The physical and community infrastructure outlined within this provision includes: <ol style="list-style-type: none"> <li>a. <i>Gawler East Link Road as illustrated on Structure Plan Map Ga/1 (Overlay 1) Enlargement G, and associated traffic interventions (road and intersection upgrades);</i></li> <li>b. <i>public lighting, pedestrian and bicycle paths and associated landscaping treatments associated with the Gawler East Link Road;</i></li> <li>c. <i>community facilities (including recreation facilities); and</i></li> <li>d. <i>stormwater management systems.</i></li> </ol> </li> <li>(c) To date this infrastructure has not been completed/provided nor secured. As such, the proposal does not satisfy Zone PDC 42 (being all elements a to d as above)</li> </ol>

490/441/2019 – 490/E006/19

**CONSTRUCTION OF A SHOPPING CENTRE COMPRISING SUPERMARKET AND SIX (6) SPECIALTY SHOPS, TWO (2) STANDALONE SHOPS, ASSOCIATED CAR PARKING AND SIGNAGE**



Element	Discussion
Land Use	<ol style="list-style-type: none"> <li>1. The proposed development comprises of the following components: <ol style="list-style-type: none"> <li>1.1 A Shopping Centre, including: <ol style="list-style-type: none"> <li>1.1.1 One (1) Supermarket (2600m<sup>2</sup>)</li> <li>1.1.2 Six (6) Specialty Shops (totalling 780m<sup>2</sup>)</li> <li>1.1.3 Two (2) Standalone Shops (680m<sup>2</sup>)</li> <li>1.1.4 217 car parks and loading dock</li> <li>1.1.5 Associated signage including two (2) Pylon Signs</li> </ol> </li> </ol> </li> <li>2. The Mixed Use Centre Policy area contemplates a wide range of land uses that serve the local community. The proposed use is in keeping with this intent and therefore satisfies Objective 1, 3 and PDC 1 and 6 of the policy area. Furthermore, the scale of the proposed the proposed retail facility is in keeping with the 10,000 square metre retail capacity provided for within the Desired Character Statement of the Policy Area.</li> </ol>

<p><b>Earthworks and Retaining</b></p>	<ol style="list-style-type: none"> <li>1. No site civil plans have been provided as part of the application. The existing and finished levels of the land are dependent on the final design and approval of the associated land division applications and completion of the Gawler East Link Road.</li> <li>2. Depending on the level of earthworks that result in both the associated land division and Gawler East Link Road works, there may be a need for additional earthworks and retaining walls.</li> <li>3. At this stage of the proposal, there is insufficient information to properly assess the development against the following PDC's: <p><b>PDC 7 (Policy Area)</b></p> <p>Filling of land and retaining walls exceeding 1.0 metre in height are appropriate where they are associated with the remediation and development of the former quarry site.</p> <p><b>PDC 29 (Zone)</b></p> <p>Retaining walls greater than 1.0 metre in height should:</p> <ol style="list-style-type: none"> <li>(a) not be directly visible from a public road or the public realm</li> <li>(b) where located between a building and a road frontage, be stepped in increments of not more than 1.0 metre</li> <li>(c) not cause significant adverse impacts on the amenity of adjacent properties, particularly in relation to overshadowing, cutting off light or views</li> </ol> <p><b>PDC 31 (Zone)</b></p> <p>Development should be designed and sited to relate to the slope of the land, so that:</p> <ol style="list-style-type: none"> <li>(a) the bulk and scale of the buildings do not dominate the landscape;</li> <li>(b) the amount of cut and fill is minimised.</li> </ol> </li> </ol>
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<p><b>Design</b></p>	<ol style="list-style-type: none"> <li>1. The subject development comprises of three detached buildings, comprising of the shopping centre and two (2) standalone shops with car parking situated centrally and to the side of the site.</li> <li>2. The shopping centre generally presents of a high architectural standard and incorporates a variety of building materials that promote visual interest. Although improvements could be sought to the East Street elevation of the shopping centre to assist in softening the buildings bulk. Whilst the proposed feature steel frames may be appropriate in achieving this intent, additional clarification on the detail of the structures should be provided (i.e. location shown on site plan).</li> <li>3. Similarly, to further align with the Desired Character statements intent to provide for a main street environment, provision of a pedestrian link from the eastern roadway to the shopping centre could be provided. This link could be achieved in the area situated between the Main Shopping Centre and specialty shops indicated as 'Retail E' and 'Retail F'.</li> <li>4. Whilst the location of the rear loading dock is generally supported, there is potential for improved frontage to the adjoining north eastern road. A frontage treatment inclusive of feature screening and dense landscape plantings or similar would be encouraged by Council. Greater pedestrian linkage from this portion of the site would also be encouraged.</li> <li>5. The two proposed standalone shops consist of a form and of materials that reflect and complement the proposed shopping centre building. The buildings provide for articulation and visual interest to the adjoining public realm and achieve strong pedestrian connectivity with the provision of the centrally located plaza. This pedestrian connectivity is improved via the provision of a designated link from the plaza to the shopping centre.</li> </ol>
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	<ol style="list-style-type: none"> <li>6. The proposed car parking area is generally well screened by the proposed built form and satisfies the Policy Area's intent for car parking to be located to the rear.</li> <li>7. Notwithstanding the above, with minor improvements to the design, the proposal is considered to satisfy Zone Objective 7, PDC 22(b) and Policy Area Objectives 2, 3 and PDC's 3, 4, 5 and 6.</li> </ol>
<b>Stormwater</b>	<ol style="list-style-type: none"> <li>1. The applicant has noted that a site specific Stormwater Management Plan will be prepared for the subject development. The plan will build upon the overall Stormwater Management Plan for the entire Springwood development which has been submitted with the concurrent land division applications.</li> <li>2. Whilst a Reserved Matter, or at the very least a condition, would generally be appropriate for the provision of the final design, a concept plan demonstrating the high level method of stormwater management should be provided for the purposes of the planning assessment prior to the issuing of any Development Plan Consent.</li> <li>3. The stormwater system should incorporate adequate detention, water quality control (i.e. Gross Pollutant Trap to separate oils and other pollutants) for the carparking area and encourage the use of Water Sensitive Urban Design (WSUD) principles.</li> <li>4. Applicable WSUD principles that could be adopted within the design are identified with CW PDC 148, 150, 151, 152, 154, 155, 157 and 158.</li> </ol>
<b>Traffic</b>	<ol style="list-style-type: none"> <li>1. The traffic volume associated with the site is considered to be in keeping with the traffic volumes proposed as part of the associated land division application 490/D025/19 and 490/D026/19 for the adjoining road network. Accordingly, the traffic network proposed as part of the associated land division applications is not considered to be exceeded by the proposal.</li> </ol>

	<p>2. Vehicle access to the site will be provided via the following locations:</p> <p>2.1 a two-way access point on the Gawler East Link Road (which will accommodate left-in, left-out, right-in and right-out movements);</p> <p>2.2 a two-way access point on the north-south collector road (which will accommodate left-in, left-out, right-in and right-out movements); and</p> <p>2.3 a two-way access point on the local road to the north which will primarily provide access for commercial vehicle movements, albeit customers will also be able to utilise the access point .</p> <p>3. The car parking area is configured in a manner to accommodate on-site vehicle manoeuvring to enable vehicles to egress the site in a forward direction. As such, the movement of vehicles in association with the site will not conflict with the flow of traffic on adjoining roads.</p> <p>4. The traffic impact has been considered by Cirqa, the applicant's independent traffic consultant and Council's traffic engineers. Both of which, consider this element of the proposal to be acceptable.</p> <p>5. Commercial vehicle access will be accommodated via the loading area located at the rear (north) of the shopping centre. Cirqa Traffic Consultants have demonstrated this area can appropriately accommodate the manoeuvring of a semi-trailer. Minor deliveries associated with the specialty shops will be achieved internally through the car parking area provided.</p> <p>Accordingly, deliveries to the proposed supermarket is not considered to result in conflict with the adjoining roadway.</p>
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	<p>6. The current plans do not provide for the construction site levels (including existing or design levels) of the adjoining roads (including the Gawler East Link Road) compared to proposed finished levels of the site and proposed development. Without this level of detail, there is concern relating to access and gradients between the subject site and the external roads (some being partially constructed or not yet approved).</p> <p>7. The proposal also lacks detail with regards to any required works relating to access and manoeuvrability into the site from any external roads (including the Gawler East Link Road) and the need for alterations to traffic devices such as any median strips, or access arrangements. Whilst it is acknowledged that not all external road infrastructure has been constructed, the associated designs may already exist and may require refinement to accommodate the proposal.</p> <p>8. Any works or refinements to roads that are required to accommodate the proposed development should be completed as part of the associated land division applications and secured via an Infrastructure Agreement (being as part of external infrastructure works) for the subject application to ensure such works (if required) are provided.</p>
<b>Car Parking</b>	<p>1. The proposal seeks to provide for 217 car parking spaces to service the proposed retail uses. The car parking is proposed in a manner that is consistent with AS2890.1:2004 and AS2891.6:2009 (being the Australian Standard for off street parking and off street parking for people with disabilities). It is noted that the width of the car parking spaces proposed exceed the minimum requirements of the associated user class under AS2890. This approach is encouraged by Council.</p>

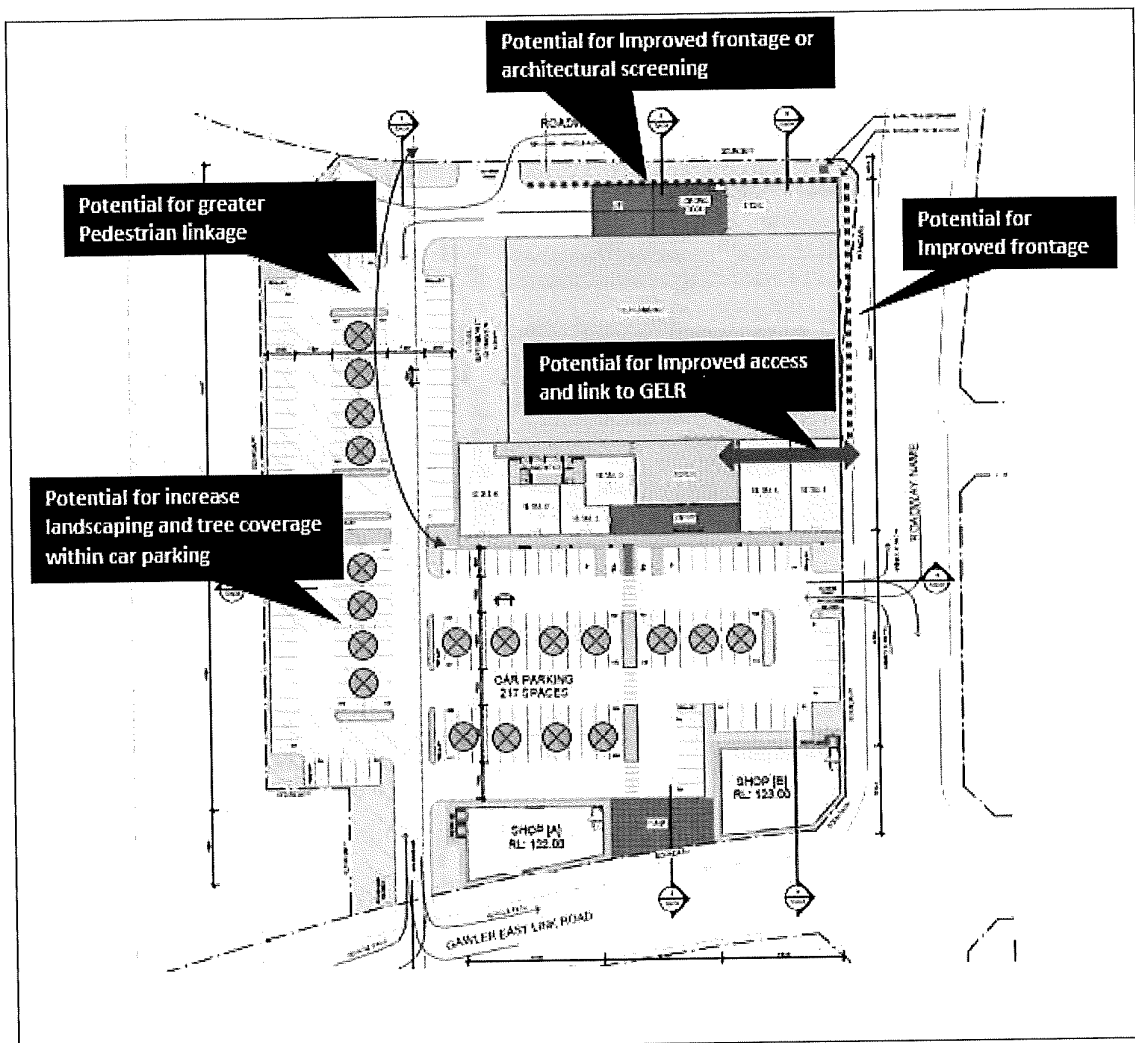
	<ol style="list-style-type: none"> <li>2. The Development Plan within the Mixed Use Centre Policy Area identifies carparking to a rate of 5.5 spaces per 100m<sup>2</sup> is required for the proposed shop/retail uses. Subsequently, a total of 224 car parking spaces are required resulting in a shortfall of 7 car parking spaces.</li> <li>3. Cirqa Traffic Consultants engaged by the applicant undertook an empirical assessment of car parking and noted that recent surveys of shopping centres indicate peak parking demands are more commonly in the range of 3.0 to 5.0 spaces per 100 m<sup>2</sup> of floor area. These rates equate to a peak demand in the range of 122 to 203 spaces. This demand is accommodated by the proposal. Notwithstanding, the theoretical 7 park shortfall can be accommodated within the adjoining road network servicing the proposed development.</li> <li>4. It is further noted that the car parking shortfall could be reduced via the removal of the designated pedestrian linkage within the site connecting the proposed plaza with the shopping centre. Such an approach however, would be to the detriment of the proposal and the minor shortfall is considered acceptable and inconsequential to the provision of car parking on site.</li> <li>5. Whilst typically considered during the Building Rules Assessment of such an application, Council would encourage the consideration of the provision of disabled car parking spaces within close proximity to both the Shopping Centre and the two detached specialty shops proposed on site. The current car parking configuration is limited to the provision of disabled car parking adjacent to the shopping centre.</li> <li>6. The indicative layout of the car parking area is sufficient to demonstrate that the appropriate amount of car parking has been provided to service the proposed use. Final detail however, is required for the design levels of the car parking area, to confirm gradients comply with both the DDA and AS2890.</li> </ol>
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<b>Bicycle Parking</b>	<ol style="list-style-type: none"> <li>1. Bike parking and storage facilities should be considered and provided to the site as an alternative mode of transport.</li> </ol>
<b>Landscaping</b>	<ol style="list-style-type: none"> <li>1. The proposal includes a conceptual landscaping plan which depicts the provision of landscaping around the perimeter of the site and within the proposed car parking area. These landscaped areas are to comprise of a range of species of trees, shrubs and groundcovers, including: <ol style="list-style-type: none"> <li>1.1 Trees: <ol style="list-style-type: none"> <li>1.1.1 Callery Pear; and</li> <li>1.1.2 Japanese Elm</li> </ol> </li> <li>2. Shrubs: <ol style="list-style-type: none"> <li>2.1 Agapanthus Small Cultivars;</li> <li>2.2 Distichlis Distichophylla;</li> <li>2.3 Lomandra Cultivars; and</li> <li>2.4 Nandina</li> </ol> </li> <li>3. Groundcovers: <ol style="list-style-type: none"> <li>3.1 Aesperula Conferta; and</li> <li>3.2 Leptospermum Lanigerum</li> </ol> </li> <li>4. Whilst the provision of landscaping is generally appropriate, increased landscaping could be provided to the road frontage adjacent to the rear loading dock and within the proposed car parking area (i.e. shading trees).</li> </ol> </li> </ol>
<b>Signage</b>	<ol style="list-style-type: none"> <li>1. The proposed architectural plans demonstrate indicative locations for signage at the main entrances to the site. Whilst this location and form of signage (flat wall) is generally appropriate, clarification on dimensions should be provided.</li> <li>2. Similarly, there is currently insufficient detail to consider the proposed pylon signs. However from the limited extent of information provided, it is noted that the pylon signage is detached form and in keeping with the scale of the proposed building. Notwithstanding, Table Ga/4 of the Development Plan seeks to limit such signs to a maximum height of 6.0 metres with a maximum advertisement area of 5.0 square metres. A reduction in</li> </ol>

	<p>the 8 metre pylon sign height would be encouraged to satisfy this provision.</p> <p>3. Consideration should also be given to whether the proposed pylon signs will maintain appropriate driver sight lines as a result of their close association with vehicle access/egress points.</p> <p>4. Signage contained within the proposal aligns with Table Ga/4 of the Town of Gawler Development Plan and does not offend CW Objectives 69, 70 nor PDC's 199, 200, 201, 202 and 204.</p>
<b>General Infrastructure Provision</b>	<p>1. Zone PDC 42 outlines that development should only occur if the required physical and community infrastructure has been provided or secured via a legally binding agreement.</p> <p>2. The physical and community infrastructure outlined within this provision includes:</p> <ul style="list-style-type: none"> <li><i>a. Gawler East Link Road as illustrated on Structure Plan Map Ga/1 (Overlay 1) Enlargement G, and associated traffic interventions (road and intersection upgrades);</i></li> <li><i>b. public lighting, pedestrian and bicycle paths and associated landscaping treatments associated with the Gawler East Link Road;</i></li> <li><i>c. community facilities (including recreation facilities); and</i></li> <li><i>d. stormwater management systems.</i></li> </ul> <p>3. To date this infrastructure has not been completed/provided nor secured. As such, the proposal does not satisfy Zone PDC 42 (being all elements a to d as above).</p>

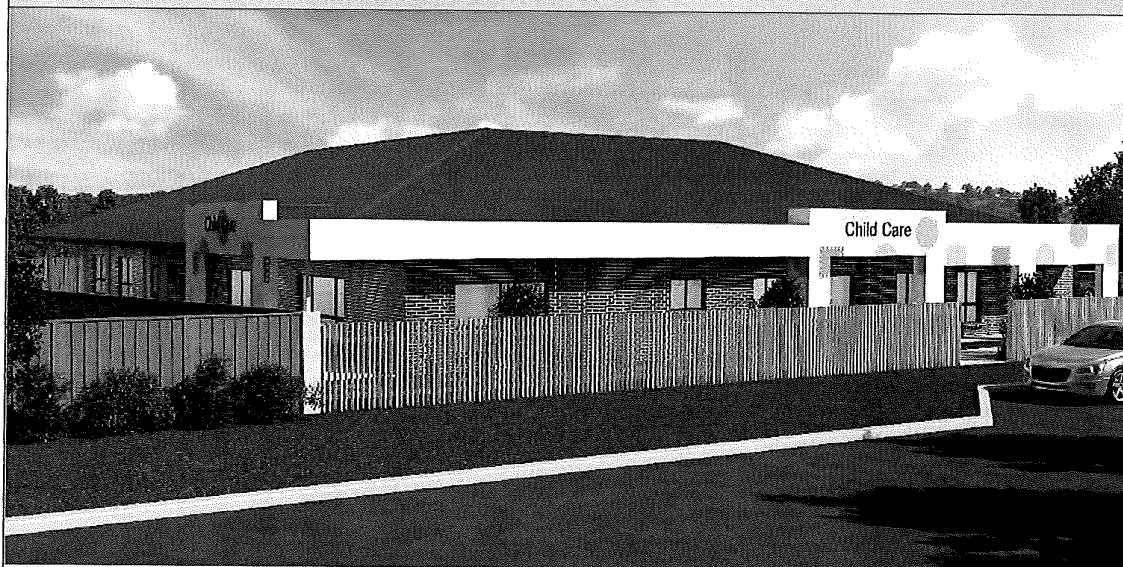
<p><b>SEAGas and ElectraNet informal referral</b></p>	<p>1. Whilst the proposed application does not trigger any referrals under Schedule 8 of the <i>Development Regulation 2008</i>, it is highly recommended that following informal referrals are sought to ensure public safety and compliance by the following utility providers:</p> <p>1.1 SEAGas to ensure compliance with AS2885 and Zone PDC 36 (b) given that the proposed shopping centre will contain high concentrations of people within close proximity to the SEA Gas high pressure pipeline.</p> <p>1.2 ElectraNet given that the proposed Shopping centre is encroaching within the transmission line corridor and in particular is currently at variance with Zone PDC 19(c) which seeks a 100metre wide corridor within the existing ElectraNet easement and transmission corridor.</p>
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Visual Representation of Recommendations as per above comments.





**CONSTRUCTION OF A SINGLE-STOREY CHILD-CARE CENTRE (PRESCHOOL)**



Element	Discussion
<b>Land Use</b>	<ol style="list-style-type: none"> <li>The proposed land use consist of a preschool being a child-care centre, which comprises                         <ol style="list-style-type: none"> <li>A single storey building (913.89m<sup>2</sup>)</li> <li>Two external Play yards (513.2m<sup>2</sup> and 326m<sup>2</sup>)</li> <li>Capacity of maximum of 110 children</li> <li>Operation Hours between 6:00am and 6:30pm</li> </ol> </li> <li>Car Park comprising 26 spaces</li> <li>The Mixed Use Centre Policy area contemplates a wide range of land uses that serve the local community. The proposed use is in keeping with this intent and is listed as an envisaged use therefore satisfying Objective 1 and 3 and PDC 1 and 6 of the Policy Area.</li> </ol>
<b>Earthworks and Retaining Walls</b>	<ol style="list-style-type: none"> <li>No site civil plans have been provided as part of the application. The existing and finished levels of the land are dependent on the final design and approval of the associated land division applications and completion of the Gawler East Link Road.</li> </ol>

	<p>2. The applicants Planning Report states that retaining walls and earthworks will be required to accommodate the proposed development. This notion is supported by the "Site &amp; Floor Plan" which depicts a retaining wall along the northern (rear) and western (Gawler East Link Road frontage) boundary.</p> <p>3. At this stage of the proposal, there is insufficient information to properly assess the development against the following PDC's:</p> <p><b>PDC 7 (Policy Area)</b></p> <p>Filling of land and retaining walls exceeding 1.0 metre in height are appropriate where they are associated with the remediation and development of the former quarry site.</p> <p><b>PDC 29 (Zone)</b></p> <p>Retaining walls greater than 1.0 metre in height should:</p> <p>(a) not be directly visible from a public road or the public realm</p> <p>(b) where located between a building and a road frontage, be stepped in increments of not more than 1.0 metre (c) not cause significant adverse impacts on the amenity of adjacent properties, particularly in relation to overshadowing, cutting off light or views.</p> <p><b>PDC 31 (Zone)</b></p> <p>Development should be designed and sited to relate to the slope of the land, so that:</p> <p>(a) the bulk and scale of the buildings do not dominate the landscape;</p> <p>(b) the amount of cut and fill is minimised.</p> <p>4. The proposal identifies the need for retaining walls with the height of these walls currently being unknown until such time as the Gawler East Link Road has been constructed and the associated land division detailed</p>
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	<p>design completed and approved. Indicative levels provided as part of the current application demonstrate potential retaining walls up to 1.76 metres (for fill) maybe required above natural ground level in order to accommodate the proposed development. The height of retaining may represent development in its own right and as such, be subject to development approval. Accordingly, the provision of such retaining walls should be considered within the subject assessment.</p> <p>5. Notwithstanding, the provision of a fill retaining wall for the duration of the Western Road frontage would be discouraged by Council, particularly if such retaining exceeded a height of 1 metre. It would be recommended that a more balanced approach to earthworks be undertaken (combination of excavation and fill) to encourage greater interaction of the site with the Western frontage and to minimise the resulting visual bulk on the future public realm. This approach is supported by Zone PDC 29 and 31. The material utilised in the wall's construction should also be considered to provide for visual interest and to align with the desired character of the zone to achieve high architectural standard outcomes.</p>
<b>Design</b>	<p>1. The desired character statement of the Policy Area seeks to create a 'main street' environment with buildings addressing the street and car parking located primarily to the rear. The appearance of siting of buildings is encouraged to be within close proximity to street frontages and be of an architecturally high standard to create diversity of interest through the appearance of an aggregation of smaller buildings.</p> <p>2. Whilst it is acknowledged that the provision of carparking to the rear of the subject site is difficult to achieve given its dual road frontage, improvements to the built form would be encouraged to further align with the intent of the Desired Character.</p>

	<p>3. The building has been designed and sited to provide for the primary frontage to the western road. This approach is supported and the close siting of the building aligns with the intent of the Desired Character Statement. Notwithstanding, this approach is somewhat diminished by the provision of the fill retaining wall along this frontage which isolates the adjoining public realm from the subject site. The provision of feature steps and a DDA compliant ramp to the western frontage of the site would be encouraged to further assist in reducing the impact of retaining and accommodate a pedestrian focused link to the site. This approach could be complimented with the provision of landscaping.</p> <p>4. The northern elevation of the building incorporates the feature clad wall as a continuation of the western frontage. This approach is encouraged and provides for articulation and visual interest for the building when travelling north to south on the adjoining road.</p> <p>5. The southern elevation forms the extent of the building visible from the carparking area and abutting southern public road. A portion of the feature cladding of the western elevation is visible as a side return and is complimented by the provision of an additional feature wall central to the southern elevation. Of concern however, is the untreated provision of a brick wall between the feature claddings. As this wall is highly visible from the adjoining public road inclusive of the intersection, additional consideration and treatment should be considered. The provided feature bulkhead above is not considered sufficient to offset the extent of untreated wall provided below. This element of the proposal in its current form is not considered to align with the intent to achieve high standard architectural outcomes and further consideration should be given.</p>
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	<p>6. Whilst the design of the building with minor design improvements would be considered to satisfy the desired character of the Policy Area and Zone, a departure from a building generally of residential appearance would be encouraged.</p> <p>7. Zone Objective 7, PDC 22(b) and Policy Area Objectives 2, 3 and PDC's 3, 4, 5 and 6 are considered to be satisfied by the proposal.</p>
<b>Stormwater</b>	<p>1. Whilst the proposed stormwater design is dependent on the approval and construction of the adjoining public stormwater system associated with the land division proposals, the subject development seeks to manage stormwater on-site via the following:</p> <ul style="list-style-type: none"> <li>1.1 Collection of roof and surface water;</li> <li>1.2 Provision of an underground detention system;</li> <li>1.3 Provision of a Gross Pollutant Trap;</li> <li>1.4 Discharge to Future Side Entry Pit located in adjoining public road.</li> </ul> <p>2. Whilst this general approach for stormwater management is appropriate, further clarification and consideration on the details of the design are required. This has been reviewed by Council's engineering staff whom noted the following:</p> <ul style="list-style-type: none"> <li>2.1 The pre-development flow rate of 37.8L/s within the provided calculations appears to be too high as it has been calculated for a developed site. The flow rate should be in the order of approximately 8L/s as the site is currently undeveloped.</li> <li>2.2 Runoff coefficient calculations appear to be too low and should be increased to the following:</li> <li>2.3 From 0.92 to 1.0 for roofed areas; and</li> <li>2.4 From 0.75 to 0.9 for hardstand areas.</li> </ul>

	<ol style="list-style-type: none"> <li>3. The extent of detention provided appears to be undersized based on miscalculation. Council's engineers have calculated that approximately 70m<sup>3</sup> with a 50mm orifice would be required.</li> <li>4. Finished levels of the site would be required to ensure appropriate gradients for stormwater management can be achieved.</li> <li>5. Whilst the proposal incorporates a Gross Pollutant Trap, further consideration of WSUD principles would be encouraged.</li> </ol>
<b>Traffic</b>	<ol style="list-style-type: none"> <li>1. The traffic volume associated with the site is considered to be in keeping with the traffic volumes proposed as part of the associated land division application 490/D025/19 and 490/D026/19 for the adjoining road network. Accordingly, the traffic network proposed as part of the associated land division applications is not considered to be exceeded by the proposal.</li> <li>2. Access and egress from the subject land to the adjoining public road network will be obtained via one two-way simultaneous crossover to the abutting southern road being a Local Road proposed as part of the associated land division 490/D025/19. The car parking area is configured in a manner to accommodate on-site vehicle manoeuvring to enable vehicles to egress the site in a forward direction. As such, the movement of vehicles in association with the site will not conflict with the flow of traffic on adjoining roads.</li> <li>3. The traffic impact has been considered by Cirqa, the applicant's independent traffic consultant and Council's traffic engineers. Both of which, consider this element of the proposal to be acceptable.</li> <li>4. Given the location of waste refuge within the car parking area, the collection of waste should occur from the site outside of operation hours to ensure no conflict with waste vehicles and vehicles parked within the car parking</li> </ol>



	<p>spaces.</p> <ol style="list-style-type: none"> <li>5. The current plans do not provide for the construction site levels (including existing or design levels) of the adjoining roads (including the Gawler East Link Road) compared to proposed finished levels of the site and proposed development. Without this level of detail, there is concern relating to access and gradients between the subject site and the external roads (some being partially constructed or not yet approved).</li> <li>6. The proposal also lacks detail with regards to any required works relating to access and manoeuvrability into the site from any external roads (including the Gawler East Link Road) and the need for alterations to traffic devices such as any median strips, or access arrangements. Whilst it is acknowledged that not all external road infrastructure has been constructed, the associated designs may already exist and may require refinement to accommodate the proposal.</li> <li>7. Any works or refinements to roads that are required to accommodate the proposed development should be completed as part of the associated land division applications and secured via an Infrastructure Agreement (being as part of external infrastructure works) for the subject application to ensure such works (if required) are provided.</li> </ol>
<b>Car Parking</b>	<ol style="list-style-type: none"> <li>1. The proposal seeks to provide for 26 car parking spaces to service the proposed child care centre in a manner that is consistent with AS2890.1:2004 and AS2891.6:2009 (being the Australian Standard for off street parking and off street parking for people with disabilities).</li> <li>2. A car parking assessment has been undertaken by Cirqa Traffic Consultants with regard to the car parking requirements outlined within Table Ga/1 of the Development Plan. Based on the carparking requirements outlined within the Development Plan, a total of 28 car</li> </ol>

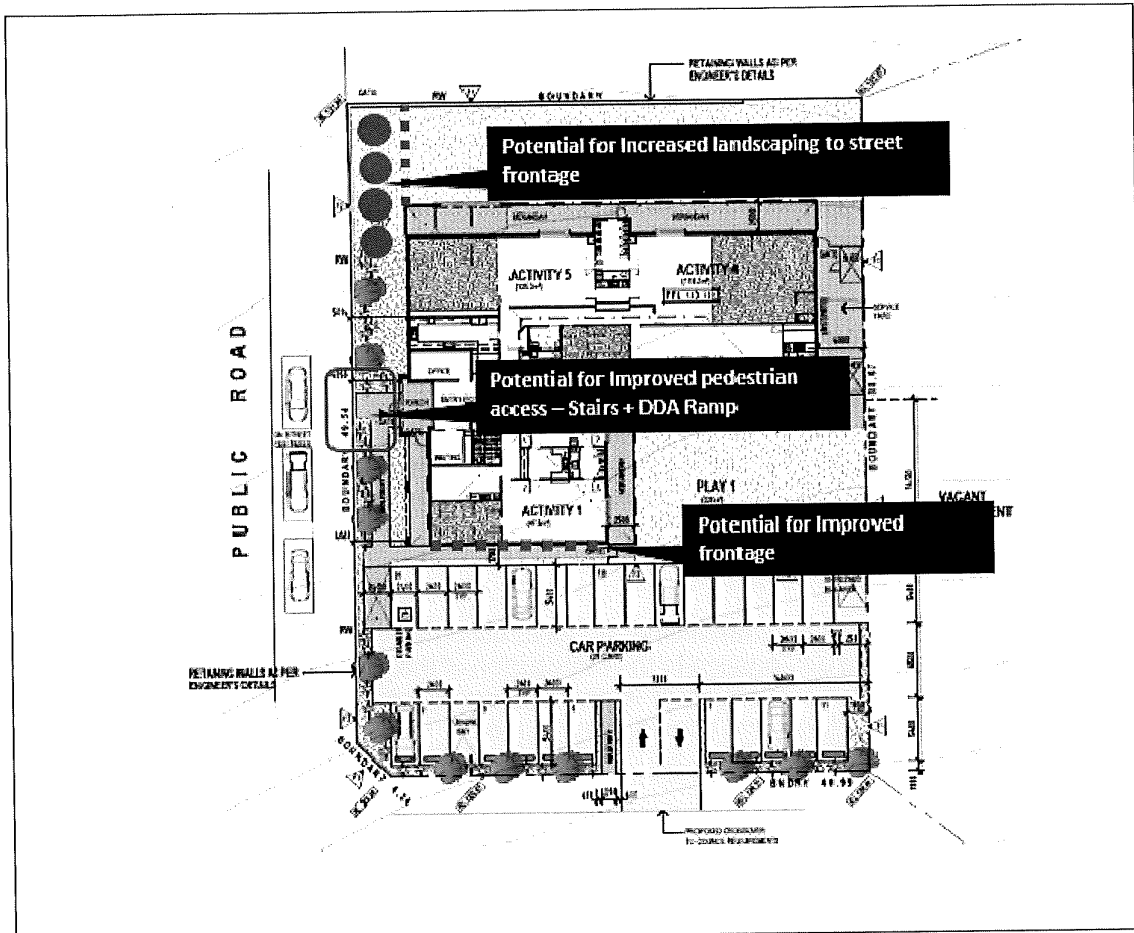
	<p>parking spaces would generally be required to service the proposed development. Therefore a shortfall of 2 carparks exists when considered against the requirements of Table Ga/1.</p> <p>3. Cirqa have further considered this shortfall against the proposed provision of carparking within the wider locality (being the future public roads) and consider that this minor shortfall can be absorbed within this future on-street provision. Given the minor shortfall, this notion is supported by Council administration.</p> <p>4. The indicative layout of the car parking area is sufficient to demonstrate that the appropriate amount of car parking has been provided to service the proposed use. Final detail however, is required for the design levels of the car parking area, to confirm gradients comply with both the DDA and AS2890.</p>
<b>Bicycle Parking</b>	<p>1. Bike parking and storage facilities should be considered and provided to the site as an alternative mode of transport.</p>
<b>Landscaping</b>	<p>1. A conceptual landscaping plan has been provided with the application, which demonstrated landscaping in the following forms:</p> <p>2. To the perimeter of the site;</p> <p>3. Adjacent to the building; and</p> <p>4. within outdoor play areas;</p> <p>5. The landscaping is proposed to comprise of a variety of irrigated trees species, shrubs and groundcovers. These specie's include:</p> <p style="padding-left: 40px;">5.1 Trees:</p> <p style="padding-left: 80px;">5.1.1 Ornamental pear; and</p> <p style="padding-left: 80px;">5.1.2 Pencil pine.</p> <p>6. Shrubs/Ground Cover:</p> <p style="padding-left: 40px;">6.1 Standard Lilly Pilly;</p> <p style="padding-left: 40px;">6.2 Westringia Fruiticosa</p>

	<p>6.3 Ornamental Grasses</p> <p>7. Whilst the provision of landscaping is generally appropriate, increased landscaping could be provided to the north-western corner of the site (between play area 2 and the road frontage) and south of play area 1.</p>
<b>Signage</b>	<ol style="list-style-type: none"> <li>1. The proposed development includes signage within the feature cladding indicated on the South, North and elevations.</li> <li>2. The proposed signage does not amount to proliferation and will not conceal any architectural elements and is in keeping with the scale of the building.</li> <li>3. Signage contained within the proposal aligns with Table Ga/4 of the Town of Gawler Development Plan and does not offend CW Objectives 69, 70 nor PDC's 199, 200, 201, 202 and 204.</li> </ol>
<b>Noise</b>	<ol style="list-style-type: none"> <li>1. The applicant has engaged Sonus Pty Ltd to undertake an Environmental Noise Assessment for the proposed childcare centre. The assessment seeks to investigate the proposals compliance against CW PDC's 107, 108, 111, 112 and 113.</li> <li>2. The report concludes that the development will minimise adverse impacts and conflict between land uses and the proposal will therefore not detrimentally affect the amenity of the locality or cause unreasonable interference through noise. The proposal therefore satisfies the relevant provisions of the Gawler Council Development Plan.</li> <li>3. The above recommendation is based on the inclusion of a 2.4 metre colorbond barrier to the boundary of the site to mitigate noise impacts pertaining to future single storey dwellings (if proposed) abutting the subject site. The report further noted that should two storey dwellings be proposed in lieu of single storey, that such dwellings would be required to incorporate reasonable and practicable acoustic treatments to account for the child</li> </ol>

	<p>care centre.</p> <ol style="list-style-type: none"> <li>4. Notwithstanding the above, it is noted that there are a few minor inconsistencies with the detail outlined within the report prepared by the applicants planning consultant and that contained within the acoustic report. This detail pertains to the hours of operation of the childcare centre. The acoustic report considers the proposal based on a 7am commencement where the planning report details a proposed opening time of 6am. This is not considered to be of any consequence as the acoustic report considered noise impact in a limited capacity before 7am.</li> <li>5. Whilst the proposed plan of division demonstrates that the subject land may in fact abut a future commercial property thus further minimising the impact of the proposed use. Further consideration should be given to the mechanism for securing such works relative to future dwellings on adjacent land to ensure the proposal satisfies the relevant Development Plan provisions.</li> <li>6. It is noted that if balanced earthworks were to be undertaken such as suggested above, the proposal would further reduce potential noise impacts through the provision of a cut retaining wall along the boundaries external to the subject site.</li> <li>7. Whilst the proposed noise attenuation measure is considered appropriate, Council would encourage the consideration of an alternative barrier to that of colourbond construction. An acoustic barrier of increased architectural standard would be encouraged given its location within the commercial precinct.</li> </ol>
<b>General Infrastructure Provision</b>	<ol style="list-style-type: none"> <li>1. Zone PDC 42 outlines that development should only occur if the required physical and community infrastructure has been provided or secured via a legally binding agreement.</li> <li>2. The physical and community infrastructure outlined within this provision includes:</li> </ol>

	<p>a) <i>Gawler East Link Road as illustrated on Structure Plan Map Ga/1 (Overlay 1) Enlargement G, and associated traffic interventions (road and intersection upgrades);</i></p> <p>b) <i>public lighting, pedestrian and bicycle paths and associated landscaping treatments associated with the Gawler East Link Road;</i></p> <p>c) <i>community facilities (including recreation facilities); and</i></p> <p>d) <i>stormwater management systems.</i></p> <p>3. To date this infrastructure has not been completed/provided nor secured. As such, the proposal does not satisfy Zone PDC 42 (being all elements a to d):</p>
<b>SEAGas informal referral</b>	<p>1. Whilst the proposed application does not trigger any referrals under Schedule 8 of the <i>Development Regulation 2008</i>, it is highly recommended that informal referral is sought by SEA Gas to ensure compliance with AS2885 and Zone PDC 36 (b) given that the proposed childcare centre will contain high concentrations of people including children within close proximity to the SEA Gas high pressure pipeline.</p>

## Visual Representation of Recommendations





## **Attachment 3 – Recommended Conditions**

### **490/440/2019 AND 490/441/2019**

#### General Conditions

1. The development hereby approved shall be undertaken in accordance with Development Application No 490/INSERT/INSERT, the approved plans, details and conditions therein.
2. Containers, bins or receptacles used for the temporary storage of garbage, waste or refuse arising from the premises, shall be located and/or screened from public view to the reasonable satisfaction of Council.
3. Mechanical plant shall be designed to be screened from public view and to the reasonable satisfaction of Council.
4. Prior to construction commencing, a Construction Environmental Management Plan shall be prepared and provide to the satisfaction of the Town of Gawler, which address the mitigation or minimisation of impacts (especially from noise and dust) and must be implemented during the construction phase.

#### Hours of Operation Conditions

5. Mechanical plant shall be designed and operate to 50 dB(A) during the day (7 am to 10 pm) and 43 dB(A) at night (10 pm to 7 am).
6. Machines and equipment used in conjunction with the use now approved being positioned and operated to minimise noise disturbance to residents in the locality to the reasonable satisfaction of Council and taking into account EPA requirements.

#### Car Parking Conditions

7. All loading and unloading of vehicles and manoeuvring of vehicles in connection with the now approved land use shall be carried out entirely within the subject land.
8. Parking layout to conform to the requirements of the most recent Australian/New Zealand Standard 2890 Parts 1 – 4 at the date of the approval
9. All car parking spaces shall be clearly delineated at all times by the use of white/yellow paint. Such works shall be completed to the Council's satisfaction prior to the occupation of the development herein approved.
10. No materials, goods or containers shall be stored in the car park or driveways at any time unless otherwise approved by Council.

11. Driveway, car parking spaces, manoeuvring areas and landscaping areas shall not be used for storage or display of materials or goods.
12. Entranceways shall be paved or sealed to the kerb and gutter, at the Developer's expense, to the satisfaction of the Council.
13. Devices to manage the access of vehicles from public roads on to reserves and other public open spaces are to be provided and installed to the reasonable satisfaction of Council.
14. All loading and unloading of vehicles and manoeuvring of vehicles in connection with the now approved land use shall be carried out entirely within the subject land.

#### Lighting Conditions

15. External lighting shall be restricted to that necessary for security and safety purposes only and shall be directed and shielded in such a manner as to not cause nuisance to adjacent properties or motorists.

#### Stormwater Conditions

16. Prior to Building Rules Consent and Full Development Approval being granted, a detailed stormwater management plan shall be provided to and approved by Council that demonstrates the management of stormwater on site in accordance with recognised engineering practices and relevant Australian Standards. The stormwater management plan shall be designed to detain the 1% Annual Exceedance Probability (AEP) post development peak flow from proposed development shall not exceed 0.2 Exceedances Per Year (EY) pre-development peak flow or the capacity of the downstream drainage infrastructure, whichever is the lesser.
17. The proposed stormwater drainage system shall include a water quality treatment facility and apply water sensitive urban design (WSUD) principles that comply with EPA Environment Protection (Water Quality) Policy 2015 and the Town of Gawler Standards and Requirements for Land Development/ Land Division.
18. All stormwater from buildings and paved areas shall be disposed of in accordance with recognised engineering practices in a manner and with materials that does not result in the entry of water onto any adjoining property or any building, and does not affect the stability of any building.
19. All stormwater runoff from the buildings shall be disposed of via public stormwater drainage system in a manner and with materials satisfactory to Council.
20. All drainage systems are to include all necessary pollution control devices at the downstream outlet of the development.

#### Landscaping Conditions

21. The proposed landscaping shall be established on the site in accordance with the approved plan prior to the occupation of the site and shall be maintained and nurtured at all times, with any diseased or dying plants replaced.

#### Advertising Conditions

22. Illuminated signs shall not be of a light intensity to cause a light overspill nuisance to adjacent occupiers, or cause a distraction to drivers on adjacent public roads.
23. The advertising approved herein shall be maintained in a state of good repair and tidy condition to the reasonable satisfaction of Council

## **Recommend Conditions 490/446/2019**

### **General Conditions**

1. The development hereby approved shall be undertaken in accordance with Development Application No 490/INSERT/INSERT, the approved plans, details and conditions therein.
2. Containers, bins or receptacles used for the temporary storage of garbage, waste or refuse arising from the premises, shall be located and/or screened from public view to the reasonable satisfaction of Council.
3. Mechanical plant shall be designed to be screened from public view and to the satisfaction of Council.
4. Prior to construction commencing, a Construction Environmental Management Plan shall be prepared and provide to the satisfaction of the Town of Gawler, which address the mitigation or minimisation of impacts (especially from noise and dust) and must be implemented during the construction phase.

### **Acoustic Conditions**

5. Prior to the use and occupation of the site, Acoustic attenuation treatments shall be installed on the northern and eastern boundaries.

### **Hours of Operation Conditions**

6. Mechanical plant shall be designed and operate to 50 dB(A) during the day (7 am to 10 pm) and 43 dB(A) at night (10 pm to 7 am).
7. Machines and equipment used in conjunction with the use now approved being positioned and operated to minimise noise disturbance to residents in the locality to the reasonable satisfaction of Council and taking into account EPA requirements.

### **Car Parking Conditions**

8. All loading and unloading of vehicles and manoeuvring of vehicles in connection with the now approved land use shall be carried out entirely within the subject land.
9. Parking layout to conform to the requirements of the most recent Australian/New Zealand Standard 2890 Parts 1 – 4 at the date of the approval

10. All car parking spaces shall be clearly delineated at all times by the use of white/yellow paint. Such works shall be completed to the Council's satisfaction prior to the occupation of the development herein approved.
11. No materials, goods or containers shall be stored in the car park or driveways at any time unless otherwise approved by Council.
12. Driveway, car parking spaces, manoeuvring areas and landscaping areas shall not be used for storage or display of materials or goods.
13. Entranceways shall be paved or sealed to the kerb and gutter, at the Developer's expense, to the satisfaction of the Council.
14. Devices to manage the access of vehicles from public roads on to reserves and other public open spaces are to be provided and installed to the reasonable satisfaction of Council.
15. All loading and unloading of vehicles and manoeuvring of vehicles in connection with the now approved land use shall be carried out entirely within the subject land.

#### Lighting Conditions

16. External lighting shall be restricted to that necessary for security and safety purposes only and shall be directed and shielded in such a manner as to not cause nuisance to adjacent properties or motorists.

#### Stormwater Conditions

17. Prior to Building Rules Consent and Full Development Approval being granted, a detailed stormwater management plan shall be provided to and approved by Council that demonstrates the management of stormwater on site in accordance with recognised engineering practices and relevant Australian Standards. The stormwater management plan shall be designed to detain the 1% Annual Exceedance Probability (AEP) post development peak flow from proposed development shall not exceed 0.2 Exceedances Per Year (EY) pre-development peak flow or the capacity of the downstream drainage infrastructure, whichever is the lesser.
18. The proposed stormwater drainage system shall include a water quality treatment facility and apply water sensitive urban design (WSUD) principles that comply with EPA Environment Protection (Water Quality) Policy 2015 and the Town of Gawler Standards and Requirements for Land Development/ Land Division.
19. All stormwater from buildings and paved areas shall be disposed of in accordance with recognised engineering practices in a manner and with materials that does not result in the entry of water onto any adjoining property or any building, and does not affect the stability of any building.
20. All stormwater runoff from the buildings shall be disposed of via public stormwater drainage system in a manner and with materials satisfactory to Council.

21. All drainage systems are to include all necessary pollution control devices at the downstream outlet of the development.

#### Landscaping Conditions

22. The proposed landscaping shall be established on the site in accordance with the approved plan prior to the occupation of the site and shall be maintained and nurtured at all times, with any diseased or dying plants replaced.

#### Advertising Conditions

23. Illuminated signs shall not be of a light intensity to cause a light overspill nuisance to adjacent occupiers, or cause a distraction to drivers on adjacent public roads.
24. The advertising approved herein shall be maintained in a state of good repair and tidy condition to the reasonable satisfaction of Council





## Attachment 4 – Council Assessment Panel Report

### DEVELOPMENT APPLICATION OVERVIEW

<b>Item Number</b>	<b>5.5</b>
<b>Development Application</b>	1) 490/440/2019 – 490/E005/19; 2) 490/441/2019 – 490/E006/19; 3) 490/446/2019 – 490/E009/19;
<b>Applicant</b>	SPRINGWOOD DEVELOPMENT NOMINEES PTY LTD LEYTON PROPERTIES PTY LTD
<b>Owner</b>	FIVE AMES FARMING PTY LTD
<b>Subject Land</b>	<ul style="list-style-type: none"> <li>• Lot 2046 within 490/D026/19</li> <li>• Lot 2048 within 490/D026/19</li> <li>• Lot 1006 within 490/D025/19</li> </ul>
<b>Certificate of Title</b>	LOT: 9010 PLN: D114845 CT: 6205/146
<b>Description of Development</b>	1) Construction of a Sales Centre (two storey) comprising Showroom, Office and Cafe with associated car park and signage; 2) Construction of a shopping centre comprising supermarket and six (6) specialty shops, two (2) standalone shops, associated car parking and signage; and 3) Construction of a single-storey child-care centre (preschool)
<b>Zone</b>	Residential (Gawler East) Zone
<b>Policy Area</b>	Mixed Use Policy Area
<b>Key Issues / Considerations</b>	<ul style="list-style-type: none"> <li>• Appropriateness of land use</li> <li>• Earthworks and Retaining</li> <li>• Design</li> <li>• Stormwater</li> <li>• Traffic</li> <li>• Carparking</li> <li>• Landscaping</li> <li>• Signage</li> <li>• General Infrastructure Provision</li> <li>• External referrals to SEA Gas and ElectraNet</li> </ul>
<b>Previous Motion</b>	Nil
<b>Lodgement Date</b>	26/09/2019
<b>Development Plan</b>	18/07/2019
<b>Attachments Under Separate Cover</b>	Attachment 1 - Application Documents 490/440/2019 – 490/E005/19 Attachment 2 - Application Documents 490/441/2019 – 490/E006/19 Attachment 3 - Application Documents 490/446/2019 – 490/E009/19 Attachment 4 - Submission to SCAP outlining Council comments Attachment 5 - Councils Procedural Concerns

	Attachment 6 - Recommended conditions for Development Plan Consent
<b>Authors</b>	Team Leader Development Services (David Bielatowicz) Senior Development Assessment Planner (Scott Twine)
<b>Reviewed</b>	Manager Development, Environment & Regulatory Services (Ryan Viney)
<b>Delegation</b>	Any form of development which the Manager –, Development, Environment and Regulatory Services deems would be more appropriately considered by the Council Development Assessment Panel.
<b>Recommendation</b>	<p><b>It is recommended that the Town of Gawler Council Assessment Panel resolves to:</b></p> <ol style="list-style-type: none"> <li><b>1. Note the report pertaining to Development Applications 490/440/2019, 490/441/2019, 490/446/2019.</b></li> <li><b>2. Endorse the subject report and the submissions outlined in Attachments 4, 5 and 6 of this report for the purpose of forwarding to SCAP in relation to Development Applications 490/440/2019, 490/441/2019, 490/446/2019.</b></li> <li><b>3. Authorise the Presiding Member and Assessment Manager to amend the endorsed submission based on the outcomes of these deliberations, and where otherwise considered necessary, prior to forward to SCAP by 7 November 2019.</b></li> <li><b>4. Authorise staff to continue to work collaboratively with SCAP as the Relevant Authority and the developer to reach agreement on all outstanding matters in order to achieve a quality planning outcome for the community.</b></li> </ol>

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## 1. DESCRIPTION OF PROPOSAL

- 1.1. Three (3) development applications have been lodged with the State Commission Assessment Panel (SCAP) and include:
- 1.1.1. **490/440/2019** – 490/E005/19 - Construction of a Sales Centre (two storey) comprising Showroom, Office and Cafe with associated car park and signage;
  - 1.1.2. **490/441/2019** – 490/E006/19 - Construction of a shopping centre comprising supermarket and six (6) specialty shops, two (2) standalone shops, associated car parking and signage; and
  - 1.1.3. **490/446/2019** – 490/E009/19 - Construction of a single-storey child-care centre (preschool) with associated car parking, landscaping, fencing and signage.
- 1.2. The above proposals have been lodged with SCAP and are all part of a 'call-in package' by the State Coordinator General within the Springwood Development.
- 1.3. Given that all three proposals share the same background, relevant history and similar development issues, the proposals are presented within one report, which will form part of Councils official referral comments.
- 1.4. Copies of the application documents are contained within **Attachments 1** (490/440/19), **2** (490/441/19) and **3** (490/446/19).

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## 2. PURPOSE OF REPORT

- 2.1. This report has been prepared for the purposes of providing Councils formal referral comments to SCAP relating to the three (3) commercial development applications that have been lodged. The following report provides the assessment conducted by Council administration and includes Council comments and recommendations in relation to the applications presented before the Gawler Council Assessment Panel.

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## 3. RELEVANT DEVELOPMENT APPLICATION HISTORY

- 3.1. The subject applications form part of the overall master planned Springwood Development, which once completed will feature an approximately 220ha residential community including a mixed use Town Centre and educational precinct and vast areas of public open space.
- 3.2. To date, approximately 350 residential allotments have already been created or approved, while an additional (approximate) 1,400 residential allotments have been sought to be created via four (4) separate land division applications.

- 3.3. These four (4) land division applications were presented to the Council Assessment Panel for consideration in August 2019 and are to be determined by SCAP at a later date.
- 3.4. Included with the proposed 1400 lots, is approximately a dozen allotments that are proposed to be created for future commercial or educational purposes.
- 3.5. Three (3) of the proposed commercial allotments, although not yet approved or created are subject to the three built form applications presented before the CAP.
- 3.6. By letter dated 15 November 2018, the State Coordinator General advised that the State Commission Assessment Panel (SCAP) would be the relevant authority to assess all of the remaining portions of the Springwood development (excluding Residential development).
- 3.7. In accordance with Schedule 10(20) of the Development Regulations 2008, the State Coordinator General determined that the overall land division and associated commercial development met specific 'call-in' criteria including that once all stages are completed, the project will exceed \$5 million and will not solely be for residential purposes (i.e. The development will contain a commercial and educational precinct).
- 3.8. In making the determination for SCAP to be the relevant authority, it is noted that the determination does not constitute a form of advocacy nor imply a favourable assessment outcome. It is noted that an assessment must still be made against the provisions of the Town of Gawler (CT) Development Plan consolidated 18 July 2019.
- 3.9. This 'call-in' by the State Coordinator General was conditional on the development proceeding via a series of 'Development Packages' as follows.
- 3.9.1. **Development Package 1** comprising the following elements:
- 3.9.1.1. Land division for all of the remaining portions of the Springwood development (including residential areas, retail areas, community areas and school precinct);
  - 3.9.1.2. Retail area (supermarket, speciality shops and associated car parks); and
  - 3.9.1.3. Sales Centre with retail component
- 3.9.2. **Development Package 2** comprising the following elements:
- 3.9.2.1. Retirement village;
  - 3.9.2.2. Childcare Centre; and
  - 3.9.2.3. Additional retail.
- 3.9.3. **Development Package 3** comprising the following elements:

3.9.3.1. School precinct.

3.10. The subject three applications are seeking Development Plan Consent for:

3.10.1. Construction of a Sales Centre (two storey) comprising Showroom, Office and Cafe with associated car park and signage;

3.10.2. Construction of a shopping centre comprising supermarket and six (6) specialty shops, two (2) standalone shops, associated car parking and signage; and

3.10.3. Construction of a single-storey child-care centre (preschool).

3.11. These developments cover Parts 2 and 3 of Development Package 1 and Part 2 of Development Package 2, with the land division for all remaining portions of Springwood already being lodged with SCAP in June 2019 and Council providing commentary back to the Department of Planning, Transport and Infrastructure back in August 2019.

3.12. A petrol filling station and car wash is proposed to complete the remainder of Development Package 2 and will be lodged with SCAP in the near future.

3.13. It is important to note, that at the time of this report no decision had been made on either of the four land division applications lodged and furthermore, no public consultation had yet be undertaken.

3.14. A history of the development of the site inclusive of the lands rezoning is provided below within table 1.

**Table 2:**

DA#	DESCRIPTION OF PROPOSAL	STATUS
Ministerial DPA	August 2010 – Ministerial Development Plan Amendment (DPA) Minister Rezones land within Gawler East from Rural to Residential	<ul style="list-style-type: none"><li>• Rezoned</li></ul>
490/D054/2010	Land Division for Approximately 225 Allotments with associated public infrastructure and areas of public open space	<ul style="list-style-type: none"><li>• Most stages constructed and subsequent dwellings approved and built.</li><li>• One Stage remaining to be constructed (consent still operative)</li></ul>
490/D038/2013 – 490/D050/2013	Variation to configuration of stages and allotments within 490/D054/2010	<ul style="list-style-type: none"><li>• Stages constructed and complete.</li><li>• Subsequent dwellings approved and built</li></ul>



490/D064/2012	Creation of 44 new allotments and two (2) development stages	<ul style="list-style-type: none"> <li>• Stages constructed and complete.</li> <li>• Most subsequent dwellings approved and built</li> </ul>
490/D038/2014	Creation of 26 Residential allotments and 2 super-lots	<ul style="list-style-type: none"> <li>• Stages constructed and complete.</li> <li>• Most subsequent dwellings approved and built</li> </ul>
490/D014/2016, 490/D015/2016, 490/D016/2016,	Creation of 10 (total) residential lots at the end of existing cul-de sacs	<ul style="list-style-type: none"> <li>• DPC and LDC Granted, stages currently under construction</li> </ul>
490/D009/2017	Land Division by Torrens Title creating a total of 62 Residential Allotments with associated public roads, open space reserves and infrastructure	<ul style="list-style-type: none"> <li>• DPC and LDC Granted, Construction commenced</li> </ul>
490/D010/17	Land Division by Torrens Title creating a total of 74 Residential Allotments with associated public roads, open space reserves and infrastructure	<ul style="list-style-type: none"> <li>• DPC and LDC Granted</li> </ul>
490/D025/2019, 490/D026/2019, 490/D027/2019, 490/D028/2019	4 Separate Land Divisions by Torrens Title creating 1,415 with associated public roads, infrastructure, public open space, bulk earthworks and the removal of 87 Regulated (40 Significant) Trees	<ul style="list-style-type: none"> <li>• Currently under assessment by SCAP as the relevant authority</li> </ul>
490/440/2019 490/E005/19	Construction of a Sales Centre (two storey) comprising Showroom, Office and Cafe with associated car park and signage;	<ul style="list-style-type: none"> <li>• Currently under assessment by SCAP as the relevant authority</li> </ul>
490/441/2019 490/E006/19	Construction of a shopping centre comprising supermarket and six (6) specialty shops, two (2) standalone shops, associated car parking and signage; and	<ul style="list-style-type: none"> <li>• Currently under assessment by SCAP as the relevant authority</li> </ul>
490/446/2019 490/E009/19	Construction of a single-storey child-care centre (preschool)	<ul style="list-style-type: none"> <li>• Currently under assessment by SCAP as the relevant authority</li> </ul>

#### 4. RELEVANT AUTHORITY

4.1. As mentioned earlier and pursuant to Schedule 10, clause 20 of the *Development Regulations 2008*, the applicant (the Developer) has made a request to the State Coordinator-General to assign the SCAP as the Planning Authority for the Springwood

development, in particular in relation to the balance of the land division and commercial development.

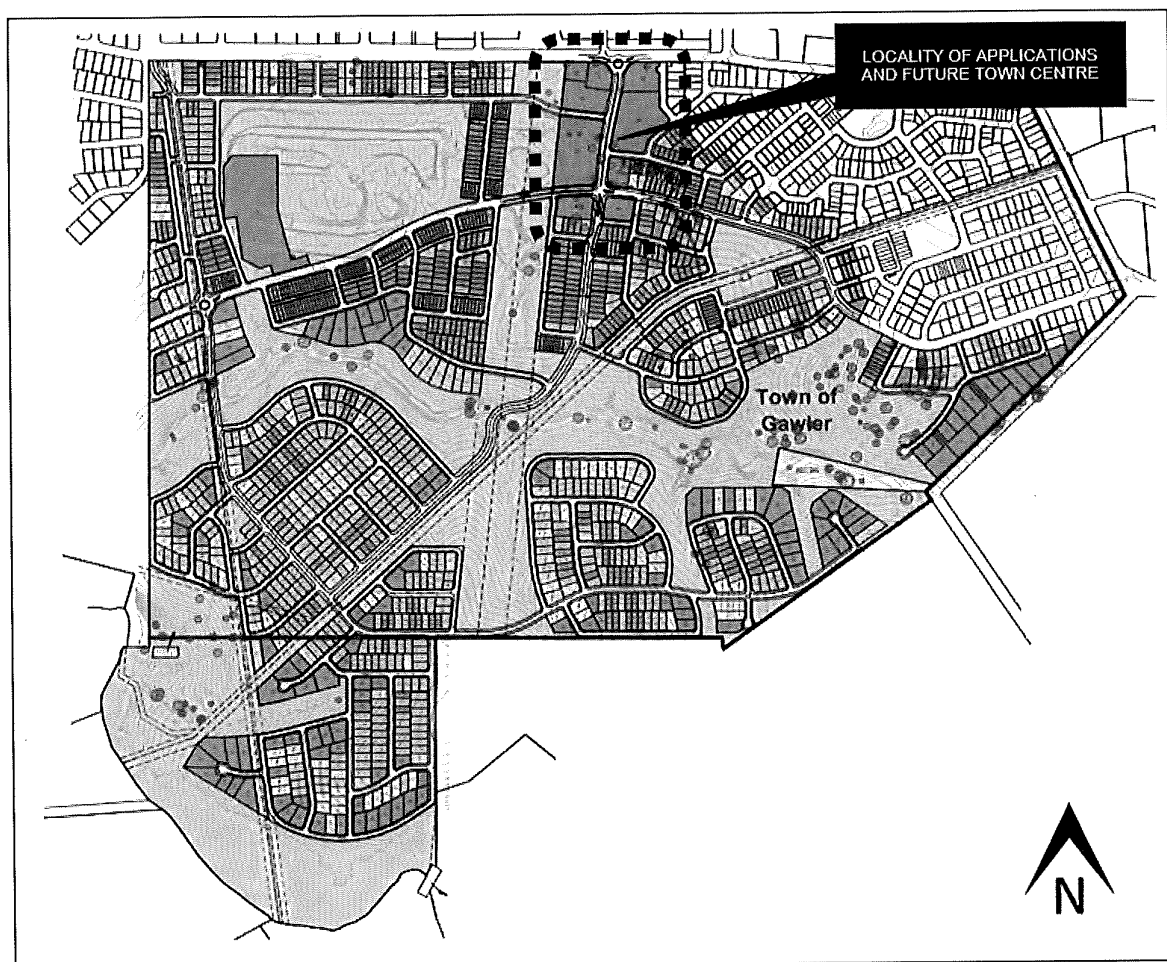
- 4.2. Given that the proposal satisfies criteria mentioned previously, the applications have been deemed by the Coordinator-General as of State Significance.
- 4.3. As the SCAP has been assigned as the relevant authority, the planning assessment process ordinarily undertaken by Council is now the responsibility of the SCAP. Accordingly, the Council's responsibility with regard to this proposal is one solely of a referral agency.
- 4.4. The process for SCAP is similar to that of Council, in that SCAP is also governed by the *Development Act 1993* and the *Development Regulations 2008*. All three subject applications will be required to be assessed against the Town of Gawler Development Plan consolidated 18 July 2019 and undertake public notification if required.
- 4.5. In accordance with 38(2)(B) of the *Development Regulations 2008*, Council have the opportunity to provide a report and recommendations to SCAP relating to the matters under section 33(1) of the *Development Act 1993* within a six (6) week period from the time the application was referred to Council.
- 4.6. The date for Council to provide official comments and its recommendations is no later than 7 November 2019.
- 4.7. It is important to note, that Council's role is as a referral body is to raise concerns, key issues or make recommendations to the SCAP given its local knowledge base. Further it is noted that Council has the opportunity to provide recommendations to the SCAP and additional conditions should the SCAP resolve to approve the development proposal. These conditions and recommendations will be considered at SCAP's discretion.
- 4.8. Given Council is only a referral agency, the Panel is advised that the applications cannot be deferred until a later date (meeting) for further consideration and all Council concerns and comments must be received by the SCAP by no later than the specified timeframes.
- 4.9. In accordance with Regulation 38 (2)(a)(ii)(B) of the *Development Regulations 2008*, SCAP may as it considers fit disregard or reject any recommendation or comments by Council.
- 4.10. It must be further noted, that Council and the Council Assessment Panel do not have the ability to request further information or amendments directly from the applicant in respect of the proposal.

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## 5. LOCALITY

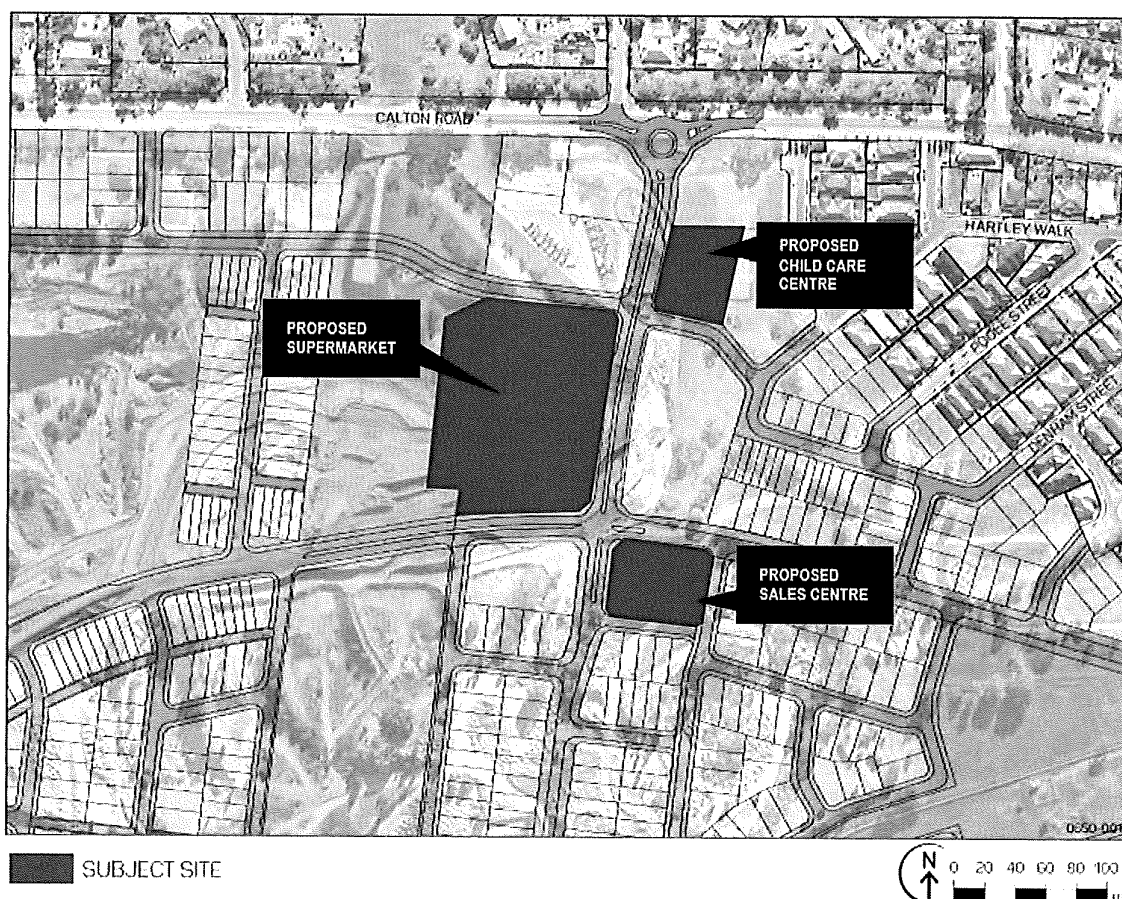
- 5.1. The existing locality for the subject proposals is the most eastern edge of the former quarry where excavated sand was processed (including washed) and stockpiled for sale.
- 5.2. The locality especially within the location of the proposed shopping centre and sales and information centre was extensively excavated as part of quarry operations and now is also within the area of heavy civil activity for the Gawler East Link Road that is currently under construction.
- 5.3. Generally speaking, the locality for the current proposals comprises of Calton Road to the north with existing residential development beyond, the initial stages of Springwood (stages 1 -10) to the east, the former quarry to the west and farmland and undulating hills which increase in gradient towards the South Para River to the south.
- 5.4. The locality will be transformed over the coming years with the completion of the Gawler East Link Road and through the delivery of housing and the proposed Town Centre as part of the associated land division applications.
- 5.5. An aerial image of the current locality and future town centre as proposed within land division 490/D025/19 and 490/D026/19 is provided below.





## 6. SUBJECT SITE(S)

- 6.1. The subject three (3) development applications are proposed on three allotments anticipated as part of land division 490/D026/19 and 490/D025/19, which both have yet been determined and therefore do not legally exist.
- 6.2. The subject proposed allotments for the following three proposals are depicted below within figure 1 within the proposed Town Centre as part of the two mentioned land divisions and include:
  - 6.2.1. Lot 2046 within 490/D026/19 for the proposed Shopping Centre (Supermarket)
  - 6.2.2. Lot 2048 within 490/D026/19 for the proposed Childcare Centre
  - 6.2.3. Lot 1006 within 490/D025/19 for the proposed Sales Centre



- 6.3. Given that the above allotments have not yet been legally be created (or approved) as part of any land divisions, all three applications legally comprise on Allotment 9010 within Deposited Plan 114845, Certificate of Title Volume 6205 Folio 146.
- 6.4. The current and proposed allotments are contained within the Residential (Gawler East) Zone, and more specifically within the Mixed Use Centre Policy Area.
- 6.5. The subject land and subsequent Certificate of Title contains a number of easements registered across the land, including:
  - 6.5.1. A 15 metre wide easement registered to the Minister for Infrastructure containing a High Pressure Gas transmission pipeline owned and operated by SEA Gas.
  - 6.5.2. A 10 metre wide easement registered to the Minister for Infrastructure containing a potable water main owned and operated by SA Water.
  - 6.5.3. A 100 metre wide easement containing a 275kv high power electricity transmission line owned and operated by ElectraNet.

- 6.5.4. A 30 metre wide easement containing a 132kv high power electricity transmission line operated by ElectraNet
- 6.5.5. Two (2) Land Management Agreements (LMA) are registered against the land, one with Renewal SA (South Australian Housing Trust) for the provision of a minimum 15% Affordable Housing. The second being a LMA with the Town of Gawler to ensure a minimum 12.5% open space is provided within the Development Area of Springwood in accordance with Section 50 of the *Development Act 1993*.
- 6.6. Whilst neither land division has yet been approved nor easements determined on each allotment, it is anticipated that proposed allotment 2046 (shopping centre) will retain an existing easement to ElectraNet as the 275kv High power transmission line traverses this allotment.
- 6.7. All three proposed allotments are located within vicinity of the former quarry where significant earthworks has previously occurred as a consequence of quarry activities. Whilst a site inspection has been unable to be undertaken, it is understood that some minor vegetation in the form of trees, shrubs and ground covers are evident.
- 6.8. All three proposed allotments seek access onto the Gawler East Link Road which is currently under construction and furthermore is resulting in additional and substantial civil and earthworks occurring within the locality.

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## **7. NATURE OF DEVELOPMENT**

- 7.1. Whilst the nature of the development for each application will be determined by SCAP, neither of the three proposals are not listed as either a complying or non-complying form of development within the Residential (Gawler East) Zone as delineated within the Gawler (CT) Development Plan. Therefore, Council administration considers that all three development applications will assessed as a merit form of development.

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## **8. CATEGORISATION (PUBLIC NOTIFICATION)**

- 8.1. The Categorisation of the applications will be undertaken by SCAP as the relevant authority.
- 8.2. In accordance with the Public Notification provisions within Principle of Development Control (PDC) 45 of the Residential (Gawler East) Zone, all three applications are considered to be Category 2 forms of Development.
- 8.3. As per PDC 45 which states:

*Further, all development listed within Principle of Development Control 1 of the Residential*



*(Gawler East) Zone, Mixed Use Policy Area 3 and/or Local Centre Policy Area 19 are designated category 2 (except where the development is classified as Category 1 or non-complying).*

All three applications are listed within PDC 1 envisaged land uses within the zone:

***Land Use***

1      *The following forms of development are envisaged in the zone:*

*Affordable housing*

*Aged persons' accommodation*

*Community facilities*

*Domestic outbuilding in association with a dwelling*

*Domestic structure*

*Dwelling Dwelling*

*addition*

*Dwelling with associated home based business uses Land  
division*

*Non-residential use that serves the local community, for example:*

*- child care facility*

*- health and welfare service*

*- open space*

*- pre-school*

*- primary and secondary school*

*- recreation area*

*- shop, office or consulting room*

*Retirement village*

*Supported accommodation*

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## **9. PROCEDURAL MATTERS**

9.1. Legal advice has been sought with regards to procedural matters in processing the subject three applications without each corresponding allotment existing for each of the proposals (including lack of legal road access) and further the need to consider earthworks.

9.2. The legal advice received in summary included:

9.2.1. Each of the respective land divisions cannot and should not be assessed and determined unless and until the divisions which create the relevant public roads from

which access will be taken, are approved. Until this occurs, none of the proposed

developments in the 3 development applications have a lawful means of access. The 3 development applications cannot be granted a Development Plan consent on this basis. To do so would be contrary to the Gawler (CT) Development Plan and would, be against general practice (see, e.g. Rohrlach & Anor v City of Unley [2011] SAERDC 19).

- 9.2.2. Further, fundamental aspects of the 3 development applications, being floor levels stormwater management and retaining wall heights are uncertain and are subject on as-yet undetermined development applications. It would be inappropriate and contrary to the Development Plan to grant Development Plan consent to the 3 development applications without certain information in this regard. Specifically, floor levels and retaining wall heights are significant planning merit considerations such that they cannot be dealt with as reserve matters for the reasons expressed in the Supreme Court judgment of Mar Mina (SA) Pty Ltd v City of Marion & Anor [2008] SASC 120.
- 9.2.3. The lack of lawful access does not render the 3 development applications hypothetical. The test for a “hypothetical” development is whether there is a reasonable prospect of the development proceeding (see Hackney Hotel Pty Ltd v Corporation of the Town of St Peters (1983) 32 SASR 145 and Rampling v City of Holdfast Bay [2010] SAERDC 60).
- 9.2.4. Once the relevant land division applications are approved and plans deposited at the Land Titles Office (LTO) such that the proposed public roads are sufficiently certain so as to facilitate future developments relying on them for access, and so that levels and retaining wall heights can be confirmed, the 3 development applications may then be granted Development Plan consent.
- 9.2.5. In conclusion neither of the 3 development applications can be determined until:
  - 9.2.5.1. The divisions creating the public roads for which each of the 3 development applications are reliant for access are approved and the relevant plans of division deposited at the LTO; or
  - 9.2.5.2. Each of the 3 development applications is amended to provide access to an existing public road and to remove all uncertainty concerning levels and retaining walls (which are currently specified as being contingent on the outcome of the relevant land division applications).

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## 10. REFERRALS

10.1. Whilst neither of the proposed applications trigger any referrals under Schedule 8 of the *Development Regulation 2008*, the following informal referrals are highly recommended by the Town of Gawler:

10.1.1. SEAGas to ensure compliance with AS2885 and Zone PDC 36 (b) given that all three applications contain high concentrations of people within close proximity to the SEA Gas high pressure pipeline.

10.1.2. ElectraNet given that the proposed Shopping centre is encroaching within the transmission line corridor and in particular is currently at variance with Zone PDC 19(c) which seeks a 100metre wide corridor within the existing ElectraNet easement and transmission corridor.

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## 11. ASSESSMENT COMMENTS TO SCAP

11.1. Council administration have undertaken a review of the proposed three development applications.

11.2. Councils overall key concerns have been provided within the following table(s) below which will form part of Council's submission to SCAP (**Attachment 4**).

11.3. Further general comments which provide additional and supporting commentary to the key issues (tabled below).

11.4. Councils procedural comments mentioned above will be included to SCAP as per **Attachment 5**.

11.5. Recommended conditions of approval should SCAP resolve to approve the three applications are provided within **Attachment 6**.

11.6. The following assessment includes consideration of:

11.6.1. Appropriateness of land use

11.6.2. Earthworks and Retaining

11.6.3. Design

11.6.4. Stormwater

11.6.5. Traffic

11.6.6. Carparking

11.6.7. Landscaping

11.6.8. Signage

11.6.9. General Infrastructure Provision

11.6.10. External referrals to SEA Gas and ElectraNet

11.7. It must be reiterated that Council's role is only that of a referral agency for the purposes of the assessment of the subject proposals and to provide comment in relation to specific areas of expertise or in a general sense to the overall proposals. The actual assessment (planning) and determination of the merits of the application will be undertaken by SCAP who do so taking into account Council's (and other referral agencies) comments and recommendations.

11.8. The following assessment was based on the current documents that have been lodged, and it should be noted that no requests for further information or amendments have been made by the Relevant Authority (SCAP) thus far.

490/440/2019 – 490/E005/19

**CONSTRUCTION OF A SALES CENTRE (TWO STOREY) COMPRISING SHOWROOM, OFFICE AND CAFE WITH ASSOCIATED CAR PARK AND SIGNAGE**



Element	Discussion
<p><b>Land Use</b></p>	<ol style="list-style-type: none"> <li>The proposed land use has been defined by the applicant as a Sales Centre and will consist of a number of associated elements which include: <ul style="list-style-type: none"> <li>A sales Centre (132.55m<sup>2</sup>)</li> <li>A Café (79.4m<sup>2</sup>)</li> <li>Office with Amenities (159.93m<sup>2</sup>)</li> <li>35 Car parks</li> </ul> </li> <li>The Mixed Use Centre Policy area contemplates a wide range of land uses that serve the local community. The proposed use is in keeping with this intent and therefore satisfies Objective 1, 3 and PDC 1 and 6 of the policy area.</li> </ol>
<p><b>Earthworks and Retaining</b></p>	<ol style="list-style-type: none"> <li>No site civil plans have been provided as part of the application. The existing and finished levels of the land are dependent on the final design and approval of the associated land division applications and completion of the Gawler East Link Road.</li> <li>Depending on the level of earthworks that result in both the associated land division and Gawler East Link Road works, there</li> </ol>



	<p>may be a need for additional earthworks and retaining walls.</p> <p>3. At this stage of the proposal, there is insufficient information to properly assess the development against the following PDC's:</p> <p><b>PDC 7 (Policy Area)</b></p> <p>Filling of land and retaining walls exceeding 1.0 metre in height are appropriate where they are associated with the remediation and development of the former quarry site.</p> <p><b>PDC 29 (Zone)</b></p> <p>Retaining walls greater than 1.0 metre in height should: (a) not be directly visible from a public road or the public realm (b) where located between a building and a road frontage, be stepped in increments of not more than 1.0 metre (c) not cause significant adverse impacts on the amenity of adjacent properties, particularly in relation to overshadowing, cutting off light or views.</p> <p><b>PDC 31 (Zone)</b></p> <p>Development should be designed and sited to relate to the slope of the land, so that:</p> <ul style="list-style-type: none"> <li>(a) the bulk and scale of the buildings do not dominate the landscape;</li> <li>(b) the amount of cut and fill is minimised.</li> </ul>
<b>Design</b>	<p>1. The desired character statement of the Policy Area seeks to create a 'main street' environment with buildings addressing the street and car parking located primarily to the rear. The appearance of siting of buildings is encouraged to be within close proximity to street frontages and be of an architecturally high standard to create diversity of interest through the appearance of an aggregation of smaller buildings.</p> <p>2. This intent is achieved through the siting of the building being closely aligned with the intersection of the Gawler East Link Road and the intersecting north-south collector road and rear located car parking area. The built form comprises of a high standard</p>

	<p>façade incorporating protruding elements and features which provide for visual interest and promote a pedestrian-friendly environment.</p> <ol style="list-style-type: none"> <li>3. The two-storey building addresses both street frontages and incorporates paving, street furniture and landscaping that integrate the adjoining public spaces with the subject development.</li> <li>4. Zone Objective 7, PDC 22(b) and Policy Area Objectives 2, 3 and PDC's 3, 4, 5 and 6 are considered to be satisfied by the proposal.</li> <li>5. The sales centre is considered to achieve a strong design outcome which sets a positive benchmark for future development within the Policy Area.</li> </ol>
<b>Stormwater</b>	<ol style="list-style-type: none"> <li>1. The applicant has noted that a site specific Stormwater Management Plan will be prepared for the subject development. The plan will build upon the overall Stormwater Management Plan for the entire Springwood development which has been submitted with the concurrent land division applications.</li> <li>2. Whilst a Reserved Matter or at the very least a condition would generally be appropriate for the provision of the final design, a concept plan demonstrating the high level method of stormwater management should be provided for the purposes of the planning assessment prior to the issuing of any Development Plan Consent.</li> <li>3. The stormwater system should incorporate adequate detention, water quality control (i.e. Gross Pollutant Trap to separate oils and other pollutants) for the carparking area and encourage the use of Water Sensitive Urban Design (WSUD) principles.</li> <li>4. Applicable WSUD principles that could be adopted within the design are identified with CW PDC 148, 150, 151, 152, 154, 155, 157 and 158.</li> </ol>
<b>Traffic</b>	<ol style="list-style-type: none"> <li>1. The traffic volume associated with the site is considered to be in keeping with the traffic volumes proposed as part of the associated land division application 490/D025/19 and 490/D026/19 for the adjoining road network. Accordingly, the traffic network proposed</li> </ol>

	<p>as part of the associated land division applications is not considered to be exceeded by the proposal.</p> <ol style="list-style-type: none"> <li>2. Access and egress from the subject land to the adjoining public road network will be obtained via two two-way simultaneous crossovers. These crossovers are situated upon a Local Road and the North-South Collector Road proposed as part of the associated land division 490/D025/19. The car parking area is configured in a manner to accommodate on-site vehicle manoeuvring to enable vehicles to egress the site in a forward direction. As such, the movement of vehicles in association with the site will not conflict with the flow of traffic on adjoining roads.</li> <li>3. The traffic impact has been considered by Cirqa, the applicant's independent traffic consultant and Council's traffic engineers. Both of which, consider this element of the proposal to be acceptable.</li> <li>4. Should waste collection not be achieved via kerb side collection, additional consideration should be given to the manner in which waste is collected from the site.</li> <li>5. The current plans do not provide for the construction site levels (including existing or design levels) of the adjoining roads (including the Gawler East Link Road) compared to proposed finished levels of the site and proposed development. Without this level of detail, there is concern relating to access and gradients between the subject site and the external roads (some being partially constructed or not yet approved).</li> <li>6. The proposal also lacks detail with regards to any required works relating to access into the site from any external roads (including the Gawler East Link Road) and the need for alterations to traffic devices such as any median strips, or access arrangements. Whilst it is acknowledged that not all external road infrastructure has been constructed, the associated designs may already exist and may require refinement to accommodate the proposal.</li> <li>7. Any works or refinements to roads that are required to accommodate the proposed development should be completed as</li> </ol>
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	<p>part of the associated land division applications and secured via an Infrastructure Agreement (being as part of external infrastructure works) for the subject application to ensure such works (if required) are provided.</p>
<b>Car Parking</b>	<ol style="list-style-type: none"> <li>1. The proposal seeks to provide for 35 car parking spaces to service the proposed use(s) in a manner that is consistent with AS2890.1:2004 and AS2891.6:2009 (being the Australian Standard for off street parking and off street parking for people with disabilities).</li> <li>2. A car parking assessment has been undertaken for the Sales Centre and associated Office and Café use with consideration to the car parking rates outlined within the Gawler East Residential Zone and Table Ga/1. Based on the carparking requirements outlined within the Development Plan, a total of 31 car parks is required to service the proposed development. Therefore a surplus of four carparks exists.</li> <li>3. It is noted that the provided Cirqa Traffic Consultant report further discusses the provision of car parking on site for a Display Village comprising of approximately 33 display homes. This Display Village and associated display homes has not been formally proposed as part of the subject application and as such, is not a consideration at this point in time. When/if a display village is lodged, then the provision of carparking can be further considered in this regard.</li> <li>4. The indicative layout of the car parking area is sufficient to demonstrate that the appropriate amount of car parking has been provided to service the proposed use. Final detail however, is required for the design levels of the car parking area, to confirm gradients comply with both the <i>Disability Discrimination Act 1992</i> (DDA) and AS2890.</li> </ol>
<b>Landscaping</b>	<ol style="list-style-type: none"> <li>1. Landscaping is provided adjacent to all four road frontages servicing the subject land. This landscaping is provided in the following forms:</li> </ol>

	<ul style="list-style-type: none"> <li>• Landscaped perimeter to car parking area;</li> <li>• Internal landscaping to car parking area;</li> <li>• Landscaping to curtilage of Sale Centre; and</li> <li>• Creation of a privately owned landscaped recreation area.</li> </ul> <p>1. Whilst a detailed landscaping plan has not been provided, the Site Plan adequately demonstrates that sufficient area has been designated for the provision of landscaping. In addition to the preparation of a detailed landscaping plan, minor improvements could be made to the car parking perimeter adjacent to the Southern Boundary road frontage. Increased tree and shrub plantings to this perimeter would be encouraged.</p>
<b>Signage</b>	<p>1. The proposed development includes signage at the upper level of the northern and western elevations which reflect the intended use of the site as a Sales Centre. Directional signage will be provided around the site.</p> <p>2. The proposed signage does not amount to proliferation and will not conceal any architectural elements and is in keeping with the scale of the building.</p> <p>3. Signage contained within the proposal aligns with Table Ga/4 of the Town of Gawler Development Plan and does not offend CW Objectives 69, 70 nor PDC's 199, 200, 201, 202 and 204.</p>
<b>General Infrastructure Provision</b>	<p>1. Zone PDC 42 outlines that development should only occur if the required physical and community infrastructure has been provided or secured via a legally binding agreement.</p> <p>2. The physical and community infrastructure outlined within this provision includes:</p> <p style="padding-left: 40px;">a) <i>Gawler East Link Road as illustrated on Structure Plan Map Ga/1 (Overlay 1) Enlargement G, and associated traffic interventions (road and intersection upgrades);</i></p>

	<p><i>b) public lighting, pedestrian and bicycle paths and associated landscaping treatments associated with the Gawler East Link Road;</i></p> <p><i>c) community facilities (including recreation facilities); and</i></p> <p><i>d) stormwater management systems.</i></p> <p>3. To date this infrastructure has not been completed/provided nor secured. As such, the proposal does not satisfy Zone PDC 42 (being all elements a to d).</p>
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490/441/2019 – 490/E006/19

**CONSTRUCTION OF A SHOPPING CENTRE COMPRISING SUPERMARKET AND SIX (6) SPECIALTY SHOPS, TWO (2) STANDALONE SHOPS, ASSOCIATED CAR PARKING AND SIGNAGE**



Element	Discussion
Land Use	<p>1. The proposed development comprises of the following components:</p> <ul style="list-style-type: none"> <li>• A Shopping Centre, including: <ul style="list-style-type: none"> <li>○ One (1) Supermarket (2600m<sup>2</sup>)</li> <li>○ Six (6) Specialty Shops (totalling 780m<sup>2</sup>)</li> </ul> </li> <li>• Two (2) Standalone Shops (680m<sup>2</sup>)</li> <li>• 217 car parks and loading dock</li> <li>• Associated signage including two (2) Pylon Signs</li> </ul> <p>2. The Mixed Use Centre Policy area contemplates a wide range of land uses that serve the local community. The proposed use is in keeping with this intent and therefore satisfies Objective 1, 3 and PDC 1 and 6 of the policy area. Furthermore, the scale of the proposed the proposed retail facility is in keeping with the 10,000 square metre retail capacity provided for within the Desired Character Statement of the Policy Area.</p>



<b>Earthworks and Retaining</b>	<ol style="list-style-type: none"> <li>1. No site civil plans have been provided as part of the application. The existing and finished levels of the land are dependent on the final design and approval of the associated land division applications and completion of the Gawler East Link Road.</li> <li>2. Depending on the level of earthworks that result in both the associated land division and Gawler East Link Road works, there may be a need for additional earthworks and retaining walls.</li> <li>3. At this stage of the proposal, there is insufficient information to properly assess the development against the following PDC's:   <b>PDC 7 (Policy Area)</b>   Filling of land and retaining walls exceeding 1.0 metre in height are appropriate where they are associated with the remediation and development of the former quarry site.   <b>PDC 29 (Zone)</b>   Retaining walls greater than 1.0 metre in height should: <ol style="list-style-type: none"> <li>a) not be directly visible from a public road or the public realm</li> <li>b) where located between a building and a road frontage, be stepped in increments of not more than 1.0 metre (c) not cause significant adverse impacts on the amenity of adjacent properties, particularly in relation to overshadowing, cutting off light or views.</li> </ol> <b>PDC 31 (Zone)</b>   Development should be designed and sited to relate to the slope of the land, so that: <ol style="list-style-type: none"> <li>a) the bulk and scale of the buildings do not dominate the landscape;</li> <li>b) the amount of cut and fill is minimised.</li> </ol> </li> </ol>
<b>Design</b>	<ol style="list-style-type: none"> <li>1. The subject development comprises of three detached buildings, comprising of the shopping centre and two (2) standalone shops with car parking situated centrally and to the side of the site.</li> </ol>

	<p>2. The shopping centre generally presents of a high architectural standard and incorporates a variety of building materials that promote visual interest. Although improvements could be sought to the East Street elevation of the shopping centre to assist in softening the buildings bulk. Whilst the proposed feature steel frames may be appropriate in achieving this intent, additional clarification on the detail of the structures should be provided (i.e. location shown on site plan).</p> <p>3. Similarly, to further align with the Desired Character statements intent to provide for a main street environment, provision of a pedestrian link from the eastern roadway to the shopping centre could be provided. This link could be achieved in the area situated between the Main Shopping Centre and specialty shops indicated as 'Retail E' and 'Retail F'.</p> <p>4. Whilst the location of the rear loading dock is generally supported, there is potential for improved frontage to the adjoining north eastern road. A frontage treatment inclusive of feature screening and dense landscape plantings or similar would be encouraged by Council. Greater pedestrian linkage from this portion of the site would also be encouraged.</p> <p>5. The two proposed standalone shops consist of a form and of materials that reflect and complement the proposed shopping centre building. The buildings provide for articulation and visual interest to the adjoining public realm and achieve strong pedestrian connectivity with the provision of the centrally located plaza. This pedestrian connectivity is improved via the provision of a designated link from the plaza to the shopping centre.</p> <p>6. The proposed car parking area is generally well screened by the proposed built form and satisfies the Policy Area's intent for car parking to be located to the rear.</p> <p>7. Notwithstanding the above, with minor improvements to the design, the proposal is considered to satisfy Zone Objective 7, PDC 22(b) and Policy Area Objectives 2, 3 and PDC's 3, 4, 5 and</p>
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<b>Stormwater</b>	<p>1. The applicant has noted that a site specific Stormwater Management Plan will be prepared for the subject development. The plan will build upon the overall Stormwater Management Plan for the entire Springwood development which has been submitted with the concurrent land division applications.</p> <p>2. Whilst a Reserved Matter or at the very least a condition would generally be appropriate for the provision of the final design, a concept plan demonstrating the high level method of stormwater management should be provided for the purposes of the planning assessment prior to the issuing of any Development Plan Consent.</p> <p>3. The stormwater system should incorporate adequate detention, water quality control (i.e. Gross Pollutant Trap to separate oils and other pollutants) for the carparking area and encourage the use of Water Sensitive Urban Design (WSUD) principles.</p> <p>4. Applicable WSUD principles that could be adopted within the design are identified with CW PDC 148, 150, 151, 152, 154, 155, 157 and 158.</p>
<b>Traffic</b>	<p>1. The traffic volume associated with the site is considered to be in keeping with the traffic volumes proposed as part of the associated land division application 490/D025/19 and 490/D026/19 for the adjoining road network. Accordingly, the traffic network proposed as part of the associated land division applications is not considered to be exceeded by the proposal.</p> <p>2. Vehicle access to the site will be provided via the following locations:</p> <ul style="list-style-type: none"> <li>• a two-way access point on the Gawler East Link Road (which will accommodate left-in, left-out, right-in and right-out movements);</li> <li>• a two-way access point on the north-south collector road (which will accommodate left-in, left-out, right-in and right-out movements); and</li> </ul>

	<ul style="list-style-type: none"> <li>• a two-way access point on the local road to the north which will primarily provide access for commercial vehicle movements, albeit customers will also be able to utilise the access point.</li> </ul> <p>3. The car parking area is configured in a manner to accommodate on-site vehicle manoeuvring to enable vehicles to egress the site in a forward direction. As such, the movement of vehicles in association with the site will not conflict with the flow of traffic on adjoining roads.</p> <p>4. The traffic impact has been considered by Cirqa, the applicant's independent traffic consultant and Council's traffic engineers. Both of which, consider this element of the proposal to be acceptable.</p> <p>5. Commercial vehicle access will be accommodated via the loading area located at the rear (north) of the shopping centre. Cirqa Traffic Consultants have demonstrated this area can appropriately accommodate the manoeuvring of a semi-trailer. Minor deliveries associated with the specialty shops will be achieved internally through the car parking area provided. Accordingly, deliveries to the proposed supermarket is not considered to result in conflict with the adjoining roadway.</p> <p>6. The current plans do not provide for the construction site levels (including existing or design levels) of the adjoining roads (including the Gawler East Link Road) compared to proposed finished levels of the site and proposed development. Without this level of detail, there is concern relating to access and gradients between the subject site and the external roads (some being partially constructed or not yet approved).</p> <p>7. The proposal also lacks detail with regards to any required works relating to access and manoeuvrability into the site from any external roads (including the Gawler East Link Road) and the need for alterations to traffic devices such as any median strips, or access arrangements. Whilst it is acknowledged that not all external road infrastructure has been constructed, the associated designs may already exist and may require refinement to</p>
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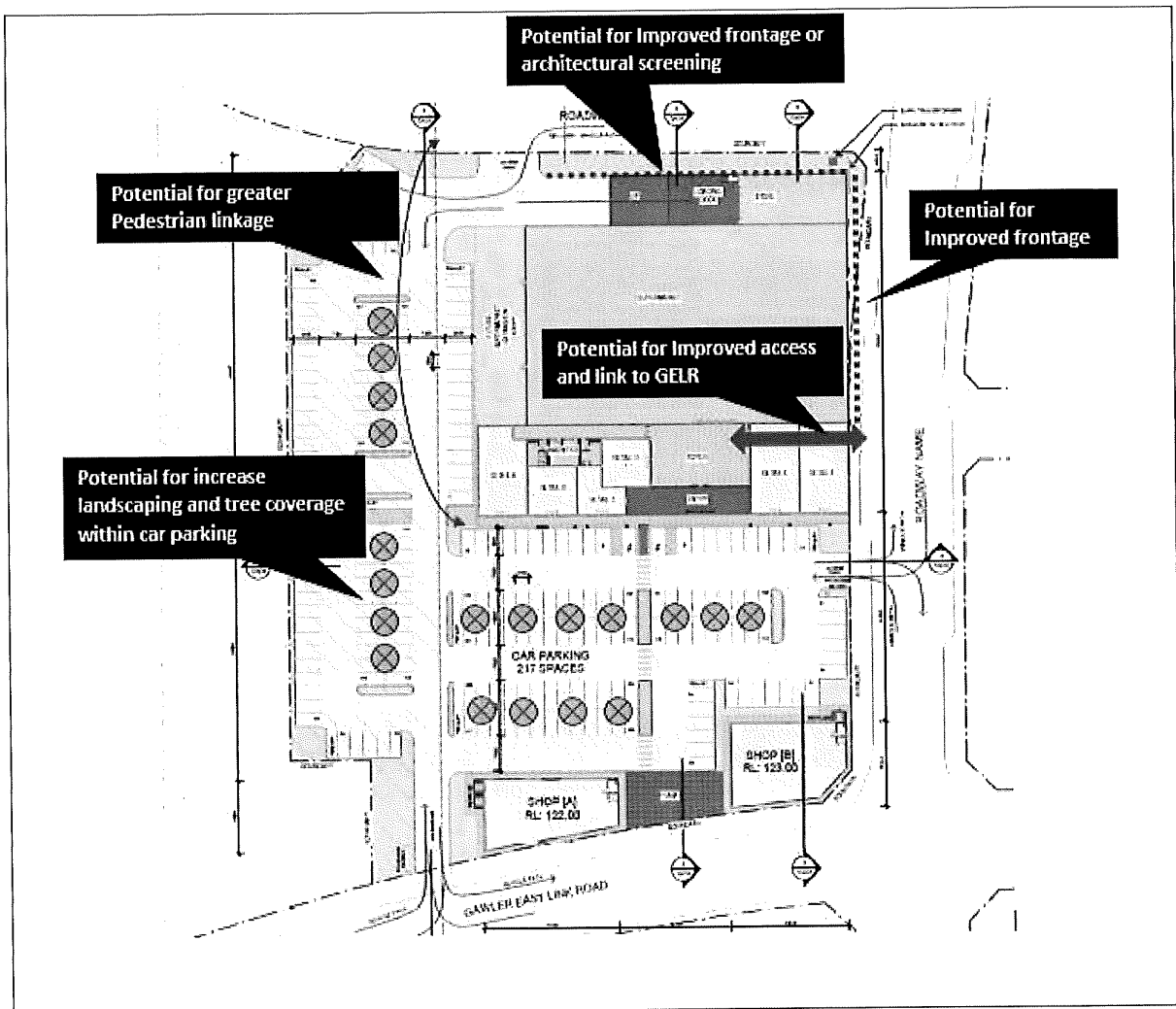
	<p>accommodate the proposal.</p> <p>8. Any works or refinements to roads that are required to accommodate the proposed development should be completed as part of the associated land division applications and secured via an Infrastructure Agreement (being as part of external infrastructure works) for the subject application to ensure such works (if required) are provided.</p>
<b>Car Parking</b>	<p>1. The proposal seeks to provide for 217 car parking spaces to service the proposed retail uses. The car parking is proposed in a manner that is consistent with AS2890.1:2004 and AS2891.6:2009 (being the Australian Standard for off street parking and off street parking for people with disabilities). It is noted that the width of the car parking spaces proposed exceed the minimum requirements of the associated user class under AS2890. This approach is encouraged by Council.</p> <p>2. The Development Plan within the Mixed Use Centre Policy Area identifies carparking to a rate of 5.5 spaces per 100m<sup>2</sup> is required for the proposed shop/retail uses. Subsequently, a total of 224 car parking spaces are required resulting in a shortfall of 7 car parking spaces.</p> <p>3. Cirqa Traffic Consultants engaged by the applicant undertook an empirical assessment of car parking and noted that recent surveys of shopping centres indicate peak parking demands are more commonly in the range of 3.0 to 5.0 spaces per 100 m<sup>2</sup> of floor area. These rates equate to a peak demand in the range of 122 to 203 spaces. This demand is accommodated by the proposal. Notwithstanding, the theoretical 7 park shortfall can be accommodated within the adjoining road network servicing the proposed development.</p> <p>4. It is further noted that the car parking shortfall could be reduced via the removal of the designated pedestrian linkage within the site connecting the proposed plaza with the shopping centre. Such an approach however, would be to the detriment of the proposal and the minor shortfall is considered acceptable and inconsequential</p>

	<p>to the provision of car parking on site.</p> <p>5. Whilst typically considered during the Building Rules Assessment of such an application, Council would encourage the consideration of the provision of disabled car parking spaces within close proximity to both the Shopping Centre and the two detached specialty shops proposed on site. The current car parking configuration is limited to the provision of disabled car parking adjacent to the shopping centre.</p> <p>6. The indicative layout of the car parking area is sufficient to demonstrate that the appropriate amount of car parking has been provided to service the proposed use. Final detail however, is required for the design levels of the car parking area, to confirm gradients comply with both the DDA and AS2890.</p>
<b>Landscaping</b>	<p>1. The proposal includes a conceptual landscaping plan which depicts the provision of landscaping around the perimeter of the site and within the proposed car parking area. These landscaped areas are to comprise of a range of species of trees, shrubs and groundcovers, including:</p> <ul style="list-style-type: none"> <li>• Trees: <ul style="list-style-type: none"> <li>○ Callery Pear; and</li> <li>○ Japanese Elm</li> </ul> </li> <li>• Shrubs: <ul style="list-style-type: none"> <li>○ Agapanthus Small Cultivars;</li> <li>○ Distichlis Distichophylla;</li> <li>○ Lomandra Cultivars; and</li> <li>○ Nandina</li> </ul> </li> <li>• Groundcovers: <ul style="list-style-type: none"> <li>○ Aesperula Conferta; and</li> <li>○ Leptospermum Lanigerum</li> </ul> </li> </ul> <p>2. Whilst the provision of landscaping is generally appropriate,</p>

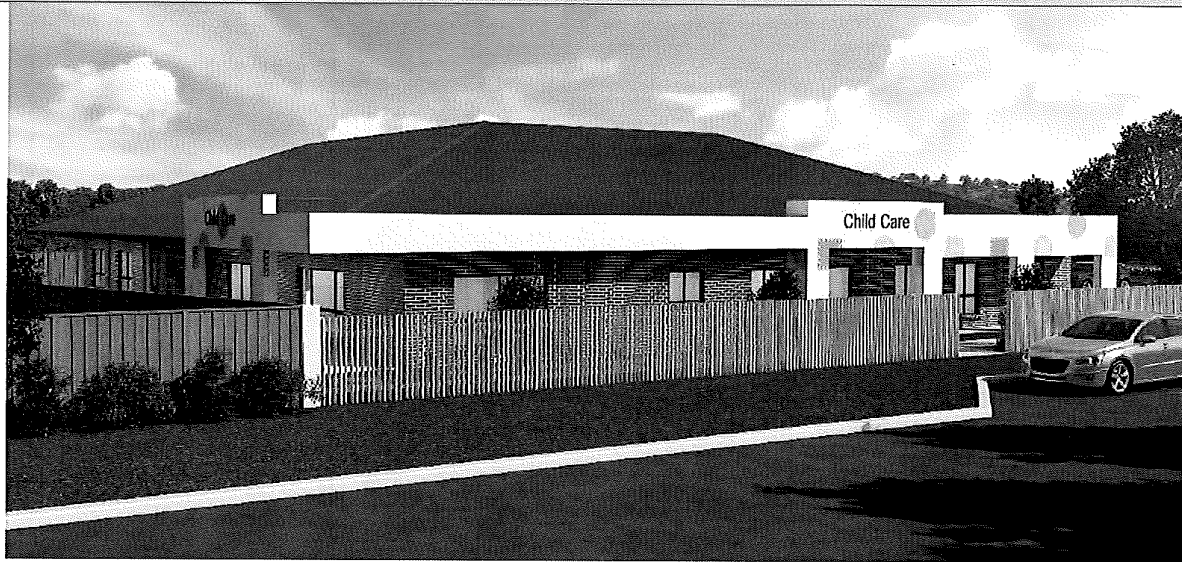
	<p>increased landscaping could be provided to the road frontage adjacent to the rear loading dock and within the proposed car parking area (i.e. shading trees).</p>
<b>Signage</b>	<ol style="list-style-type: none"> <li>1. The proposed architectural plans demonstrate indicative locations for signage at the main entrances to the site. Whilst this location and form of signage (flat wall) is generally appropriate, clarification on dimensions should be provided.</li> <li>2. Similarly, there is currently insufficient detail to consider the proposed pylon signs. However from the limited extent of information provided, it is noted that the pylon signage is detached form and in keeping with the scale of the proposed building. Notwithstanding, Table Ga/4 of the Development Plan seeks to limit such signs to a maximum height of 6.0 metres with a maximum advertisement area of 5.0 square metres. A reduction in the 8 metre pylon sign height would be encouraged to satisfy this provision.</li> <li>3. Consideration should also be given to whether the proposed pylon signs will maintain appropriate driver sight lines as a result of their close association with vehicle access/egress points.</li> <li>4. Signage contained within the proposal aligns with Table Ga/4 of the Town of Gawler Development Plan and does not offend CW Objectives 69, 70 nor PDC's 199, 200, 201, 202 and 204.</li> </ol>
<b>General Infrastructure Provision</b>	<ol style="list-style-type: none"> <li>1. Zone PDC 42 outlines that development should only occur if the required physical and community infrastructure has been provided or secured via a legally binding agreement.</li> <li>2. The physical and community infrastructure outlined within this provision includes: <ol style="list-style-type: none"> <li>a) <i>Gawler East Link Road as illustrated on Structure Plan Map Ga/1 (Overlay 1) Enlargement G, and associated traffic interventions (road and intersection upgrades);</i></li> </ol> </li> </ol>



	<p><i>b) public lighting, pedestrian and bicycle paths and associated landscaping treatments associated with the Gawler East Link Road;</i></p> <p><i>c) community facilities (including recreation facilities); and</i></p> <p><i>d) stormwater management systems.</i></p> <p>1. To date this infrastructure has not been completed/provided nor secured. As such, the proposal does not satisfy Zone PDC 42 (being all elements a to d).</p>
<b>SEAGas and ElectraNet informal referral</b>	<p>1. Whilst the proposed application does not trigger any referrals under Schedule 8 of the <i>Development Regulation 2008</i>, it is highly recommended that following informal referrals are sought to ensure public safety and compliance by the following utility providers:</p> <p>1.2 SEAGas to ensure compliance with AS2885 and Zone PDC 36 (b) given that the proposed shopping centre will contain high concentrations of people within close proximity to the SEA Gas high pressure pipeline.</p> <p>1.3 ElectraNet given that the proposed Shopping centre is encroaching within the transmission line corridor and in particular is currently at variance with Zone PDC 19(c) which seeks a 100metre wide corridor within the existing ElectraNet easement and transmission corridor.</p>
Visual Representation of Recommendations as per above comments.	



**CONSTRUCTION OF A SINGLE-STOREY CHILD-CARE CENTRE (PRESCHOOL)**



Element	Discussion
<b>Land Use</b>	<p>1. The proposed land use consist of a preschool being a child-care centre, which comprises</p> <ul style="list-style-type: none"> <li>• A single storey building (913.89m<sup>2</sup>)</li> <li>• Two external Play yards (513.2m<sup>2</sup> and 326m<sup>2</sup>)</li> <li>• Capacity of maximum of 110 children</li> <li>• Operation Hours between 6:00am and 6:30pm</li> <li>• Car Park comprising 26 spaces</li> </ul> <p>2. The Mixed Use Centre Policy area contemplates a wide range of land uses that serve the local community. The proposed use is in keeping with this intent and is listed as an envisaged use therefore satisfying Objective 1 and 3 and PDC 1 and 6 of the Policy Area.</p>
<b>Earthworks and Retaining Walls</b>	<p>1. No site civil plans have been provided as part of the application. The existing and finished levels of the land are dependent on the final design and approval of the associated land division applications and completion of the Gawler East Link Road.</p>

	<p>2. The applicants Planning Report states that retaining walls and earthworks will be required to accommodate the proposed development. This notion is supported by the "Site &amp; Floor Plan" which depicts a retaining wall along the northern (rear) and western (Gawler East Link Road frontage) boundary.</p> <p>3. At this stage of the proposal, there is insufficient information to properly assess the development against the following PDC's:</p> <p><b>PDC 7 (Policy Area)</b></p> <p>Filling of land and retaining walls exceeding 1.0 metre in height are appropriate where they are associated with the remediation and development of the former quarry site.</p> <p><b>PDC 29 (Zone)</b></p> <p>Retaining walls greater than 1.0 metre in height should:</p> <ul style="list-style-type: none"> <li>a) not be directly visible from a public road or the public realm</li> <li>b) where located between a building and a road frontage, be stepped in increments of not more than 1.0 metre</li> <li>(c) not cause significant adverse impacts on the amenity of adjacent properties, particularly in relation to overshadowing, cutting off light or views.</li> </ul> <p><b>PDC 31 (Zone)</b></p> <p>Development should be designed and sited to relate to the slope of the land, so that:</p> <ul style="list-style-type: none"> <li>a) the bulk and scale of the buildings do not dominate the landscape;</li> <li>b) the amount of cut and fill is minimised.</li> </ul> <p>4. The proposal identifies the need for retaining walls with the height of these walls currently being unknown until such time as the Gawler East Link Road has been constructed and the associated land division detailed design completed and approved. Indicative levels provided as part of the current application demonstrate</p>
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	<p>potential retaining walls up to 1.76 metres (for fill) maybe required above natural ground level in order to accommodate the proposed development. The height of retaining may represent development in its own right and as such, be subject to development approval. Accordingly, the provision of such retaining walls should be considered within the subject assessment.</p> <p>5. Notwithstanding, the provision of a fill retaining wall for the duration of the Western Road frontage would be discouraged by Council, particularly if such retaining exceeded a height of 1 metre. It would be recommended that a more balanced approach to earthworks be undertaken (combination of excavation and fill) to encourage greater interaction of the site with the Western frontage and to minimise the resulting visual bulk on the future public realm. This approach is supported by Zone PDC 29 and 31. The material utilised in the wall's construction should also be considered to provide for visual interest and to align with the desired character of the zone to achieve high architectural standard outcomes.</p>
<b>Design</b>	<p>1. The desired character statement of the Policy Area seeks to create a 'main street' environment with buildings addressing the street and car parking located primarily to the rear. The appearance of siting of buildings is encouraged to be within close proximity to street frontages and be of an architecturally high standard to create diversity of interest through the appearance of an aggregation of smaller buildings.</p> <p>2. Whilst it is acknowledged that the provision of carparking to the rear of the subject site is difficult to achieve given its dual road frontage, improvements to the built form would be encouraged to further align with the intent of the Desired Character.</p> <p>3. The building has been designed and sited to provide for the primary frontage to the western road. This approach is supported and the close siting of the building aligns with the intent of the Desired Character Statement. Notwithstanding, this approach is somewhat diminished by the provision of the fill retaining wall</p>

	<p>along this frontage which isolates the adjoining public realm from the subject site. The provision of feature steps and a DDA compliant ramp to the western frontage of the site would be encouraged to further assist in reducing the impact of retaining and accommodate a pedestrian focused link to the site. This approach could be complimented with the provision of landscaping.</p> <p>4. The northern elevation of the building incorporates the feature clad wall as a continuation of the western frontage. This approach is encouraged and provides for articulation and visual interest for the building when travelling north to south on the adjoining road.</p> <p>5. The southern elevation forms the extent of the building visible from the carparking area and abutting southern public road. A portion of the feature cladding of the western elevation is visible as a side return and is complimented by the provision of an additional feature wall central to the southern elevation. Of concern however, is the untreated provision of a brick wall between the feature claddings. As this wall is highly visible from the adjoining public road inclusive of the intersection, additional consideration and treatment should be considered. The provided feature bulkhead above is not considered sufficient to offset the extent of untreated wall provided below. This element of the proposal in its current form is not considered to align with the intent to achieve high standard architectural outcomes and further consideration should be given.</p> <p>6. Whilst the design of the building with minor design improvements would be considered to satisfy the desired character of the Policy Area and Zone, a departure from a building generally of residential appearance would be encouraged.</p> <p>7. Zone Objective 7, PDC 22(b) and Policy Area Objectives 2, 3 and PDC's 3, 4, 5 and 6 are considered to be satisfied by the proposal.</p>
<b>Stormwater</b>	<p>1. Whilst the proposed stormwater design is dependent on the approval and construction of the adjoining public stormwater system associated with the land division proposals, the subject</p>

	<p>development seeks to manage stormwater on-site via the following:</p> <ul style="list-style-type: none"> <li>• Collection of roof and surface water;</li> <li>• Provision of an underground detention system;</li> <li>• Provision of a Gross Pollutant Trap;</li> <li>• Discharge to Future Side Entry Pit located in adjoining public road.</li> </ul> <p>2. Whilst this general approach for stormwater management is appropriate, further clarification and consideration on the details of the design are required. This has been reviewed by Council's engineering staff whom noted the following:</p> <ul style="list-style-type: none"> <li>• The pre-development flow rate of 37.8L/s within the provided calculations appears to be too high as it has been calculated for a developed site. The flow rate should be in the order of approximately 8L/s as the site is currently undeveloped.</li> <li>• Runoff coefficient calculations appear to be too low and should be increased to the following: <ul style="list-style-type: none"> <li>○ From 0.92 to 1.0 for roofed areas; and</li> <li>○ From 0.75 to 0.9 for hardstand areas.</li> </ul> </li> <li>• The extent of detention provided appears to be undersized based on miscalculation. Council's engineers have calculated that approximately 70m<sup>3</sup> with a 50mm orifice would be required.</li> <li>• Finished levels of the site would be required to ensure appropriate gradients for stormwater management can be achieved.</li> </ul> <p>3. Whilst the proposal incorporates a Gross Pollutant Trap, further consideration of WSUD principles would be encouraged.</p>
<b>Traffic</b>	<p>1. The traffic volume associated with the site is considered to be in keeping with the traffic volumes proposed as part of the</p>



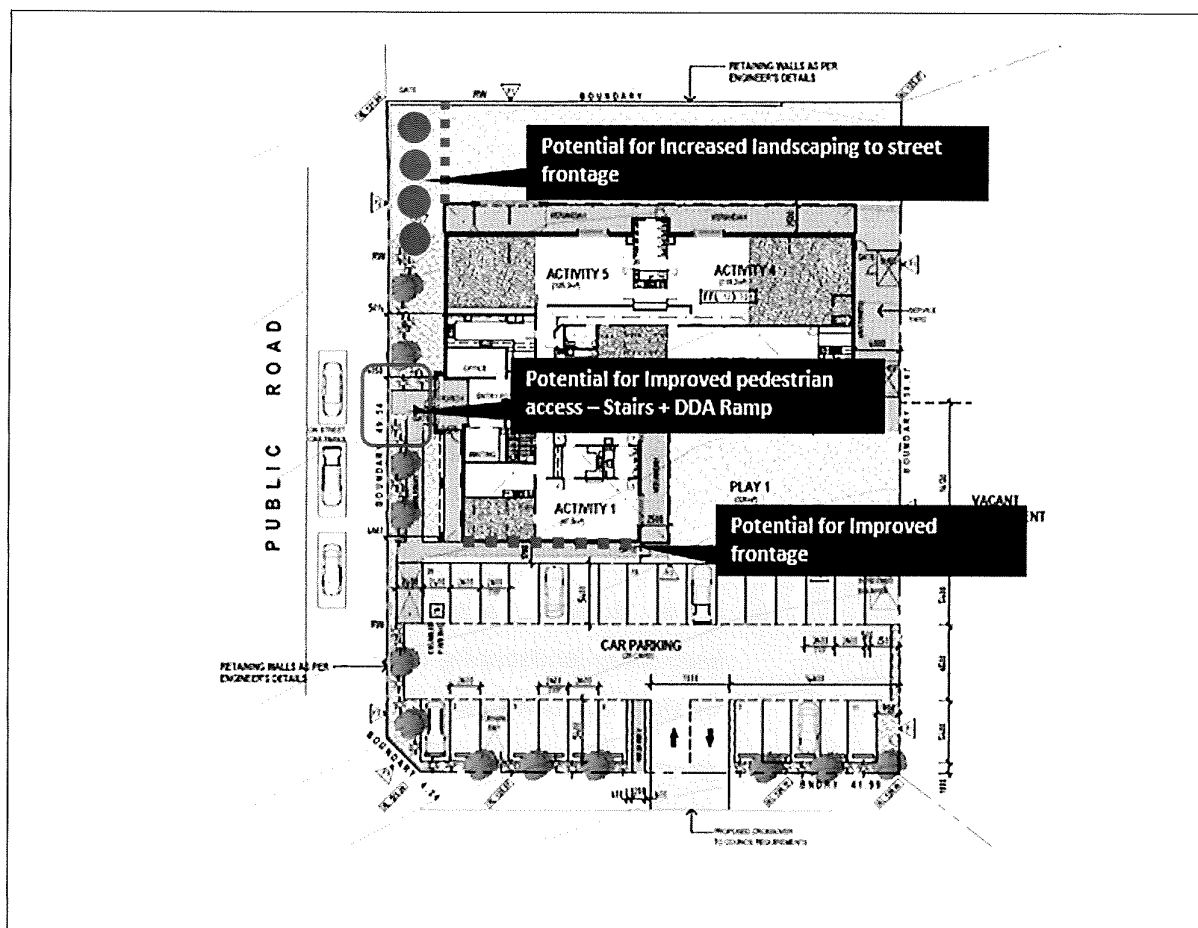
	<p>associated land division application 490/D025/19 and 490/D026/19 for the adjoining road network. Accordingly, the traffic network proposed as part of the associated land division applications is not considered to be exceeded by the proposal.</p> <p>2. Access and egress from the subject land to the adjoining public road network will be obtained via one two-way simultaneous crossover to the abutting southern road being a Local Road proposed as part of the associated land division 490/D025/19. The car parking area is configured in a manner to accommodate on-site vehicle manoeuvring to enable vehicles to egress the site in a forward direction. As such, the movement of vehicles in association with the site will not conflict with the flow of traffic on adjoining roads.</p> <p>3. The traffic impact has been considered by Cirqa, the applicant's independent traffic consultant and Council's traffic engineers. Both of which, consider this element of the proposal to be acceptable.</p> <p>4. Given the location of waste refuge within the car parking area, the collection of waste should occur from the site outside of operation hours to ensure no conflict with waste vehicles and vehicles parked within the car parking spaces.</p> <p>5. The current plans do not provide for the construction site levels (including existing or design levels) of the adjoining roads (including the Gawler East Link Road) compared to proposed finished levels of the site and proposed development. Without this level of detail, there is concern relating to access and gradients between the subject site and the external roads (some being partially constructed or not yet approved).</p> <p>6. The proposal also lacks detail with regards to any required works relating to access and manoeuvrability into the site from any external roads (including the Gawler East Link Road) and the need for alterations to traffic devices such as any median strips, or access arrangements. Whilst it is acknowledged that not all external road infrastructure has been constructed, the associated</p>
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	<p>designs may already exist and may require refinement to accommodate the proposal.</p> <p>7. Any works or refinements to roads that are required to accommodate the proposed development should be completed as part of the associated land division applications and secured via an Infrastructure Agreement (being as part of external infrastructure works) for the subject application to ensure such works (if required) are provided.</p>
<b>Car Parking</b>	<p>1. The proposal seeks to provide for 26 car parking spaces to service the proposed child care centre in a manner that is consistent with AS2890.1:2004 and AS2891.6:2009 (being the Australian Standard for off street parking and off street parking for people with disabilities).</p> <p>2. A car parking assessment has been undertaken by Cirqa Traffic Consultants with regard to the car parking requirements outlined within Table Ga/1 of the Development Plan. Based on the carparking requirements outlined within the Development Plan, a total of 28 car parking spaces would generally be required to service the proposed development. Therefore a shortfall of 2 carparks exists when considered against the requirements of Table Ga/1.</p> <p>3. Cirqa have further considered this shortfall against the proposed provision of carparking within the wider locality (being the future public roads) and consider that this minor shortfall can be absorbed within this future on-street provision. Given the minor shortfall, this notion is supported by Council administration.</p> <p>4. The indicative layout of the car parking area is sufficient to demonstrate that the appropriate amount of car parking has been provided to service the proposed use. Final detail however, is required for the design levels of the car parking area, to confirm gradients comply with both the DDA and AS2890.</p>
<b>Landscaping</b>	<p>1. A conceptual landscaping plan has been provided with the application, which demonstrated landscaping in the following</p>

	<p>forms:</p> <ul style="list-style-type: none"> <li>• To the perimeter of the site;</li> <li>• Adjacent to the building; and</li> <li>• within outdoor play areas;</li> </ul> <p>2. The landscaping is proposed to comprise of a variety of irrigated trees species, shrubs and groundcovers. These specie's include:</p> <ul style="list-style-type: none"> <li>• Trees: <ul style="list-style-type: none"> <li>○ Ornamental pear; and</li> <li>○ Pencil pine.</li> </ul> </li> <li>• Shrubs/Ground Cover: <ul style="list-style-type: none"> <li>○ Standard Lilly Pilly;</li> <li>○ Westringia Fruiticosa</li> <li>○ Ornimental Grasses</li> </ul> </li> </ul> <p>3. Whilst the provision of landscaping is generally appropriate, increased landscaping could be provided to the north-western corner of the site (between play area 2 and the road frontage) and south of play area 1.</p>
<b>Signage</b>	<p>1. The proposed development includes signage within the feature cladding indicated on the South, North and elevations.</p> <p>2. The proposed signage does not amount to proliferation and will not conceal any architectural elements and is in keeping with the scale of the building.</p> <p>3. Signage contained within the proposal aligns with Table Ga/4 of the Town of Gawler Development Plan and does not offend CW Objectives 69, 70 nor PDC's 199, 200, 201, 202 and 204.</p>
<b>Noise</b>	<p>1. The applicant has engaged Sonus Pty Ltd to undertake an Environmental Noise Assessment for the proposed childcare centre. The assessment seeks to investigate the proposals compliance against CW PDC's 107, 108, 111, 112 and 113.</p> <p>2. The report concludes that the development will minimise adverse</p>

	<p>impacts and conflict between land uses and the proposal will therefore not detrimentally affect the amenity of the locality or cause unreasonable interference through noise. The proposal therefore satisfies the relevant provisions of the Gawler Council Development Plan.</p> <p>3. The above recommendation is based on the inclusion of a 2.4 metre colorbond barrier to the boundary of the site to mitigate noise impacts pertaining to future single storey dwellings (if proposed) abutting the subject site. The report further noted that should two storey dwellings be proposed in lieu of single storey, that such dwellings would be required to incorporate reasonable and practicable acoustic treatments to account for the child care centre.</p> <p>4. Notwithstanding the above, it is noted that there are a few minor inconsistencies with the detail outlined within the report prepared by the applicants planning consultant and that contained within the acoustic report. This detail pertains to the hours of operation of the childcare centre. The acoustic report considers the proposal based on a 7am commencement where the planning report details a proposed opening time of 6am. This is not considered to be of any consequence as the acoustic report considered noise impact in a limited capacity before 7am.</p> <p>5. Whilst the proposed plan of division demonstrates that the subject land may in fact abut a future commercial property thus further minimising the impact of the proposed use. Further consideration should be given to the mechanism for securing such works relative to future dwellings on adjacent land to ensure the proposal satisfies the relevant Development Plan provisions.</p> <p>6. It is noted that if balanced earthworks were to be undertaken such as suggested above, the proposal would further reduce potential noise impacts through the provision of a cut retaining wall along the boundaries external to the subject site.</p> <p>7. Whilst the proposed noise attenuation measure is considered appropriate, Council would encourage the consideration of an</p>
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	<p>alternative barrier to that of colourbond construction. An acoustic barrier of increased architectural standard would be encouraged given its location within the commercial precinct.</p>
<b>General Infrastructure Provision</b>	<ol style="list-style-type: none"> <li>1. Zone PDC 42 outlines that development should only occur if the required physical and community infrastructure has been provided or secured via a legally binding agreement.</li> <li>2. The physical and community infrastructure outlined within this provision includes: <ol style="list-style-type: none"> <li>a) <i>Gawler East Link Road as illustrated on Structure Plan Map Ga/1 (Overlay 1) Enlargement G, and associated traffic interventions (road and intersection upgrades);</i></li> <li>b) <i>public lighting, pedestrian and bicycle paths and associated landscaping treatments associated with the Gawler East Link Road;</i></li> <li>c) <i>community facilities (including recreation facilities); and</i></li> <li>d) <i>stormwater management systems.</i></li> </ol> </li> <li>3. To date this infrastructure has not been completed/provided nor secured. As such, the proposal does not satisfy Zone PDC 42 (being all elements a to d).</li> </ol>
<b>SEAGas informal referral</b>	<ol style="list-style-type: none"> <li>1. Whilst the proposed application does not trigger any referrals under Schedule 8 of the <i>Development Regulation 2008</i>, it is highly recommended that informal referral is sought by SEA Gas to ensure compliance with AS2885 and Zone PDC 36 (b) given that the proposed childcare centre will contain high concentrations of people including children within close proximity to the SEA Gas high pressure pipeline.</li> </ol>
Visual Representation of Recommendations	



## 12. CONCLUSION

- 12.1. In summary, the three development applications proposed have been assessed by Council and a view on the current proposals has been formed.
- 12.2. Council has sought legal advice with regards to procedural matters relating to the determination of the three subject applications. Council is of the opinion that a decision on the three applications cannot be made until such time as land divisions 490/D025/19 and 490/D026/19 have been approved and relevant allotments lodged with the Lands Titles Office.
- 12.3. It is further the opinion of Council, that consideration of site levels and earthworks including the level of retaining is fundamental to the assessment and determination of each of the applications.
- 12.4. Council has tabled its concerns and recommendations in relation to each of the proposed developments which form part of Council's official response to SCAP.

- 12.5. While Council is supportive of the proposed developments in-principle, at this stage and until such time as the appropriate allotments, public road access and associated site levels have been established, together with those matters raised within its tabled comments being resolved, Council is not able to provide its full support.
- 12.6. Council has further included a number of conditions should the SCAP deem the applications appropriate.
- 12.7. Council is committed to working collaboratively with both SCAP and the developer in order to resolve Council's initial concerns and ultimately achieve the best possible planning outcome for the developer and both the new and existing community.

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### **13. RECOMMENDATION**

It is recommended that the Town of Gawler Council Assessment Panel resolves to:

- 1. Note the report pertaining to Development Applications 490/440/2019, 490/441/2019, 490/446/2019.**
- 2. Endorse the subject report and the submissions outlined in Attachments 4, 5 and 6 of this report for the purpose of forwarding to SCAP in relation to Development Applications 490/440/2019, 490/441/2019, 490/446/2019.**
- 3. Authorise the Presiding Member and Assessment Manager to amend the endorsed submission based on the outcomes of these deliberations, and where otherwise considered necessary, prior to forward to SCAP by 7 November 2019.**
- 4. Authorise staff to continue to work collaboratively with SCAP as the Relevant Authority and the developer to reach agreement on all outstanding matters in order to achieve a quality planning outcome for the community.**



## Attachment 5 – Council Assessment Panel Minutes

- 5.5 Development Application:** 490/440/2019 (490/E006/19)  
490/441/2019 (490/E006/19)  
490/446/2019 (490/E009/19)
- Applicant:** SPRINGWOOD DEVELOPMENT NOMINEES PTY LTD  
LEYTON PROPERTIES PTY LTD
- Address:** Balmoral Road GAWLER EAST
- Nature of Development:**
- 1) Construction of a Sales Centre (two storey) comprising Showroom, Office and Café with associated car park and signage;
  - 2) Construction of a shopping centre comprising supermarket and six (6) specialty shops, two (2) standalone shops, associated car parking and signage; and
  - 3) Construction of a single-story child-care centre (preschool)

Moved: Nathan Shanks  
Seconded: Ross Bateup  
Motion No CAP: 2019:10:55

**That the Town of Gawler Council Assessment Panel resolves to:**

1. Note the report pertaining to Development Applications 490/440/2019, 490/441/2019, 490/446/2019.
2. Endorse the subject report and the submissions outlined in Attachments 4, 5 and 6 of this report for the purpose of forwarding to SCAP in relation to Development Applications 490/440/2019, 490/441/2019, 490/446/2019.
3. Authorise the Presiding Member and Assessment Manager to amend the endorsed submission based on the outcomes of these deliberations, and where otherwise considered necessary, prior to forward to SCAP by 7 November 2019.
4. Authorise staff to continue to work collaboratively with SCAP as the Relevant Authority and the developer to reach agreement on all outstanding matters in order to achieve a quality planning outcome for the community.

CARRIED

6 March 2020

REF No.: 00739-002

State Commission Assessment Panel  
GPO Box 1815  
ADELAIDE SA 5000

**Attention:** Karen Ferguson, Senior Specialist Planning Officer  
By Email: karen.ferguson@sa.gov.au

Dear Karen,

**RE: DA 490/E006/19 - PROPOSED SUPERMARKET & SPECIALITY SHOPS AT CALTON ROAD, GAWLER EAST – RESPONSE TO CATEGORY 2 REPRESENTATIONS**

We refer to the proposed development involving the construction of a supermarket and speciality shops with associated car parking, landscaping and signage at Calton Road, Gawler East. More specifically, we refer to the representations received during the Category 2 notification process. Ekistics Planning and Design has been engaged by Springwood Nominees Pty Ltd to respond to the representations.

Please note that, following receipt of the representations, the plans and elevations have been amended slightly (see **Appendix 1**). The amended plans now:

- Clearly identify the heights of the various buildings;
- Reduce the height of the pylon sign at the southern entrance to the site down to 6m;
- Identify the location of bicycle parking;
- Identify the steel feature frames on the Site Plan; and
- Remove any reference to a potential expansion area for the supermarket.

The proposed supermarket and speciality shops will form part of the new Springwood Town Centre which is being developed in accordance with:

- A detailed Master Plan prepared by Tract Consulting; and
- The recently approved land division which has created the site of the supermarket and the adjoining public roads.

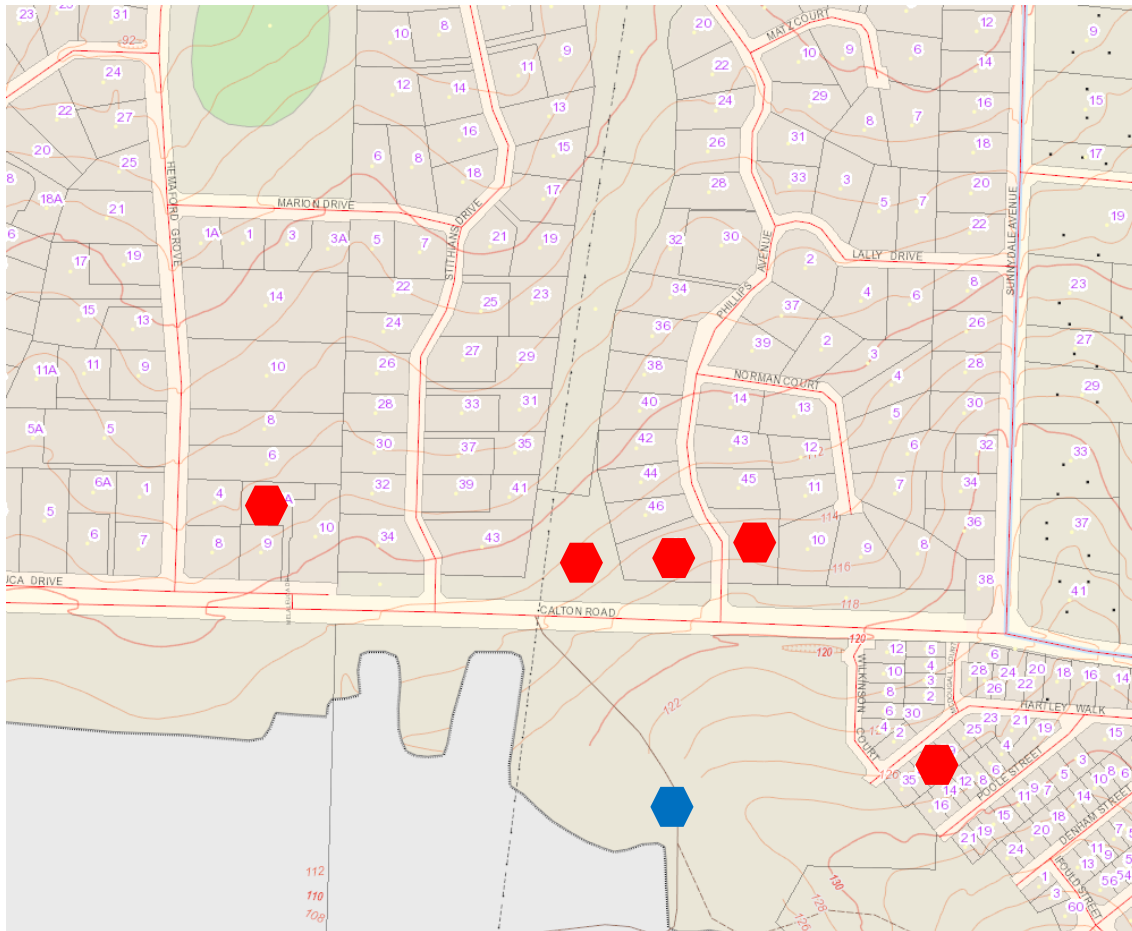
The subject land is within the '**Mixed Use Centre Policy Area 3**' of the '**Residential (Gawler East) Zone**' in which a wide range of land uses are contemplated including a "*... mix of commercial, retail, recreation, community, residential, office, consulting rooms and educational uses*" (Objective 1).

Importantly, a 'supermarket' and 'shop' are specifically listed as envisaged forms of development in the Policy Area. For this reason, the proposed land use is appropriate in this location and is consistent with the wide range of land uses sought by the Development Plan and will complement other proposed uses in the emerging Springwood Town Centre.

## 1. Response to Representations

Following the Category 2 public notification process, five (5) representations were forwarded for the applicant's response. The location of the five (5) representations received is identified on **Figure 2.1** and **Table 2.1** below.

**Figure 2.1** Location of representors (red) with indicative location of supermarket (blue)



**Table 1.1** Representors

Name	Address	Wish to be heard? (Y/N)
Deborah Russell	9a Melaleuca Drive, Gawler East	Y
Chris and Lyn Gill	47 Phillips Avenue, Gawler East	Y
Narene Wardleworth	48 Phillips Avenue, Gawler East	Y
Cesar Chaparro	31 Hartley Walk, Gawler East	N
Town of Gawler	Owner of adjoining public roads and public reserve	Y

The representations raised a number of concerns which we have summarised under the following key planning themes:

- Impact on existing shopping centres in Gawler (increased competition);
- Increased traffic;

- Increased noise;
- Visual impact (including lighting and signage);
- Stormwater;
- Infrastructure provision; and
- Procedural matters.

The Town of Gawler has also provided a comprehensive assessment of the proposed development which is generally supportive, but suggests a number of relatively minor amendments.

Each theme and the specific comments of the Town of Gawler are addressed respectively below.

## 1.2 Impact on existing shopping centres in Gawler

One representor has suggested that Gawler already has a significant number of shopping centres and that further competition is not needed. In response, it is noted that the proposed development is intended to serve the retail needs of the existing and future population of Springwood. Once completed, Springwood will have approximately 1,750 dwellings which will house in the order of 6,000 people. Accordingly, the Residential (Gawler East) Zone and Mixed Use Centre Policy Area 3 establishes a policy framework which identifies an appropriate location for a 'town centre' in which retail and other forms of non-residential development can be developed. This policy framework is articulated in the Desired Character of the Mixed Use Centre Policy Area 3 which states (in part):

*A retail facility, not exceeding 10 000 square metres in floor area, will comprise a supermarket and a range of specialty shops to serve the weekly shopping needs of the community. Restaurants, cafes, hotels (incorporating dining) and take away outlets are envisaged and will develop the centre as a destination point and provide uses that extend the hours of the centres operation to promote surveillance and safety of the adjacent recreation and education facilities.*

For the above reasons, it is clear that the policy framework expressly contemplates a supermarket and speciality shops in this location.

## 1.3 Increased Traffic

Several representors raised concerns in relation to increased traffic levels. In response, we note that CIRQA Traffic Engineers has assessed the traffic impacts of the proposed supermarket and speciality shops. CIRQA has quite rightly noted that the shopping centre will be serviced by the Gawler East Link Road which is under construction. CIRQA further note that:

*The Gawler East Link Road is a proposed major link road which will provide access to/from Main North Road without the need to travel through the centre of the Gawler township. In the vicinity of the subject site, the road will generally comprise a single traffic lane and bicycle lane in each direction separated by a raised*

*central median (with opportunities for sheltered right turn lanes where required). Footpaths will be provided on both sides of the Gawler East Link Road. Previous traffic forecasts (by others) indicate volumes on the Gawler East Link Road will ultimately be in the order of 12,500 vehicles per day (vpd).*

CIRQA also note that:

*Traffic impacts associated with the proposal have been assessed as part of the broader land division application and planning for the Gawler East Link Road. The proposed access arrangements and associated traffic impacts will be in line with the previous assessments. Additional SIDRA modelling of the primary access points indicates that the forecast movements will be adequately accommodated.*

Based on CIRQA's assessment, as well as the previous assessments that have been undertaken as part of the land division process, the proposed development appropriately addresses the key provisions in the Development Plan in relation to transportation and access.

#### 1.4 Increased noise

Two of the representors raised concerns about increased levels of noise. In response, we note that the subject site is located within the Mixed Use Centre Policy Area 3 which clearly anticipates a neighbourhood level centre (including a supermarket) in this area. Currently, the site and nearby land is vacant but will be developed over time to create a master-planned community comprising residential development supported by a range of non-residential land uses. This includes a petrol filling station and car wash further to the north on Calton Road. On this basis, potential interface issues, including the transfer of noise, have been carefully considered during the preparation of the Springwood masterplan and as part of the urban design of the whole project.

Importantly, the supermarket and potential noise generating sources such as the loading dock will be located a considerable distance away from any existing or proposed residential development. Similarly, the car park and associated access points will not front any existing or proposed residential areas. More specifically, the ElectraNet easement to the rear of the supermarket provides a buffer of approximately 100m to potential future residential development to the west. Existing residential development to the north, on the northern side of Calton Road, is located approximately 150m away from the subject site and will be separated from the proposed supermarket by two roads, a large development site (Lot 2045), as well as a proposed petrol filling station and car wash which are proceeding via separate development applications.

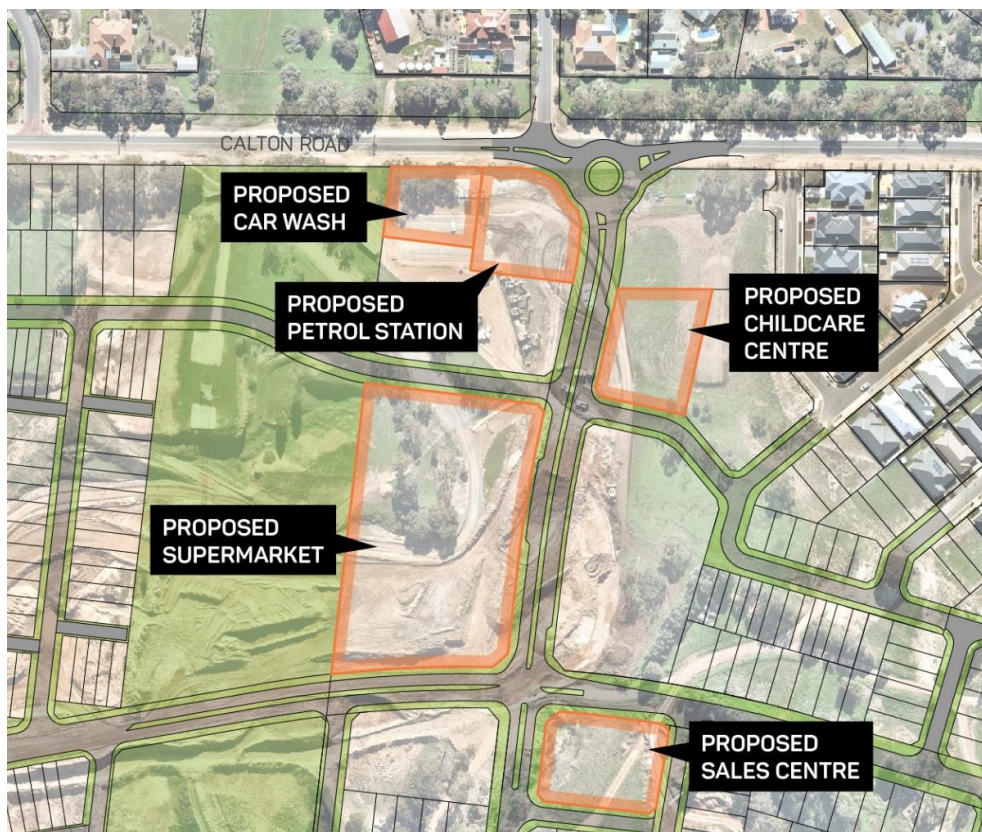
#### 1.5 Visual impact

Two of the representors raised a concern about the visual impact of the proposed supermarket. In response to this concern, we once again note that the subject land sits within the Mixed Use Centre Policy Area 3 which contemplates a supermarket in this location. We also note that the proposed supermarket and speciality shops is an architecturally designed development which includes a variety of building materials including painted concrete panels, face brickwork, feature stone facing and light weight timber cladding. In addition, a number of architectural features have been included in the design to create visual interest including feature steel frames and canopies. In this way, the proposed supermarket and speciality shops provides an attractive and high quality built form outcome which:

- Assists to create a 'main street' environment through the siting of buildings close to the Gawler East Link Road and the proposed collector road;
- Locates the majority of the car parking area behind a strong built form presence along the road frontages;
- Addresses the street frontages and will incorporate "... *protruding elements in order to create a pedestrian-friendly environment and outdoor dining opportunities*";
- Offers a façade and building materials which have been designed to reflect the high-profile location of the subject site in the 'village centre' with additional visual interest created by architectural features such as feature steel frames and canopies; and
- Assists to activate the street frontages by providing a plaza along the Gawler East Link Road and through the inclusion of high quality paving, street furniture and landscaping.

Further to the above, it is noted that almost all of the representors are separated and shielded from the subject site by a 20m wide vegetated reserve running along the northern side of Calton Road and are also separated by the proposed petrol filling station, car wash and future development site (see Figure 2). This separation distance, as well as the vegetated buffer, provides an effective screen thereby reducing the visual impact of the proposed development.

**Figure 2** Locality showing 20m vegetated reserve on the northern side of Calton Road



## 1.6 Signage

Two of the representors raised a concern about signage. In response, we note that the signage reflects the retail use of the buildings and is designed to easily identify the future tenants while also responding to the overall architectural theme of the 'village centre'. The design, extent and size of the proposed signage is relatively modest and does not visually dominate the buildings on which they will be attached. Further, the proposed signage will:

- Complement the design of the building;
- Be contained within the boundaries of the site;
- Not affect existing street trees or obscure views to attractive landscaping;
- Not cause a distraction for drivers or obstruct their views of other vehicles; and
- Be limited to information relating to the proposed use of the land.

We also note that the proposed pylon sign at the southern entrance to the car park has been reduced in height down to 6m which aligns the height guideline for signs associated with supermarkets contained in *Table Ga/4 – Advertising and Advertising Displays Definitions and Design Criteria*.

## 1.7 Town of Gawler comments

The Town of Gawler has provided a comprehensive assessment of the proposed development. While the Council is generally supportive of the proposed supermarket and speciality shops, it has suggested a number of potential amendments which have been summarised and addressed below:

- **Earthworks and Retaining** – the Council has noted that a site civil plan has not been provided with the application and has further noted that there may be a need for additional earthworks and retaining walls on the site associated with the land division and Gawler East Link Road. In response, we note that the associated land division has now been approved which means that the site levels for the supermarket will be determined by the works required to implement the land division and associated infrastructure. This includes the vehicular access arrangements. If considered necessary, the applicant would be willing to accept a Condition of Consent requiring the preparation of a Site Works Plan to the satisfaction of SCAP prior to the issue of Development Approval;
- **Feature steel frames** – we note that the Council has requested additional clarification on the detail of the steel feature frames on the eastern side of the supermarket. In response, we note that the Site Plan has been updated to show the steel feature frames. They have also been identified on the elevations and the 3D images (see Figure 3). On this basis, we consider that sufficient information has been provided.



Figure 3 3D Image of eastern side of the supermarket showing steel feature frames



- **Additional pedestrian link** – we note that the Council has suggested that an additional pedestrian link could be provided to the eastern roadway. However, we note that the plans already show a pedestrian linkage from the eastern boundary (Gawler East Link Road) which runs along the front of the shops. This link is also clearly shown on the 3D image (see Figure 3). For this reason, another pedestrian linkage is not considered warranted;
- **Treatment of the loading dock** – we note that the Council is supportive of the location of the loading dock but has suggested that additional landscaping be provided along with greater pedestrian linkage. In response we note that a Landscaping Plan was provided which shows that the area to the north of the loading dock will be screened with dense plantings. For this reason, additional screening is not warranted. Also, we do not consider that an additional pedestrian linkage through this area would be appropriate given the heavy vehicle movements that will occur on this portion of the site;
- **Stormwater** – in response to the Council's queries about stormwater, WGA has now prepared a Stormwater Management Plan which has been submitted to SCAP. This Plan shows how stormwater will be managed in accordance with the arrangements agreed with DPTI;
- **Traffic** – we note that the Council has suggested that additional details should be provided in relation to the site levels of adjoining roads. Given that the associated land division has now been approved, the site levels will be governed by the works required to implement the land division and associated infrastructure. This includes the vehicular access arrangements. If considered necessary, the applicant would be willing to accept a Condition of Consent requiring the preparation of a Site Works Plan to the satisfaction of SCAP prior to the issue of Development Approval;
- **Car parking** – the Council has suggested that additional parking for those with a disability should be provided near the two standalone speciality shops. We note that this is a minor matter which can be dealt with during the detailed design stage and the Building Rules Consent process;

- **Bicycle parking** – as suggested by the Council, bike parking has been identified on the amended plans;
- **Landscaping** – we note that the Council has suggested that additional landscaping could be provided to further screen the loading dock and also within the car park. In response, we note that a 4m wide landscaped strip will be provided to the north of the loading dock. In addition, substantial landscaping is provided throughout the car park in the form of trees, shrubs, grasses and groundcovers;
- **Signage** – in response to the Council's concerns about the height of pylon sign at the southern entrance to the car park, its height has been reduced down to 6m; and
- **Infrastructure provision** – the infrastructure noted by the Council will generally be established via the recently approved land division.

## 2. Conclusion

Following our review of the representations received during the Category 2 notification process, we remain of the view that the proposal warrants Development Plan Consent for the following reasons:

- A 'supermarket' and 'shop' are envisaged forms of development in the Mixed Use Policy Area 3 of the Residential (Gawler East) Zone;
- The general area of the subject site is indicatively identified as a Neighbourhood Centre within the *Gawler East Structure Plan Map Ga/1 (Overlay 1) Enlargement G* as well as the Springwood Masterplan;
- The location and design of the proposed development will complement other proposed development in the Springwood 'village centre' such as the Sales Centre on the opposite side of the Gawler East Link Road;
- The proposed development has been designed to address both road frontages, and the building exhibits a high degree of architectural merit achieved by using a variety of complementary colours, building materials and articulation;
- A coordinated signage theme is proposed, with the indicative signage shown on the plans designed in accordance with the relevant provisions of the Development Plan;
- Large windows along the southern elevation will maximise opportunities for casual surveillance across the car park;
- The proposed development has been designed to accommodate safe and convenient movements for vehicles, and the CIRQA report concludes that sufficient onsite parking will be provided to cater for the anticipated parking demand;
- The additional traffic volumes to be generated by the development will not have a significant impact on the road network;
- Stormwater will be managed appropriately in accordance stormwater management plan which ties in with the overall management of stormwater for the Springwood development; and
- The proposed landscaping will assist to create a sense of place and enhance the amenity of the locality.

The application is closely aligned within the relevant provisions of the Development Plan and therefore warrants Development Plan Consent subject to reasonable and relevant conditions.

We trust that you now have sufficient information to finalise your assessment of the proposed development to enable the application to be presented at the next available meeting of the SCAP. We would appreciate the opportunity to address the SCAP and answer any questions that Members may have.

If any further information is required, please contact the undersigned on (08) 7231 0286.

Yours Sincerely

A handwritten signature in blue ink, appearing to read 'Kieron Barnes'.

**Kieron Barnes**  
Senior Associate

## Appendix 1. Amended Plans and Elevations

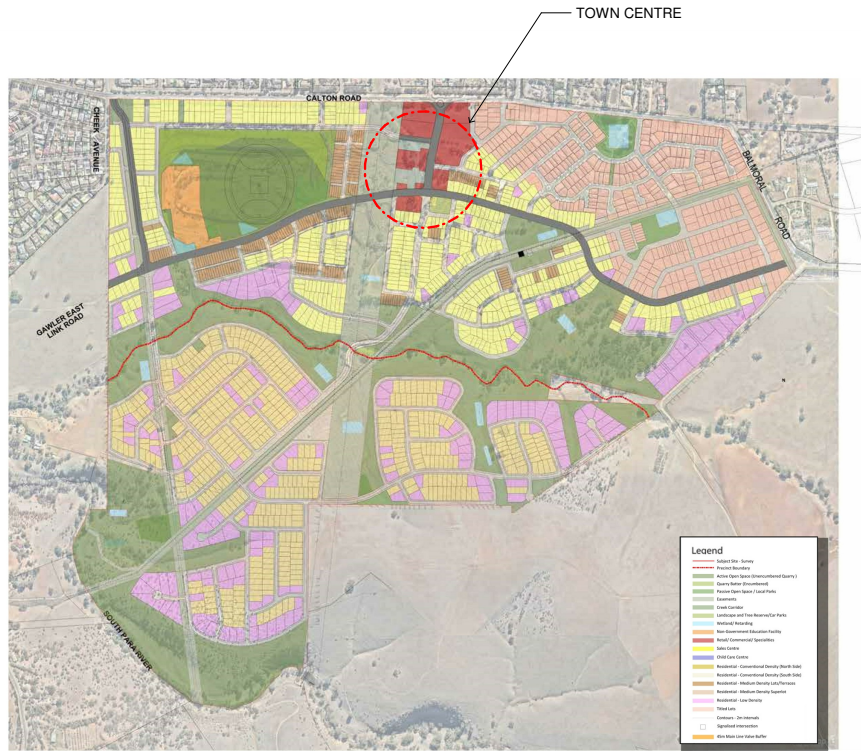


Figure 1. Springwood Masterplan

Text

316-0320-00-4-00-8P00\_SCAP\_Report\_Landscape and UD

13 May 2019

17 / 61

## LOCALITY PLAN

# SPRINGWOOD TOWN CENTRE

## PLANNING APPLICATION PACKAGE

LODGEMENT ISSUE 14 JUNE 2019

DA01	COVER SHEET & LOCATION PLAN	1
DA02	SITE PLAN	1
DA03	ELEVATIONS	1
DA04	SITE SECTIONS	1
DA05	3D IMAGES	1
DA06	LANDSCAPING PLAN	1



3D VIEW STREET VIEW TO NORTH WEST

**SPRINGWOOD**  
GAWLER EAST

**BROOK FALCONER**  
28 Chesser Street, Adelaide, South Australia 5000  
Telephone: 08 8223 5800 Facsimile: 08 8223 2440  
ABN 65 007 846 588  
brookfalconer.com.au

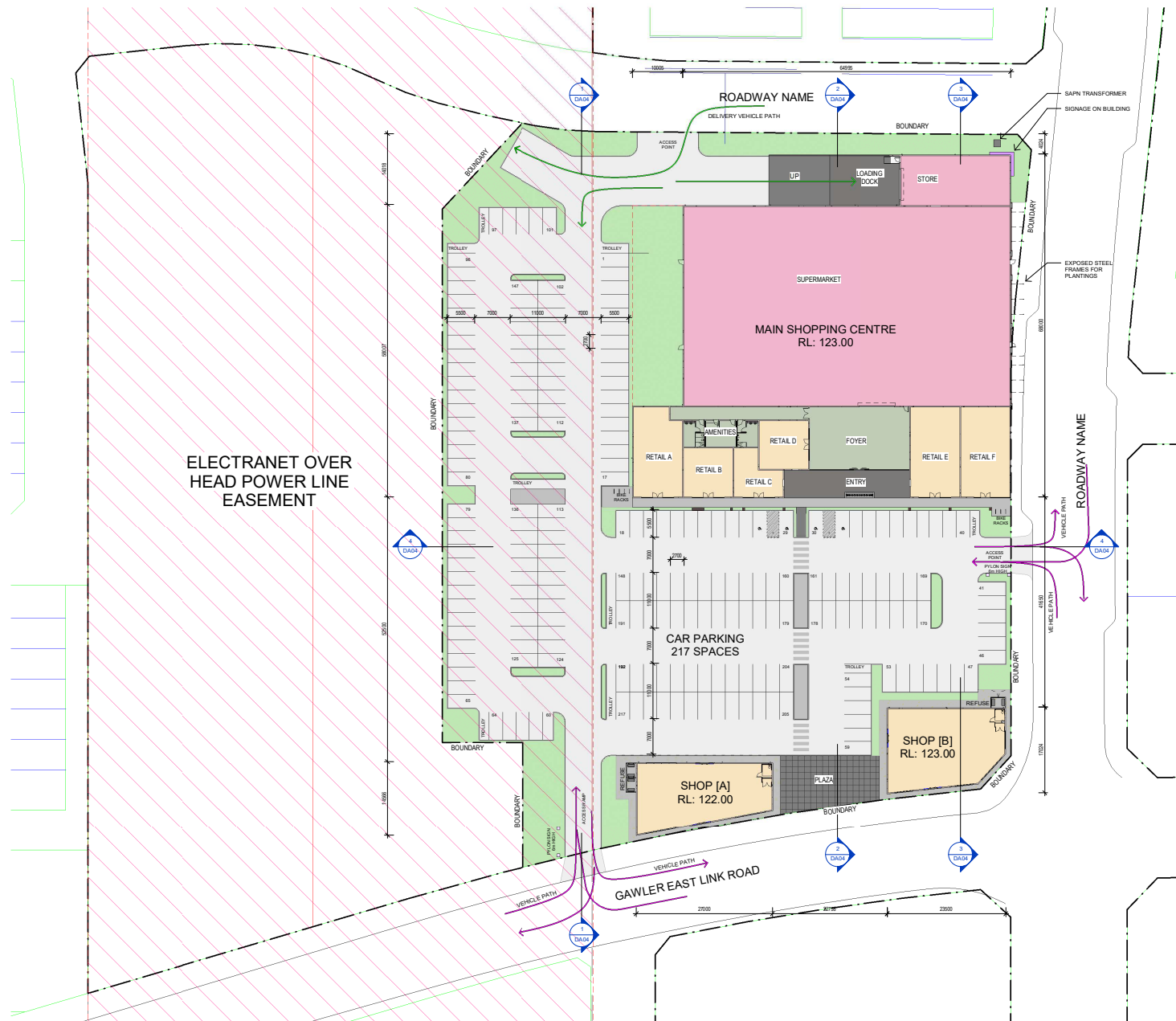
ARCADIAN PROPERTY

SPRINGWOOD TOWN CENTRE

COVER SHEET & LOCATION PLAN

Scale  
Drawn JSM  
Date 16/05/2019  
Job No. 2016049  
Dwg No. **2569 DA01** Rev: **1** A1 SHEET

SITE PLAN  
1:400



# DA ISSUE

ISSUED FOR DEVELOPMENT APPROVAL  
03/03/2019 05:18 PM

Rev	Description	Date
1	LODGEMENT ISSUE	14/01/19
2	MINOR AMENDMENTS	05/03/20

## SITE SUMMARY

SITE AREA	27,980m <sup>2</sup>
SUPERMARKET	2,600m <sup>2</sup>
RETAIL (A-F)	780m <sup>2</sup>
SHOP A	325m <sup>2</sup>
SHOP B	355m <sup>2</sup>
TOTAL BUILDING AREA	4,060m <sup>2</sup>
CARPARKING SPACES	217

**SPRINGWOOD**  
GAWLER EAST

**BROOK FALCONER**  
28 Chesser Street, Adelaide, South Australia 5000  
Telephone: 08 8223 5800 Facsimile: 08 8223 2440  
ABN 65 007 846 586 brookfalconer.com.au

ARCADIAN PROPERTY

SPRINGWOOD TOWN CENTRE

SITE PLAN

Scale As indicated

Drawn JSM

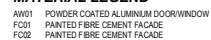
Date 16/05/2019

Job No. 20160-40

Dwg No. 2569 DA02 Rev. 2

AT SHEET





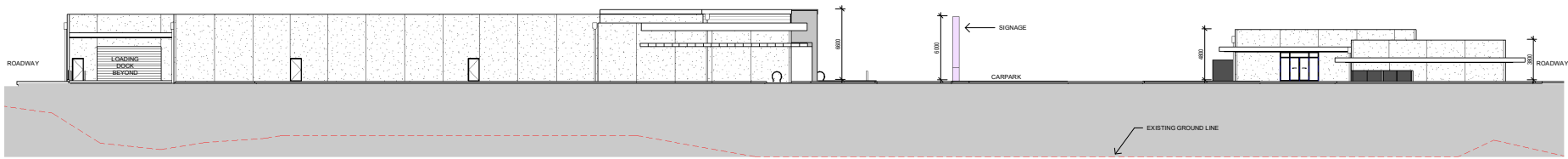
Drawn: JSM  
 Date: 16/05/2019  
 Job No.: 2016040  
 Dag No.: **2569 DA03**    Rev: **2**    A1 SHEET



# DA ISSUE

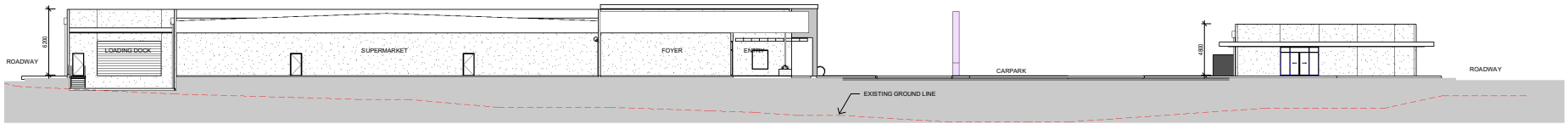
ISSUED FOR DEVELOPMENT APPROVAL  
09/09/2019 09:34 PM

Rev	Description	Date
1	LOADING DOCK	14/09/19
2	MINOR AMENDMENTS	09/09/20



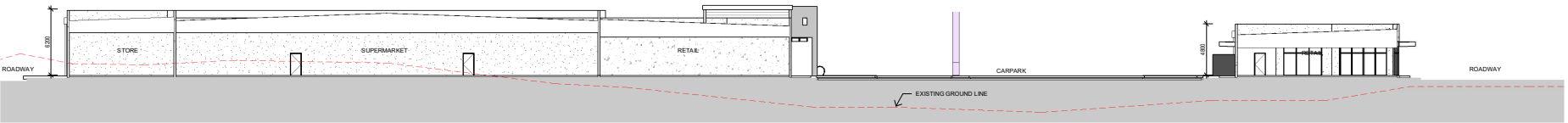
DA - Section 1

1:200



DA - Section 2

1:200



DA - Section 3

1:200



DA - Section 4

1:200

**SPRINGWOOD**  
CAWIFR FAST

**BROOK FALCONER**  
28 Chesser Street, Adelaide, South Australia 5000  
Telephone: 08 8223 5800 Facsimile: 08 8223 2440  
ABN 65 007 846 586 brookfalconer.com.au

ARCADIAN PROPERTY

SPRINGWOOD TOWN CENTRE

SITE SECTIONS

Scale 1:200  
Drawn JSM  
Date 16/05/2019  
Job No. 2016040  
Dwg No. 2569 DA04 Rev. 2 A1 SHEET

# DA ISSUE

ISSUED FOR DEVELOPMENT APPROVAL  
\$59,232.00 0.00 38 PM

No.	Amendment	Date
1	LODGE/MENT ISSUE	14/07/19
2	MINOR AMENDMENTS	05/03/20

## LANDSCAPE LEGEND

NEW LANDSCAPING  
WITHIN RELIEVED  
BEDS  
AREAS WITHIN  
PROPOSED SITE ONLY

NOTES  
➤ ALL LANDSCAPING TO BE IRRIGATED  
➤ MULCH TO BE INSTALLED TO ALL  
GARDEN BEDS

## TREE SCHEDULE

T1 CALLERY PEAR  
T2 JAPANESE ELM



## PLANT SCHEDULE

ALL TUBE STOCK

## BEDGES / GRASSES

AGAPANTHUS SMALL CULTIVARS	ASC	2 per m²
DISTICHLIS DISTICHOPHYLLA	DD	2 per m²
LOMANDRA CULTIVARS	LC	2 per m²
NANDINA SPP	NS	2 per m²
POA SPP	PS	2 per m²

## GROUND COVERS

ASPERULA CONFERTA	AC	2 per m²
LEPTOSPERMUM LAMBERGII	LL	2 per m²



**SPRINGWOOD**  
GAWLER EAST

**BROOK FALCONER**  
28 Chesser Street, Adelaide, South Australia 5000  
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ARCADIAN PROPERTY

SPRINGWOOD TOWN CENTRE

LANDSCAPING PLAN

Scale As indicated

Drawn Author

Date 06/07/19

Job No. 20160-43

Dwg No. 2569 DA05 Rev. 2



AT SHEET

ELECTRANET OVER  
HEAD POWER LINE  
EASEMENT

ROADWAY NAME

SAPN TRANSFORMER

UP

LOADING  
DOCK

STORE

SUPERMARKET

RETAIL A

RETAIL B

RETAIL C

RETAIL D

FOYER

RETAIL E

RETAIL F

ENTRY

CAR PARKING

SHOP [B]

SHOP [A]

PLAZA

GAWLER EAST LINK ROAD

ROADWAY NAME

LANDSCAPING PLAN

1:400

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# Development Plan

## Gawler (CT)

Consolidated – 18 July 2019

Please refer to the Gawler (CT) page at [www.sa.gov.au/developmentplans](http://www.sa.gov.au/developmentplans) to see any amendments not consolidated.



**Government of South Australia**

Department of Planning,  
Transport and Infrastructure

Consolidated - 18 July 2019



## RESIDENTIAL (GAWLER EAST) ZONE

### Introduction

The Objectives and Principles of Development Control that follow apply in the Residential (Gawler East) Zone shown on Gawler [Maps Ga/5, 6, 9 and 20](#) and Policy Areas [Map Ga/15 and 17](#). They are additional to those expressed for the whole of the Council area.

### OBJECTIVES

- Objective 1:** A predominately residential area comprising a range of low and medium-density dwellings, with associated integrated infrastructure, retail, commercial, recreational, educational and community development in master-planned locations in accordance with Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#).
- Objective 2:** Development sensitive to the particular topography of the area and which has minimal visual and environmental impacts.
- Objective 3:** A residential zone comprising a range of dwellings types, including a minimum of 15 percent affordable housing.
- Objective 4:** Increased dwelling densities in close proximity to centres, future public transport routes and public open spaces.
- Objective 5:** Open space systems designed to provide multiple use reserve areas that promote water management, habitat retention and enhancement, and recreational linkages.
- Objective 6:** Development that achieves co-ordination in allotment configuration and physical and community infrastructure provision.
- Objective 7:** Development that contributes to the desired character of the zone.

### DESIRED CHARACTER

The Residential (Gawler East) Zone is located within both the Town of Gawler and The Barossa Council. The Gawler East area encompasses broad hectare land which is expected to support a population of approximately 10 000 persons.

The zone will develop in accordance with Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#) and be undertaken in an orderly manner that achieves the most efficient use of land, the extension or expansion of infrastructure services and the timely provision of community facilities. No more than 1000 allotments should be created within the area defined by Gawler East Development Constraints Concept Plan [Figure CoP/5](#) until such time as the Gawler East Link Road is complete.

The zone will accommodate a diversity of housing forms. The Mixed Use Centre Policy Area 3 will comprise a mix of residential development and a range of commercial, retail, educational, recreational and community facilities. A smaller range of retail facilities, together with compact housing are anticipated within the Local Centre Policy Area 19.

It is essential that development respects and enhances the natural attributes of the zone through the retention of significant views, creek lines, native vegetation and locations of ecological significance. Innovative and best practice solutions in respect to water reuse, grey water supply and stormwater management will be implemented. Retention of on-site stormwater will be maximised where practical. Any stormwater leaving the site will be dispersed to reduce the risk of erosion to the natural surroundings and impacts to the downstream receiving waters.

Dwellings will range between 1 and 3 storeys in height; however buildings at the interface with adjoining zones other than the Open Space Zone will not exceed 2 storeys.

Buildings of between 3 and 5 storeys, such as apartment buildings, will be located in the Mixed Use Centre Policy Area 3, the former quarry area, adjacent to open space, and where necessary to frame the end of important or significant vistas. It is important that development achieve a clear transition in building height for a cohesive streetscape.

Housing forms will be simple and incorporate a high degree of articulation to the street façade while delivering a mix of housing types and forms to provide interesting streetscapes and promote social interaction. This will include the provision of recessed vehicle garaging and the inclusion of front verandas/porticos and appropriate landscaping.

The delivery of housing diversity will require innovative solutions for front or rear access and parking. Rear access will be provided in the form of 'service lanes', which support vehicular access requirements at reduced speeds providing a safe pedestrian environment. At the land division stage, on-street car parking should be provided near areas of proposed medium to high density developments.

Allotment configuration is envisaged to be compact particularly within the Mixed Use Centre Policy Area 3, with building setbacks minimised to assist in facilitating an enclosed and active street. This will incorporate opportunities for multi-storey apartments, terrace and rear lane mews housing that will achieve a range of housing types within a single street.

The slope of the land will dictate the location of particular dwelling types, with some more compact dwelling types located on relatively flat sites, whilst more traditional dwelling types will be located on those portions of the site with moderate to high slope. On topographically steep sites alternative design solutions, such as split level dwellings, that are able to meet desired earthworks and driveway gradient outcomes are appropriate.

The form and distribution of major open space will be influenced by Council Wide Open Space Development Plan provisions and by the need for integrated stormwater detention, treatment and re-use given limitations on the potable water supply for the area. It will also be influenced by the location of natural drainage corridors, and the need to integrate with existing corridors, including the eastern escarpment at Evanston Park. Public open space areas will need to accommodate both active and passive recreation opportunities and the retention of identified conservation areas.

A co-ordinated network of linear parks, including cohesive pedestrian and bicycle movement corridors and visual links, will be established between the new development and adjoining natural creek lines, public recreation areas, local shopping and community services and surrounding road networks.

Steeper environments and watercourses that are proposed to be vested to Council as reserves and/or easements, will be restored to an appropriate natural state in consultation with the Town of Gawler.

Steeper slopes result in higher bushfire risk. Providing vehicle access between the Open Space Zone and residential development together with appropriate buffers assists in managing bushfire risk. Reference to Australian Standard AS3959 – 2009 (incorporating amendments to 2009 and 2011) is recommended to inform and reduce bushfire risk. Any revegetation of areas will take into account the level of bushfire risk, local topography and the need for ongoing management of bushfire risk, ensuring significant hazards are neither created nor exacerbated.

Reserves for public purposes such as active and passive recreation and stormwater management, including the provision of associated infrastructure, will be designed to an appropriate standard and level of service suitable to the locality to ensure long term economic, environmental and social sustainability.

Portion of the eastern boundary of the zone is located adjacent to the Para Woodland Reserve. It is essential that development form an appropriate interface with the Para Woodland Reserve. The interface will act as a buffer between the residential area and the Reserve, balancing access, management of bushfire risk, management of potential invasion by pest plants, minimising the impact of domestic pets on native wildlife and as a provision of open space. The interface will vary in width as appropriate to meet the above criteria and will comprise of a combination of roads, paths, public open space and, where appropriate, areas of natural character for stormwater management. Where housing is included in the interface area it is expected that houses will address the Reserve. The interface area will be planted with locally indigenous species (mainly groundcovers and low shrubs) selected to minimise the bushfire risk by providing an area of reduced fuel hazard.

The north-eastern, eastern and southern boundary of the zone adjoins agricultural, rural and rural living land. It is essential that development provide an appropriate buffer between dwellings and land used for agriculture. Larger allotments together with open space and road networks and increased dwelling setbacks will be established at the peripheries of the zone boundary in order to provide an appropriate low density transition and interface with adjacent rural and rural living land.

The Gawler East Link Road shown on Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#) is intended to have a boulevard character comprising wide footpaths and cycle paths on both sides and substantive street tree plantings. Dwellings will front and address the road with setbacks to contribute to the boulevard character. In addition, dwellings fronting the road will be sited, designed and constructed to mitigate traffic noise impacts.

Infrastructure Corridors have been created in response to the presence of key public infrastructure, namely 132 kV and 275 kV transmission lines. This infrastructure comprises a significant component of the State's high voltage power transmission network. The zone provisions are aimed at protecting this significant public infrastructure from encroachment by incompatible land uses and protecting the infrastructure corridors from being fragmented by land division and therefore ensuring on-going access for maintenance is available and that the security and reliability of the power network is not compromised. No residential allotments should infringe on the corridors or existing easements. These corridors provide the opportunity for co-locating compatible land use activities such as other appropriate infrastructure, at-grade car parking and roads, a linear park or a cycling/walking trail.

Development should have regard to the impact of high noise emission and adverse air quality sources, such as from the high-pressure gas transmission pipeline valve, non-residential development and the Gawler East Link Road. Dwellings fronting this road will be sited, designed and constructed to mitigate traffic noise and adverse air quality impacts.

An Aboriginal heritage survey has been undertaken for the Gawler East area. It indicates that prior to colonial contact the South Para River and its tributaries were associated with long-term seasonal Aboriginal occupation, with adjoining hill crests providing views of culturally significant landscape features and surveillance for the Kaurna Nation peoples. While there has since been a high level of ground disturbance in the area, there is the potential for future activities to disturb sub-surface archaeological deposits particularly along the banks of the South Para River. Activities within this area which may damage, disturb or interfere with any Aboriginal site or object need to be undertaken in accordance with the provisions of the *Aboriginal Heritage Act 1988*.

## PRINCIPLES OF DEVELOPMENT CONTROL

### Land Use

- 1 The following forms of development are envisaged in the zone:

- Affordable housing
- Aged persons' accommodation
- Community facilities
- Domestic outbuilding in association with a dwelling
- Domestic structure
- Dwelling
- Dwelling addition
- Dwelling with associated home based business uses
- Land division
- Non-residential use that serves the local community, for example:
  - child care facility
  - health and welfare service
  - open space
  - pre-school
  - primary and secondary school
  - recreation area
  - shop, office or consulting room
- Retirement village
- Supported accommodation



- 2 Development listed as non-complying is generally inappropriate.

### Form and Character

- 3 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 4 Development should occur in accordance with Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#).
- 5 Road reserves should be of a width, design and alignment that can:
- (a) provide for safe and convenient movement and parking of vehicles and other users according to projected vehicle volumes, speeds and the character of the road;
  - (b) accommodate bus routes where required;
  - (c) provide for shared, on-street parking bays for nearby residents and visitors wherever practical to achieve unrestricted movement along collector roads;
  - (d) allow vehicles to enter or reverse from an allotment or garage in a single movement, allowing for cars parked on the opposite side of the road (where applicable) or fixed infrastructure on the street;
  - (e) allow for the efficient movement of service and emergency vehicles; and
  - (f) accommodate street planting, landscaping, street furniture and utilities infrastructure.
- 6 **Direct allotment access may be provided to the Gawler East Link Road:**
- (a) where the Road gradient is not more than 8 percent at the point the driveway accesses the Road; or
  - (b) where the Road gradient is more than 8 percent if the allotment has a regular frontage of at least 25 metres.
- 7 Dwellings fronting the Gawler East Link Road should be sited, designed and constructed to mitigate traffic noise and adverse air quality impacts.
- 8 The use and placement of outbuildings should be ancillary to and in association with a dwelling or dwellings.

### Land Division

- 9 Land division should facilitate the provision of a broad range of housing options, including affordable housing.
- 10 Land division should accommodate open space and movement networks that provide for strong connections and safe and convenient access to public facilities, public transport and potential future development of adjoining sites.
- 11 Land division:
- (a) should not exceed 1000 allotments until at least the following infrastructure indicated by Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#) has been constructed:
    - (i) the Gawler East Link Road between Calton Road and Gawler - One Tree Hill Road; and
    - (ii) the Gawler East Link Road between Gawler - One Tree Hill Road and Potts Road; and

- (iii) an upgrade of Potts Road and its intersection with Main North Road to accommodate the traffic flows associated with further continued development.
  - (b) should not prejudice the construction of the Gawler East Link Road as detailed in the Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#) and associated pedestrian and bicycle paths, landscaping treatments and public lighting.
  - (c) should facilitate the construction of on and off-site stormwater management systems and community facilities.
- 12** Land division creating reserves containing steeper environments and watercourses, that are proposed to be vested to Council, should be restored to an appropriate natural state through the removal of declared plant species, control of other environmental weeds and the planting and management of native vegetation in order to achieve desired amenity, recreation, conservation, biodiversity, management of bushfire risk and ongoing management outcomes.
- 13** Land division creating reserves for public purposes such as stormwater management and active and passive recreation, including the provision of associated infrastructure, should be designed to an appropriate standard and level of service suitable for the locality to ensure long term economic, environmental and social sustainability.
- 14** Rear lanes should:
  - (a) have a minimum reserve width of 6.5 metres;
  - (b) be limited in length to a maximum of 100 metres;
  - (c) have a minimum carriageway width of 5.5 metres;
  - (d) include protuberances to accommodate landscaping and lighting should not exceed 1.0 metre;
  - (e) landscaping should be in the form of tall vertical trees in preference to low level shrubs;
  - (f) be designed to accommodate garbage trucks and emergency service vehicles.
- 15** Public lighting should be provided to all public roads, laneways, paths and open spaces.
- 16** Development with frontage to the eastern side of the Gawler – One Tree Hill scenic road should be established on allotments of no less than 1000 square metres in area and incorporate high quality of building articulation and appropriate landscaping between buildings and the road in order to retain distant vistas, ensure visual interest and a scenic landscape character along this corridor.
- 17** Land located west of the South Para River should not be divided for the purpose of creating additional allotments unless forming part of an integrated development scheme where all infrastructure is delivered to service the land in an orderly and economic manner including potable water supply, grey water supply, waste water disposal, formed all-weather public roads and access, and stormwater disposal.
- 18** Detention and/or retention basins should incorporate good design techniques that:
  - (a) allow sediments to settle so as to treat stormwater prior to discharge into watercourses;
  - (b) allow for sediments to be easily removed from the basin and disposed of off-site;
  - (c) ensure human health and safety, particular with respect to high velocity drainage points;
  - (d) ensures the control of mosquitoes and nuisance insects (e.g. midges); and

- (e) ensure detention and retention infrastructure (in conjunction with Water Sensitive Design) is designed to have a capacity that limits outflow to predevelopment flow rates in a 100 year ARI event
  - (f) improve water quality through treatment of detained water.
- 19 Transmission lines should be protected from encroachment through the provision of:
- (a) a 30 metre wide corridor (15 metres each side from the centreline) for the 132kV line;
  - (b) a 100 metre wide corridor for the 275kV line as required by relevant authorities.
- 20 Residential allotments should not be created within the Major Transmission Infrastructure Corridors shown on Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#), or within the existing easements for the 132kV and 275kV transmission lines.

### Land Use and Density

- 21 Housing with an average site area for dwellings less than 250 square metres should be located within the Mixed Use Centre Policy Area and Local Centre Policy Area or within walking distance of public open space, local shops and public facilities.

### Built Form/Setbacks

- 22 Buildings should not exceed the following heights:
- (a) two storey development for properties adjacent to the boundary of adjoining zones other than the Open Space Zone;
  - (b) three storeys for the balance of the zone, other than apartment/mixed use buildings within the Mixed Use Centre Policy Area where a 5 storey limit applies.
- 23 Where allotments have direct frontage to an open space reserve, housing should address the reserve and incorporate a high quality design outcome (i.e. articulation of building facades) and that promotes casual surveillance (i.e. windows and open style fencing that promotes surveillance).
- 24 Where an allotment immediately adjoins public open space, clear, safe and convenient pedestrian access should be provided to the dwelling.
- 25 Residential building setbacks should satisfy the minimum dimensions outlined in Table 1 except where a proposed plan of division is accompanied by a building envelope plan that demonstrates that lesser building setbacks will contribute to the achievement of the desired character for the zone:

**Table 1**

Parameter	Value
Primary Street Frontage (excluding arterial or collector roads forming the zone boundary and the One Tree Hill Scenic Road shown on <a href="#">Map Ga/1 (Overlay 1) Part A and Part B</a> )	3.0 metres to front facade 1.5 metres for dwellings where vehicle access obtained from the rear or side 1.5 metres to veranda/balcony elements 0.5 metres to entry porch and portico
Secondary Street Frontage (Corner Lots)	1.5 metres to façade 0.6 metres for dwellings on allotments with a frontage equal to or less than 9.0 metres 0.5 metres for veranda/balcony elements 0 metres for entry porch/portico

Parameter	Value
Side Boundary (excluding road frontage)	0.0 metres for dwellings on allotments with a road frontage equal to or less than 9.0 metres; 0.9 metres for dwellings on allotments with a frontage greater than 9.0 metres, other than a garage wall with a maximum length of 6.0 metres
Rear Boundary (other than rear lane)	0.9 metres
Open Space Reserve Frontage	1.5 metres where dwellings front the reserve 0.5 metres to entry porch and portico, veranda and balcony elements
Single Carport/Garage	metres from primary street frontage 0.5 metres for laneway frontage
Double Carport/Garage	5.0 metres from primary street frontage 0.5 metres for laneway frontage
Arterial or Collector Roads	5.0 metres to front façade 5.5 metres to carport/garage
Gawler - One Tree Hill Scenic Road shown on <a href="#">Map Ga/1 (Overlay 1) Part A and Part B</a>	15.0 metres to all buildings (excluding minor protrusions such as porches, porticos, eaves, verandas, balconies or similar)
Rear Lane (upper level dwelling)	0.5 metres for laneway frontage

### Private Open Space

26 Dwellings should include private open space which conforms to the requirements of Table 2:

**Table 2**

Site area of dwelling	Minimum area of private open space	Provisions
Greater than 250 square metres	60 square metres (minimum dimension of 2.5 metres)	(a) Balconies, roof patios, decks and the like, can comprise part of this area provided the area of each is 8 square metres or greater and has a minimum dimension of 2.0 metres. (b) One part of the space should be directly accessible from a living room and have an area of 25 square metres with a minimum dimension of 4 metres and a maximum gradient of 1-in-10.
250 square metres and less	35 square metres (minimum dimension of 2.5 metres)	(a) Balconies, roof patios and the like can comprise part of this area provided the area of each is 8 square metres or greater and has a minimum dimension of 2.0 metres. (b) One part of the space is directly accessible from a living room and has an area of 16 square metres with a minimum dimension of 4 metres and a maximum gradient of 1-in-10.
	25 square metres (minimum dimension of 2.5 metres) where:	(a) The dwelling has no more than two bedrooms (or rooms that could reasonably be used as bedrooms) and a total floor area of not more than 110 square metres (b) Separate areas are provided for the provision of a rainwater tank and the storage of refuse and recycling bins.

Site area of dwelling	Minimum area of private open space	Provisions
Upper level dwellings	Minimum area of private open space	(a) 8 square metres and accessible from a living room.

### Amenity and Public Spaces

- 27** Residential development should have regard to existing and possible future noise and adverse air quality sources, such as from the high pressure gas transmission pipeline valve, the Gawler East Link Road and non-residential developments, with respect to site layout, orientation, design and construction to ensure a safe and comfortable residential environment and to minimise conflict with existing non-residential activities
- 28** Development should balance the desire for an open streetscape and passive surveillance with the need for functional privacy. Clear delineation should be provided between public and private spaces, which may incorporate fencing, landscaping or a combination of these elements.
- 29** Retaining walls greater than 1.0 metre in height should:
- (a) not be directly visible from a public road or the public realm
  - (b) where located between a building and a road frontage, be stepped in increments of not more than 1.0 metre
  - (c) not cause significant adverse impacts on the amenity of adjacent properties, particularly in relation to overshadowing, cutting off light or views.
- 30** Residential development should provide an area for the storage of waste receptacles that is screened from primary and secondary street frontages.
- 31** Development should be designed and sited to relate to the slope of the land, so that:
- (a) the bulk and scale of the buildings do not dominate the landscape;
  - (b) the amount of cut and fill is minimised.
- 32** Fences should be located to minimise their visual impact and be constructed of materials which are of a low light-reflective nature.

### Bushfire Protection

- 33** To protect against bushfire, dwellings should not be sited within 40 metres of a slope greater than 20 degrees, where the length of the slope is greater than 10 metres and covered by unmanaged vegetation.

### Separation of Use

- 34** Development should be designed and sited to minimise negative impact on existing and potential future land uses considered appropriate in the locality.
- 35** Residential development adjacent to non-residential zones and land uses should be located, designed and/or sited to protect residents from potential adverse impacts from non-residential activities.

### High Pressure Gas Pipeline

- 36** Development, including land division, in the vicinity of a high pressure pipeline, as depicted on Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#), should:

- (a) not affect the continued operation of the pipeline infrastructure
- (b) be located, designed, and developed having regard to the location of the infrastructure and the potential public safety impacts.

### Car Parking

- 37** For each dwelling, the maximum width (including the width of any support structure) of any garage or carport opening that faces a street, should be no greater than six metres or 50 percent of the frontage width, whichever is the lesser, except where a site has a frontage of less than 12 metres and the dwelling is:
- (a) two or more storeys; and
  - (b) incorporates protrusions such as verandas, projecting windows, porches, balconies etc. which provide articulation in the building as it presents to the street, in which case garages or carports should have a maximum width of 6 metres or 80 percent of the width of the site, whichever is the lesser.
- 38** No maximum width applies to garage or carport openings where a site has rear vehicular access and from which vehicular access is obtained.
- 39** Development within Mixed Use Centre Policy Area 3 and Local Centre Policy Area 19 should have car parking in accordance with the following table based on calculations relating to net-leasable floor areas:

Form of Development	No. of required car spaces
Apartment	1 space for every unit plus 1 additional space for every 5 2-bedroom units plus 1 additional space for every 3 bedroom unit with a visitor park of 1 space for every 5 units.
Bank	5.5 spaces per 100 square metres
Office	4 spaces per 100 square metres
Shop	5.5 spaces per 100 square metres

### Affordable Housing

- 40** Development should include a minimum 15 percent of residential dwellings for affordable housing.
- 41** Affordable housing should be distributed throughout the zone to avoid over-concentration of similar types of housing in a particular area.

### Infrastructure Coordination

- 42** Development, including land division, should only occur once required physical and community infrastructure is either provided, or a legally binding agreement has been executed for the provision of the infrastructure items, including the following:
- (a) Gawler East Link Road as illustrated on Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#), and associated traffic interventions (road and intersection upgrades);
  - (b) public lighting, pedestrian and bicycle paths and associated landscaping treatments associated with the Gawler East Link Road;
  - (c) community facilities (including recreation facilities); and
  - (d) stormwater management systems.

## PROCEDURAL MATTERS

### Complying Development

- 43** Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

### Non-complying Development

- 44** Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Crematorium

Development in the form of land division in the area defined by the Gawler East Development Constraints Concept Plan [Figure CoP/5](#) is non-complying if:

- (a) there exist 1000 allotments within the area defined by the Gawler East Development Constraints Concept Plan [Figure CoP/5](#); and
- (b) the following infrastructure has not been completed
  - (i) the Gawler East Link Road between Calton Road and Gawler - One Tree Hill Road;
  - (ii) the Gawler East Link Road between Gawler - One Tree Hill Road and Potts Road;
  - (iii) an upgrade of Potts Road and its intersection with Main North Road to accommodate the traffic flows associated with further continued development.

Fuel Depot

Horticulture

Hospital except where located within Mixed Use Policy Area 3 Industry

Intensive animal keeping Junk Yard

Major public service depot

Office greater than 150 square metres, except where located within Mixed Use Centre Policy Area 3 and/or Local Centre Policy Area 19

Prescribed mining operations Refuse destructor

Shop or group of shops with a gross leasable floor area exceeding 250 square metres, except where located within Mixed Use Centre Policy Area 3 and/or Local Centre Policy Area 19

Telecommunications Facility above 30 metres in height Warehouse

Waste reception, storage, treatment or disposal, except a sewerage treatment plant.

### Public Notification

- 45** Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development (except where the development is non-complying) are designated:

#### Category 1

Fencing (including a combination of fencing and retaining walls) up to a maximum height of 2.8 metres

#### Category 2

All development listed within Principle of Development Control 1 of the Residential (Gawler East) Zone, Mixed Use Policy Area 3 and Local Centre Policy Area 19.

Fencing (including a combination of fencing and retaining walls) with a height in excess of 2.8 metres.



## Mixed Use Centre Policy Area 3

### Introduction

The Objectives and Principles of Development Control that follow apply in the Mixed Use Centre Policy Area 3 of the Residential (Gawler East) Zone shown on Policy Areas [Map Ga/15](#). They are additional to those expressed for the whole of the Council area.

### OBJECTIVES

- Objective 1:** A functional and diverse zone accommodating a mix of commercial, retail, recreation, community, residential, office, consulting rooms and educational uses.
- Objective 2:** Development that minimises any adverse impacts upon the amenity of the locality within the policy area.
- Objective 3:** Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

Mixed Use Centre Policy Area 3 will accommodate retail, commercial, community, education and formal recreation facilities and clubrooms to service the local community. It is envisaged that a full range of residential development will form an integral component of this policy area to activate it outside of commercial and retail business hours. Housing forms will include, but not be limited to, mixed use development within a single building where dwellings will typically be established above non-residential land uses.

Low impact, commercial business activities that provide employment opportunities for the local population are envisaged. Such development will need to have particular regard to ensuring that minimal off-site impacts occur with respect to noise, air, water and waste emissions, commercial traffic generation and movement.

A retail facility, not exceeding 10 000 square metres in floor area, will comprise a supermarket and a range of specialty shops to serve the weekly shopping needs of the community. Restaurants, cafes, hotels (incorporating dining) and take away outlets are envisaged and will develop the centre as a destination point and provide uses that extend the hours of the centres operation to promote surveillance and safety of the adjacent recreation and education facilities.

Commercial business activities and retail facilities will be established in the Neighbourhood Centre area indicated on Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#).

It is envisaged that community facilities and additional educational establishments will be established to complement the existing role of the Gawler Township. Future educational establishments will be located in accordance with the Structure Plan [Map Ga/1 \(Overlay 1\)](#). Schools are expected to form a focal point for the new community providing opportunities for the establishment of significant buildings and shared open space.

In terms of urban design and built form, a 'main street' environment will be created, where buildings address the street and car parks are primarily located to the rear. In order to minimise the overall extent of off street parking shared car parking is encouraged, and the convenience, availability and function of on street parking in mixed use environments will be recognised.

Gathering points for formal and informal community events will be established, either by means of a central pedestrian plaza, a village green or series of nodes.

Commercial and mixed use buildings will be established close to the street frontage and incorporate verandas and other protruding elements in order to create a pedestrian-friendly environment and outdoor dining opportunities. These areas will be enhanced by large street trees, high quality paving, lighting and street furniture. Building facades will be designed in a manner to create diversity of interest through the appearance of an aggregation of smaller buildings.

## PRINCIPLES OF DEVELOPMENT CONTROL

### Land Use

- 1 The following forms of development are envisaged in the policy area:

Advertisement  
Bank  
Child care centre  
Community facility  
Consulting room  
Detached dwelling  
Educational establishment  
Group dwelling  
Health centre  
Home activity  
Hospital  
Hotel  
Indoor recreation centre  
Library  
Licensed premises  
Motel  
Nursing home  
Office  
Office and dwelling  
Personal service establishment  
Petrol filling station  
Place of worship  
Pre-school  
Recreation area  
Residential flat building  
Restaurant  
Retail showroom  
Row dwelling  
Semi-detached dwelling  
Serviced accommodation  
Shop  
Shop and dwelling  
Supermarket.

- 2 Development listed as non-complying is generally inappropriate.

### Form and Character

- 3 Development should be designed to ensure that:
- (a) buildings are designed to address the street frontage with servicing areas located internal to the centre and appropriately screened from public view;
  - (b) the establishment of shared car parking areas to the rear of buildings and on-street.
- 4 Public space established should be activated by uses around its edges.
- 5 Buildings should have a maximum of 5 storeys in height.
- 6 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 7 Filling of land and retaining walls exceeding 1.0 metre in height are appropriate where they are associated with the remediation and development of the former quarry site.

## Contaminated Land

### OBJECTIVES

- Objective 18:** Protection of human health and the environment wherever site contamination has been identified or is suspected to have occurred.
- Objective 19:** Appropriate assessment and remediation of site contamination to ensure land is suitable for the proposed use and provides a safe and healthy living and working environment.

### PRINCIPLE OF DEVELOPMENT CONTROL

- 41** Development, including land division, should not occur where site contamination has occurred unless the site has been assessed and remediated as necessary to ensure that it is suitable and safe for the proposed use.

## Crime Prevention

### OBJECTIVE

- Objective 20:** A safe, secure, crime resistant environment where land uses are integrated and designed to facilitate community surveillance.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 42** Development should be designed to maximise surveillance of public spaces through the incorporation of clear lines of sight, appropriate lighting and the use of visible permeable barriers wherever practicable.
- 43** Buildings should be designed to overlook public and communal streets and public open space to allow casual surveillance.
- 44** Development should provide a robust environment that is resistant to vandalism and graffiti.
- 45** Development should provide lighting in frequently used public spaces including those:
- (a) along dedicated cyclist and pedestrian pathways, laneways and access routes
  - (b) around public facilities such as toilets, telephones, bus stops, seating, litter bins, automatic teller machines, taxi ranks and car parks.
- 46** Development, including car park facilities should incorporate signage and lighting that indicate the entrances and pathways to, from and within sites.
- 47** Landscaping should be used to assist in discouraging crime by:
- (a) screen planting areas susceptible to vandalism
  - (b) planting trees or ground covers, rather than shrubs, alongside footpaths
  - (c) planting vegetation other than ground covers a minimum distance of 2 metres from footpaths to reduce concealment opportunities.
- 48** Site planning, buildings, fences, landscaping and other features should clearly differentiate public, communal and private areas.
- 49** Buildings should be designed to minimise and discourage access between roofs, balconies and windows of adjoining dwellings.

- 104** Development in proximity to infrastructure facilities should be sited and be of a scale to ensure adequate separation to protect people and property.
- 105** Existing transmission lines should not be encroached upon by incompatible uses.
- 106** Provision should be made for new distribution substations and overhead major electricity line corridors (having a capacity greater than or equal to 33kv) in optimum locations with adequate access.

## Interface Between Land Uses

### OBJECTIVES

- Objective 42:** Development located and designed to minimise adverse impact and conflict between land uses.
- Objective 43:** Protect community health and amenity from adverse impacts of development.
- Objective 44:** Protect desired land uses from the encroachment of incompatible development.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 107** Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:
- (a) the emission of effluent, odour, smoke, fumes, dust or other airborne pollutants;
  - (b) noise;
  - (c) vibration;
  - (d) electrical interference;
  - (e) light spill;
  - (f) glare;
  - (g) hours of operation;
  - (h) traffic impacts.
- 108** Development should be sited and designed to minimise negative impacts on existing and potential future land uses desired in the locality.
- 109** Development adjacent to a **Residential Zone** or residential area within a **Township Zone** should be designed to minimise overlooking and overshadowing of adjacent dwellings and private open space.
- 110** Residential development adjacent to non-residential zones and land uses should be located, designed and/or sited to protect residents from potential adverse impacts from non-residential activities.
- 111** Sensitive uses likely to conflict with the continuation of lawfully existing developments and land uses desired for the zone should be designed to minimise negative impacts.
- 112** Non-residential development on land abutting a residential zone should be designed to minimise noise impacts to achieve adequate levels of compatibility between existing and proposed uses.

### Noise Generating Activities

- 113** Development that emits noise (other than music noise) should include noise attenuation measures that achieve the relevant *Environment Protection (Noise) Policy* criteria when assessed at the nearest existing noise sensitive premises.
- 114** Development with the potential to emit significant noise (e.g. industry) should incorporate noise attenuation measures that prevent noise from causing unreasonable interference with the amenity of noise sensitive premises.
- 115** Outdoor areas (such as beer gardens or dining areas) associated with licensed premises should be designed or sited to minimise adverse noise impacts on adjacent existing or future noise sensitive development.
- 116** Development proposing music should include noise attenuation measures that achieve the following desired noise levels:

Noise level assessment location	Desired noise level
Adjacent existing <i>noise sensitive development</i> property boundary	Less than 8 dB above the level of background noise ( $L_{90,15min}$ ) in any octave band of the sound spectrum and Less than 5 dB(A) above the level of background noise ( $LA_{90,15min}$ ) for the overall (sum of all octave bands) A-weighted level
Adjacent <i>land</i> property boundary	Less than 65dB(Lin) at 63Hz and 70dB(Lin) in all other octave bands of the sound spectrum or Less than 8 dB above the level of background noise ( $L_{90,15min}$ ) in any octave band of the sound spectrum and 5 dB(A) overall (sum of all octave bands) A-weighted level

### Air Quality

- 117** Development with the potential to emit harmful or nuisance-generating air pollution should incorporate air pollution control measures to prevent harm to human health or unreasonable interference with the amenity of sensitive uses within the locality.
- 118** Chimneys or exhaust flues associated with commercial development (including cafes, restaurants and fast food outlets) should be designed to ensure they do not cause a nuisance or health concerns to nearby sensitive receivers by:
- (a) incorporating appropriate treatment technology before exhaust emissions are released to the atmosphere;
  - (b) ensuring that the location and design of chimneys or exhaust flues maximises dispersion and takes into account the location of nearby sensitive uses.

### Rural Interface

- 119** The potential for adverse impacts resulting from rural development should be minimised by:
- (a) not locating horticulture or intensive animal keeping on land adjacent to townships;
  - (b) maintaining an adequate separation between horticulture or intensive animal keeping and townships, other sensitive uses and, where desirable, other forms of primary production.

## PRINCIPLES OF DEVELOPMENT CONTROL

- 137** Development should preserve and enhance the character and amenity of land within the Metropolitan Open Space System as shown on [Map Ga/1 \(Overlay 2\)](#).
- 138** Publicly owned land within the Metropolitan Open Space System should be used to provide natural or landscaped open space, accommodate a range of public recreation, sporting and institutional facilities and uses and to accommodate stormwater management.
- 139** Publicly owned land within the Metropolitan Open Space System should also be used to conserve wildlife habitats and areas of natural vegetation, to allow for movement of wildlife, to conserve sites of scientific, cultural or heritage interest and for re-vegetation.
- 140** Privately owned land within the Metropolitan Open Space System should be used for rural activities and agriculture (but not intensive animal keeping), very low-density residential development, low-impact tourist, or sporting facilities, or conservation purposes.
- 141** Buildings and structures erected on land within the Metropolitan Open Space System should be designed, located and screened so as to be unobtrusive and not detract from the open natural or landscaped character of these areas.
- 142** When land fronting watercourses within the Metropolitan Open Space System is divided land adjoining the watercourse should become a public reserve.
- 143** The width of reserves abutting watercourses within the Metropolitan Open Space System should be sufficient to allow for flood control, stormwater management, retention of the riverine ecosystem and to provide areas of open space which can be used to accommodate a range of recreational and sporting facilities.

## Natural Resources

### OBJECTIVES

- Objective 52:** Retention, protection and restoration of the natural resources and environment.
- Objective 53:** Protection of the quality and quantity of South Australia's surface waters, including inland and underground waters.
- Objective 54:** The ecologically sustainable use of natural resources including water resources, ground water, surface water and watercourses.
- Objective 55:** Natural hydrological systems and environmental flows reinstated, and maintained and enhanced.
- Objective 56:** Development consistent with the principles of water sensitive design.
- Objective 57:** Development sited and designed to:
- (a) protect natural ecological systems;
  - (b) achieve the sustainable use of water;
  - (c) protect water quality, including receiving waters;
  - (d) reduce runoff and peak flows and prevent the risk of downstream flooding;
  - (e) minimise demand on reticulated water supplies;
  - (f) maximise the harvest and use of stormwater;

- (g) protect stormwater from pollution sources.

**Objective 58:** Storage and use of stormwater which avoids adverse impact on public health and safety.

**Objective 59:** Native flora, fauna and ecosystems protected, retained, conserved and restored.

**Objective 60:** Restoration, expansion and linking of existing native vegetation to facilitate habitat corridors for ease of movement of fauna.

**Objective 61:** Minimal disturbance and modification of the natural landform.

**Objective 62:** Protection of the physical, chemical and biological quality of soil resources.

**Objective 63:** Protection of areas prone to erosion or other land degradation processes from inappropriate development.

**Objective 64:** Protection of the scenic qualities of natural and rural landscapes.

## **PRINCIPLES OF DEVELOPMENT CONTROL**

- 144** Development should be undertaken with minimum impact on the natural environment, including air and water quality, land, soil, biodiversity, and scenically attractive areas.
- 145** Development should ensure that South Australia's natural assets, such as biodiversity, water and soil, are protected and enhanced.
- 146** Development should not significantly obstruct or adversely affect sensitive ecological areas such as creeks and wetlands.
- 147** Development should be appropriate to land capability and the protection and conservation of water resources and biodiversity.

## **Water Sensitive Design**

- 148** Development should be designed to maximise conservation, minimise consumption and encourage reuse of water resources.
- 149** Development should not take place if it results in unsustainable use of surface or underground water resources.
- 150** Development should be sited and designed to:
  - (a) capture and re-use stormwater, where practical;
  - (b) minimise surface water runoff;
  - (c) prevent soil erosion and water pollution;
  - (d) protect and enhance natural water flows;
  - (e) protect water quality by providing adequate separation distances from watercourses and other water bodies;
  - (f) not contribute to an increase in salinity levels;
  - (g) avoid the water logging of soil or the release of toxic elements;
  - (h) maintain natural hydrological systems and not adversely affect:
    - (i) the quantity and quality of groundwater;



- (ii) the depth and directional flow of groundwater;
- (iii) the quality and function of natural springs.

**151** Water discharged from a development site should:

- (a) be of a physical, chemical and biological condition equivalent to or better than its pre-developed state;
- (b) not exceed the rate of discharge from the site as it existed in pre-development conditions.

**152** Development should include stormwater management systems to protect it from damage during a minimum of a 1-in-100 year average return interval flood.

**153** Development should have adequate provision to control any stormwater over-flow runoff from the site and should be sited and designed to improve the quality of stormwater and minimise pollutant transfer to receiving waters.

**154** Development should include stormwater management systems to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure the carrying capacities of downstream systems are not overloaded.

**155** Development should include stormwater management systems to minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants to the stormwater system.

**156** Stormwater management systems should preserve natural drainage systems, including the associated environmental flows.

**157** Stormwater management systems should:

- (a) maximise the potential for stormwater harvesting and re-use, either on-site or as close as practicable to the source;
- (b) utilise, but not be limited to, one or more of the following harvesting methods:
  - (i) the collection of roof water in tanks;
  - (ii) the discharge to open space, landscaping or garden areas, including strips adjacent to car parks;
  - (iii) the incorporation of detention and retention facilities;
  - (iv) aquifer recharge.

**158** Where it is not practicable to detain or dispose of stormwater on site, only clean stormwater runoff should enter the public stormwater drainage system.

**159** Artificial wetland systems, including detention and retention basins, should be sited and designed to:

- (a) ensure public health and safety is protected;
- (b) minimise potential public health risks arising from the breeding of mosquitoes.

#### **Water Catchment Areas**

**160** Development should ensure watercourses and their beds, banks, wetlands and floodplains are not damaged or modified and are retained in their natural state, except where modification is required for essential access or maintenance purposes.

- (e) be designed and selected to minimise maintenance costs;
- (f) provide habitat for local fauna.

**197** Development of recreational activities in areas not zoned for that purpose should be compatible with surrounding activities.

**198** Recreation facilities development should be sited and designed to minimise negative impacts on the amenity of the locality.

## Outdoor Advertisements

The context for advertising or advertising displays is critical in assessing the compatibility of the development with the Desired Future Character or character of a locality or zone. Signage types are defined in [Table Ga/4](#).

### OBJECTIVES

**Objective 69:** An urban environment and rural landscape not disfigured by advertisements.

**Objective 70:** Advertisements in retail, commercial and industrial urban areas, and centre zones, designed to enhance the appearance of those areas.

**Objective 71:** Advertisements not hazardous to any person.

### PRINCIPLES OF DEVELOPMENT CONTROL

**199** Advertising and advertising displays should, in addition to general provisions, conform with the performance criteria in [Table Ga/4](#).

**200** The location, siting, size, shape and materials of construction, of advertisements should be:

- (a) consistent with the desired character of areas or zones as described by their objectives;
- (b) consistent with the predominant character of the urban or rural landscape; or
- (c) in harmony with any building or site of historic significance or heritage value in the locality.

**201** Advertisements should not detrimentally affect by way of their siting, size, shape, scale, glare, reflection or colour the amenity of areas, zones, or localities, in which they are situated.

**202** Advertisements should not impair the amenity of areas, zones, or localities, in which they are situated by creating, or adding to, clutter, visual disorder and the untidiness of buildings and spaces.

**203** Advertisements should not obscure views of attractive landscapes or particular trees or groups of trees.

**204** The scale of advertisements should be compatible with the buildings on which they are situated and with nearby buildings and spaces.

**205** Advertisements should be constructed and designed in a workmanlike manner.

**206** Advertisements wholly or partly consisting of bunting, streamers, flags, windvanes, and the like should not detrimentally affect the amenity of areas, zones or localities in which they are situated.

**207** Advertisements on buildings that have a single architectural theme but which contain a number of tenancies, should be attached and displayed so as to be co-coordinated with that theme.

- 208** Advertisements should not be erected in positions close to existing electricity mains so that potentially hazardous situations are created.
- 209** Advertisements should not create a hazard to persons travelling by any means.
- 210** Advertisements should not obscure a driver's view of other road vehicles, of rail vehicles at or approaching level crossings, of pedestrians and of features of the road such as junctions, bends, changes in width, traffic control devices and the like that are potentially hazardous.
- 211** Advertisements should not be so highly illuminated as to cause discomfort to an approaching driver, or create difficulty in his perception of the road, or of persons or objects on it.
- 212** Advertisements should not be liable to interpretation by drivers as an official traffic sign, or convey to drivers information that might be confused with instructions given by traffic signals or other control devices, or impair the conspicuous nature of traffic signs or signals.
- 213** Advertisements should not detract drivers from the primary driving task at a location.
- 214** Advertisements or advertising displays should:
- (a) convey in clear and concise language, symbols, print style, layout, and in a limited number of colours, the owner/occupier and/or generic type of business, merchandise or services;
  - (b) be minimised in number to avoid proliferation of advertisements or advertising displays;
  - (c) be of a form, style, scale and size and in locations appropriate to the desired character and future use of land or character of the locality or zone and to preserve reasonable exposure to the public for all adjacent sites;
  - (d) not have an adverse impact on the amenity of adjacent land uses and areas from which they are visible and without limiting the foregoing, no bunting, whirligigs or other like displays should be displayed;
  - (e) conceal the supporting structure from view wherever possible;
  - (f) be completely contained within the boundaries of the subject property, and if road widening is applicable, be within the proposed property boundary realignment;
  - (g) be unified in colour, style, placement and proportion to present a coordinated theme and design for a site, centre or building; and
  - (h) be designed and located to avoid damage to, pruning or lopping of on-site landscaping or street trees.
- 215** Free-standing advertisements and advertising displays:
- (a) should be of a consistent design theme with other advertising on buildings within the site or complex; and
  - (b) should be of a scale and size compatible and complementary with development on the site.
- 216** Advertisements or advertising displays attached to buildings should:
- (a) be of appropriate colour, scale and proportion, and of an integrated design to be coordinated and complementary with the architectural form and design of the building the advertisement or advertising display is located upon; and
  - (b) not be located upon the roof or above the walls of a building.

**217** Advertisements or advertising displays attached to a verandah or which project from a building wall should have a minimum clearance over a footway of 2.3 metres.

**218** Advertisements or advertising displays should not be erected upon:

- (a) a vehicle carriageway, dividing strip of traffic island;
- (b) a vehicle adapted and exhibited primarily as an advertisement; or
- (c) residential land, unless erected to fulfill a statutory requirement, or as an advertising display associated with the business use of the land.

**219** Portable, easel or A-frame advertisements should only be displayed when:

- (a) no other appropriate opportunity exists for an adequate coordinated and permanent advertisement or advertising display;
- (b) no obstruction or infringement of safety occurs to pedestrians or vehicle movement;
- (c) no unnecessary duplication or proliferation of information or advertisements or advertising displays results; and
- (d) the advertisement is:
  - (i) coordinated and in conformity with the theme and design of all other advertisements on the subject site or building;
  - (ii) restricted to only one per site, or one per major road frontage if located on a corner site;
  - (iii) less than 0.54 square metres in advertisement area per face and 0.9 metres in height; and
  - (iv) only displayed during the hours when the subject premises is open for trading.

**220** Advertisements or advertising on buildings of nominated historic character, (State Heritage Items, Local Heritage Items and Contributory Items) or other buildings in Historic (Conservation) Zones, should be erected, displayed and positioned to:

- (a) not conceal architectural detailing;
- (b) be in scale with the building or architectural elements to which it is attached;
- (c) be removable without leaving any permanent damage to the building fabric;
- (d) not obscure the original frontage;
- (e) limit use of colours that relate to the building to which the sign is attached; and
- (f) not use reflective and opalescent paints or materials.

**221** Advertisements or advertising displays should only identify premises or the nature of business, goods or services conducted on the land or building.

**222** In residential and other non-business zones, advertisements or advertising displays should only be erected on land upon which a lawful land-use exists and in all respects should comply with:

- (a) [Table Ga/4](#); and
- (b) standards applicable to appropriate zones.

- 343** Development should discourage commercial and industrial vehicle movements through residential streets and adjacent other sensitive land uses such as schools.
- 344** Industrial/commercial vehicle movements should be separated from passenger vehicle car-parking areas.
- 345** Development should make sufficient provision on site for the loading, unloading and turning of all traffic likely to be generated.

### **Cycling and Walking**

- 346** Development should ensure that a permeable street and path network is established that encourages walking and cycling through the provision of safe, convenient and attractive routes with connections to adjoining streets, paths, open spaces, schools, public and community transport stops and activity centres.
- 347** Development should provide access, and accommodate multiple route options, for cyclists by enhancing and integrating with:
- (a) open space networks, recreational trails, parks, reserves and recreation areas;
  - (b) Adelaide's Metropolitan Open Space System.
- 348** Cycling and pedestrian networks should be designed to be permeable and facilitate direct and efficient passage to neighbouring networks and facilities.
- 349** New developments should give priority to and not compromise existing designated bicycle routes.
- 350** Where development coincides with, intersects or divides a proposed bicycle route or corridor, development should incorporate through-access for cyclists.
- 351** Developments should encourage and facilitate cycling as a mode of transport by incorporating end-of journey facilities including:
- (a) showers, changing facilities, and secure lockers;
  - (b) signage indicating the location of bicycle facilities;
  - (c) secure bicycle parking facilities.
- 352** Pedestrian facilities and networks should be designed and provided in Accordance with relevant provisions of the *Australian Standards and Austroads Guide to Traffic Engineering Practice Part 13*.
- 353** Cycling facilities and networks should be designed and provided in accordance with the relevant provisions of the *Australian Standards and Austroads Guide to Traffic Engineering Practice Part 14*.

### **Access**

- 354** Development should have direct access from an all weather public road.
- 355** Development should be provided with safe and convenient access which:
- (a) avoids unreasonable interference with the flow of traffic on adjoining roads;
  - (b) accommodates the type and volume of traffic likely to be generated by the development or land use and minimises induced traffic through over-provision;
  - (c) is sited and designed to minimise any adverse impacts on the occupants of and visitors to neighbouring properties.

- 356** Development should not restrict access to publicly owned land.
- 357** The number of vehicle access points onto arterial roads should be minimised, and where possible access points should be:
- (a) limited to local roads;
  - (b) shared between developments.
- 358** The number of access points for cyclists and pedestrians onto all adjoining roads should be maximised.
- 359** Development with access from roads with existing or projected traffic volumes exceeding 6000 vehicles per day should be sited to avoid the need for vehicles to reverse on to the road.
- 360** Driveways, access tracks and parking areas should be designed and constructed to:
- (a) follow the natural contours of the land;
  - (b) minimise excavation and/or fill;
  - (c) minimise the potential for erosion from run-off;
  - (d) avoid the removal of existing vegetation;
  - (e) be consistent with *Australian Standard AS 2890 Parking facilities*.

#### **Access for People with Disabilities**

- 361** Development should be sited and designed to provide convenient access for people with a disability.

#### **Vehicle Parking**

- 362** Development should be consistent with *Australian Standard AS 2890 Parking facilities*.
- 363** Vehicle parking areas should be sited and designed in a manner that will:
- (a) facilitate safe and convenient pedestrian linkages to the development and areas of significant activity or interest in the vicinity of the development;
  - (b) include safe pedestrian and bicycle linkages that complement the overall pedestrian and cycling network;
  - (c) not inhibit safe and convenient traffic circulation;
  - (d) result in minimal conflict between customer and service vehicles;
  - (e) avoid the necessity to use public roads when moving from one part of a parking area to another;
  - (f) minimise the number of vehicle access points to public roads;
  - (g) avoid the necessity for backing onto public roads;
  - (h) provide the opportunity for shared use of car parking and integration of car parking areas with adjoining development to reduce the total extent of vehicle parking areas and the requirement for access points;
  - (i) not dominate the character and appearance of a centre when viewed from public roads and spaces;

- (j) provide landscaping that will shade and enhance the appearance of the vehicle parking areas.

**364** Vehicle parking areas should be designed to reduce opportunities for crime by:

- (a) maximising the potential for passive surveillance by ensuring they can be overlooked from nearby buildings and roads;
- (b) incorporating walls and landscaping that do not obscure vehicles or provide potential hiding places;
- (c) being appropriately lit;
- (d) having clearly visible walkways.

**365** Where parking areas are not obviously visible or navigated, signs indicating the location and availability of vehicle parking spaces associated with businesses should be displayed at locations readily visible to customers.

**366** Parking areas that are likely to be used during non daylight hours should provide floodlit entrance and exit points and site lighting directed and shaded in a manner that will not cause nuisance to adjacent properties or users of the car park.

**367** Parking areas should be sealed or paved in order to minimise dust and mud nuisance.

**368** To assist with stormwater detention and reduce heat loads in summer, vehicle parking areas should include soft (living) landscaping.

**369** Parking areas should be line-marked to indicate parking bays, movement aisles and direction of traffic flow.

## **Infrastructure**

**370** A Traffic Impact Study should be undertaken to determine the potential impact of developments on the surrounding arterial road network. Works that are required as a direct result of providing safe and efficient access to any proposed development should be provided.

## **Telecommunications Facilities**

### **OBJECTIVES**

**Objective 95:** Telecommunications facilities provided to meet the needs of the community.

**Objective 96:** Telecommunications facilities located and designed to minimise visual impact on the amenity of the local environment.

Telecommunications facilities are an essential infrastructure required to meet the rapidly increasing community demand for communications technologies. To meet this demand there will be a need for new telecommunications facilities to be constructed.

The Commonwealth Telecommunications Act 1997 is pre-eminent in relation to telecommunications facilities. The Telecommunications (Low-impact Facilities) Determination 1997 identifies a range of facilities that are exempt from State planning legislation. The development of low impact facilities to achieve necessary coverage is encouraged in all circumstances where possible to minimise visual impacts on local environments.

Where required, the construction of new facilities is encouraged in preferred industrial and commercial and appropriate non-residential zones. Recognising that new facility development will be unavoidable in more sensitive areas in order to achieve coverage for users of communications technologies, facility design and location in such circumstances must ensure visual impacts on the amenity of local environments are minimised.